



Replacement of the John Coffee Memorial Bridge (Tennessee River Bridge) Environmental Assessment - Appendices



November 2024

APPENDIX A:
TOPICS DISMISSED FROM DETAILED ANALYSIS

Air Quality

Section 118 of the Clean Air Act requires the NPS to meet all federal, state, and local air pollution standards (42 United States Code [USC] 7401 et seq.). During replacement of the Tennessee River Bridge, construction activities would generate emissions associated with hauling materials, power equipment, fugitive dust, and the application of hot mix asphalt paving. In addition, expected travel delays on the detour route during the full bridge closure period would increase travel time, delays, and congestion, subsequently increasing vehicle emissions. However, the project would not increase vehicle trips to the Parkway, and emissions and fugitive dust would occur only during the construction period and would dissipate quickly. Following the completion of the project, air quality in the area of the Tennessee River Bridge would return to preconstruction conditions, and no long-term impacts on air quality are expected. Therefore, this topic was dismissed from detailed analysis.

Climate Change

Climate change refers to any significant changes in average climatic conditions (such as mean temperature, precipitation, or wind) or variability (such as seasonality and storm frequency) lasting for an extended period (decades or longer). Recent reports by the US Climate Change Science Program, the National Academy of Sciences, and the United Nations Intergovernmental Panel on Climate Change provide evidence that climate change is occurring as a result of rising greenhouse gas emissions and could accelerate in the coming decades. While climate change is a global phenomenon, its impacts vary based on regional and local factors. Construction activities associated with the proposed bridge replacement would contribute to increased greenhouse gas emissions, but such emissions would be short term, concluding with the completion of construction. Where feasible, the proposed bridge replacement would include sustainable design and materials to help offset the contribution to climate change. Any effects on climate change from the proposed action would not be discernible at a regional scale. Therefore, this topic was dismissed from detailed analysis.

Groundwater

Groundwater refers to the water that exists underground in saturated zones beneath the land surface. It is supplied by water sources such as rain or snowmelt. The rate at which water infiltrates the soil depends on factors such as type of soil, land use, and precipitation. The project area is underlain by the Tusculumbia-Fort Payne aquifer, which contains large amounts of limestone and is characterized by karst development. Karst and karst-like features are defined as points where surface water may transition to and could potentially impact groundwater, which includes sinkholes and caves. There are no active sinkholes present in the project area, there is an Undisclosed Cave located outside the project area, and there are no other karstic surface to subsurface connections in the project area. Drilling and other activities associated with pier installation would not affect bedrock in such a way that groundwater would be contaminated nor would the flow to nearby wells be impacted. Drilling would occur at or below the groundwater/surface water interface (i.e., below the river bottom) where groundwater is strongly discharging into the Tennessee River. Contamination would not be able to travel “backward” into the aquifer because of the magnitude of groundwater flow into the Tennessee River.

Other construction activities associated with the proposed bridge replacement would create minimal risk to groundwater. The potential for erosion and sediment introduction into the river from construction activities would be mitigated through permanent and temporary erosion and sediment control measures during site preparation and throughout construction. These measures may include slope reinforcement, riprap for armoring, retaining walls, silt fences, timber mats, slope stabilization, cofferdams, and turbidity controls. The project would not result in short- or long-term impacts that would substantially affect groundwater in the region. Therefore, this topic was dismissed from detailed analysis.

Lightscares

The project does not include installation of permanent lighting. Given the construction duration, some nighttime construction work likely would be necessary to minimize bridge closure times. Construction lighting would be required to safely accomplish nighttime work. Changes to lightscares would be localized and short term. A list of mitigation measures for short-term impacts on lightscares are detailed in Appendix B. Construction lighting would follow any applicable local, state, or federal requirements regarding nighttime construction lighting. Therefore, this topic was dismissed from further analysis.

Paleontological Resources

NPS *Management Policies 2006* states that paleontological resources (i.e., fossils), including both organic and mineralized remains in body or trace form, will be protected, preserved, and managed for public education, interpretation, and scientific research. These resources are managed on NPS lands under the requirements of the Paleontological Resources Preservation Act (2022). The NPS Geological Resources Division, including the NPS Senior Paleontologist conducted a paleontological review of existing paleontological digital data and a field survey. These efforts, coupled with analysis of proposed construction, determined that ground disturbance associated with the project is not expected to disturb paleontological resources because the fossils found in Fort Payne Formation clasts on the northeastern shore and the Tusculumbia limestone clasts in the southwestern shore overbank deposits are of poor quality and are encountered infrequently. Therefore, this topic was dismissed from detailed analysis.

River Commerce/Navigation

The Tennessee River is a navigable river, with commercial vessels transiting under the bridge daily. Annually, 12 million tons of economically beneficial commerce, valued at approximately \$4 billion, transits the Jamie L. Wilson Lock at Florence, Alabama. The Port of Florence offers intermodal transportation that links river, rail, and highways along with 40 acres of land and a 40-ton bridge crane.

Maintaining safe access for barges under the Tennessee River Bridge is important for regional, statewide, and national river commerce. Under both action alternatives, it is assumed that the existing 37 piers would remain in place in some manner. For example, they could be removed to the mud line, but remain in the ground, or they could be repurposed as dolphins in the existing navigation channel. If left in place, the NPS would coordinate with the USCG, USACE and TVA to ensure all in-water activities that have the potential to affect watercraft do not create hazardous conditions for boat traffic. Lighting would be included during bridge construction to maintain the navigation channel. Because USCG monitors the demolition activities on-site to provide a safe environment during demolition and salvage, this operation is anticipated to have minimal impacts on commercial river traffic.

During construction, there may be periods when the navigation channel would be closed at the bridge for the removal or installation of dolphins or bridge spans, which could affect commerce traveling along the Tennessee River. Under both action alternatives, demolition activities are anticipated to take approximately 220 days. Prior to construction, the NPS would coordinate with USCG, USACE, and TVA to schedule dates for the closures of the navigation channel, including the duration of these closures.

Efforts would be made to minimize the interference to the navigation channel in the Tennessee River from the presence of temporary platforms and barges during bridge demolition and construction. Should there be a temporary disruption to the navigation channel due to construction, the NPS would coordinate with the USCG District 8 to issue a Local Notice to Mariners. The Notice to Mariners provides timely marine safety information for the correction of all US government navigation charts. To ensure safety of commercial river users, the NPS will coordinate with USCG to halt river traffic during demolition activities.

The replacement bridge includes a minimum horizontal clearance of 350 feet for the main navigation channel under both action alternatives. The existing navigation channel width is 650 feet; The contractor's site-specific erection plan would provide adequate horizontal clearance within the navigation channel span to allow safe passage of river traffic during construction of the superstructure. Neither of the action alternatives would affect the location of the navigation channel. The existing vertical clearance is sufficient, and both action alternatives can achieve this clearance; therefore, they would satisfy the reasonable needs of navigation.

With the implementation of the safety measures described in this section, adverse impacts to health, safety, and river commerce would be minimized to the greatest extent practicable and are expected to be short term and minor. Therefore, concerns over impacts to health, safety, and river commerce related to construction and demolition would be temporary during construction activities. Along with USCG coordination and efforts related to the implementation of mitigation measures on potential impacts, river commerce has been dismissed from further analysis.

Socioeconomics

The implementation of the project would result in short-term construction-related spending. Construction expenditures would be used for labor, supplies, equipment, and other services. Labor would likely come from nearby communities and possibly from the Florence or Nashville metropolitan areas. However, the number of workers would be minimal, and most of them would be employed already. There would be no permanent effect on the population, income, or employment base of the surrounding community. Secondary economic effects from construction-related spending would generate minimal economic benefits to the region. Construction-related spending would have a short-term, beneficial effect on the regional economy. Construction activities and the proposed detour may inconvenience and possibly deter some visitors from using the Parkway; however, no substantial change in visitor use is anticipated along the Parkway. Maintaining traffic flow and visitor access over the long term would help sustain Parkway visitation and tourist-related spending. During construction, the proposed project would result in short term, beneficial effects on the regional economy but would not result in short- or long-term impacts that would substantially affect socioeconomics in the region. Therefore, this topic was dismissed from detailed analysis.

Vegetation

NPS *Management Policies 2006* states that the agency will strive to maintain all components and processes of naturally evolving ecosystems, including the natural abundance, diversity, and ecological integrity of plants (NPS 2006). The project area is surrounded by a vegetated landscape, and the NPS landscapes and maintains vegetation in the project area. The proposed project could require trimming and removal of trees and shrubs to accommodate construction equipment. Revegetation of the affected areas would be carried out once the proposed project was complete. Reestablishment of the vegetation after the use of the site would stabilize the soils. Additionally, fill material used during the project would meet FHWA Eastern Federal Lands Highway Division and NPS requirements to avoid the introduction of nonnative invasive plants. No known rare plants occur in the project area. The NPS would maintain the vegetated landscape during and after construction. Therefore, this topic was dismissed from detailed analysis.

Visual Resources

NPS *Management Policies 2006* states that a park's scenery and scenic features are included among the resources and values to be protected and conserved unimpaired for enjoyment by current and future generations. Scenic features are critical to the visual landscape in conjunction with other resources and values. Visual impacts from construction activities would be localized. Repair treatments to the existing road in the project area would become permanent features on the landscape, potentially detracting from

the scenic resources of the Parkway and affecting visual quality. However, the project would occur in areas where the presence of the road has already altered the natural setting. Visual impacts that would represent a noticeable departure from the historical visual conditions and the cultural landscape of the project area are discussed and addressed in the “Cultural Resources” section of this document. Therefore, this topic was dismissed from detailed analysis.

Wetlands

Wetlands and waters of the United States are regulated by the US Environmental Protection Agency and the USACE under Section 404 of the Clean Water Act. The Alabama Department of Environmental Management is the state agency responsible for implementing water quality regulations in wetlands not under the USACE’s jurisdiction. Consideration of impacts on wetlands is also required under Executive Order 11990. In addition, NPS Director’s Order 77-1 calls for no net loss of wetlands and strives for a longer-term goal of a net gain of wetlands throughout NPS-managed lands. An aquatic resource inventory was conducted in the project area in August 2022 to delineate wetlands and other waters of the United States. The area reviewed included the project area, which totals approximately 148 acres. The survey identified five palustrine wetlands in the project area, including three wetlands that meet all three USACE wetland criteria, and two additional wetlands that meet the definitions included in Director’s Order 77-1. Replacement of the bridge would require construction activities within the river and along its banks. The NPS would obtain the required permits from USACE under Section 404 the Clean Water Act for the discharge of dredged or fill material into waters of the United States, and under the River and Harbors Act for work or structures in, over, or under navigable waters of the United States. Under Section 401 of the Clean Water Act, the NPS would obtain any required state water quality certification or a waiver of certification from the Alabama Department of Environmental Management. The NPS would adhere to the terms and conditions of the required permits from USACE and/or the Alabama Department of Environmental Management and satisfy the conditions and best management practices referred to in Section 4.2.2 and listed in Appendix 2 of NPS Procedural Manual #77-1.

The NPS does not manage the Tennessee River, or the water or the riverbed substrate below the river. Therefore, impacts within the Tennessee River to remove and construct bridge abutments and piers (below the ordinary high-water mark) are not subject to Director’s Order 77-1 and would not count against the allowable 0.25-acre threshold for bridge replacements in accordance with NPS Procedural Manual #77-1. Table A-1 provides the acreage of the remaining wetlands within the project area subject to Director’s Order 77-1. Impacts to these wetlands would be avoided and minimized to the extent practicable during final design, and construction activities would not affect more than 0.25 acres. Therefore, the project would be excepted from the NPS’s Statement of Findings and compensation requirements. Therefore, this topic was dismissed from detailed analysis.

TABLE A-1. APPROXIMATE WETLAND AREA SUBJECT TO DIRECTOR’S ORDER 77-1

Wetland ID	Wetland Classification Standard	Approximate Acreage
WL1	NPS (FGDC) ^a	0.01
WL2a	NPS (FGDC) ^a	0.14
WL2b	NPS (FGDC) ^a	0.02
SUBTOTAL		0.17
WL3	USACE Wetlands Delineation Manual	0.03
WL4	USACE Wetlands Delineation Manual	0.06
WL5	USACE Wetlands Delineation Manual	0.10
SUBTOTAL		0.19

^a Any location classified as a wetland according to the Federal Geographic Data Committee (FGDC) Wetlands Classification Standard (FGDC 2013) is subject to Director’s Order 77-1, except those below the ordinary high-water mark of the Tennessee River.

APPENDIX B:
RESOURCE PROTECTION MEASURES
AND BEST MANAGEMENT PRACTICES

RESOURCE PROTECTION MEASURES AND BEST MANAGEMENT PRACTICES

Topic	Resource Protection Measure	Authority	Responsible Party
General	Clearly state all resource protection measures in the construction specifications and instruct workers to avoid conducting activities outside the project area. Limit disturbances to roadsides and other areas inside the project area. Clearly indicate areas of concern, such as historic culverts, on construction drawings.	NHPA, NEPA	FHWA
General	Hold a preconstruction meeting to inform contractors about sensitive areas, including natural and cultural resources, and provide procedures for identifying and addressing any unanticipated discoveries.	NPS Director's Order 2, Section 3.4	FHWA
General	Site staging and storage areas for construction vehicles, equipment, materials, and soils in previously disturbed or paved areas approved by the NPS. Locate these areas outside high visitor use areas and clearly identified in advance of construction.	NPS <i>Management Policies 2006</i> , Section 4.8.2.4	NPS/FHWA
General	Identify construction zones with construction fence, silt fence, or some similar material prior to any construction activity. Use fencing to define the construction zone and confine activity to the minimum area required for construction. Clearly state all protection measures in the construction specifications, and instruct workers to avoid conducting activities beyond the construction zone. Limit disturbances to areas inside the designated construction limits.	23 CFR 650.203; NPS <i>Management Policies 2006</i> , Section 9.1.3.1	FHWA
General	Inform construction workers and supervisors about the special sensitivity of the Parkway's values, regulations, and appropriate housekeeping. Require the contractor to remove food trash daily.	NPS <i>Management Policies 2006</i> , Section 1.4.3	FHWA
General	Comply with all legal load restrictions regarding material and equipment hauling. Remove all tools, equipment, barricades, signs, surplus materials, and rubbish from the project work limits upon project completion.	Alabama Code § 32-9-20	FHWA
General	Provide a copy of all environmental permits to the Parkway Chief of Resource Management for the project's decision file.	NEPA	FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
General	Implement a project-specific revegetation plan to address reestablishing vegetation and vegetative materials for former hard-scaped (paved or improved) areas that are abandoned as part of any proposed roadway realignment. Restore the agricultural lease area as natural/riparian forest habitat following completion of construction/staging activities to ensure sustained natural conditions, establishment of native vegetation, and prevention of nonnative vegetation growth. Conduct restoration efforts to conform with the parkway's character and cultural landscape.	NPS <i>Management Policies 2006</i> , Sections 4.1.5 and 4.4.2.4	NPS
General	Coordinate with and notify local emergency service providers and local schools prior to construction to minimize potential delays for emergency services and school-related transportation.	NPS <i>Management Policies 2006</i> , Section 1.6; NPS Director's Order 75A	FHWA
General	Meaningfully engage with protected populations, visitors, residents, and business owners in the project area in advance of construction activities, including providing detailed construction and detour plans to the public, installing safe and visible directional signage, and working with public information officers to disseminate construction information to the public.	NPS <i>Management Policies 2006</i> , Section 1.7; NPS Director's Order 75A	NPS
Cultural Resources	Complete a NRHP nomination for the Chikasha Aiasha TCL.	NHPA	NPS
Cultural Resources	Conduct archeological monitoring by a Secretary of Interior-qualified archeologist during construction of bridge elements that occurs within the boundaries of the George Colbert Archeological District.	NHPA	FHWA
Cultural Resources	Document all historic properties within the project area and include treatment measures such as: <ul style="list-style-type: none"> • Developing measures to avoid or minimize effects to historic properties. • Recommending construction techniques that follow the Secretary of the Interior's Standards for Treatment of Historic Properties. • Developing and implementing an inadvertent discovery plan, including adhering to requirements in the NAGPRA (1990). 	NHPA, NAGPRA	NPS/FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
Cultural Resources	Conduct additional mitigation and minimization measures agreed to through consultation with the public, Tribes, the Alabama SHPO, NPS, construction team, and potentially the Advisory Council on Historic Preservation.	NHPA	NPS
Cultural Resources	Identify and delineate archeological or structural resources near the project area prior to project work. Employ an archeologist who meets the Secretary of the Interior's professional qualification standards to monitor specific areas of ground disturbance indicated by NPS cultural resources staff or Tribal partners. Identify archeological sites on construction drawings to identify areas for avoidance and indicate where archeological monitoring would be focused.	NHPA	FHWA
Cultural Resources	Continue to coordinate with the Alabama SHPO, Tribal Historic Preservation Officers, and TVA (for work within their jurisdictions) throughout the course of the project if unknown cultural resources are discovered as a result of the actions associated with the alternatives.	NHPA	NPS
Cultural Resources	In the unlikely event that previously unknown archeological resources are discovered during construction, halt all work in the immediate vicinity of the discovery until the resources are identified and documented and, if the resources cannot be preserved in situ, develop an appropriate mitigation strategy in consultation with the Alabama SHPO and, if necessary, associated Tribal Historic Preservation Officers. Allow members of Native American Tribes to monitor excavation activities during construction for the presence of cultural resources.	NHPA, NAGPRA	NPS/FHWA
Cultural Resources	Stop all work on the project and contact law enforcement and the coroner immediately if human remains are discovered during construction activities. Follow all provisions outlined in NAGPRA.	NAGPRA	NPS/FHWA
Cultural Resources	Ensure that all contractors and subcontractors are informed of the penalties for illegally collecting artifacts or intentionally damaging archeological sites or historic properties. Instruct contractors and subcontractors on procedures to follow if previously unknown archeological resources are uncovered during construction.	NHPA	FHWA
Cultural Resources	Locate equipment and material staging areas to avoid known archeological resources.	NHPA	FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
Geological Resources	Avoid or minimize disturbance to soils as much as possible.	NPS <i>Management Policies 2006</i> , Section 4.8.2.4	FHWA
Geological Resources	Evaluate new or locally sourced topsoil for nonnative invasive plant infestations. Prohibit the use of infested soil for any construction or post-construction use, including revegetation.	Federal Noxious Weed Act	FHWA
Geological Resources	Wash and inspect construction equipment to remove potential nonnative invasive plant seeds.	Federal Noxious Weed Act	FHWA
Geological Resources	Implement erosion control measures that provide for soil stability and prevent movement of soils during rain events (i.e., silt fences and tarps).	NPS <i>Management Policies 2006</i> , Section 4.8.2.4	FHWA
Geological Resources	Aerate any ground surface temporarily disturbed during construction and revegetate with native plants to reduce compaction and prevent erosion.	NPS <i>Management Policies 2006</i> , Section 4.4.2.5	FHWA
Geological Resources	Develop a stormwater pollution prevention plan to identify potential sources of dust and stormwater pollution at the construction site, including active haul roads and staging areas. Engage a qualified stormwater practitioner to ensure compliance.	Clean Water Act	NPS/FHWA
Geological Resources	Delineate construction zones outside existing disturbed areas with flagging and confine all surface disturbance to the construction zone.	23 CFR 650.203	FHWA
Geological Resources	Require contractors to properly maintain construction equipment to minimize potential for accidental spills.	40 CFR 450.21	FHWA
Geological Resources	Follow best management practices to control fugitive dust, including wetting soils to suppress dust, maintaining the existing vegetation to the extent possible, limiting speed limits on unpaved roads, and limiting demolition work in high-wind conditions.	EPA AP-42, Section 13.2	FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
Geological Resources	Exercise care during construction to prevent direct secondary impacts to the cave and karst environment. Primary impacts thought to represent a hazard to the cave are from rock excavation to reach proposed finished grades, drilling during exploration, and groundwater/erosion.	NPS <i>Management Policies 2006</i> , Sections 4.8.1.2 and 4.8.2.2	FHWA
Geological Resources	Exercise caution during excavation to remove rock (if required) to limit disturbance to the cave morphology and its associated fauna. If rock removal is anticipated during construction, consider pre-splitting or chipping methods as lower-impact rock removal techniques. Avoid higher-impact rock removal techniques such as blasting.	NPS <i>Management Policies 2006</i> , Section 4.8.2.2	FHWA
Geological Resources	Do not fill subsurface voids encountered during exploratory drilling as part of borehole abandonment. Avoid filling subsurface voids by using floats or stemming plugs installed in the borehole above the void to reduce impacts to the subterranean environment.	NPS <i>Management Policies 2006</i> , Section 4.8.2.2	FHWA
Geological Resources	Consider construction equipment fuel spills and other groundwater impacts should as part of the construction planning process. Karst environments are sensitive and generally poorly defined with respect to groundwater transport. Exercise care during construction to limit these impacts to the protected resource, as required by the NPS.	NPS <i>Management Policies 2006</i> , Sections 4.6.3 and 4.8.1.2	NPS/FHWA
Geological Resources	Implement karst-specific measures: <ul style="list-style-type: none"> Clearly identify the extent of karst features on all relevant construction drawings and maps. Use construction techniques that minimize changes to natural karst processes (e.g., avoid deeply cutting into soils and blasting to minimize changes to predevelopment surface and subsurface drainage patterns). Limit equipment storage and access in the area of extensive erosion to avoid possible sinkhole formation northwest of the western edge of the area of interest. Maintain, to the maximum extent technically feasible, the predevelopment hydrology of the property, including surface drainage 	NPS <i>Management Policies 2006</i> , Section 4.8.1.2	NPS/FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
	<p>patterns, groundwater recharge, and the quantity and quality of stormwater runoff.</p> <ul style="list-style-type: none"> • Develop a karst management plan specifying methods for protecting karst features from removal, replacement, and construction staging activities as well as for mitigating unavoidable impacts. • Avoid disturbance to known karst features where feasible and design stormwater management measures following karst-specific guidelines. • Minimize ponding, widely distribute infiltration, and treat runoff using small runoff reduction methods. • Avoid discharge of road runoff directly to karst features connected to the subsurface. 		
Soundscapes and Night Skies	<p>Implement standard noise abatement measures during construction, including:</p> <ul style="list-style-type: none"> • Require contractors to properly maintain construction equipment (i.e., mufflers and brakes) to minimize noise. Do not allow construction vehicle engines to idle for extended periods. • Close engine housing doors during operation. • Cover portable noise-generating equipment with noise-insulating fabric and blankets, where feasible and practicable. 	NPS Director's Order 47; <i>NPS Management Policies 2006</i> , Section 4.9	FHWA
Soundscapes and Night Skies	<p>Do not allow trucks, other than trucks loading, unloading, or processing material (e.g., concrete mixing trucks), to idle any longer than is necessary for safety and/or mechanical reasons; no unattended motors/engines are to be permitted.</p>	<i>NPS Management Policies 2006</i> , Sections 4.10 and 8.2.3	FHWA
Soundscapes and Night Skies	<p>Consider best available construction technologies regarding noise abatement including:</p> <ul style="list-style-type: none"> • Replace diesel- or gas-powered equipment with electrically powered equipment. • Use Occupational Safety and Health Administration-approved broadband backup alarms. • Implement fixed or portable noise barriers/shields. 	NPS Director's Order 47; <i>NPS Management Policies 2006</i> , Section 4.9	FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
Soundscapes and Night Skies	Follow NPS best practices regarding light-emitting equipment, fixtures, or devices (e.g., plant lights) used during night-time operations and require NPS approval prior to implementing night-time operations. Consider allowing night-time operations only when there is a documented case that construction timeframes can be expedited by a minimum of 150%.	NPS <i>Management Policies 2006</i> , Section 4.10	FHWA
Soundscapes and Night Skies	At the end of each workday, except where night-time operations are approved, turn-off or remove any light-emitting devices or equipment before leaving the construction site.	NPS <i>Management Policies 2006</i> , Section 4.10	FHWA
Wildlife, Including Threatened/Endangered Species	Conduct a preconstruction survey along the length of the existing bridge to determine the presence or absence of sensitive and/or protected species and reduce impacts on a variety of species within the project area.	ESA	NPS
Wildlife, Including Threatened/Endangered Species	Consider in the replacement bridge design potential enhancements for wildlife species within the project area, where feasible.	ESA	FHWA
Wildlife, Including Threatened/Endangered Species	Perform site inspections for potential bat roosting prior to any bridge removal. If bats are using the bridge, initiate demolition work between November 15 and March 31 to avoid and minimize disturbance. If a maternity colony is present, do not initiate construction work between May 15 and August 15.	ESA	NPS/FHWA
Wildlife, Including Threatened/Endangered Species	Conduct tree clearing between November 15 and March 31 to avoid impacts on federally listed bats and nesting birds. During future project phases, if it is determined that clearing or construction is needed during these seasons, coordinate with USFWS to ensure no impacts would occur.	ESA	NPS

Topic	Resource Protection Measure	Authority	Responsible Party
Wildlife, Including Threatened/ Endangered Species	Use fill material that meets FHWA Eastern Federal Lands Highway Division and NPS requirements to avoid the introduction of nonnative invasive plants.	Federal Noxious Weed Act	FHWA
Wildlife, Including Threatened/ Endangered Species	Revegetate disturbed areas with a native seed mix.	NPS <i>Management Policies 2006</i> , Sections 4.4.2.4 and 9.1.3.2	NPS
Wildlife, Including Threatened/ Endangered Species	Restore the agricultural lease area to natural/riparian forest habitat upon completion of construction/staging activities.	NPS <i>Management Policies 2006</i> , Sections 4.1.5 and 4.4.2.5	NPS
Wildlife, Including Threatened/ Endangered Species	Comply with all relevant requirements under the Clean Water Act, including management of stormwater-related nonpoint source pollutants under the National Pollutant Discharge Elimination System.	Clean Water Act	FHWA
Wildlife, Including Threatened/ Endangered Species	Implement best management practices for drainage erosion and sediment control to prevent or reduce nonpoint source pollution and minimize soil loss and sedimentation in drainage areas, wetlands, and waters of the US. Document these procedures (e.g., silt fencing, erosion control fabrics/blankets, straw wattles or fiber rolls, temporary sediment ponds or sediment traps, inlet protection, sedimentation basins, temporary or permanent check dams, turbidity barriers, revetments, interim or permanent seeding and/or mulching of exposed areas, top-seeding or over-seeding) in a stormwater pollution prevention plan with design details incorporated in the construction documents on erosion control plan sheets. Follow NPS standards and best practices with regard to products and materials to prevent unintended impacts to wildlife or other resources.	Clean Water Act; 23 CFR 650.203	FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
Wildlife, Including Threatened/ Endangered Species	Ensure that permanent vegetation and permanent erosion control best management practices are well-established and in a post-commissioned condition during final inspections and prior to final acceptance to prevent adverse impacts on water resources. Require the contractor to remove all temporary erosion control best management practices prior to final acceptance.	23 CFR 650.203	NPS/FHWA
Wildlife, Including Threatened/ Endangered Species	Implement wetland protections in accordance with NPS Director's Order 77-1: Wetland Protection.	NPS Director's Order 77-1	NPS
Wildlife, Including Threatened/ Endangered Species	Consult with the USFWS if threatened or endangered mussels are observed during the preconstruction survey.	ESA	NPS
Visitor Use and Experience	Inform visitors in advance of construction activities via multiple methods, including the website, various signs, and the visitor center. Have Parkway staff available to address visitor questions during construction and provide regular updates to the public about project progress and any associated delays.	NPS <i>Management Policies 2006</i> , Section 1.9.2	NPS
Visitor Use and Experience	Develop provisions for emergency vehicle access through construction zones, where feasible.	NPS <i>Management Policies 2006</i> , Section 8.2.5.2	FHWA
Visitor Use and Experience	Implement a traffic control plan and a detailed detour plan during construction.	NPS <i>Management Policies 2006</i> , Section 9.2.1	FHWA
Visitor Use and Experience	Coordinate with the city of Florence, city of Muscle Shoals, town of Cherokee, and ALDOT on installing appropriate signage regarding construction routes for public safety.	NPS <i>Management Policies 2006</i> , Section 1.6	NPS

Topic	Resource Protection Measure	Authority	Responsible Party
Visitor Use and Experience	Incorporate aesthetic treatment measures throughout the project area corridor where project treatments, like retaining walls, are visible.	NPS <i>Management Policies 2006</i> , Section 9.1.1.2	NPS
Water Resources and Water Quality	Implement floodplain protections in accordance with NPS Director's Order 77-2: Floodplain Management.	NPS Director's Order 77-2	NPS
Water Resources and Water Quality	Design the bridge and road structures to withstand flood events while impeding flow as little as possible. Adhere to applicable floodplain standards. Use sustainable design principles and implement best management practices during and after construction to minimize storm damage.	NPS Director's Order 77-2	FHWA
Water Resources and Water Quality	Prepare and implement an erosion control plan and appropriate best management practices to control erosion during construction to prevent or minimize the potential for erosion and transport of sediments to the Tennessee River and adjacent streams.	Clean Water Act; 23 CFR 650.203	NPS/FHWA
Water Resources and Water Quality	Screen native soils excavated for construction, and reuse suitable soils to restore temporary disturbance areas, as needed, in accordance with the NPS floodplain guidelines and Executive Order 11988. Use fill material that meets FHWA Eastern Federal Lands Highway Division and NPS requirements to avoid the introduction of nonnative invasive plants during floodplain restoration.	Executive Order 11988; NPS <i>Management Policies 2006</i> , Section 4.8.2.4	NPS/FHWA
Water Resources and Water Quality	Temporarily suspend the operation of ground-disturbing equipment during large precipitation events to reduce the production of sediment that may be transported to streams.	Clean Water Act; 23 CFR 650.203	FHWA
Water Resources and Water Quality	Require a hazardous spill plan or a spill prevention control and countermeasures plan from the contractor prior to the start of construction stating actions that would be taken in the case of a spill and preventive measures that would be implemented. Store hazardous spill clean-up materials on-site at all times to avoid and minimize the introduction of chemical contaminants associated with machinery (e.g., fuel, oil, and hydraulic fluid) used in project implementation.	Clean Water Act; 40 CFR 450.21	FHWA

Topic	Resource Protection Measure	Authority	Responsible Party
Water Resources and Water Quality	Maintain all equipment in a clean and well-functioning state to avoid or minimize contamination from fluids and fuels. Prior to starting work each day, inspect all machinery for leaks (e.g., fuel, oil, and hydraulic fluid) and make all necessary repairs before commencing work.	Clean Water Act	FHWA
Water Resources and Water Quality	Use qualified NPS staff and/or certified wetland scientists to protect wetlands and floodplains during project implementation. Perform construction activities with caution to prevent damage to wetlands and floodplains caused by equipment, erosion, siltation, or pollutant discharges.	NPS Director's Order 77-2	NPS/FHWA
Water Resources and Water Quality	Design structures and facilities to be consistent with the intent of the standards and criteria of the National Flood Insurance Program.	44 CFR Part 60	FHWA
Water Resources and Water Quality	Limit construction or clearing of vegetation within the floodplains to the greatest extent possible. If use of herbicide is needed, limit application to maximum extent practicable in floodplains.	NPS Director's Order 77-2	NPS
Water Resources and Water Quality	Restore all temporary disturbance areas to preconstruction conditions (or better). Revegetate disturbed areas with native plant species that reflect surrounding floodplain vegetation.	NPS <i>Management Policies 2006</i> , Sections 4.1.5 and 4.4.2.4	NPS
Water Resources and Water Quality	Restore watershed conditions after temporary impacts to reduce accelerated runoff caused by soil compaction; poor vegetation cover; or unnatural conveyance of water by roads, ditches, or trails.	NPS <i>Management Policies 2006</i> , Section 4.1.5	NPS

**APPENDIX C:
ALTERNATIVES CONSIDERED BUT DISMISSED
FROM FURTHER ANALYSIS**

The following alternatives were considered in the preliminary planning process but dismissed from further analysis because they were not considered reasonable alternatives (e.g., they did not meet purpose and need or were determined not to be technically or economically feasible).

Do Not Replace Bridge When No Longer Operational

As noted in the park's foundation document, "The Natchez Trace Parkway commemorates 10,000 years of travel old the Old Natchez Trace corridor and provides a 444-mile journey linking culture, nature, and history along one of the oldest transportation routes on the North American continent" (NPS 2014). The park was created in 1938 to commemorate the historic travel corridor with transportation being a key component of the park's significance for its establishment. As such, allowing the bridge to deteriorate and close, without eventually replacing it, would be contrary to the reason why the Parkway was established. For this reason, the alternative of not replacing the bridge when it is no longer operational was dismissed from consideration.

Consideration of Alternative Alignments for a New Bridge

During the planning process, multiple alignments were considered for the construction of a new bridge, including replacing it on a new alignment north of the existing bridge and replacing the bridge on the current alignment. The NPS identified criteria to evaluate preliminary alternatives including safety, emergency access, bicycle and pedestrian access, resource protection, cultural resources, community access, resiliency, longevity, sustainability, maintainability, and impacts resulting from construction closures. Based on these criteria, two action alternatives were carried forward for detailed analysis. Other alignments considered but dismissed from detailed analysis include those below with their corresponding dismissal rationales:

- **Replace Bridge on New Alignment North of Existing Bridge.** Constructing a new bridge on a new alignment north of the existing bridge would require elevation changes to achieve the necessary horizontal and vertical alignment. These elevation changes and the proximity of the drainage basin would require significant clearing, as well as tie-in requirements, which would result in additional cut and fill and could affect landslide formations. The placement and constructability of this alignment would also be limited by the proximity to nearby terrestrial archeological sites and could result in significant adverse impacts to known cultural resources and the cultural landscape. Therefore, this alignment option would have more cultural and natural resource impacts than Alternatives 1 and 2 and was dismissed from consideration.
- **Replace Bridge on Current Alignment.** Replacing the bridge on the current alignment would result in a much longer construction time because the entire bridge would need to be closed for demolition and then constructed on the same alignment. While this alternative would minimize impacts to some natural and cultural resources, it would result in up to a 1-hour detour each way for more than four years, which would have significant adverse impacts on the surrounding community in terms of accessing employment centers, community resources, and access to emergency services. This alternative would have the largest potential for disturbance to turbidity and water quality, as well as potential impacts to known submerged archeological resources from the removal of the existing piers and replacement of new piers in the river. Therefore, when compared to Alternatives 1 and 2, this alignment would have greater impacts to water and archeological resources, as well as the surrounding community due to bridge closure duration and was therefore dismissed from consideration.

Rehabilitate the Existing Bridge on Existing Alignment

Under this alternative, the Tennessee River Bridge would be rehabilitated on the existing alignment by replacing the bridge using new design standards. This alternative would rehabilitate and repair the steel girder bridge and associated bridge features. As noted in Chapter 1, the Tennessee River Bridge has exceeded its 60-year design lifespan, and rehabilitation of the bridge would extend this lifespan by an unknown but limited amount of time. The life cycle cost for this alternative assumes a full bridge structure replacement would be needed in approximately 20 years, even with bridge rehabilitation. Therefore, rehabilitation of the bridge would be a short-term solution. Furthermore, this alternative would result in closures for both the rehabilitation and reconstruction, resulting in impacts from closure and detour for twice as long as the other alternatives considered. Therefore, this alternative was considered but dismissed from consideration.

APPENDIX D:
PAST, ONGOING, AND REASONABLY FORESEEABLE
ACTIONS

This appendix summarizes the past, ongoing, and reasonably foreseeable projects that were considered in the analyses of the cumulative effects for the resource topics presented in Chapter 3 of the EA.

Dam Construction (Completed)

The USACE and TVA constructed an extensive network of dams throughout the Tennessee River basin. These dams date back to the early 1930s and 1940s. Stream impoundments have affected species composition in the Tennessee River by reducing flows and altering temperatures.

River Heritage Trail Project (Completed 2024)

The City of Florence finished construction of a bicycle and pedestrian trail from River Heritage Park to the Patton Island Overlook in May 2024. The project includes several overlook points on the Tennessee River, as well as exhibits featuring the significance of the river from Native American influence and other topics.

Routine Maintenance, Repairs, and Safety Improvements on Parkway (Ongoing, As Needed)

The NPS will continue regularly scheduled routine maintenance, repairs, and safety improvement activities for the bridge, motor roadway, recreation areas, and facilities on the Parkway. Activities include:

- **Cave Prescribed Burns:** As part of its ongoing forest management efforts, the Parkway conducts prescribed burning in the forests on the south side of the bridge. The NPS consults with USFWS prior to each burn to avoid potential impacts on listed species.
- **Hazard Tree Removal:** To protect public safety, dead trees that could fall within the roadway are removed, as needed. Trees are marked for removal during the dormant season and removed later in the year to avoid potential disturbances to roosting bats during the maternal season. Trees are generally felled and left to decompose in the woods along the Parkway corridor. If a storm blows trees down in the road, they are cleared from the road and sometimes hauled away.
- **Audible Pavement Markings Pilot Project (Completed 2023):** The NPS installed audible pavement markings from mile marker 291 to 331, roughly 20 miles south of the bridge and 8 to 10 miles to the north in Colbert and Lauderdale Counties in Alabama, and in Prentiss and Tishomingo Counties in Mississippi. FHWA and Parkway staff plan to consider audible pavement markers application on future projects based on the best research available.
- **Colbert Ferry Gully Erosion Project (Future):** The NPS completed a study for hydrologic and hydraulic design and engineering services necessary for the preparation of conceptual design alternatives for two gully erosion sites at Colbert Ferry Park in Colbert County, Alabama. Multiple washout gullies have formed within the wooded areas of the park. Project construction is unknown at this time.
- **Habitat Enhancement at an Undisclosed Cave (Construction Completed, Research Ongoing):** The NPS recently installed a cave gate to protect park resources and stop trespassing. A perimeter fence was also removed to improve habitat conditions for the endangered gray bat, which had used the Undisclosed Cave in the past. Future research, monitoring, and habitat improvements are planned to protect the cave resources.

Local and Municipal Insecticide, Pesticide, and Fertilizer Application (Ongoing, As Needed)

Municipalities spray annually (as needed) for mosquitos, generally from mid-March to late October. City personnel also apply chemicals to known breeding areas to kill the larvae or eggs. Local agricultural areas spray pesticides and apply fertilizer to soils.

River Cleanups and Related Programming (Ongoing, As Needed)

Local organizations, such as Keep the Tennessee River Beautiful, host river cleanups and related programs to remove garbage from the river and improve local water quality.

Dam Improvements (Ongoing, As Needed)

Improvements in the design and operation of dams have reduced impacts on aquatic species in the Tennessee River basin. For instance, TVA is improving tailwater conditions downstream of certain dams in the Tennessee River drainage that may benefit freshwater mussels by increasing temperatures through multiport releases, increasing oxygenation with aerators, establishing minimum flow schedules, and reducing or eliminating hydropeaking.

Highway 72 Road Construction Projects (Future)

The ALDOT has three road construction projects underway in Colbert and Lauderdale Counties, all on HWY 72 within the immediate vicinity of Florence and Muscle Shoals. The road construction projects include replacing a bridge over Ashe Boulevard in Sheffield, replacing a bridge over HWY 20 in Florence, and widening a road east of Florence (ALDOT 2023). These projects are located outside the project area, but are located along the proposed detour route. Additionally, in 2022 and 2023, ALDOT solicited bids for several additional road projects in Colbert and Lauderdale Counties, including a structure demolition project along HWY 72 in Florence.

At-Grade Railroad Crossing (Future)

In 2022, the Northwest Alabama Council of Local Governments received funding to produce a study and design for the at-grade railroad crossing near Montgomery Avenue in Sheffield (USDOT 2022)

APPENDIX E:
DRAFT PROGRAMMATIC AGREEMENT

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE, NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S. COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

WHEREAS, the NPS plans to reconstruct the John Coffee Memorial Bridge along the Natchez Trace Parkway (Parkway), spanning across the Tennessee River, approximately 6 miles north of Cherokee, Alabama. The bridge is a contributing element of the National Register of Historic Places (NRHP)-listed Natchez Trace Parkway and the NRHP-eligible John Coffee Memorial Bridge Cultural Landscape; and

WHEREAS, the NPS is funding the proposed project on lands under its jurisdiction, which constitutes a federal Undertaking subject to compliance with Section 106 of the National Historic Preservation Act, as amended (NHPA; 54 USC § 306108), and its implementing regulations at 36 CFR § 800; and

WHEREAS, the NPS has worked in collaboration with the following Cooperating Agencies: Federal Highway Administration (FHWA), U.S. Coast Guard (USCG), Tennessee Valley Authority (TVA), and U.S. Army Corps of Engineers (USACE) on the bridge design and environmental compliance; however, the NPS retains responsibility for compliance with Section 106; and

WHEREAS, the project is on the Parkway in Colbert and Lauderdale Counties and the NPS has defined the Undertaking's Area of Potential Effects (APE) as approximately 300 feet from either side of the existing Parkway centerline, from just west of the Colbert Ferry Recreation Area entrance to just east of the ramps connecting the Parkway to Lauderdale County Road (CR) 2. An agricultural lease within the project study area, just southeast of the John Coffee Memorial Bridge, could serve as a construction staging area. The APE is shown in *Appendix A*, and encompasses all direct, indirect, and cumulative effects of the Undertaking; and

WHEREAS, the NPS has identified potential consulting parties in the Section 106 process. In addition to the Alabama State Historic Preservation Office (SHPO), TVA was invited to participate in the Section 106 process. Also, on behalf of the NPS, in keeping with a government-to-government relationship and in compliance with 36 CFR 800, applicable federally recognized Tribal governments were invited to participate, including: Absentee-Shawnee Tribe of Indians of Oklahoma; Alabama-Coushatta Tribe Texas; Cherokee Nation; The Chickasaw Nation; Coushatta Tribe of Louisiana; Eastern Band of Cherokee Indians; Eastern Shawnee Tribe of Oklahoma; Jena Band of Choctaw Indians; Kialegee Tribal Town; The Muscogee (Creek) Nation; Poarch Band of Creek Indians; The Seminole Nation of Oklahoma; Shawnee Tribe; Thlopthlocco Tribal Town; and United Keetoowah Band of Cherokee Indians in Oklahoma; and

WHEREAS, the following federally recognized Tribes chose to consult on the project: The Chickasaw Nation; Muscogee (Creek) Nation; Poarch Band of Creek Indians; Cherokee Nation; and Shawnee Tribe. In accordance with 36 CFR 800.2, these consulting parties were informed of efforts to locate previously identified historic properties and the results of those efforts and were asked to provide information on any unidentified National Register-listed or potentially eligible historic properties located within the proposed project's APE; and

WHEREAS, the NPS will continue Tribal consultation throughout the duration of the Undertaking; and

WHEREAS, the NPS had cultural resources surveys performed to identify historic properties within the APE. These determinations of eligibility (DOE) resulted in the NPS determining that the following historic properties are eligible for the NRHP:

(1) The Chikasha Aiasha Traditional Cultural Landscape (TCL) is a Traditional Cultural Property (TCP) located throughout the APE and includes Colbert/Koger's Island and the George Colbert Archaeological District. This property was determined eligible under Criteria A, B, and D for local and state level of significance in the areas of Ethnic Heritage-Native American, Exploration/Settlement, Transportation, Agriculture, Commerce, and Archeology/Prehistoric/(Precontact).

(2) The George Colbert Archeological District encompasses the original George Colbert Plantation and Ferry and includes the following contributing resources: NATR00403, NATR00652, NATR00406; NATR00653, NATR00331, the Old Natchez Trace; NATR00031/1LU48, NATR00420/1LU312, NATR00421/1LU313, NATR00654/1LU802, NATR00034/1CT38, NATR00036/1CT40, NATR00035/1CT39, NATR00164/1LU49, NATR00178/1CT141, NATR000165/1CT31, NATR00169/1CT37, NATR00175/1LU45; and the location of the ferry, which is likely submerged and was not identified in the underwater archeology survey. In addition, the district includes Georgetown Cave (NATR00/1CT42), North Cave (1CT182), Chert Cave (1CT183), and South Cave (1CT151). The district as a whole is considered eligible for the NRHP under Criterion D.

(3) The John Coffee Memorial Bridge Cultural Landscape is an NRHP-eligible site located within the APE (Attachment A). The eligible National Register boundary comprises approximately 498 acres and contains all National Register qualifying characteristics and features of the property contributing specifically to various points in time during the period of significance from 1801 to 1968. The John Coffee Memorial Bridge cultural landscape is significant under Criterion C as embodying the distinctive characteristics of Parkway construction during the twentieth century.

(4) The Natchez Trace Parkway is an NRHP-eligible district that runs between Natchez, Mississippi, and Nashville, Tennessee (Attachment A). The entire length of the Parkway has been determined eligible for listing in the National Register under Criteria A and C, and the John Coffee Memorial Bridge is considered a contributing resource; and

WHEREAS, the SHPO concurred on _____, that the project constitutes an Undertaking and that the APE encompasses all direct, indirect, and cumulative effects and concurred that the

Chikasha Aiasha TCL, George Colbert Archeological District, John Coffee Memorial Bridge Cultural Landscape, and the Parkway are historic properties listed, or eligible for listing, on the NRHP; and

WHEREAS, the NPS determined that implementation of the Undertaking would result in a finding of Adverse Effect to the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway. The AL SHPO concurred with this assessment on _____ (SHPO # _____); and

WHEREAS, the NPS has determined that the Undertaking will not adversely affect the Chikasha Aiasha TCL and the George Colbert Archeological District. It is anticipated that there would be physical effects inside the NRHP-eligible boundary of the George Colbert Archeological District; however, current planning seeks to avoid contributing resources in the district. If, during the design phase, plans require that there would be direct physical effects to contributing portions of the district, then NPS would be required to consult with SHPO and THPOs and revisit the No Adverse Effect determination for both the Chikasha Aiasha TCL and the George Colbert Archeological District. The AL SHPO concurred with this assessment on _____; and

WHEREAS, the NPS has developed this Programmatic Agreement (PA) to resolve adverse effects to the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway through the development of avoidance, minimization, and mitigation measures in accordance with 36 CFR §§ 800.6(b) and 800.6(c); and

WHEREAS, the AL SHPO is authorized to enter into this PA in order to fulfill its role of advising and assisting federal agencies in carrying out their responsibilities under Sections 110 and 106 of the NHPA (36 CFR §§ 800.2[c][1][i] and 800.6[b]), and AL SHPO is participating as a signatory; and

WHEREAS, the FHWA, TVA, USCG, and USACE are all cooperating agencies, have worked collaboratively with NPS on the bridge design and environmental compliance, and are participating as signatories; and

WHEREAS, in accordance with 36 CFR § 800.6(a)(1), the NPS notified the Advisory Council on Historic Preservation (ACHP) of its adverse effect determination on _____, with specified documentation, and no response was received after 15 calendar days had elapsed by _____, indicating that the ACHP chose not to participate in this PA pursuant to 36 CFR § 800.6(a)(1)(iii); and

WHEREAS, pursuant to 36 CFR § 800.2(d), the NPS solicited and considered public comment throughout the Section 106 process, including consulting party review of a draft of this PA. Additionally, it was included as an appendix in the public review copy of the Environmental Assessment (EA) in November 2024. The public review period for the EA was ___ days, and comments were considered and integrated into the current design for the Undertaking; and

WHEREAS, the definitions in this PA follow 36 CFR § 800.16; and

NOW, THEREFORE, the NPS, AL SHPO, TVA, FHWA, USCG, and USACE agree that the Undertaking shall be implemented in accordance with the following stipulations to resolve the adverse effects on historic properties.

STIPULATIONS

The NPS shall ensure that the following measures are carried out:

I. APPLICABILITY AND SCOPE

This PA applies to the planning, design, and construction associated with the various project components of the undertaking identified in Attachment A.

II. ADMINISTRATIVE STIPULATIONS

- A. In the event that another federal agency, not initially a party to this PA, receives an application for funding/license/permit for the Undertaking, as it is described in this PA, that agency may fulfill its Section 106 responsibilities by stating in writing that it concurs with the terms of this PA and by notifying the Signatories that it intends to do so. Such agreement shall be evidenced by execution of a Signature Page and filing it with the ACHP, and implementation of the terms of this PA.
- B. This PA may be executed in counterparts, with a separate page for each Signatory and Invited Signatory.
- C. Email will be an acceptable form of communication between the Consulting Parties and is an appropriate method of “notification” or “in writing” where it is called for in this PA, unless otherwise described. If a Consulting Party does not have access to email or consistently available internet service, then the NPS will ensure that other forms of communication are made available. All the Consulting Parties should immediately notify the NPS if a point of contact within their organization changes and provide updated information.

III. ROLES AND RESPONSIBILITIES

The NPS is the lead agency for administering and implementing this PA. These responsibilities include, but are not limited to, the following: consulting and coordinating with the consulting parties; conducting Government-to-Government consultation with the Tribes; overseeing all cultural resources work; assembling all submissions to the consulting parties, including cultural resources monitoring and reporting; and seeking AL SHPO concurrence with all agency compliance decisions. It is anticipated that monitoring and associated documentation while being overseen by the NPS, would be completed by a qualified contractor.

IV. PROFESSIONAL QUALIFICATIONS

- A. All work carried out pursuant to this PA shall meet the Secretary of the Interior’s Professional Qualification Standards for Archaeology and Historic Preservation, as per

Section 112(a)(1)(A) of the NHPA and 36 CFR § 800.2(a)(1) of the implementing regulations.

- B. All work to address historic buildings or structures will be conducted by or under the direct supervision of a Historian, Historic Architect, or Historic Landscape Architect that *meets the Secretary of Interior's Professional Qualification Standards (Federal Register Vol. 48, No. 190:44738-44739)*.

V. ALTERNATE PROCEDURES

- A. NPS, in consultation with AL SHPO, TVA, FHWA, USCG, and USACE, will follow an alternate procedure to comply with Section 106 of the National Historic Preservation Act regarding undertakings for the new bridge that are included in Attachments B and C.

- 1. The Parkway Cultural Resource Management (CRM) Team will review undertakings to determine if activities are covered in Attachments B and C. If so, the NATR CRM Team will document to file the findings and include the undertaking in the annual report.

- 2. If the project activities include ground disturbance, then archaeological monitoring may be appropriate throughout the ground-disturbing activities, in accordance with any recommendation of the CRM Team. When monitoring is recommended, members of any appropriate Federally recognized Indian Tribes may be invited to participate in monitoring.

- 3. NATR may not use alternative procedures if an undertaking's APE has not been inventoried for historic properties or if there is potential to adversely affect a known historic property that is not part of the bridge replacement APE.

- B. NPS will use standard Section 106 (36 CFR 800) for undertakings that are not included in Attachments B and C, or are otherwise precluded from this PA.

VI. ACTIVITIES WITH NO POTENTIAL FOR ADVERSE EFFECTS

See Attachment B

VII. ACTIVITIES THAT MAY RESULT IN ADVERSE EFFECTS

See Attachment C

VIII. RESOLUTION OF ADVERSE EFFECTS

Pursuant to 36 CFR § 800.6(a), this PA commits NPS to measures to resolve adverse effects to historic properties. The measures discussed below focus on the John Coffee Memorial Bridge Cultural Landscape and the Parkway, including the treatment and documentation of NRHP-contributing features of these historic properties. Additional measures are provided for the protection and documentation of any unanticipated

discoveries. The NPS shall ensure all construction documents and/or contracts include applicable provisions, and herein commits to the following:

- A. Continued consultation with AL SHPO, TVA, USCG, FHWA, and USACE regarding design, construction, circulation, landscape, reclamation, and mitigation.
- B. Efforts made during design development to avoid and minimize adverse effects and efforts to maintain aspects of integrity for each contributing resource, including the landscape.
- C. Consideration of the Secretary of the Interior's Standards for the Treatment of Historic Buildings for proposed work, including guidelines for appropriate design and construction techniques within the boundaries of the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway.
- D. Consideration of relevant comments received during the Section 106 public review period.
- E. A general schedule to implement cultural resources continued consultation, reporting, and reviews.
- F. Procedures for archeological monitoring, evaluating, and treating discoveries of unanticipated or newly identified cultural resources during construction associated with the Undertaking, including consultation with appropriate parties (See Stipulation IX). This may include additional archeological testing and data recovery for sites within or potentially outside of the area of potential effects (APE) for the undertaking, or additional cultural landscape, or historic built environment research and interpretation of historic properties within and potentially outside of the APE.
- G. Adherence to requirements in the Native American Graves Protection and Repatriation Act (NAGPRA) (2023) for any inadvertent discoveries;
- H. Completion of an NRHP nomination form for Chikasha Aiasha TCL in consultation with the Chickasaw Nation for submission to the Keeper of the Register and listing on the NRHP.
- I. Level III Historic American Landscapes Survey (HALS) documentation shall be completed for this project. See Stipulation XI below.
- J. *Additional mitigation measures may be agreed to through consultation with the public, Tribes, SHPO, NPS, the construction team, and potentially the ACHP.

CULTURAL RESOURCES MONITORING GENERAL PROVISIONS

- A. The NPS will ensure that the location of contributing elements of the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway within or adjacent to the project area are incorporated into construction design documents. This process will make sure to safeguard this information so as to not identify

sensitive resources to personnel not directly involved with the Undertaking. Historic features will be labeled with their associated treatment measure. Having the location and extent of these resources in the designs will allow for more efficient communication and ensure that crew members coordinate with cultural monitors, as necessary.

- B. The NPS will ensure that construction crews provide a minimum 14 calendar-day notice to NATR before conducting activities that disturb structures or ground-disturbing activities within or adjacent to contributing features of the eligible historic properties and shall specify the estimated length of time and extent of ground-disturbing and structural actions (e.g., _____). This information will be shared with the associated monitors to determine if their presence is required during construction. Multiple potential monitors and clear communication protocols in the event of potential delays or lack of availability of a monitor will be addressed prior to the start of construction in sensitive areas.
- C. If previously unreported cultural resources are encountered, the work shall be halted until the discovery is documented and evaluated for its significance and NRHP eligibility in coordination with the NPS subject matter expert. The NPS will notify AL SHPO and Tribes of the discovery, and consultation will follow, as appropriate, in accordance with Stipulation VI.

IX. HISTORIC PROPERTY TREATMENT, CONSTRUCTION MONITORING, AND REPORTING

- A. The NPS will include the expertise of the NATR Resource Management staff on the design team to ensure that the Secretary of the Interior's Standards for Rehabilitation, Standard 9, are met, specifically: *New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to project the historic integrity of the project and its environment*, where possible. Findings from these efforts will be incorporated into the regular quality control assessments during construction.
- B. The monitor and construction team will ensure the repair of contributing features of the historic district, and the placement of new masonry will be designed to minimize the effects on the historic character of contributing elements and the overall historic setting of the district. This includes the use of masonry techniques that replicate the aesthetics of existing significant features. Masonry shall utilize techniques that include matching mortar type, composition, color, and joint profile for each feature.
- C. Any source of building materials will be assessed for potential disturbance to historic properties and fully comply with the associated requirements of the NHPA.

- D. Depending on the organizational composition of the contractor providing the monitoring services (i.e., if the archeological work is subcontracted), an Archeological Resources Protection Act permit may be required. If required, this permit will be obtained through the NPS Southeast Archeological Center. All monitoring will include review of the archeological investigation findings completed by New South Associates (Meyers 2024), which provide context for surface and subsurface findings. An archeological monitor will be on hand for a project kick-off meeting at the project area to identify the location and extent of archeological resources, provide an educational briefing describing the types of archeological materials that have been found and could be encountered during construction, and provide guidance for protecting and reporting any discoveries during construction. Alternative monitors will also be identified to address any personnel shifts or unanticipated logistical challenges or emergencies. The portions of terrestrial sites outside of the APE will be marked with flagging or plastic fencing, where practical, to ensure ground disturbance does not occur in these areas. Monitoring will include observation of the ground disturbance during construction and sampling of displaced soils using ¼-inch wire mesh throughout the APE. Once construction reaches the full extent of ground disturbance in any location, archeological monitoring during these phases will not be required, unless significant cultural materials are identified. The monitor will be authorized to halt ground-disturbing activities within the associated specific location of the findings while recovering materials and data. If, through this process, minimal or no cultural materials are encountered, the soil sampling may be significantly reduced or discontinued. Monitoring will not be recommended in areas where previous archeological controlled excavations were completed with no associated findings. Monitoring will include digital photographs of construction in progress, diagnostic or rare artifacts not previously documented, archeological screening in progress, and site overviews once construction is complete.
- E. The archeological monitor will complete an update to the Alabama state site cards for all sites disturbed during construction activities to reflect monitoring findings and changes, including updates to the site sketch map for construction actions and new findings. The archeological monitor will also compose a report of construction monitoring findings, to include an introduction, project background, and summary of findings, field methods, results, and recommendations for further site investigation and management will be included. This reporting will incorporate information from the NSA report (Meyers 2024) and be submitted to NATR, NPS Southeast Archeological Center, Tennessee Valley Authority (for any updates to sites within the Tennessee River, and the Alabama SHPO for 30-day review and comment after completion of all ground disturbance within or adjacent to archeological sites. All review comments and edits will be integrated into final reporting.

- F. Archeological monitors will consider if artifacts should be collected or left in place based on their potential to address significant research questions or if they represent rare or unique items that would be damaged during construction. Further discussion on collections may require additional consultation with the Tribes, Tennessee Valley Authority, and Alabama SHPO, as tribes have previously recommended returning materials to the ground where possible. An associated collection and discard policy will be integrated into the archeological monitoring contract, and artifact collection will also be discussed with the tribal monitor. All collections will include initial processing at the archeological contractor's facility and will be submitted to the Southeast Archeological Center for final curation.
- G. Archeological and tribal monitoring will occur in partnership with construction crews. The NPS and tribes can decide, through discussion with the monitors in the field, if the proposed work requires monitoring in certain areas and not in others. This decision will be based on the location of work, findings during previous surveys and excavations, findings from previous days of monitoring, the extent of ground disturbance, and the nature of the proposed work. This will allow for flexibility during the months of implementation of the project and unanticipated challenges such as construction delays, shifts in schedule, and differences in the location, extent, and significance of findings during monitoring.
- H. The NPS, tribes, and construction contractor will ensure that efforts to communicate with the monitors are thorough and that the correct contact information is shared between parties prior to construction. This includes any changes in monitor personnel, as it is anticipated that through the lengthy period of construction, there will be shifts in who is conducting the monitoring. Monitors and construction crews should also ensure that monitors are regularly briefed as to the nature of work to occur on a given day and that any change in personnel includes a briefing to that new person to include personnel contact information, the work location, scheduling, anticipated duration, and previous findings.
- I. If all communication provisions in this agreement are adhered to and the archeological or tribal monitor does not appear for scheduled work, and alternative monitors are not available, the work may proceed. However, the NPS contracting officer shall be notified by the contractor at the start of the workday, and the contractor and contracting officer shall attempt to clarify why the monitor and any alternates were not available and resolve, where possible, any communication or logistical challenges that may have contributed to the monitors' absence. In cases of a monitor being delayed for a reasonable and short-term duration, the contractor and NPS contracting officer shall coordinate to attempt to conduct work in non-sensitive areas not requiring monitoring where feasible, to allow for the monitor to be present during work in sensitive areas. Written documentation of this effort and outcome will be composed by the contractor, monitor, and NPS for project records and to address any lingering issues.

X. POST REVIEW DISCOVERIES

The NPS shall ensure that all construction documents include the following provisions:

- A. If previously unidentified historic properties or unanticipated effects to historic properties are discovered during construction activities, the contractor shall immediately halt all activity within a 100-foot radius of the discovery, notify the NPS of the discovery, and implement interim measures to protect the finding(s) in place.
- B. Immediately upon receipt of the notification required in Stipulation X.A of this document, the NPS shall:
 - 1. Inspect the site to determine the extent of the discovery and ensure that construction activities have halted.
 - 2. Clearly mark the area of the discovery.
 - 3. Implement additional measures, as appropriate, to protect the discovery from looting and vandalism.
 - 4. Have a qualified subject matter expert (archeologist, historian, historic landscape architect, historic architect, etc., as appropriate) inspect the construction site to determine the extent of the discovery and provide recommendations regarding its NRHP eligibility and treatment.
 - 5. Depending on recommendations from the qualified subject matter expert after discussion with NPS cultural resources personnel, for resources that could potentially be eligible for the NRHP the NPS will notify AL SHPO and Tribes and traditionally associated with NATR. Notification will include a description of the finding and the measures that have been implemented to comply with Stipulations X.B.1-4 of this document.
- C. Within 48 hours of receipt of the notification described in Stipulation X.B.5 of this document, the NPS shall provide the AL SHPO and Tribes traditionally associated with NATR with its assessment of the NRHP eligibility of the discovery and the measures it proposes to take to resolve adverse effects. In making its official evaluation, the NPS, in consultation with the AL SHPO and Tribes may assume the discovery to be NRHP eligible for the purposes of Section 106 pursuant to 36 CFR § 800.13(c). The AL SHPO and tribes shall respond within 48 hours of receipt.
- D. The NPS, which shall take into account the consulting parties' recommendations on eligibility and treatment of the discovery, shall ensure that appropriate actions are carried out and provide the AL SHPO and the other consulting parties with a report on these actions when they have been implemented.

- E. Construction activities may only proceed after NPS has determined that implementation of any actions necessary to address the discovery pursuant to Stipulation X.A-D have been completed.
- F. The NPS will comply with the requirements of NAGPRA, including the 2023 updates to NAGPRA implementing regulations, and will adhere to the principles outlined in the ACHP's Policy Statement on Burial Sites, Human Remains, and Funerary Objects, dated March 1, 2023. No human remains or NAGPRA cultural items are known to exist in the APE; however, any such encounters would be treated in accordance with a NAGPRA Plan of Action as developed among the NPS and Tribes, as necessary.

XI. LEVEL III HALS DOCUMENTATION AND NRHP NOMINATION

- A. Descriptions of the three levels of HALS documentation were published in the Federal Register on July 21, 2003, and are available at https://www.nps.gov/hdp/standards/standards_regs.pdf. Level III HALS documentation will consist of the following:
 - 1. Photographs: photographs with large-format negatives of the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway/John Coffee Memorial Bridge. A sketch plan showing locations and directions of views will be included in the photography index.
 - 2. Written data: a short form historical report.
- B. Documentation will be prepared in accordance with the HALS Guidelines for history, drawing, photography, and transmittal available at <https://www.nps.gov/hdp/standards/halsguidelines.htm>.
- C. Preparation of HALS documentation is currently being coordinated in advance with AL SHPO and the NPS National Park Service, Interior Region 2, Heritage Preservation Partnerships Program (HPP). Photographic documentation of the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway/John Coffee Memorial Bridge is complete, and revised draft documentation is currently being reviewed based on NPS HPP comments. It is likely the reporting will be submitted in approximately Fall 2024. Following HPP acceptance, final documentation will be provided to HPP for transmittal to the Library of Congress for archiving and public access.
- D. The NRHP nomination form for the Chikasha Aiasha TCL and revised draft HALS documentation will be complete before ground disturbance or other modification of contributing features of the archeological district and cultural landscape. Draft reporting will be submitted to NATR and the AL SHPO for 30-day review and comment.
- E. No physical impacts to contributing elements of the John Coffee Memorial Bridge Cultural Landscape and the Natchez Trace Parkway will occur until documentation has been accepted in writing by the AL SHPO and HPP.

XII. REVIEW AND COMMENT

The NPS will submit all documentation related to the Undertaking (e.g., cultural landscape inventory and NRHP nominations, Alabama site form updates, HALS documentation, etc.) to the consulting parties for review and comment unless stipulated elsewhere in this PA. Consulting parties shall have 30 days from receipt to provide written comments. If a party does not comment on a submittal during this period, the NPS will follow up by telephone or e-mail with the party. If, after such reasonable and good-faith efforts to reach an unresponsive consulting party, there has still been no response, the NPS will proceed to the next step.

XIII. REPORTING AND REVIEW

- A. Following the execution of this PA, the NPS and FHWA will convene prior to construction to ensure that stipulations in this agreement are understood and associated requirements for documentation prior to construction are completed. The NPS will then compose a letter report (Annual Report) upon commencement of construction, which is anticipated to be within one year of execution of this PA. Following this first year, until this PA expires or is terminated, the NPS will compose additional brief Annual Reports to review the progress under this PA and associated treatment measures. The Annual Report will include an update on the project schedule, status, and any ongoing cultural resources monitoring or mitigation activities, discovery situations, proposed future actions, or outstanding tasks to be completed under this PA. Consulting parties will have 30 calendar days to review the Annual Report and provide comments to the NPS, who will then address the comments. The NPS will share the report with consulting parties to this PA and ask if parties are interested in attending a virtual annual meeting.
- B. If consulting parties request an annual meeting, the NPS will address the comments on the annual report to develop the meeting agenda. The meeting shall include a discussion of construction progress, any scheduling changes proposed, any problems encountered, associated findings for any disturbances or enhancements to historic properties, identification of any new discoveries, and any disputes and objections received in NPS's efforts to carry out the terms of this PA.
- C. Within 14 days after the annual meeting, the NPS will summarize the meeting, including proposed action items and how they are to be addressed, in a letter to consulting parties. Consulting parties will have 20 days to review and comment on the meeting notes and, if necessary, provide the NPS with any edits to the meeting notes. If changes are needed, the NPS will produce revised meeting notes within 30 days of receipt of comments and will provide the final notes to the consulting parties.

XIV. DISPUTE RESOLUTION

Should any signatory to this PA object at any time to any actions proposed or the manner in which the terms of this PA are implemented, the NPS shall consult with such party to resolve the objection. If NPS determines that such an objection cannot be resolved, NPS will follow the process outlined in 36 CFR §800.7:

- A. Forward all documentation relevant to the dispute, including NPS's proposed resolution, to the ACHP. The ACHP shall provide the NPS with its advice on the resolution of the objection within 45 days of receiving adequate documentation. Prior to reaching a final decision on the dispute, the NPS shall prepare a written response that considers any timely advice or comments regarding the dispute from the ACHP, signatories, and concurring parties, and provide them with a copy of this written response. The NPS will then proceed according to its final decision.
- B. If the ACHP does not provide its advice regarding the dispute within the 45-day period, the NPS may make a final decision on the dispute and proceed accordingly. Prior to reaching such a final decision, the NPS shall prepare a written response that considers any timely comments regarding the dispute from the signatories to the PA and provide them and the ACHP with a copy of such written response.
- C. The NPS's responsibility to carry out all other actions subject to the terms of this PA that are not the subject of any dispute shall remain unchanged.

XV. CONFIDENTIALITY

To the maximum extent allowed by federal and state law, the NPS will maintain confidentiality of sensitive information regarding historic properties that could be damaged through looting or disturbance, and/or to help protect a historic property to which a Tribe attaches religious or cultural significance. However, any documents or records the NPS has in its possession are subject to the Freedom of Information Act (FOIA) (5 USC 552 et seq.) and its exemptions, as applicable. The NPS shall evaluate whether a FOIA request for records or documents would involve a sensitive historic property, or a historic property to which a Tribe attaches religious or cultural significance, and if such documents contain information that the NPS is authorized to withhold from disclosure by other statutes including the Section 304 of the NHPA, as well as the Archeological Resources Protection Act. If this is the case, then the NPS will consult with the Keeper of the National Register of Historic Places and the ACHP regarding withholding the sensitive information. If a Tribally sensitive property is involved, the NPS will also consult with the relevant Tribe(s) prior to making a determination in response to a FOIA request.

XVI. AMENDMENTS

This PA may be amended when such an amendment is agreed to in writing by all signatories. The amendment will be effective on the date a copy signed by all the signatories is filed with the ACHP.

A. PA Attachments may be amended with a streamlined process as follows. Any PA Signatory may propose an amendment to an Attachment by submitting a request in writing to NPS. If NPS concurs that the amendment improves or updates the Attachment(s), NPS will share the proposed amendment with the Consulting Parties for a 45-day review and comment period. If no comments are received at the end of the review period, NPS will move forward with the proposed amendment and will provide Consulting Parties with a revised version of the Attachment(s).

XVII. TERMINATION

- A. If any signatory to this PA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other parties to attempt to develop an amendment per Stipulation XVI of this PA. If an amendment cannot be reached within 30 days, any signatory may terminate the PA upon written notification to the other signatories and concurring parties.
- B. Once the PA is terminated, and prior to work continuing on the Undertaking, the NPS must either (a) execute a new PA pursuant to 36 CFR § 800.14(b), or (b) comply with 36 CFR 800 with regard to each undertaking pursuant to 36 CFR 800.14(b)(2)(v). The NPS shall notify the signatories as to the course of action it will pursue.

XVIII. DURATION

This PA will expire if its terms are not carried out within 10 years from the date of its execution. Prior to such time, the signatories may consult and agree in writing to an extension for carrying out the terms of the PA in accordance with Stipulation XVI above.

XIX. ANTI-DEFICIENCY ACT

The NPS's obligations under this PA are subject to the availability of appropriated funds, and the stipulations of this PA are subject to the provisions of the Anti-Deficiency Act (31 U.S.C. Section 1341). The NPS shall make reasonable and good-faith efforts to secure the necessary funds to implement this PA in its entirety. If compliance with the Anti-Deficiency Act alters or impairs the NPS's ability to implement the stipulations of this PA, the NPS shall consult in accordance with the amendment and termination procedures found at Stipulations XVI and XVII of this PA.

Execution of this PA by the NPS, AL SHPO, FHWA, TVA, USCG, and USACE and implementation of its terms are evidence that the NPS has taken into account the effects of this Undertaking on historic properties and afforded the ACHP an opportunity to comment and that the NPS has satisfied its Section 106 responsibilities for the Undertaking.

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

SIGNATORY:

National Park Service – Natchez Trace Parkway

_____, NATR Superintendent

Date: _____

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

SIGNATORY:

Alabama State Historic Preservation Office

_____, Alabama State Historic Preservation Officer
Date: _____

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

SIGNATORY:

Federal Highway Administration

_____, Federal Highway Administration, [Title] Date: _____

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

SIGNATORY:

U.S. Coast Guard

_____, U.S. Coast Guard, [Title] Date: _____

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

SIGNATORY:

Tennessee Valley Authority

_____, Tennessee Valley Authority, [Title] Date: _____

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

SIGNATORY:

U.S. Army Corps of Engineers

_____, U.S. Army Corps of Engineers, _____
Date: _____

**PROGRAMMATIC AGREEMENT
BETWEEN THE NATIONAL PARK SERVICE NATCHEZ TRACE PARKWAY
(NATR), THE TENNESSEE VALLEY AUTHORITY, THE FEDERAL HIGHWAY
ADMINISTRATION, THE U.S.COAST GUARD, THE U.S. ARMY CORPS OF
ENGINEERS, AND THE ALABAMA STATE HISTORIC PRESERVATION OFFICE
REGARDING THE REPLACEMENT OF THE JOHN COFFEE MEMORIAL
BRIDGE/TENNESSEE RIVER, COLBERT AND LAUDERDALE COUNTIES,
ALABAMA**

CONCURRING PARTIES:

Chickasaw Nation

_____ Date _____
Bill Anoatubby, Governor

Muscogee (Creek) Nation

_____ Date _____
David Hill, Principal Chief

Poarch Band of Creek Indians

_____ Date _____
Stephanie A. Bryan, Tribal Chair and CEO

Cherokee Nation

_____ Date _____
Chuck Hoskin, Jr., Principal Chief

Shawnee Tribe

_____ Date _____
Benjamin Barnes, Chief

Appendix A: Description of the Undertaking and Area of Potential Effects

Introduction

The NPS is proposing to reconstruct the John Coffee Memorial Bridge along the Natchez Trace Parkway (Parkway), spanning across the Tennessee River, approximately 6 miles north of Cherokee, Alabama. While the Tennessee Valley Authority (TVA) manages the river, the Parkway manages the John Coffee Memorial Bridge and areas on both sides of the river. Design and construction for the project is being conducted by the Federal Highway Administration (FHWA) Eastern Federal Lands Highway Division. The U.S. Coast Guard (USCG) oversees the use of this navigable waterway within the jurisdiction of the 8th District. The Parkway is consulting with the USCG and U.S. Army Corps of Engineers (USACE) regarding the bridge design and environmental analysis. TVA, FHWA, USACE, and USCG are all cooperating agencies in the National Environmental Policy Act (NEPA) process.

The project study area includes approximately 148 acres, bounded by the Tennessee River to the east and west (Figure 1). The primary feature within the project area is the John Coffee Memorial Bridge. The project study area covers areas where construction and staging would occur on the riverbanks and within the river.

Purpose and Need

The purpose and need of the project is to maintain a safe and reliable bridge crossing for Parkway users over the Tennessee River, while minimizing effects on Parkway resources. It is needed because this 60-year-old bridge has exceeded its 50-year design life. The John Coffee Memorial Bridge is currently safe to drive on, and the bridge would continue to be inspected annually for safety. However, the bridge is a fracture-critical structure, which means there are no redundancies in the bridge members and the failure of one girder would cause one or multiple spans to fail leading to the potential for collapse. In addition, there is widespread cracking in the bridge deck and notable deterioration of the bridge piers, and routine maintenance can no longer address these structural issues. The NPS and FHWA estimate that the bridge would be determined unsafe for vehicle travel in approximately 10 years.

Project Undertaking and Area of Potential Effects

The project involves replacement of the Tennessee River Bridge on the Parkway. This includes construction of new abutments, removal of existing piers, installation of new piers, construction of a new roadway to connect the bridge to adjacent areas, and extensive staging of equipment and materials over a construction period lasting multiple years. The undertaking is located approximately 6 miles north of Cherokee, Alabama, and overlaps Colbert and Lauderdale counties. The area of potential effects includes all areas within 100 meters of the centerline of the Tennessee River Bridge and up to 100 meters from the centerline of the Parkway on either side of the bridge up to approximately 0.5 mile away from the river, covering approximately 90 acres. It also includes the staging area southeast of the bridge and west of Lauderdale County Road 2 to the Tennessee River, covering approximately 14 acres. The depth of excavation required for construction will vary based on the nature of construction, but in terrestrial areas may extend to approximately 50 feet below surface and in marine environments to the mudline approximately 10 feet below the river bottom.

Figure 1. Project Location Map

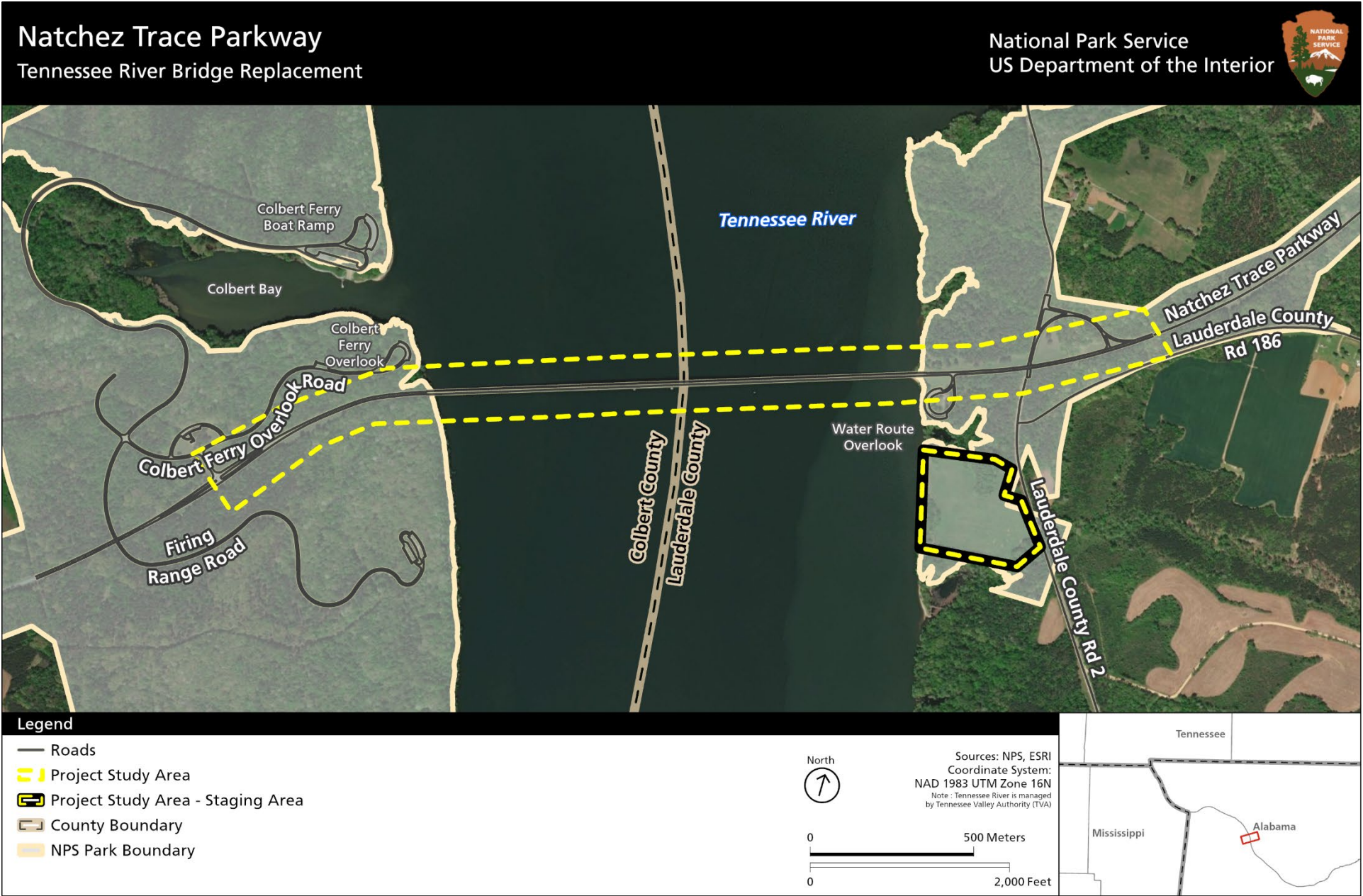


Figure 2. Survey Area Alternative 1

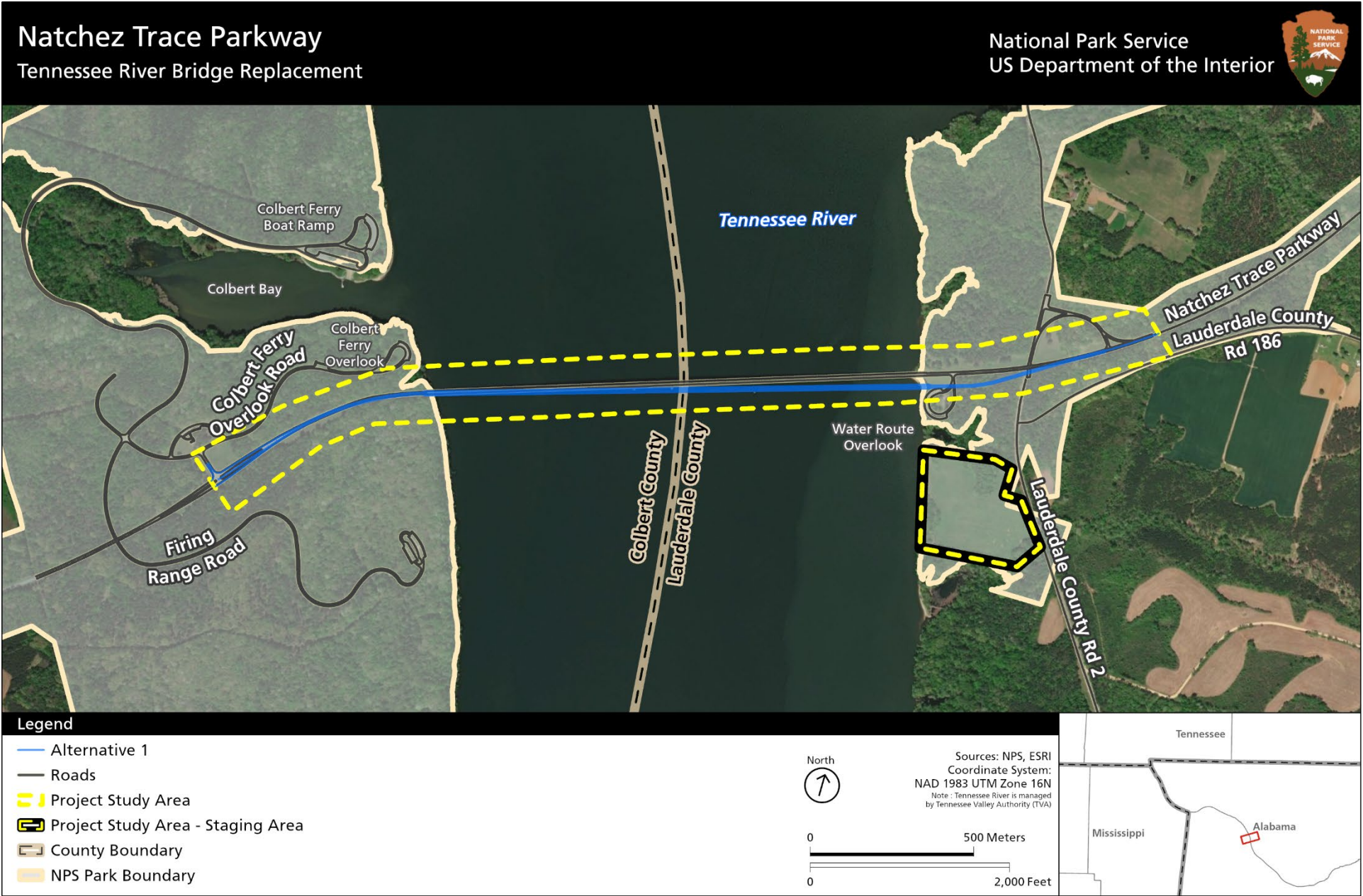
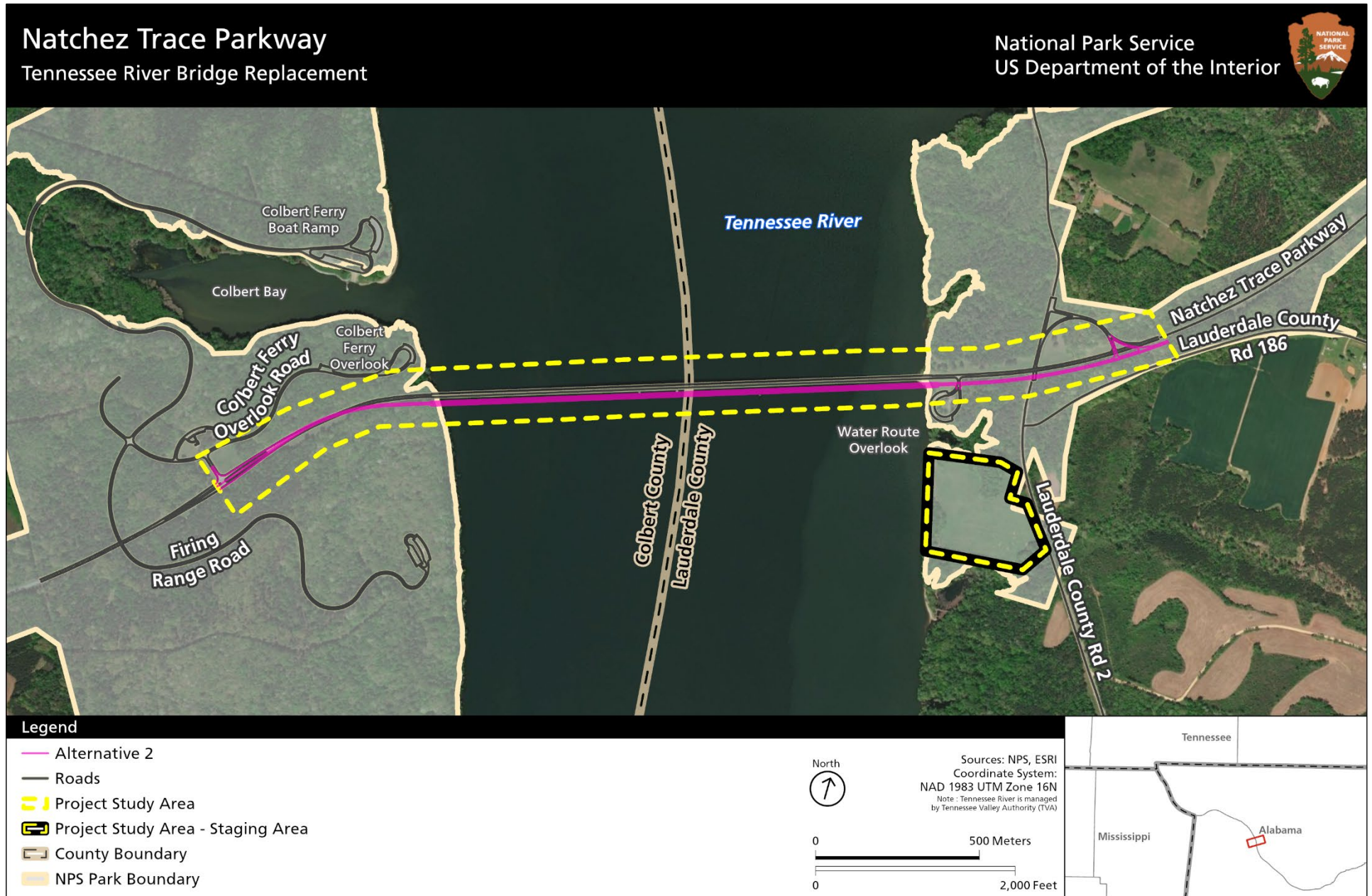


Figure 3. Survey Area Alternative 2



Appendix B: Activities with No Potential for Adverse Effect

- A. The following activities do not have a potential for adverse effect as they take place within the existing road corridor, within previously disturbed areas, are small in size, and/or temporary, and present no lasting visual impact on historic properties. Implementation of these activities will require no further consultation with SHPO or other parties other than annual reporting.
- B. When possible, any installations will be painted to match the existing terrain and will be sited in such a way as to make them less visible from the Parkway.
- C. These actions include the following undertakings as well as others that are comparable in scope, scale, and impact:

1. Road-Related Monitoring:

- a. Geotechnical Drilling and Installations. This action includes the following installations as well as others that are comparable in scope, scale, and impact:
 - i. Drilling and installation of subsurface instrumentation within the road structure or outside of archeological site boundaries without substantial new vegetation or ground disturbance for the purpose of understanding geologic material integrity and its relationship to other geologic layers.
 - ii. Boreholes may be up to 8” in diameter, have caps flush with the surface, and may have steel casing.
 - iii. Instrumentation and subsurface installations must be recovered when possible or safely abandoned in place when necessary.
 - iv. All holes without instrumentation will be backfilled at the conclusion of drilling, and the grout at the surface will be stained to match the surrounding rock.

2. Road Maintenance:

- a. Installation of engineered materials such as geotextile fabric placed under the substrate within the road structure that is not plainly visible to visitors.
- b. Placement of temporary safety-related signage within the roadway (including ditches and associated structures). Temporary is defined as the time required to complete the task and reopen the road without undue risk to visitor or staff safety.
- c. Routine maintenance and repair activities as covered under the _____ NATR Standards and the NATR Road Design Standards (Road Design Standards), and to include dust suppressant activity. This excludes new construction of bridges, new (not replacement) culverts, and permanent signage.
- d. Engineered internal slope reinforcement structures may be pursued where conditions resulting in a geohazard event pose a threat to the road structure and when the installation of the structures would not adversely affect any historic properties, as determined by the NATR CRM Team.
 - i. Installation of stabilizing structures such as gabions, crib-block walls, anchors, bolts, pins, retention nets, fences, walls, ferro-cement coatings, benches,

retaining walls, piles, piers, porous fill, concrete cribbing, check dams (various materials and designs), concrete and reinforced earth retaining walls, large diameter caissons, counterweight berms, buttress counterforts, ground anchors, lime/cement stabilization, grouting, soil nailing, steel pipe grid dams, heat treatment, and freezing outside of archeological site boundaries.

- ii. Re-grading (slope geometry modification) outside of archeological site boundaries.
- iii. Removal of unstable material, biotechnical slope stabilization (combination of vegetative and structures).
- iv. Culverts. Activities to include installation of new culverts outside of archeological site boundaries, with the smallest appropriately sized culvert to be used. Culvert placements should minimize impacts to road character.
- v. Ditches and drains. Activities to include installation of new drainage including: ditches, rockfall ditches, surface drains, French drains, trench drains, counterfort drains, deep drains, drainage tunnels, drainage wells, horizontal and sub-horizontal drainage bores, vacuum and siphon drains outside of archeological site boundaries. Any new drainage should not impact historic property, as determined by the NATR CRM Team.

3. Storage of Heavy Equipment and Road Repair Materials:

- a. Heavy equipment and road construction materials may be temporarily staged or stored at established pullouts or other locations as safe and practical. Visual impacts to historic properties will be minimized as much as possible.

4. Road Closures

- a. Road closures are considered temporary, and associated road barriers/signs will follow the Road Design Standards or Road MRO Standards.

Appendix C: Activities That May Result in Adverse Effects

This includes the construction of bridge and road components involving substantial new ground disturbance within the Tennessee River and in adjacent terrestrial areas, particularly within archeological sites.

- a. Significant changes to the road alignment, due to removal of the historic Parkway bridge and replacement with a new bridge.
- b. Installation of engineered materials within the road structure that are plainly visible to visitors and permanent.
- c. Temporary monitoring and related equipment and or signs that present adverse visual impacts to the historic district and which become permanent.
- d. Replacement or new culverts that are significantly larger, or are of a significantly different type, that either individually or cumulatively have an adverse visual impact to the historic district or cultural landscape.
- e. Alterations to historic ditches due to the need for modern drainage operations.

Appendix D: Contributing and Non-Contributing Features Historic Properties in the Project Area

The following table is condensed from the Cultural Landscape Report (CLR) for the John Coffee Memorial Bridge/Colbert Ferry Park/Water Route Overlook cultural landscape - *National Park Service Cultural Landscapes Inventory, John Coffee Memorial Bridge/Colbert Ferry Park/Water Route Overlook 2024* (Heritage Landscapes, LLC).

Feature Name	Feature Contribution
Natural Systems and Features	
Tennessee River	Contributing
Native Hardwood Forest	Contributing
Colbert Bay	Contributing
Colbert Creek	Contributing
Rock Bluffs	Contributing
Spatial Organization	
Curving Roadway with pullouts for overlooks and recreation areas	Contributing
Distributed recreational uses (picnic, boat launch, campground) and historic spaces	Contributing
Land Use	
Recreation	Contributing

Feature Name	Feature Contribution
Transportation	Contributing
Interpretation	Contributing
Topography	
Designed topography of raised roadbed and drainage system / Manipulated topography to move drainage away from roads	Contributing
Grade change at bridge abutments that allow the parkway to “float” above the adjacent terrain	Contributing
Vegetation	
Designed pattern of lawn and forested edge along roadway	Contributing
Free-standing planted vegetation along the roadway	Contributing
Pine trees along the edges of the Parkway in Lauderdale Co. (east)	Contributing
Privet shrub edge along the Parkway (east)	Unconfirmed
Elm at Colbert’s Stand	Contributing
Planted trees at Visitor Contact Station (red pine, juniper, hackberry, dogwood, cherry)	Contributing

Feature Name	Feature Contribution
Planted trees along internal roadways within Colbert Ferry Park (dogwoods, redbuds, cherry)	Contributing
Circulation	
Natchez Trace Parkway Motor Road	Contributing
Old Natchez Trace	Contributing
Water Route Overlook Parking Area	Contributing
Informal paths at Water Route Overlook Parking Area	Non-contributing
Parking Area at Visitor Contact Station and sidewalk	Contributing
Colbert Spur Road (Colbert Ferry Overlook Road)	Contributing
Pullout at Colbert's Stand and sidewalk	Contributing
Colbert's Stand Trail	Non-contributing
Colbert Ferry Overlook Parking Area and sidewalks	Contributing
Colbert Ferry Trail from Colbert Ferry Overlook to Georgetown Caves along River Bluff	Contributing

Feature Name	Feature Contribution
Stone Steps, Colbert Ferry Trail, three flights	Contributing
North Park Road	Contributing
Parking Area at Picnic/Swimming Beach and sidewalk	Contributing
Picnic Area ABA access to accessible picnic spot	Non-contributing
Parking Area at Boat Launch	Contributing
Sidewalk and boardwalk to Colbert Ferry Dock	Non-contributing
Concrete Steps at Boat Launch	Contributing
South Park Road/Firing Range Road	Contributing
Parking Area at South Park Road and sidewalk	Contributing
Campground Road	Contributing
Buildings and Structures	
John Coffee Memorial Bridge	Contributing
Lauderdale County Road 2 Bridge	Contributing
Abandoned County Road 2 Bridge (outside of CLI project area)	Non-contributing

Feature Name	Feature Contribution
South Park Road/Firing Range Road Bridge	Contributing
Culverts and Headwalls	Contributing
Drainage system of concrete swales and box culvert west side of John Coffee Memorial Bridge	Contributing
Colbert Creek Bridge	Contributing
Visitor Contact Station/Restroom	Non-contributing
Pump House/ Colbert Ferry Utility Building	Contributing
Pump House/ North Park Road Utility Building	Contributing
Colbert Ferry Boat Launch Comfort Station	Non-contributing
Colbert Ferry Boat Launch	Contributing
Colbert Ferry Dock	Non-contributing
NPS NATR Cherokee Alabama Gun Range	Non-contributing
Views and Vistas	
View to John Coffee Memorial Bridge and Tennessee River from Parkway (both directions)	Contributing

Feature Name	Feature Contribution
View, 360 degrees from Bridge	Contributing
View to John Coffee Memorial Bridge from Water Route Overlook Parking Area	Contributing
View to the Tennessee River from Water Route Overlook Parking Area	Contributing
View to John Coffee Memorial Bridge and Tennessee River from Colbert Ferry Overlook	Contributing
View to John Coffee Memorial Bridge and Tennessee River from Boat Launch and Swimming Area	Contributing
Views to the Old Natchez Trace	Contributing
Small-Scale Features	
Signs, Parkway wayfinding, wood	Contributing
Sign to Colbert Ferry, wood	Contributing
Signs, modern wayfinding	Non-contributing
Signs, modern educational and interpretive	Non-contributing
Signs, regulatory at Boat Launch	Non-contributing

Feature Name	Feature Contribution
Wayside exhibit at Water Route Overlook Parking Area	Non-contributing
Sign at Colbert's Stand, wood	Contributing
TVA Markers, concrete/brass	Contributing
DAR Monument at Colbert's Stand parking area	Non-contributing
Oval marker at Old Natchez Trace and Colbert's Stand	Contributing
Picnic Tables (Visitor Contact Station; Colbert Ferry Overlook; Picnic/Swimming area; Walk-in Campground; Water Route Overlook Parking Area)	Non-contributing
Picnic table concrete piers	Contributing
Picnic Grills	Non-contributing
Trash receptacles (Visitor Contact Station; Colbert Ferry Overlook; Boat Launch; Water Route Overlook Parking Area)	Non-contributing
Firepits (Picnic Area; Campground)	Unconfirmed
Site Lighting (at Visitor Contact Station; Boat Launch)	Non-contributing

Feature Name	Feature Contribution
Drinking Fountains (at Visitor Contact Station; Boat Launch)	Non-contributing
Split-rail fence at Water Route Overlook Parking Area	Non-contributing
Large boulder at Campground Road	Unconfirmed

APPENDIX F: SOUNDSCAPES STUDY

**United States Department of the Interior
National Park Service**

Natchez Trace Parkway

John Coffee Memorial Bridge Replacement

Soundscape Study Report

October 2024

CONTENTS

1.0 INTRODUCTION	1
2.0 ACOUSTICAL TERMINOLOGY.....	1
3.0 KEY SOUND LEVEL VALUES	3
4.0 ANALYSIS AREA	4
5.0 EXISTING ACOUSTIC ENVIRONMENT	9
6.0 METHODOLOGY AND ASSUMPTIONS	10
7.0 SUMMARY OF MODELING RESULTS	22
7.1 BRIDGE FOUNDATION CONSTRUCTION	22
7.1.1 Alternative 1	22
7.1.2 Alternative 2	23
7.2 BRIDGE SUPERSTRUCTURE CONSTRUCTION	23
7.2.1 Alternative 1	23
7.2.2 Alternative 2	23
7.3 BRIDGE DEMOLITION	24
7.3.1 Alternative 1	24
7.3.2 Alternative 2	24
7.4 TRAFFIC OPERATIONS	24
7.4.1 Existing No-Build Traffic Operations	25
7.4.2 Future No-Build Traffic Operations	28
7.4.3 Future Build Alternative 1 Traffic Operations	31
7.4.4 Future Build Alternative 2 Traffic Operations	34
8.0 IMPACT ANALYSIS	36
8.1 IMPACTS OF NO-ACTION ALTERNATIVE	37
8.2 IMPACTS OF ALTERNATIVE 1	37
8.3 IMPACTS OF ALTERNATIVE 2	40
8.4 COMPARISON OF IMPACTS FROM ALTERNATIVES 1 AND 2.....	42
9.0 POTENTIAL MITIGATION MEASURES	43
9.1 BRIDGE CONSTRUCTION AND DEMOLITION	43
9.2 TRAFFIC OPERATIONS	43
10.0 CONCLUSIONS	44
10.1 CONSTRUCTION NOISE	44
10.2 FUTURE OPERATIONAL NOISE.....	44
10.3 IMPACTS ON WILDLIFE	44
11.0 ACRONYMS AND ABBREVIATIONS.....	46
12.0 REFERENCES.....	47

LIST OF TABLES

Table 1. Key Sound Level Values for Generalized Effects of Sound Pressure Levels on Humans and Wildlife	3
Table 2. Mean Percent Time Audible for Anthropogenic Noise Sources.....	9
Table 3. Measured Existing and Natural Ambient Sound Levels	9
Table 4. Construction Equipment Type and Quantity for Each Phase and Activity.....	12
Table 5. Summary of Construction Equipment, Usage Factors and Sound Data	15
Table 6. Alternative 1 – Monthly Cumulative Sound Levels For The Duration of Construction.....	16
Table 7. Alternative 2 – Monthly Cumulative Sound Levels For The Duration of Construction.....	16
Table 8. Adjustment to Estimated L_{A10} and L_{A50} Traffic Noise Levels from Predicted L_{Aeq} Traffic Noise Level	17
Table 9. Traffic Data for Noise Modeling	19
Table 10. Measured RPS Sound Pressure Levels	22
Table 11. Alternative 1 – Bridge Foundation Construction Noise Levels	22
Table 12. Alternative 2 – Bridge Foundation Construction Noise Levels	23
Table 13. Alternative 1 – Bridge Superstructure Construction Noise Levels	23
Table 14. Alternative 2 – Bridge Superstructure Construction Noise Levels	23
Table 15. Alternative 1– Bridge Demolition Noise Levels.....	24
Table 16. Alternative 2– Bridge Demolition Noise Levels.....	24
Table 17. Existing No-Build – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement	25
Table 18. Existing No-Build – Operational Traffic Noise Levels, L_{A10} , Average Pavement	25
Table 19. Existing No-Build – Operational Traffic Noise Levels, L_{A50} , Average Pavement	25
Table 20. Existing No-Build – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement With RPS	26
Table 21. Existing No-Build – Operational Traffic Noise Levels, L_{A10} , Average Pavement With RPS	26
Table 22. Existing No-Build – Operational Traffic Noise Levels, L_{A50} , Average Pavement With RPS	26
Table 23. Existing No-Build – Operational Traffic Noise Levels, L_{Aeq} , Porous Pavement With RPS.....	27
Table 24. Existing No-Build – Operational Traffic Noise Levels, L_{A10} , Porous Pavement with RPS.....	27
Table 25. Existing No-Build – Operational Traffic Noise Levels, L_{A50} , porous Pavement with RPS.....	27
Table 26. Future No-Build – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement	28
Table 27. Future No-Build – Operational Traffic Noise Levels, L_{A10} , Average Pavement	28
Table 28. Future No-Build – Operational Traffic Noise Levels, L_{A50} , Average Pavement	28
Table 29. Future No-Build – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement With RPS	29
Table 30. Future No-Build – Operational Traffic Noise Levels, L_{A10} , Average Pavement With RPS	29
Table 31. Future No-Build – Operational Traffic Noise Levels, L_{A50} , Average Pavement With RPS	29
Table 32. Future No-Build – Operational Traffic Noise Levels, L_{Aeq} , Porous Pavement With RPS.....	30
Table 33. Future No-Build – Operational Traffic Noise Levels, L_{A10} , Porous Pavement with RPS	30
Table 34. Future No-Build – Operational Traffic Noise Levels, L_{A50} , porous Pavement with RPS.....	30
Table 35. Alternative 1 – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement.....	31

Table 36. Alternative 1– Operational Traffic Noise Levels, L_{A10} , Average Pavement.....	31
Table 37. Alternative 1– Operational Traffic Noise Levels, L_{A50} , Average Pavement.....	31
Table 38. Alternative 1– Operational Traffic Noise Levels, L_{Aeq} , Average Pavement With RPS	32
Table 39. Alternative 1– Operational Traffic Noise Levels, L_{A10} , Average Pavement With RPS.....	32
Table 40. Alternative 1 – Operational Traffic Noise Levels, L_{A50} , Average Pavement With RPS.....	32
Table 41. Alternative 1 – Operational Traffic Noise Levels, L_{Aeq} , Porous Pavement With RPS	33
Table 42. Alternative 1– Operational Traffic Noise Levels, L_{A10} , Porous Pavement with RPS	33
Table 43. Alternative 1– Operational Traffic Noise Levels, L_{A50} , porous Pavement with RPS	33
Table 44. Alternative 2 – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement.....	34
Table 45. Alternative 2– Operational Traffic Noise Levels, L_{A10} , Average Pavement.....	34
Table 46. Alternative 2– Operational Traffic Noise Levels, L_{A50} , Average Pavement.....	34
Table 47. Alternative 2 – Operational Traffic Noise Levels, L_{Aeq} , Average Pavement with RPS.....	35
Table 48. Alternative 2– Operational Traffic Noise Levels, L_{A10} , Average Pavement with RPS.....	35
Table 49. Alternative 2– Operational Traffic Noise Levels, L_{A50} , Average Pavement with RPS.....	35
Table 50. Alternative 2– Operational Traffic Noise Levels, L_{Aeq} , Porous Pavement with RPS	36
Table 51. Alternative 2 – Operational Traffic Noise Levels, L_{A10} , Porous Pavement with RPS	36
Table 52. Alternative 2 – Operational Traffic Noise Levels, L_{A50} , Porous Pavement with RPS	36
Table 53. Measured Existing Ambient Levels.....	37
Table 54. Traffic Noise Impacts of the No-Action Alternative	37
Table 55. Construction Noise Impacts of Alternative 1	38
Table 56. Future Operational Noise Impacts of Alternative 1	38
Table 57. Construction Noise Impacts of Alternative 2.....	40
Table 58. Operational Noise Impacts of Alternative 2	40
Table 59. Comparison of Construction Noise Impacts from Alternatives 1 and 2	42
Table 60. Comparison of Operational Noise Impacts from Alternatives 1 and 2	42

LIST OF FIGURES

Figure 1. View of Existing Bridge From Colbert Park	5
Figure 2. West View from the Center of the Bridge with Barge Passing Underneath.....	5
Figure 3. Alternative 1 – Replace Bridge on a Skewed Alignment Partially South of the Existing Bridge.....	6
Figure 4. Alternative 2 – Replace Bridge on New Alignment South of the Existing Bridge	7
Figure 5. Noise Study Area and Receptors	8
Figure 6. Alternative 1 – Conceptual Construction Schedule.....	11
Figure 7. Alternative 2 – Conceptual Construction Schedule.....	11
Figure 8. Predicted Noise Contours for Alternative 1, Future Traffic	39
Figure 9. Predicted Noise Contours for Alternative 2, Future Traffic	41

1.0 INTRODUCTION

The National Park Service (NPS) is proposing to replace the John Coffee Memorial Bridge (Tennessee River Bridge or bridge) along the Natchez Trace Parkway (Parkway) to provide a safe and reliable crossing. The bridge spans the Tennessee River (river mile 410.7), approximately 6 miles north of Cherokee, Alabama. This report documents the results of the acoustical study conducted for the project. As part of this project, the NPS is proposing to construct a new bridge and demolish the existing one.

The NPS considers the natural soundscapes of its parks as a “resource and value” under NPS *Management Policies 2006*, and its goal is to preserve and restore the natural soundscape and acoustic environment. Natural soundscape and acoustic environment refer to the combination of all natural sounds, as well as the physical capacity of transmitting those natural sounds, and the interrelationships among park natural sounds of different frequencies and volumes. An intact natural soundscape allows for natural functioning of wildlife communications and enhances visitors’ experience.

The intent of the soundscape study is to predict future noise conditions at sensitive receptors near the project area to understand potential impacts on park resources from construction noise and typical traffic conditions during operation of the replacement bridge. This report describes the design alternatives that were assessed, key sound level values, acoustical modeling methods, assumptions, and results.

2.0 ACOUSTICAL TERMINOLOGY

This report uses the following terms to describe acoustical features:

A-weighting: A-weighting is applied to sound levels to account for the sensitivity of the human ear. To approximate human hearing sensitivity, A-weighting discounts sounds below 1 kilohertz (kHz) and above 6 kHz.

Acoustic Environment: A combination of all the physical sound resources within a given area. This includes natural sounds, cultural sounds, and non-natural human-caused sounds. The acoustic environment of a park can be divided into two main categories: intrinsic and extrinsic.

Acoustic Resources: Includes natural sounds, like wind, water, and wildlife, as well as cultural and historic sounds, like Tribal ceremonies, quiet reverence, and battle reenactments.

Amplitude: The relative strength of a sound wave, described in decibels (dB). Amplitude is related to what is commonly called loudness or volume.

Audibility: The ability of animals with normal hearing, including humans, to hear a given sound. It can vary depending on the frequency content, amplitude of sound, and the hearing ability of the individual.

Decibel (dB): A unit of sound energy. Sound levels are measured on a logarithmic scale relative to the reference sound pressure for atmospheric sources, 20 μ Pa. The logarithmic scale is a useful way to express the wide range of sound pressures perceived by the human ear. Sound levels are reported as dB. Every 10 dB increase represents a tenfold increase in energy. Therefore, a 20 dB increase represents a hundredfold increase in energy.

Day-Night Average Sound Level (Ldn): Average equivalent sound level over a 24-hour period, with a 10 dB penalty added for sound levels between 10:00 p.m. and 7:00 a.m. The Ldn is typically A-weighted and used to assess noise impacts on humans.

Existing Ambient Sound Level (L_{A50}): Sound level ($L_{Aeq, 1s}$) exceeded 50% of the time (50th percentile) for a specified duration. This level is referred to as the existing ambient sound level and the preferred metric for chronic conditions because it is insensitive to infrequent loud events.

Extrinsic Sound: Any sounds not forming an essential part of the park system unit, or a sound originating from outside the park boundary. This could include voices, radio music, or aircraft flying thousands of feet above the park.

Energy Equivalent Sound Level (L_{Aeq}): The sound energy level averaged over the measurement period. Generally, refers to A-weighted, 1-second, time-averaged sound levels measured between 12.5 hertz (Hz) and 20 kHz. This is a standard measurement collected using the Natural Sounds and Night Skies Division's (NSNSD) acoustic monitoring protocol for sound level meters. Sound levels measured over 1-second intervals are used to calculate summary statistics, specifically percent of the time a sound level of interest is exceeded.

Maximum and Minimum Sound Level (L_{Amax} and L_{Amin}): L_{Amax} and L_{Amin} are the loudest and quietest instant sound levels occurring during a period of time, respectively.

Frequency: Expressed in Hz, frequency is related to the pitch of a sound and defined as the number of times per second that the wave of sound repeats itself. Sound levels are often adjusted (weighted) to match the hearing abilities of a given animal. In other words, different species of animals and humans are capable of hearing (or not hearing) at different frequencies. Humans with normal hearing can hear sounds between 20 Hz and 20,000 Hz, and as low as 0 dB at 1,000 Hz. Bats, on the other hand, can hear sounds between 20 Hz and 200,000 Hz.

Intrinsic Sound: Belongs to a park by the park's very nature, based on its purposes, values, and establishing legislation. Intrinsic sounds can include natural, cultural, and historic sounds that contribute to the acoustic environment of the park.

Natural Ambient Sound Level (L_{Anat}): The natural sound conditions in parks, which would exist in the absence of any human-caused noise sources. L_{Anat} is the preferred metric to represent baseline or reference conditions for determining the affected environment. Quantitatively, the NPS considers the natural ambient level to be the 50th percentile sound level that exists in the absence of anthropogenic noise.

Noise-Free Interval (NFI): The length of the continuous period of time during which no human-caused sounds are audible.

Percentile Sound Levels (L_{A10} , L_{A50} , L_{A90}): Metrics used to describe A-weighted sound pressure levels (L), in decibels, exceeded 10%, 50%, and 90% of the time, respectively. Put another way, half of the time the measured levels of sound are greater than the L_{A50} value, while 90 percent of the time the measured levels are higher than the L_{A90} value, and 10 percent of the time measured levels are higher than the L_{A10} value.

Sound Power (L_w): The Sound Power Level of a noise source is the strength or intensity of noise that the source produces/emits regardless of the environment in which it is placed. Sound power is a property of the source, and therefore is independent of distance.

Sound Pressure: Minute change in atmospheric pressure due to passage of sound over a distance that can be detected by microphones.

Sound vs. Noise: Sound and noise are often used interchangeably to describe an acoustic source. A common definition of noise is unwanted sound or sounds that interfere with a signal of interest. However, noise is not a purely subjective designation. Any sound that serves no function is noise. Most sounds produced by human transportation and other machinery are unintended and serve no function; therefore, they are noise regardless of the attitudes of the listener. While there are unintended sounds in nature, like the footfalls of an animal, these sounds provide vital cues for some receivers and are therefore considered sounds to the receiver, yet noise from the perception of the producer.

Soundscape: The human perception of physical sound resources.

Time Above: Within a defined time period, the percent of the time sound levels ($L_{Aeq, 1s}$) are above a specified sound level ($L_{Aeq, 1s}$). Commonly used levels are 35, 45, 52 dB ($L_{Aeq, 1s}$).

Time Audible: The amount of time that various sound sources are audible to humans with normal hearing, commonly expressed in percentage of day, or percentage of daytime hours and nighttime hours. A sound may be above natural ambient sound pressure levels, but still not audible. Similarly, some sounds that are below the natural ambient can be audible. Time Audible is useful because of its simplicity. It is a measure that correlates well with visitor complaints of excessive noise and annoyance. Most noise sources are audible to humans at lower levels than virtually all wildlife species. Therefore, time audible is a protective proxy for wildlife. These data can be collected either by a trained observer (on-site listening) or by making high-quality digital recordings for later playback (off-site listening).

3.0 KEY SOUND LEVEL VALUES

In considering the current conditions of the acoustical environment and assessing potential future impacts, examining how often sound pressure levels exceed certain values is helpful. **Table 1** shows a set of key sound level values for the potential effects of noise on humans and other animal species.

TABLE 1. KEY SOUND LEVEL VALUES FOR GENERALIZED EFFECTS OF SOUND PRESSURE LEVELS ON HUMANS AND WILDLIFE

Sound Level, dB	Metric	Relevance
31	Nighttime L_{Anat}	Natural ambient sound level estimated at a monitoring location in the project area during nighttime (7:00 p.m.–7:00 a.m.) that would exist in the absence of any anthropogenic noise sources. Quantitatively, L_{Anat} is the 50th percentile sound level that exists in the absence of anthropogenic noise.
34	Daytime L_{Anat}	Natural ambient sound level estimated at a monitoring location in the project area during daytime (7:00 a.m.–7:00 p.m.) that would exist in the absence of any anthropogenic noise sources.
35	$L_{Aeq, 1s}$	Maximum recommended background noise level inside classrooms (American National Standards Institute S12.60). Sound level at which blood pressure and heart rate increase in sleeping humans (Haralabidis et al. 2008).
40	L_{Aeq}^*	Threshold of noise-induced responses for terrestrial wildlife, including potential decline in species diversity for birds (Shannon et al. 2016).
52	L_{Aeq}	Speech interference for interpretive programs or time-sensitive safety communication during backcountry travel (USEPA 1974)
60	$L_{Aeq, 1s}$	Speech interruption for normal conversation (USEPA 1974).

Sound Level, dB	Metric	Relevance
66	L_{Aeq}	The Federal Highway Administration's (FHWA) recommended outdoor noise abatement criteria that defines impact for residences.

*This metric is a composite of measurements with varying time averages.

NPS *Management Policies 2006* states that the natural ambient sound level (L_{Anat}) is the baseline condition that the current and future soundscape should be evaluated against (NPS 2006). Therefore, long-term noise impacts from proposed new bridge operations are compared against measured natural ambient sound level. The natural ambient sound level in the project area is discussed in Section 5. In addition, Federal Highway Administration (FHWA) traffic noise policy identifies outdoor peak hour traffic noise levels that approach or exceed 67 dB $L_{Aeq, 1hr}$ as the noise abatement criteria that defines impact for residences (FHWA 2010). Most states (including Alabama) interpret “approach” as being within 1 dB of the 67 dB standard, so that 66 dB $L_{Aeq, 1hr}$ is the applicable outdoor residential noise abatement criteria (ALDOT 2021).

Measured existing sound levels (Section 5), construction noise levels and predicted future traffic noise levels (Section 7) are compared to these key sound level criteria to assess potential impacts on the acoustical environment (Section 8).

4.0 ANALYSIS AREA

The project is located along the Parkway, spanning the Tennessee River, approximately 7 miles due north of the US 72 Highway ramp intersecting the Parkway. The Parkway segment located within the project area is a recreational road and scenic drive that serves park visitors, permitted commercial users, and local commuter and non-commuter traffic. The existing bridge has two lanes (one lane in each direction) with an asphalt surface and metal expansion joints over the piers. A 3-foot concrete parapet is present on both edges of the bridge. The area surrounding the Parkway to the west side of the Tennessee River is covered with dense vegetation, while east of the river is comparatively open land. An undisclosed cave is located on the west of the bridge and is reportedly inhabited by federally listed and state-protected bat species. Commercial shipping occurs along the navigable central portion of the river. **Figure 1** shows the view of the existing bridge from Colbert Park, and **Figure 2** shows the view from the center of the existing bridge looking west with a barge passing underneath.

The project area covers approximately 300 feet from either side of the existing Parkway centerline, from just west of the Colbert Ferry Recreation Area entrance to just east of the ramps connecting the Parkway to Lauderdale County Road 2 (see **Figures 3 and 4**). To assess future noise conditions, a 500-meter buffer from the project area was established as the noise study area. Five locations within the noise study area were identified as noise sensitive receptors. These receptors are: (1) Colbert Ferry Overlook, (2) Colbert Ferry Visitor Contact Station, (3) Colbert Ferry Boat Ramp and Picnic Area, (4) Undisclosed Cave Entrance on the southwest with bat gate, and (5) Southeast Parking Area. **Figure 5** shows the location of receptors and the noise study area (outlined in green).



FIGURE 1. VIEW OF EXISTING BRIDGE FROM COLBERT PARK



FIGURE 2. WEST VIEW FROM THE CENTER OF THE BRIDGE WITH BARGE PASSING UNDERNEATH

Natchez Trace Parkway Tennessee River Bridge Replacement

National Park Service
US Department of the Interior

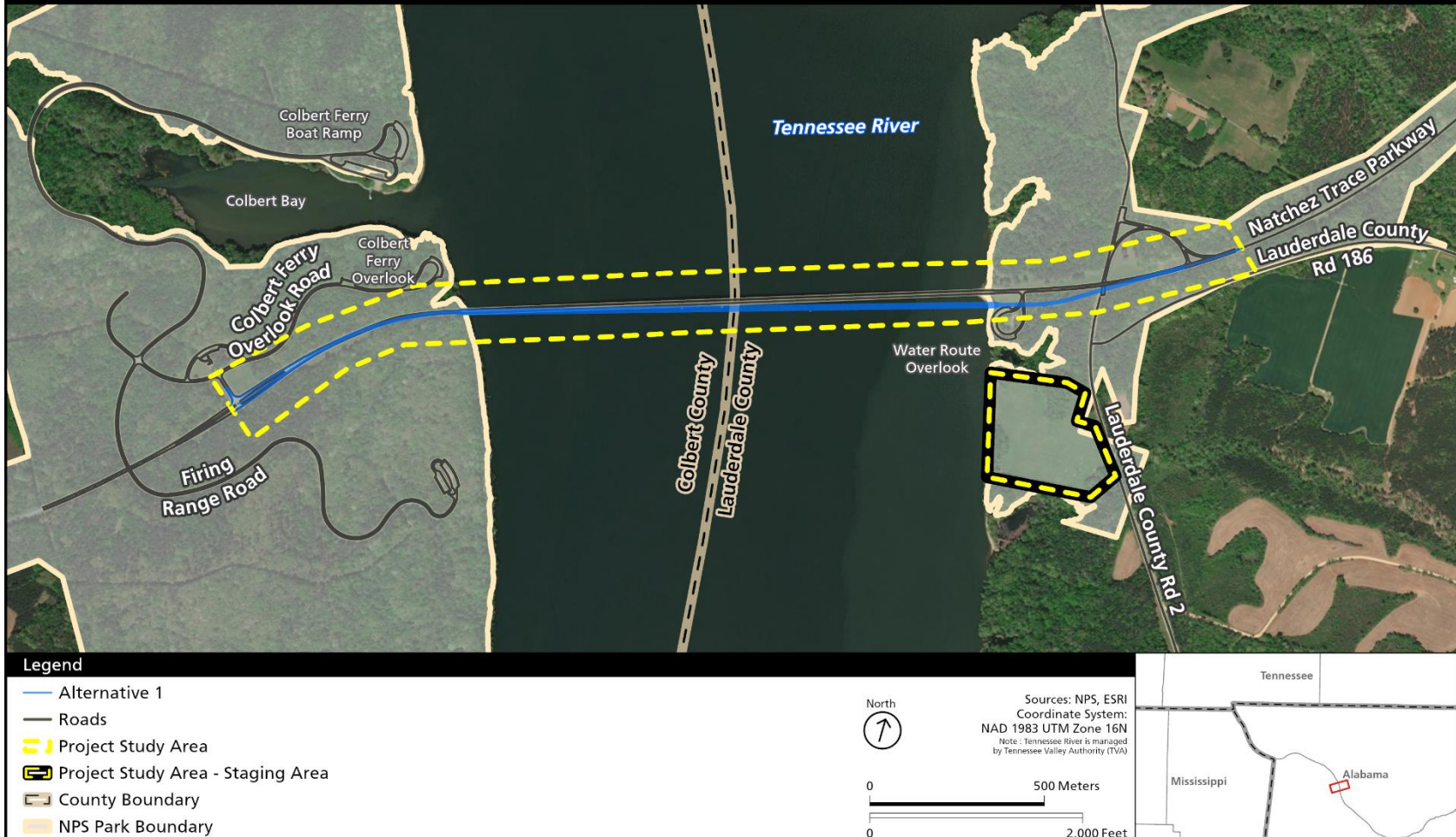


FIGURE 3. ALTERNATIVE 1 – REPLACE BRIDGE ON A SKEWED ALIGNMENT PARTIALLY SOUTH OF THE EXISTING BRIDGE

Natchez Trace Parkway Tennessee River Bridge Replacement

National Park Service
US Department of the Interior

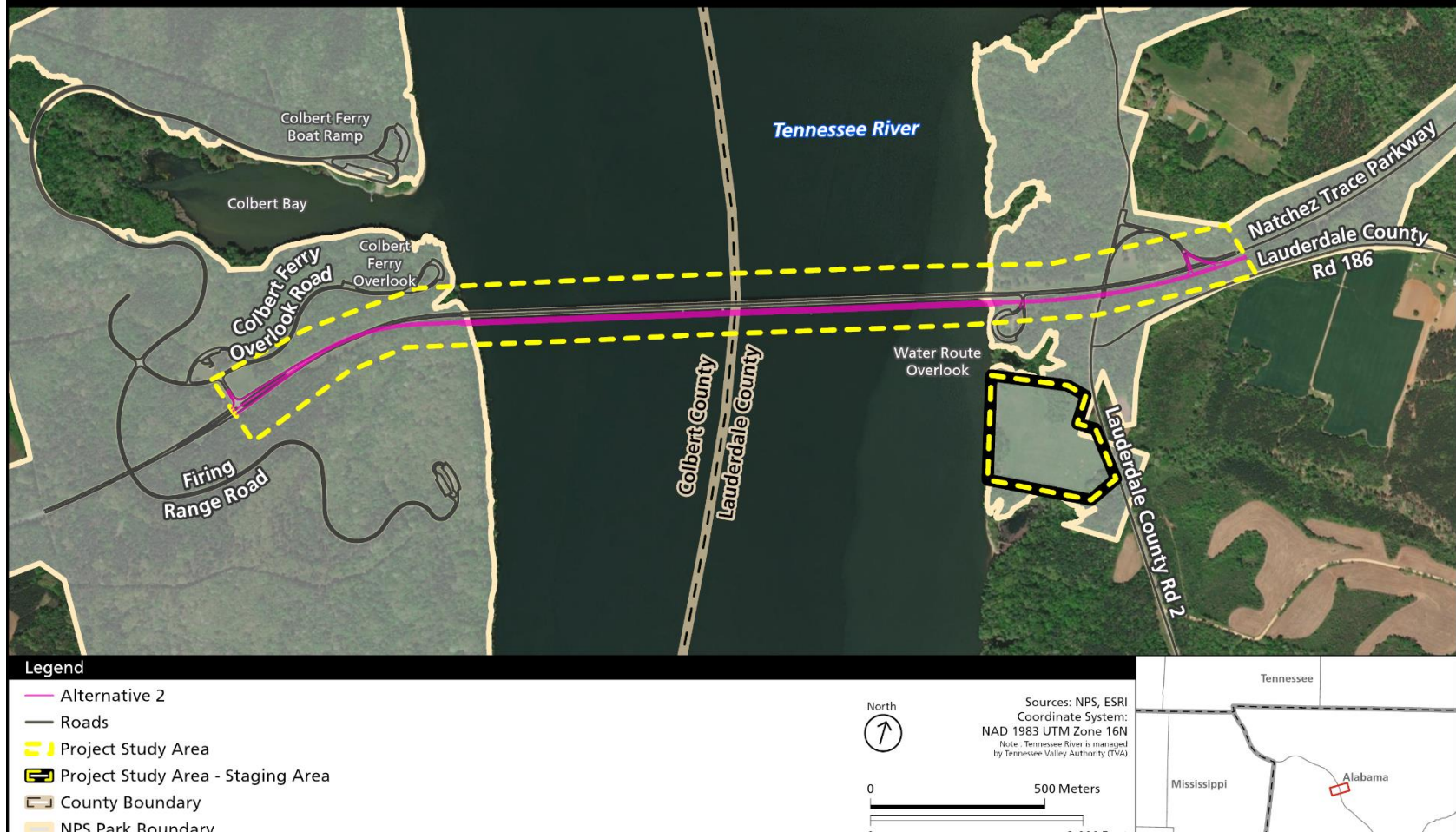


FIGURE 4. ALTERNATIVE 2 – REPLACE BRIDGE ON NEW ALIGNMENT SOUTH OF THE EXISTING BRIDGE

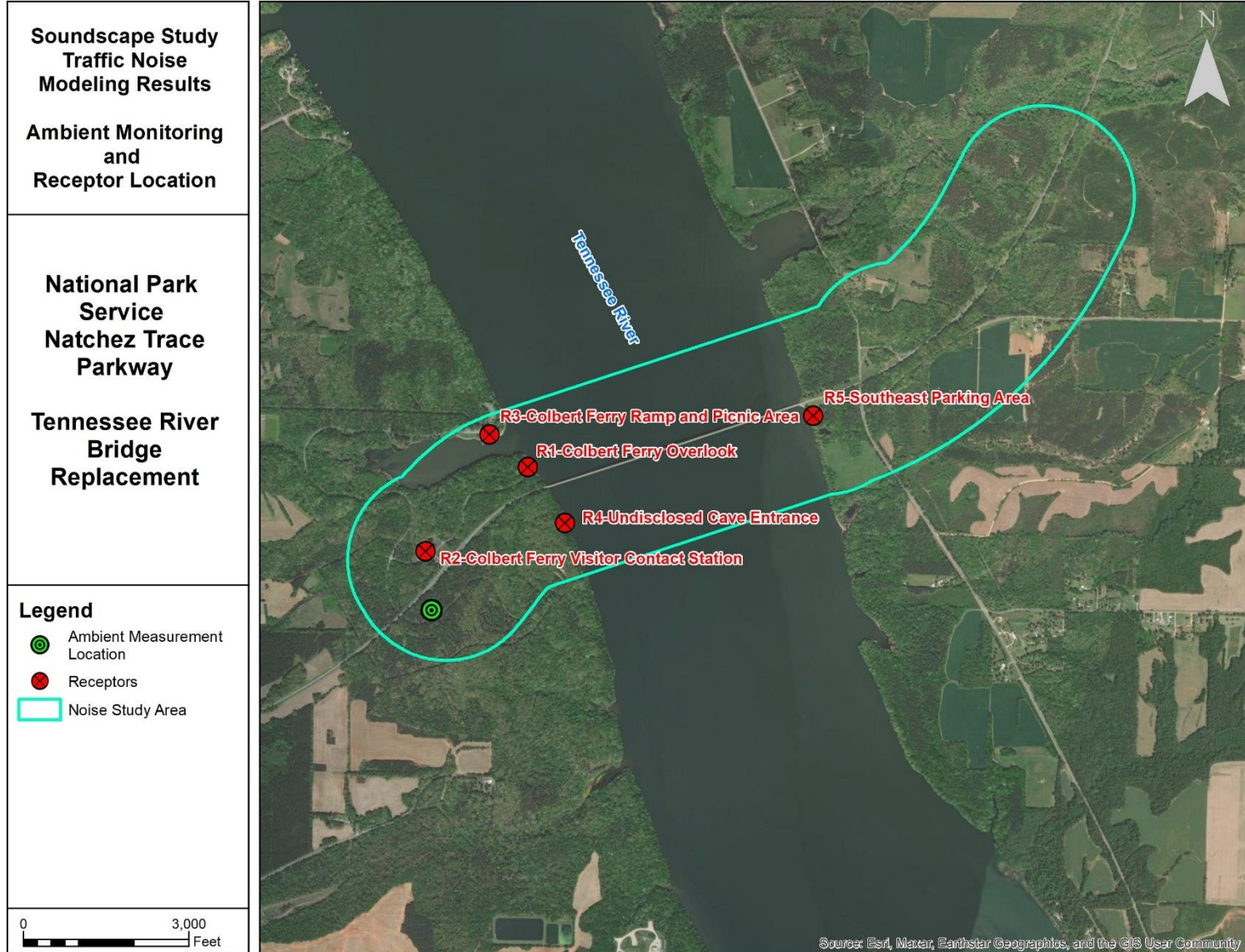


FIGURE 5. NOISE STUDY AREA AND RECEPTORS

5.0 EXISTING ACOUSTIC ENVIRONMENT

The NPS's Natural Sounds and Night Skies Division (NSNSD) conducted baseline acoustic monitoring in May 2021 at two locations along the Parkway. The first monitoring location, NATR001, was at Bear Creek Mound located 19 miles southwest of the project area. The second monitoring location, NATR002, was within the noise study area, about 0.06 miles from the edge of the Parkway and southeast of Colbert Ferry Overlook. Data from NATR002 were only considered for this study because of the monitoring location's proximity to the project. The NSNSD provided post-processed data to WSP USA Inc. (WSP) with sound sources identified, and L_{Aeq} , L_{A50} , L_{A10} , and L_{Anat} levels estimated.

L_{Aeq} is the average A-weighted equivalent sound level measured over a duration of time such as 1-hour, 8-hours, 1-day, or 1-week. L_{A50} is the median overall A-weighted sound level exceeded 50% of the ambient measurement duration. All anthropogenic sounds, such as vehicle traffic, aircraft overpasses, and mechanical systems, are considered extrinsic to the Parkway and are part of the existing ambient noise level. Because the existing ambient noise level tends to fluctuate due to traffic or aircraft flybys or other influences, L_{A50} provides a statistical mid-point of the noise measurement representing the fluctuating noise level. L_{A10} is the existing ambient A-weighted sound level exceeded for 10% of the measurement duration. For the rest of the time, sound levels are below L_{A10} . These higher L_{A10} sound levels are most likely representative of sporadic or intermittent events.

L_{Anat} is the natural ambient sound level audible that is intrinsic to the park. Quantitatively, L_{Anat} is the 50th percentile sound level that exists in the absence of anthropogenic noise.

The primary sources of human-caused sound within the noise study area are roadway vehicles and aircraft. When averaged across all hours of the day, sound source analysis revealed that anthropogenic noise was audible for 77.1% of the day, while natural ambient (i.e., NFIs) made up the remaining 22.9%. **Table 2** presents mean percent time audible for anthropogenic noise sources. **Table 3** presents the measured median ambient sound levels at the monitoring location as L_{A50} and L_{Anat} .

TABLE 2. MEAN PERCENT TIME AUDIBLE FOR ANTHROPOGENIC NOISE SOURCES

Site ID	Site Name	Mean Percentage Audible for Noise (% of 24-Hour Time Period) ^a		
		All Noise ^b	All Aircraft	Roadway Vehicle
NATR002 ^c	Colbert Ferry	77.1	8.0	62.8

Source: Data provided by the NPS NSNSD 2022

^a Based on sound source identification analysis developed by the Volpe Center, US Department of Transportation.

^b Total human-caused percent time audible (including all types of aircraft, roadway vehicles, and other sources).

^c Measurement location ID nomenclature determined and provided by the NSDSD.

TABLE 3. MEASURED EXISTING AND NATURAL AMBIENT SOUND LEVELS

Site ID	Site Name	Median Existing Ambient (L_{A50}) in dB		Median Natural Ambient (L_{Anat}) in dB	
		Day ^a	Night ^a	Day	Night
NATR002 ^b	Colbert Ferry	41.7	34.5	33.7	30.7

Source: Data provided by NPS NSNSD 2022

^a Day hours are 7:00 a.m.–7:00 p.m.; night hours are 7:00 p.m.–7:00 a.m.

^b Measurement location ID provided by the NSDSD.

6.0 METHODOLOGY AND ASSUMPTIONS

Two design alternatives, Alternative 1 (the Skewed Alignment) and Alternative 2 (the South Alignment), were evaluated for the noise assessment. Alternative 1 would be constructed on a skewed alignment, with the western end of the bridge remaining on the existing alignment and the eastern end of the bridge partially skewed to the south. Alternative 2 would be constructed immediately south of and parallel to the existing bridge.

Figures 3 and 4 show the two design alternatives with respect to the existing bridge. The western end of the new bridge for Alternative 1 would start at the same location as the existing bridge using the same roadway alignments on the west and gradually skews to the south, realigning with the existing roadway beyond the intersection with Lauderdale County Road 2. For Alternative 2, the new bridge would run parallel to the south of the existing bridge and would require a new connecting roadway on currently forested land on the west.

To predict noise levels for construction and future traffic conditions, the Datakustik CadnaA noise model was used. CadnaA is a comprehensive three-dimensional model that can predict noise from many sources in areas with complex terrain.

The topography throughout the noise study area was incorporated into CadnaA by importing contour lines derived from the US Geological Survey as GeoTiff (USGS 2021). The future roadways were modeled based on preliminary designs of the proposed roadway alignment and bridge profile. Pavement type on the future roadway would affect the generated noise levels. Therefore, future traffic noise levels were modeled using both “average” and “porous” pavements to estimate the difference in noise levels that would result from using a porous pavement on the new bridge roadway. Potential changes to terrain (e.g., embankment slopes) adjacent to the future roadway were also considered by modeling contour lines at the approximate limits of cut-and-fill roadway sections.

Bridge construction noise modeling was segmented into three major phases: (1) foundation construction, (2) superstructure construction, and (3) demolition of the existing bridge. FHWA would manage initial design planning and oversee the contracted design, construction, and demolition. FHWA provided a conceptual construction schedule and estimate of construction equipment quantity for this study. After the design phase, construction is estimated to occur for over a period of five and half years for Alternative 1 and close to six and half years for Alternative 2. The conceptual schedule for each construction phase is detailed in **Figures 6 and 7**.

As confirmed by FHWA, the type and quantity of construction equipment required for both alternatives would be the same and are summarized in **Table 4** for each phase. Equipment sound levels (L_{Amax}) in the form of octave bands provided in FHWA's Roadway Construction Noise Model (RCNM) V2.0 database were used to estimate potential noise impacts. The fuel type, engine horsepower ratings, and equipment usage factors are conservatively assumed for this quantitative construction noise analysis.

During construction activities, it is unlikely that every piece of construction equipment would operate 100% of the time. Equipment usage would generally change in any given time depending on the phase of the construction. Therefore, commonly acceptable FHWA construction equipment usage factors were used for this analysis to estimate reasonable operating times and the resulting noise levels. **Table 5** summarizes the construction equipment, usage factors, and broadband sound levels used in this study.

TABLE 4. CONSTRUCTION EQUIPMENT TYPE AND QUANTITY FOR EACH PHASE AND ACTIVITY

Phase	Activity	Unit	Construction Equipment Types and Typical Quantities for Each Activity	Duration per Unit (days)	Total Duration (days)
Foundations	Install temporary work platforms for water depth 12 feet and below	Temporary work platforms	Equipment: Construction QTY: 1 Crane 1 Pile rigs Equipment: Hauling QTY: 1 Truck for platform materials 2 Trucks for piles	35	35
Foundations	Install abutment drilled shafts	Per abutment	Equipment: Construction QTY: 1 Drilled Shaft Rig 1 Crane Equipment: Hauling QTY: 4 Trucks for drilled shaft casing 2 Truck for reinforcing steel 4 Trucks for concrete	10	20
Foundations	Construct abutment and wingwalls	Per abutment	Equipment: Construction QTY: 1 Excavator 1 Crane Equipment: Hauling QTY: 35 Trucks for excavated material 6 Trucks for formwork (3 haul in; 3 haul out) 2 Trucks for reinforcing steel 45 Trucks for concrete 35 Trucks for backfill material (Assuming 10 cubic yards per truck)	30	60
Foundations	Install pier drilled shafts (27 piers)	Per pier	Equipment: Construction QTY: 1 Drilled Shaft Rig 1 Crane 2 Barges (1 for Crane; 1 for Drilled Shaft Rig) 2 Tugboats (Drilled shafts in shallow water areas would be installed from a temporary work platform) Equipment: Hauling QTY: 4 Trucks for drilled shaft casing 2 Truck for reinforcing steel 4 Trucks for concrete 1 Barge 1 Tugboat	15 (Main Span) 10 (other piers)	290

Phase	Activity	Unit	Construction Equipment Types and Typical Quantities for Each Activity	Duration per Unit (days)	Total Duration (days)
Foundations	Construct pier footing (27 piers)	Per pier	Equipment: Construction QTY: 1 Crane 1 Barge (for Crane) 1 Tugboat (Pier footings in shallow water areas will need to be installed from a temporary work platform) Equipment: Hauling QTY: 3 Trucks for formwork tub 2 Truck for reinforcing steel 40 Trucks for concrete 1 Barge 1 Tugboat	20	540
Superstructure	Set precast pier segments and cap (27 piers)	Per pier	Equipment: Construction QTY: 1 Crane 1 Barge (for Crane) 1 Tugboat (Piers located in shallow water areas will need to be installed from a temporary work platform) Equipment: Hauling QTY: 5 Trucks for precast pier segments 1 Truck for post tensioning steel 1 Barge 1 Tugboat	5	135
Superstructure	Cast-in-place balanced cantilever pier table (2 piers)	Per pier	Equipment: Construction QTY: 1 Crane 1 Barge (for Crane) 1 Tugboat (Piers located in shallow water areas will need to be installed from a temporary work platform) Equipment: Hauling QTY: 6 Trucks for formwork (3 Haul in; 3 Haul out) 2 Trucks for reinforcing steel 45 Trucks for concrete 1 Truck for post tensioning steel 1 Barge 1 Tugboat	90	180
Superstructure	Construct precast balanced cantilever superstructure segments & closure pour	Main span	Equipment: Construction QTY: 1 Crane 1 Barge (for Crane) 1 Tugboat Equipment: Hauling QTY: 0.2 Trucks for post tensioning steel (12 trucks total for entire balanced cantilever spans) 1 Barge 1 Tugboat	230	230

Phase	Activity	Unit	Construction Equipment Types and Typical Quantities for Each Activity	Duration per Unit (days)	Total Duration (days)
Superstructure	Install precast girders	Per span	Equipment: Construction QTY: 2 Cranes 2 Barges (for Crane) 2 Tugboats Equipment: Hauling QTY: 6 Trucks for precast girders 1 Barge 1 Tugboat	2	50
Demolition	Remove existing bridge	Total	<u>Remove Deck:</u> Equipment: Construction QTY: 6 Concrete Saws 2 Cranes 2 Barges (for Crane) 2 Tugboats 2 Jackhammers Equipment: Hauling QTY: 2 Dump Trucks or 2 Barges 2 Tugboats <u>Remove Beams:</u> Equipment: Construction QTY: 2 Cranes 2 Barges (for Crane) 2 Tugboats 2 Cutting Torches Equipment: Hauling QTY: 2 Dump Trucks or 2 Barges 2 Tugboats <u>Remove Piers:</u> Equipment: Construction QTY: 6 Concrete Saws 2 Cranes 2 Barges (for Crane) 2 Tugboats 2 Jackhammers Equipment: Hauling QTY: 2 Dump Trucks or 2 Barges <u>Remove Foundations:</u> Equipment: Construction QTY: 2 Ho-Ram and/or Explosives Equipment: Hauling QTY: 2 Dump Trucks or 2 Barges 2 Tugboats 2 Jackhammers Cofferdams Pile Rigs Equipment: Hauling QTY: 2 Dump Trucks or 2 Barges <u>Remove Foundations:</u> Equipment: Construction QTY: 2 Ho-Ram and/or Explosives Equipment: Hauling QTY: 2 Dump Trucks or 2 Barges 2 Tugboats	220	220

TABLE 5. SUMMARY OF CONSTRUCTION EQUIPMENT, USAGE FACTORS AND SOUND DATA

Equipment Type	Construction Equipment Noise Emission Levels (RCNM, FHWA)	Usage Factor (%)	L _{max} @ 50 ft (dB)	Sound Power Level with Usage Factor, L _w (dB)
Dump Truck	Truck	50	92	120
Flatbed Truck	Truck	50	92	120
Impact Pile-Driver	Pile-Driver (Impact)	20	105	130
Concrete Truck	Concrete Mixer Truck	40	82	110
Excavator	Dozer	40	87	114
Hydraulic Crane (450 ton)	Crane, Derrick	16	76	100
Barge	Barge	50	85	114
Tugboat	Ferry Boat-Federal Transit Administration Manual	50	78	107
Skid Loaders	Backhoe	40	84	111
Concrete Saw	Concrete Saw	20	88	113
Jackhammers	Jack Hammer	20	95	120
Ho-Ram	Hydra Break Ram	10	98	120
Roller	Roller Machines	20	88	113
Paver	Paver	20	82	106

Note: L_w = Sound Power Level

Various phases of construction would overlap, and equipment locations and quantity would change as construction progresses. Consequently, construction noise impacts would change throughout the duration of the project. To predict the reasonable worst-case construction noise impacts, the total quantity of equipment and type, as provided in the construction schedule, were determined to estimate a cumulative sound level (at 50 feet) for each month. The month with highest estimated sound level is considered as the representative month for modeling the major construction phase.

The noise model represents conservative, worst-case, and loudest potential condition by assuming that all equipment on-site during a given phase of work would be operating simultaneously under their respective usage factors. This was accomplished by logarithmically summing the sound power levels of individual equipment for each month. Because the construction schedule is in a conceptual stage and equipment locations are not finalized, equipment locations were conservatively assumed for modeling purpose.

Trees would need to be removed for the construction of either alternative, which would require the use of chainsaws and associated equipment. Details such as locations of tree removal, extent of removal, duration, and quantity of equipment to be used are not yet available; therefore, noise impacts from tree removal activities as part of construction are not analyzed in this study. Nevertheless, because of its design, Alternative 2 would have more tree removal in the forested land on the southwest of the bridge, compared to Alternative 1.

Tables 6 and 7 show the estimated sound levels for each month for the entire duration of construction for Alternative 1 and Alternative 2.

TABLE 6. ALTERNATIVE 1 – MONTHLY CUMULATIVE SOUND LEVELS FOR THE DURATION OF CONSTRUCTION

Construction Phase	Year2												Year3												Year4												Year5												Year6																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																				
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
Foundation-Install temporary work platforms	-	-	-	-	-	-	99	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																												
Foundation-Install abutment drilled shafts	-	-	-	-	-	-	-	-	101	101	-	-	-	-	-	-	-	-	-	-	-	101	101	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
Foundation-Construct abutment and wingwalls	-	-	-	-	-	-	-	102	102	-	-	-	-	-	-	-	102	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	102	96	96	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
Foundation-Install pier drilled shafts	-	-	-	-	-	-	-	-	-	100	100	100	100	100	100	100	100	100	100	100	100	100	100	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
Foundation-Construct pier footing	-	-	-	-	-	-	-	-	-	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	98	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
Superstructure-Set precast pier segment and cap	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	97	-	97	97	97	97	97	97	97	97	97	97	97	97	97	92	97	97	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
Superstructure-Cast-in-place balanced cantilever pier table	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100	100	100	100	100	100	100	-	-	-	-	100	100	100	100	100	100	95	95	95	95	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																			
Superstructure-Construct precast balanced cantelever superstructure segment & Closure Pour	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	90	90	90	90	90	90	90	-	-	-	-	-	-	-	-	-	-	-	-	-	90	90	90	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																							
Superstructure-Install Precast Griders	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97	97																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																		
Demolition-Remove existing bridge	-	-	-	-	-	-	-	102	102	102	102	102	102	102	102	102	102	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

Note: Actual construction is scheduled to begin from year 2, while year 1 is for bridge design. Cumulative sound levels in red represent the month with the highest level for that year.

TABLE 7. ALTERNATIVE 2 – MONTHLY CUMULATIVE SOUND LEVELS FOR THE DURATION OF CONSTRUCTION

[illegible]

Note: Actual construction is scheduled to begin from year 2, while year 1 is for bridge design. Cumulative sound levels in red represent the month with the highest level for that year.

The FHWA Traffic Noise Model (TNM) v2.5 roadway noise calculation method within the CadnaA model was used for traffic operations. This method implements the full FHWA TNM v2.5 traffic noise prediction algorithm, including the standard FHWA vehicle noise emission database that is used for all FHWA-funded traffic noise studies in the United States. The TNM algorithm accounts for vehicle class, volume, speed, engine cruise and throttle conditions, and roadway pavement type. Sound propagation within the model accounts for acoustical attenuation due to geometrical spreading (divergence), ground absorption, and shielding from intervening terrain or solid objects (barriers) between the roadway and receiver locations. The CadnaA implementation of TNM also corrects several errors in the TNM v2.5 algorithm that have been corrected in TNM v3.2.

Consistent with TNM v2.5, CadnaA only predicts hourly noise levels in the L_{Aeq} metric ($L_{Aeq, 1hr}$). However, the recently released TNM v3.2 model also predicts hourly A-weighted noise levels in the L_{A10} and L_{A50} percentile metrics.¹ Percentile sound level metrics are commonly used in NPS soundscape studies; predicting future traffic noise in hourly percentile sound level metrics allows for a more accurate comparison to the existing ambient sound level, which the NPS defines using the L_{A50} metric. Therefore, a simple model was created using TNM v3.2 to determine the adjustment factors for L_{A50} and L_{A10} . These adjustment factors were applied to the hourly L_{Aeq} noise levels predicted by CadnaA, so that hourly L_{A10} and L_{A50} noise levels could be estimated. TNM v3.2 is still not an officially approved FHWA model for traffic noise assessments, thus it was only used to determine the correction factors for this study. The L_{A10} and L_{A50} adjustment factors differ depending on the roadway traffic volume, vehicle classification mix, and speed. **Table 8** shows the L_{A10} and L_{A50} adjustment factors for the receptors.

TABLE 8. ADJUSTMENT TO ESTIMATED L_{A10} AND L_{A50} TRAFFIC NOISE LEVELS FROM PREDICTED L_{Aeq} TRAFFIC NOISE LEVEL

	R1, Colbert Ferry Overlook	R2, Colbert Ferry Visitor Contact Station	R3, Colbert Ferry Boat Ramp and Picnic Area	R4, Undisclosed Cave Entrance	R5, Southeast Parking
L_{A50} (dB)	-2.7	-4.51	-1.13	-2.26	-7.58
L_{A10} (dB)	3.8	4.29	2.89	3.58	4.52

The TNM vehicle noise emission database includes three different roadway pavement types that can be selected: portland cement concrete (PCC), dense graded asphalt concrete (DGAC), and open-graded asphalt concrete (OGAC). PCC results in the loudest traffic noise levels, while OGAC, also referred to as porous pavement, results in the quietest. The TNM calculation allows users to model roadways using the average of PCC and DGAC pavements, which is the standard approach for FHWA-funded traffic noise studies. For this project, future traffic noise levels were predicted using both average and porous pavement types to estimate pavement noise levels on the bridge roadway. TNM v2.5 was used to determine the appropriate adjustment factor with porous pavement based on roadway traffic volume, vehicle classification mix, and speed. This resulted in a -0.8 dB average adjustment factor for porous pavement, relative to traffic noise levels predicted with average pavement.

The traffic noise modeling was conducted for the existing (2021) peak hour and for forecasted future 2045 peak hour traffic data, based on the vehicle volumes for weekday conditions from the project traffic study (WSP 2023). The 2022 and 2023 peak hour traffic data was affected due to road closures, therefore,

¹ L_{A10} and L_{A50} describe the A-weighted sound pressure levels as those that exceed 10% and 50% of the measurement period, respectively.

the 2021 peak hour data was used in the analysis. Vehicle mix data were obtained from the Alabama Department of Transportation-published Hourly Class Report for station 637 on September 7, 2021, that includes the noise study area.² Traffic forecast for years earlier than 2045 were not provided for this study. The existing and future posted speed limit is 50 miles per hour, and heavy trucks are not allowed on the Parkway; however, the NPS informed WSP that heavy trucks are present on the Parkway. **Table 9** summarizes the data used in the analysis for existing and future traffic forecast.³

Other assumptions for the traffic noise modeling include the following:

- No sound attenuation from trees or foliage was assumed in the model to simulate the winter season when leaves have fallen from trees.
- Acoustically soft ground (TNM “lawn” ground type, absorption factor 1) was assumed for all ground surfaces except roadways, where acoustically hard ground (TNM “pavement” ground type) was assumed.
- Acoustic reflections (absorption factor 0.3) were assumed for water surface of the Tennessee River in the study area.

The NPS indicated that the Parkway has raised profile strips (RPS) along the inner and outer edges of each lane to alert vehicles when they veer from their respected lanes. These RPS generate noise on top of typical roadway noise. The NSNSD conducted noise measurements under controlled conditions to evaluate the change in noise levels when vehicles hit the RPS. Two classes of vehicles, passenger car and pickup truck with trailer, were used for the study, and measurements were recorded at 50 foot and 100 foot setbacks. Maximum sound pressure levels were measured at 50-feet for a passenger car and at 100 feet for a pickup truck with trailer. Using the maximum sound pressure levels measured, sound power levels of RPS hit were calculated for each class of vehicle. WSP traffic study field staff observed that the probability of a vehicle hitting the RPS is 20%; therefore, the estimated sound power levels were adjusted to a 20% probability factor, using the equation $SPL + 10 \cdot \log(20\%)$.

² [TDM Public \(state.al.us\)](https://tmdmpublic.state.al.us)

³ 2045 traffic data were available for this study, data for years earlier than 2024 were not available.

TABLE 9. TRAFFIC DATA FOR NOISE MODELING

Roadway	Segment	No. of Lanes (each direction)	Peak Hour Volume (vph)		Posted Speed (mph)	Vehicle Percentage				
			Existing (year 2022)	Future (year 2045)		Auto	Medium Truck	Heavy Truck	Buses	Motorcycle
Tennessee River Bridge, Natchez Trace Parkway	Eastbound (upstream of Segment 1)	1	75	112	50	86%	0%	9%	0%	5%
Tennessee River Bridge, Natchez Trace Parkway	Eastbound (downstream of Segment 1)	1	76	113	50	86%	0%	9%	0%	5%
Tennessee River Bridge, Natchez Trace Parkway	Eastbound (upstream of Segment 2)	1	76	113	50	86%	0%	9%	0%	5%
Tennessee River Bridge, Natchez Trace Parkway	Eastbound (downstream of Segment 2)	1	71	106	50	86%	0%	9%	0%	5%
Tennessee River Bridge, Natchez Trace Parkway	Eastbound (upstream of Segment 3)	1	71	105	50	86%	0%	9%	0%	5%

Roadway	Segment	No. of Lanes (each direction)	Peak Hour Volume (vph)		Posted Speed (mph)	Vehicle Percentage				
			Existing (year 2022)	Future (year 2045)		Auto	Medium Truck	Heavy Truck	Buses	Motorcycle
Tennessee River Bridge, Natchez Trace Parkway	Eastbound (downstream of Segment 3)	1	57	84	50	86%	0%	9%	0%	5%
Tennessee River Bridge, Natchez Trace Parkway	Westbound (upstream of Segment 1)	1	76	113	50	85%	5.9%	9.1%	0%	0%
Tennessee River Bridge, Natchez Trace Parkway	Westbound (downstream of Segment 1)	1	76	113	50	85%	5.9%	9.1%	0%	0%
Tennessee River Bridge, Natchez Trace Parkway	Westbound (upstream of Segment 2)	1	71	106	50	86%	5.9%	9.1%	0%	0%
Tennessee River Bridge, Natchez Trace Parkway	Westbound (downstream of Segment 2)	1	76	113	50	86%	5.9%	9.1%	0%	0%
Tennessee River Bridge, Natchez Trace Parkway	Westbound (upstream of Segment 3)	1	59	88	50	86%	5.9%	9.1%	0%	0%

Roadway	Segment	No. of Lanes (each direction)	Peak Hour Volume (vph)		Posted Speed (mph)	Vehicle Percentage				
			Existing (year 2022)	Future (year 2045)		Auto	Medium Truck	Heavy Truck	Buses	Motorcycle
Tennessee River Bridge, Natchez Trace Parkway	Westbound (downstream of Segment 3)	1	71	106	50	86%	5.9%	9.1%	0%	0%
Lauderdale County Rd 2	Northbound (downstream of Segment 4)	1	7	17	50	96%	2.97%	0.03%	0%	1%
Lauderdale County Rd 2	Northbound (upstream of Segment 4)	1	12	10	35	96%	2.97%	0.03%	0%	1%
Lauderdale County Rd 2	Southbound (downstream of Segment 4)	1	15	22	35	96%	2.97%	0.03%	0%	1%
Lauderdale County Rd 2	Southbound (upstream of Segment 4)	1	6	9	35	96%	2.97%	0.03%	0%	1%
Lauderdale County Rd 2	Parkway off-ramp	1	17	25	35	96%	2.97%	0.03%	0%	1%
Lauderdale County Rd 2	Parkway on-ramp	1	13	19	35	96%	2.97%	0.03%	0%	1%

To account for RPS noise in the model, a separate line source was created along the outer and inner edges of the Parkwayroad segments in the CadnaA model. Adjusted sound power levels for passenger car and pickup trucks were logarithmically summed and applied as 1/3 octave sound levels to the RPS line source with a 20% probability factor in the model. This method resulted in predicting combined sound levels from traffic and from RPS. The NSNSD-measured 1/3 octave sound pressure levels converted to 1/1 octave sound pressure levels, with a 20% probability factor are summarized in **Table 10**.

TABLE 10. MEASURED RPS SOUND PRESSURE LEVELS

Vehicle Type	Max Sound Pressure Level with 20% RPS Hit (dBA)								Overall Sound Power, Lw (dBA)
	63 Hz	125 Hz	250 Hz	500 Hz	1000 Hz	2000 Hz	4000 Hz	8000 Hz	
Passenger Car at 50 feet	48	53	55	46	54	51	40	35	88
Pickup Truck at 100 feet	56	60	63	56	51	50	45	39	97

Note: Lw = Sound Power Level

The navigable channel would remain in the same location, and barge traffic would continue in the same route during and after construction of the new bridge.

7.0 SUMMARY OF MODELING RESULTS

Figure 3 shows the noise study area and receptor locations. **Figures 4** and **5** show proposed roadway alignments. Southeast Parking Area (R5) is within the construction area and would be closed for visitors for the duration of construction. Consequently, R5 is not included in the construction noise impact assessment. The predicted L_{Aeq} metric is considered for construction noise and compared against measured daytime L_{Anat} and L_{Aeq} because construction is not expected to occur during nighttime hours. It should be noted that predicted L_{Aeq} (1-hour equivalent sound level) is estimated by using FHWA maximum sound levels, (L_{Amax}) for the construction equipment.

The measured L_{Anat} of 34 dB at Colbert Ferry Overlook is considered as daytime L_{Anat} throughout the noise study area. **Tables 11** through **52** present the acoustical modeling results at the receptors. For the details of each construction phase, see Section 6, Methodology and Assumptions.

7.1 Bridge Foundation Construction

7.1.1 Alternative 1

TABLE 11. ALTERNATIVE 1 – BRIDGE FOUNDATION CONSTRUCTION NOISE LEVELS

Receptor ID	Overall Level, (L_{Aeq}) (dB)	1/1 Octave Levels, (Hz) (L_{Aeq}) (dB)								
		31.5	63	125	250	500	1000	2000	4000	8000
R1	72	37	44	54	56	65	67	67	57	34
R2	53	18	25	37	43	49	47	43	21	0
R3	63	29	36	46	52	59	58	57	39	0
R4	74	35	43	55	60	67	68	69	57	27

7.1.2 Alternative 2

TABLE 12. ALTERNATIVE 2 – BRIDGE FOUNDATION CONSTRUCTION NOISE LEVELS

Receptor ID	Bridge Foundation Construction Noise Levels, (Hz) (L _{Aeq})(dB)									
	Overall Level, (L _{Aeq})(dB)	31.5	63	125	250	500	1000	2000	4000	8000
R1	76	41	48	58	59	68	71	72	62	36
R2	58	24	30	42	46	55	52	49	27	0
R3	67	33	40	49	54	62	62	61	44	0
R4	74	35	43	55	61	67	69	70	60	33

7.2 Bridge Superstructure Construction

7.2.1 Alternative 1

TABLE 13. ALTERNATIVE 1 – BRIDGE SUPERSTRUCTURE CONSTRUCTION NOISE LEVELS

Receptor ID	Bridge Superstructure Construction Noise Levels, (Hz) (L _{Aeq})(dB)									
	Overall Level, (L _{Aeq})(dB)	31.5	63	125	250	500	1000	2000	4000	8000
R1	69	30	37	50	53	65	62	62	54	37
R2	45	12	18	29	34	42	34	27	1	0
R3	54	20	27	36	42	50	44	38	20	0
R4	59	22	30	39	43	52	49	45	31	0

7.2.2 Alternative 2

TABLE 14. ALTERNATIVE 2 – BRIDGE SUPERSTRUCTURE CONSTRUCTION NOISE LEVELS

Receptor ID	Bridge Superstructure Construction Noise Levels, (Hz) (L _{Aeq})(dB)									
	Overall Level, (L _{Aeq})(dB)	31.5	63	125	250	500	1000	2000	4000	8000
R1	60	21	32	39	43	50	53	54	46	25
R2	41	0	12	21	29	35	35	31	10	0
R3	52	15	23	31	37	44	43	39	25	0
R4	60	17	27	34	41	48	45	38	25	0

7.3 Bridge Demolition

7.3.1 Alternative 1

TABLE 15. ALTERNATIVE 1– BRIDGE DEMOLITION NOISE LEVELS

Receptor ID	Bridge Demolition Noise Levels, (Hz) (L _{Aeq})(dB)									
	Overall Level, (L _{Aeq})(dB)	31.5	63	125	250	500	1000	2000	4000	8000
R1	71	26	35	52	55	61	66	68	57	32
R2	49	10	19	32	40	40	45	42	15	0
R3	60	18	27	41	49	53	56	55	37	0
R4	73	27	39	52	58	62	68	70	58	31

7.3.2 Alternative 2

TABLE 16. ALTERNATIVE 2– BRIDGE DEMOLITION NOISE LEVELS

Receptor ID	Existing Bridge Demolition Noise Levels, (Hz) (L _{Aeq})(dB)									
	Overall Level, (L _{Aeq})(dB)	31.5	63	125	250	500	1000	2000	4000	8000
R1	71	26	35	52	55	61	66	68	57	32
R2	49	10	19	32	40	40	45	42	15	0
R3	60	18	27	41	49	53	56	56	37	0
R4	73	27	39	52	58	62	68	70	58	31

7.4 Traffic Operations

Noise impacts from existing No-Build, Future No-Build, and Future Build operations for Alternatives 1 and 2 were modeled. Three variations of each were analyzed consisting of (1) roadways with average pavement; (2) roadways with average pavement and RPS; and (3) roadways with porous pavement and RPS. Predicted results for three variations for existing No-Build, Future No-Build, and Future Build for Alternative 1 and 2 are shown below.

7.4.1 Existing No-Build Traffic Operations

TABLE 17. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT

Receptor ID	Overall L_{Aeq} , (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	47	26	35	39	42	42	38	32	24
R2	44	26	35	38	34	36	36	33	24
R3	37	18	28	31	29	33	28	14	5
R4	44	23	32	36	35	40	35	25	18
R5	50	30	39	41	43	45	43	36	30

TABLE 18. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A10} , (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	51	30	39	43	46	46	42	36	28
R2	48	30	39	42	39	40	41	37	29
R3	40	21	31	34	32	36	30	17	8
R4	47	26	36	39	39	44	39	29	21
R5	55	34	44	46	47	50	47	41	34

TABLE 19. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A50} , (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	44	23	33	36	39	39	35	29	21
R2	39	21	31	34	30	31	32	28	20
R3	36	17	27	30	28	32	26	13	4
R4	41	21	30	33	33	38	33	23	15
R5	42	22	32	33	35	38	35	29	22

TABLE 20. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{Aeq} , (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	48	27	36	40	42	43	40	33	24
R2	45	26	36	39	35	38	38	33	24
R3	39	19	29	33	31	34	29	15	5
R4	45	24	33	38	37	41	37	27	18
R5	51	31	40	42	43	46	44	37	30

TABLE 21. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{A10} , (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	52	30	40	44	46	47	43	36	28
R2	49	30	40	43	39	42	42	37	29
R3	41	22	31	36	34	37	32	18	8
R4	49	27	37	42	41	45	41	30	21
R5	55	35	44	46	48	50	49	42	34

TABLE 22. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{A50} , (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	45	24	33	37	40	40	37	30	21
R2	40	21	31	34	30	33	34	29	20
R3	37	18	27	32	30	33	28	14	4
R4	43	21	31	36	35	39	35	24	15
R5	43	23	32	34	36	38	37	29	22

TABLE 23. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{Aeq} (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	47	25	35	39	42	41	37	30	23
R2	43	25	35	38	35	37	36	31	24
R3	37	18	27	31	30	33	26	12	4
R4	43	23	32	35	36	40	35	23	17
R5	49	29	38	41	43	45	41	34	29

TABLE 24. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{A10} (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	51	30	39	43	46	45	41	34	27
R2	48	30	39	42	39	41	40	35	28
R3	41	21	30	35	34	36	30	16	7
R4	48	27	36	41	41	43	39	28	20
R5	54	35	43	46	48	49	46	39	33

TABLE 25. EXISTING NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{A50} (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	44	23	32	37	40	39	35	28	21
R2	39	21	31	34	30	33	32	26	19
R3	37	17	26	31	30	32	26	12	3
R4	42	21	30	35	35	38	33	22	14
R5	42	22	31	33	36	37	34	27	21

7.4.2 Future No-Build Traffic Operations

TABLE 26. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT

Receptor ID	Overall L_{Aeq} , (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	49	28	37	41	44	44	40	34	26
R2	45	27	37	40	36	38	38	35	26
R3	39	20	30	33	31	35	29	16	7
R4	45	25	34	37	37	42	37	27	19
R5	52	32	41	43	45	47	44	38	32

TABLE 27. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A10} , (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	53	31	41	45	47	48	43	37	30
R2	49	32	41	44	40	42	42	39	30
R3	42	23	32	36	34	38	32	19	9
R4	49	28	38	41	40	45	41	31	23
R5	56	36	45	47	49	52	49	43	36

TABLE 28. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A50} , (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	46	25	34	38	41	41	37	31	23
R2	41	23	32	35	31	33	33	30	22
R3	38	19	28	32	30	34	28	15	5
R4	43	22	32	35	34	40	35	25	17
R5	44	24	41	35	37	40	37	30	24

TABLE 29. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{Aeq} , (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	49	28	38	41	44	44	41	34	26
R2	46	28	37	40	36	39	39	35	26
R3	40	20	30	34	32	35	30	17	7
R4	46	25	35	39	39	42	39	28	19
R5	52	32	41	43	45	48	45	39	32

TABLE 30. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{A10} , (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	53	32	41	45	48	48	45	38	30
R2	50	32	42	44	41	43	44	39	30
R3	43	23	33	37	35	38	33	19	9
R4	50	29	38	43	42	46	42	32	23
R5	57	37	46	48	49	52	50	43	36

TABLE 31. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{A50} , (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	47	25	35	39	41	42	38	31	23
R2	41	23	33	36	32	35	35	30	22
R3	39	19	29	33	31	34	29	15	5
R4	44	23	33	37	36	40	36	26	17
R5	45	25	34	36	37	40	38	31	24

TABLE 32. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{Aeq} (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	48	27	37	41	44	43	38	32	25
R2	45	27	36	40	37	38	37	33	25
R3	39	20	29	34	32	34	28	14	6
R4	45	25	34	39	39	41	36	26	18
R5	51	31	40	43	45	46	43	36	31

TABLE 33. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{A10} (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	52	31	40	45	48	47	42	36	29
R2	49	31	41	44	41	42	41	37	30
R3	42	23	32	36	35	37	31	17	9
R4	49	28	37	42	42	45	39	29	22
R5	55	36	45	47	49	51	47	41	35

TABLE 34. FUTURE NO-BUILD – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{A50} (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	46	25	34	38	41	40	36	29	22
R2	40	22	32	35	32	34	32	28	21
R3	38	19	28	32	31	33	27	13	5
R4	43	22	32	36	36	39	34	23	16
R5	43	24	33	35	37	39	35	29	23

7.4.3 Future Build Alternative 1 Traffic Operations

TABLE 35. ALTERNATIVE 1 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT

Receptor ID	Overall L_{Aeq} , (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	49	27	37	41	43	44	40	35	27
R2	47	27	36	40	41	41	39	35	27
R3	39	20	29	33	31	35	29	16	7
R4	45	25	34	37	37	42	37	28	20
R5	55	34	44	47	47	50	47	42	35

TABLE 36. ALTERNATIVE 1– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A10} , (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	53	31	41	45	47	48	43	38	31
R2	51	31	41	44	45	45	43	39	31
R3	42	23	32	36	34	38	32	19	9
R4	49	28	38	41	40	45	41	31	23
R5	59	39	48	51	52	54	52	46	39

TABLE 37. ALTERNATIVE 1– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A50} , (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	46	25	34	39	41	42	37	32	24
R2	42	22	32	35	36	36	34	30	22
R3	38	19	28	32	30	34	28	15	5
R4	43	22	32	35	35	40	35	25	17
R5	47	27	36	39	40	42	40	34	27

TABLE 38. ALTERNATIVE 1– OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq}, AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L _{Aeq} , (dB)	1/1 Octave Levels (Hz), L _{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	49	28	37	42	44	45	41	35	27
R2	48	27	37	40	41	42	40	36	27
R3	40	20	30	34	32	35	30	16	7
R4	47	25	35	39	39	43	39	29	20
R5	55	35	44	47	48	50	49	43	35

TABLE 39. ALTERNATIVE 1– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10}, AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L _{A10} , (dB)	1/1 Octave Levels (Hz), L _{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	53	32	41	46	47	49	45	39	31
R2	52	31	41	45	45	46	45	40	31
R3	43	23	33	37	35	38	33	19	9
R4	50	29	38	43	42	46	42	32	23
R5	60	39	49	52	52	55	53	47	40

TABLE 40. ALTERNATIVE 1 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50}, AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L _{A50} , (dB)	1/1 Octave Levels (Hz), L _{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	47	25	35	39	41	42	38	32	24
R2	43	23	32	36	36	37	36	31	22
R3	39	19	29	33	31	34	29	15	5
R4	44	23	33	37	36	40	36	26	17
R5	48	27	37	40	40	42	41	35	27

TABLE 41. ALTERNATIVE 1 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{Aeq} (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	49	27	36	41	44	43	38	32	26
R2	47	27	36	40	41	41	38	33	26
R3	39	20	29	34	32	34	28	14	6
R4	46	25	34	39	39	41	36	26	19
R5	54	34	43	47	48	49	46	41	34

TABLE 42. ALTERNATIVE 1– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{A10} (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	52	31	40	45	48	47	42	36	30
R2	51	31	40	44	45	45	43	37	30
R3	42	22	32	36	35	37	30	17	9
R4	49	28	37	42	42	45	40	29	22
R5	59	39	48	51	52	53	50	45	39

TABLE 43. ALTERNATIVE 1– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , POROUS PAVEMENT WITH RPS

Receptor ID	Overall L_{A50} (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	46	24	34	39	41	41	35	30	24
R2	42	22	32	35	37	36	34	29	22
R3	38	18	28	32	31	33	26	13	5
R4	43	22	32	36	37	39	34	24	17
R5	46	27	36	39	40	41	38	33	27

7.4.4 Future Build Alternative 2 Traffic Operations

TABLE 44. ALTERNATIVE 2 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT

Receptor ID	Overall L_{Aeq} (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	48	26	36	39	42	43	39	32	24
R2	47	28	37	41	40	41	39	35	27
R3	38	19	29	32	29	34	29	13	4
R4	46	25	35	38	38	43	37	29	22
R5	54	33	42	45	47	49	46	40	34

TABLE 45. ALTERNATIVE 2– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A10} (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	51	30	40	43	46	47	43	36	28
R2	51	32	42	45	44	45	44	39	31
R3	41	22	32	35	32	37	31	16	7
R4	50	29	38	41	42	47	40	33	25
R5	58	38	47	49	51	54	51	45	38

TABLE 46. ALTERNATIVE 2– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT

Receptor ID	Overall L_{A50} (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	45	24	33	36	40	40	36	29	22
R2	43	23	33	36	35	37	35	31	22
R3	37	18	28	31	28	33	27	12	3
R4	44	23	32	35	36	41	34	27	19
R5	46	26	42	37	39	41	39	33	26

TABLE 47. ALTERNATIVE 2 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{Aeq} (dB)	1/1 Octave Levels (Hz), L_{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	48	27	36	39	43	43	40	32	24
R2	48	28	38	41	40	42	41	36	27
R3	39	20	30	34	31	35	30	13	4
R4	47	26	35	40	40	44	38	30	22
R5	54	34	43	45	47	50	48	41	34

TABLE 48. ALTERNATIVE 2– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{A10} (dB)	1/1 Octave Levels (Hz), L_{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	52	31	40	43	47	47	44	36	28
R2	52	32	42	45	44	46	45	40	31
R3	42	23	32	36	34	38	32	16	7
R4	51	29	39	43	43	47	42	33	25
R5	59	38	48	50	52	54	52	46	38

TABLE 49. ALTERNATIVE 2– OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50} , AVERAGE PAVEMENT WITH RPS

Receptor ID	Overall L_{A50} (dB)	1/1 Octave Levels (Hz), L_{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	45	24	34	37	40	41	37	30	22
R2	43	24	33	36	35	37	36	31	22
R3	38	19	28	32	30	34	28	12	3
R4	45	23	33	37	37	41	36	28	19
R5	47	26	35	38	40	42	40	34	26

TABLE 50. ALTERNATIVE 2— OPERATIONAL TRAFFIC NOISE LEVELS, L_{Aeq}, POROUS PAVEMENT WITH RPS

Receptor ID	Overall L _{Aeq} (dB)	1/1 Octave Levels (Hz), L _{Aeq} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	47	26	35	39	43	42	37	30	24
R2	47	28	37	41	40	41	38	33	26
R3	38	19	29	33	31	33	27	11	3
R4	46	25	34	39	40	42	36	27	21
R5	53	33	42	45	47	48	45	39	33

TABLE 51. ALTERNATIVE 2 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A10}, POROUS PAVEMENT WITH RPS

Receptor ID	Overall L _{A10} (dB)	1/1 Octave Levels (Hz), L _{A10} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	51	30	39	43	47	46	41	34	28
R2	51	32	41	45	44	45	43	38	31
R3	41	22	31	36	34	36	30	14	6
R4	50	29	38	43	43	46	40	31	24
R5	58	38	47	49	52	53	50	44	37

TABLE 52. ALTERNATIVE 2 – OPERATIONAL TRAFFIC NOISE LEVELS, L_{A50}, POROUS PAVEMENT WITH RPS

Receptor ID	Overall L _{A50} (dB)	1/1 Octave Levels (Hz), L _{A50} , (dB)							
		63	125	250	500	1000	2000	4000	8000
R1	45	23	33	36	40	39	34	27	21
R2	42	23	32	36	35	36	34	29	22
R3	37	18	27	32	30	32	26	10	2
R4	44	23	32	37	37	40	34	25	18
R5	46	26	34	37	40	41	37	31	25

8.0 Impact Analysis

The following sections assess the potential impact to the acoustical environment in the noise study area under the no-action alternative and action alternatives.

The L_{Aeq} metric is considered appropriate and commonly used for construction noise analysis. Furthermore, FHWA's RCNM V2.0 does not provide L_{A50} sound levels for construction equipment; it only publishes L_{Aeq} and L_{Amax} sound levels. Therefore, predicted L_{Aeq} construction noise levels are compared to the existing ambient L_{Aeq} sound levels in addition to L_{Anat}. FHWA confirmed that construction is predominantly anticipated to occur during daytime hours (7:00 a.m. to 7:00 p.m.); therefore, construction noise levels are compared against ambient daytime levels. Analysis of impacts

from major phases of construction found that the bridge foundation phase is predicted to have the most impact at the receptors that are close to the bridge.

Operational noise analysis compared no-action and with-action impacts. Predicted future traffic noise levels would be similar for both design alternatives mainly because of identical traffic data. These impacts are based on projected peak daytime hour traffic with average pavement having RPS. Potential reduction in noise from the use of porous pavement on the noise study area is discussed in Section 9.

The predicted L_{A50} traffic noise level is considered the most relevant metric for this study because it can be directly compared to the existing baseline sound level (L_{A50}) and the natural ambient sound level (L_{Anat}).

8.1 Impacts of No-Action Alternative

Under the no-action alternative, the project would not be constructed. Roadway vehicles and aircrafts would remain the most audible sources of noise and have the largest effect on the existing acoustical environment in the noise study area. Exposure of park resources to traffic noise would remain the same under the no-action alternative.

Under the no-action alternative, audible noise from anthropogenic sources, primarily roadway vehicles, would continue to impact the noise study area, similar to existing conditions. As discussed in Section 5 of this report, measured existing sound levels are summarized in **Table 53**.

TABLE 53. MEASURED EXISTING AMBIENT LEVELS

L_{Aeq} (dB)		L_{A10} (dB)		L_{A50} (dB)		L_{Anat} (dB)	
Day	Night	Day	Night	Day	Night	Day	Night
50	46	53	44	42	35	34	31

Under the no-action alternatives, traffic noise impacts at the receptors for average pavement with RPS for existing no-build for 2022 and future no-build for 2045 are summarized in **Table 54**.

TABLE 54. TRAFFIC NOISE IMPACTS OF THE NO-ACTION ALTERNATIVE

Receptors	No-Build Existing Traffic Noise Impacts for Year 2022, L_{A50} , (dB)	No-Build Future Traffic Noise Impacts for Year 2045, L_{A50} , (dB)	Noise Increment, L_{A50} (dB)
R1	45	47	2
R2	40	41	1
R3	37	39	2
R4	43	44	1
R5	43	45	2

8.2 Impacts of Alternative 1

Construction noise impacts of Alternative 1 compared with measured ambient L_{Aeq} and L_{Anat} are summarized in **Table 55**. Operational noise impacts of Alternative 1 for average pavement with RPS

compared against measured L_{A50} and L_{Anat} are summarized in **Table 56**. The NPS confirmed that the new bridge would have RPS under both alternatives.

TABLE 55. CONSTRUCTION NOISE IMPACTS OF ALTERNATIVE 1

Receptors	Alt 1 Construction Noise Impacts L_{Aeq} , (dB)			Daytime Existing Ambient L_{Aeq} , (dB)	Daytime Natural Ambient L_{Anat} , (dB)
	Bridge Foundations	Bridge Superstructure	Existing Bridge Demolition		
R1	72	69	71	50	34
R2	53	45	49	50	34
R3	63	54	61	50	34
R4	74	59	73	50	34

TABLE 56. FUTURE OPERATIONAL NOISE IMPACTS OF ALTERNATIVE 1

Receptors	Alt 1 Operational Noise Impacts L_{A50} , (dB)	No-Build Operational Noise Impacts L_{A50} , (dB)	Existing Ambient L_{A50} , (dB)		Natural Ambient L_{Anat} , (dB)	
			Daytime	Nighttime	Daytime	Nighttime
R1	47	47	42	35	34	31
R2	43	41	42	35	34	31
R3	39	39	42	35	34	31
R4	44	44	42	35	34	31
R5	48	45	42	35	34	31

Construction noise impacts for Alternative 1 would exceed the measured ambient L_{Aeq} and L_{Anat} noise levels at all receptors.

Future peak hour traffic operational L_{A50} noise levels for Alternative 1 are predicted to exceed the measured daytime ambient L_{A50} levels at four of five receptors and at all receptors for nighttime. Future Build noise levels exceed future no-build noise levels at two of five receptors. Predicted noise levels would also exceed measured L_{Anat} levels at all receptors for both daytime and nighttime.

Operational traffic noise would not exceed FHWA or the Alabama Department of Transportation's outdoor residential noise abatement criteria of 66 dB $L_{Aeq, 1hr}$. Predicted noise level contours for Alternative 1 traffic operations for average pavement with RPS are shown in **Figure 8**.

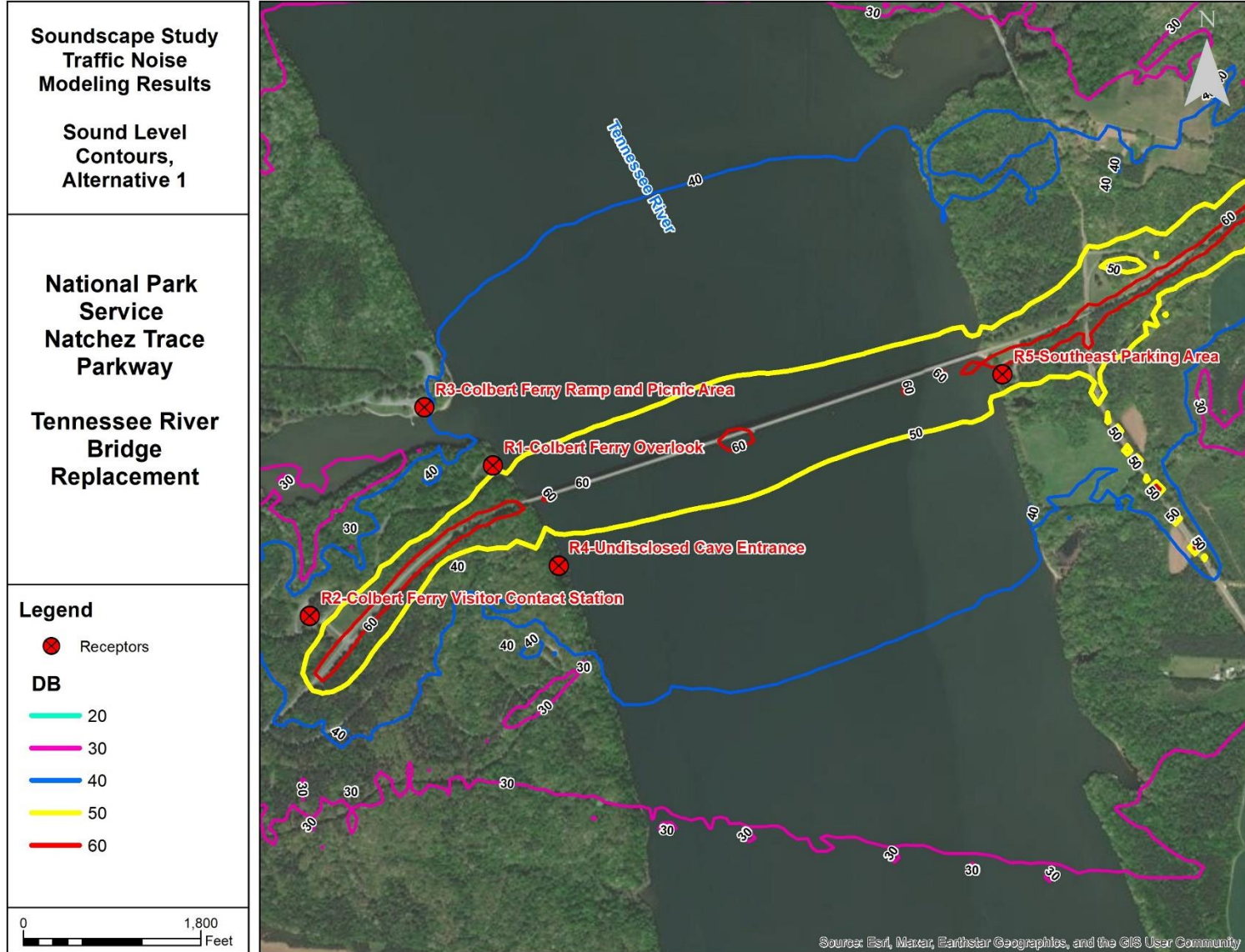


FIGURE 8. PREDICTED NOISE CONTOURS FOR ALTERNATIVE 1, FUTURE TRAFFIC

8.3 IMPACTS OF ALTERNATIVE 2

Construction noise impacts of Alternative 2 were also compared against measured ambient L_{Aeq} and L_{Anat} and are summarized in **Table 57**. Operational noise impacts of Alternative 2 for average pavement with RPS were compared with measured L_{A50} and L_{Anat} and are summarized in **Table 58**.

TABLE 57. CONSTRUCTION NOISE IMPACTS OF ALTERNATIVE 2

Receptors	Alternative 2 Construction Noise Impacts L_{Aeq} , (dB)			Daytime Ambient L_{Aeq} , (dB)	Daytime Natural Ambient L_{Anat} , (dB)
	Bridge Foundations	Bridge Superstructure	Existing Bridge Demolition		
R1	76	60	71	50	31
R2	58	41	49	50	31
R3	65	52	60	50	31
R4	74	60	73	50	31

TABLE 58. OPERATIONAL NOISE IMPACTS OF ALTERNATIVE 2

Receptors	Alternative 2 Operational Noise Impacts L_{A50} , (dB)	No-Build Operational Noise Impacts L_{A50} , (dB)	Existing Ambient L_{A50} , (dB)		Natural Ambient L_{Anat} , (dB)	
			Daytime	Nighttime	Daytime	Nighttime
R1	45	47	42	35	34	31
R2	43	41	42	35	34	31
R3	38	39	42	35	34	31
R4	45	44	42	35	34	31
R5	47	45	42	35	34	31

Construction noise impacts for Alternative 2 would exceed the measured ambient L_{Aeq} and L_{Anat} noise levels at all receptors.

Future peak hour traffic operational L_{A50} noise levels for Alternative 2 are predicted to exceed the measured daytime ambient L_{A50} levels at four out of five receptors and at all receptors for nighttime. Future Build noise levels exceeded future no-build noise levels at four of five receptors. Predicted noise levels would also exceed measured L_{Anat} levels at all receptors for both daytime and nighttime.

Like Alternative 1, operational traffic noise would not exceed FHWA or the Alabama Department of Transportation's outdoor residential noise abatement criteria of 66 dB $L_{Aeq, 1hr}$. Predicted noise level contours for Alternative 2 traffic operations with average pavement and RPS are shown in **Figure 9**.

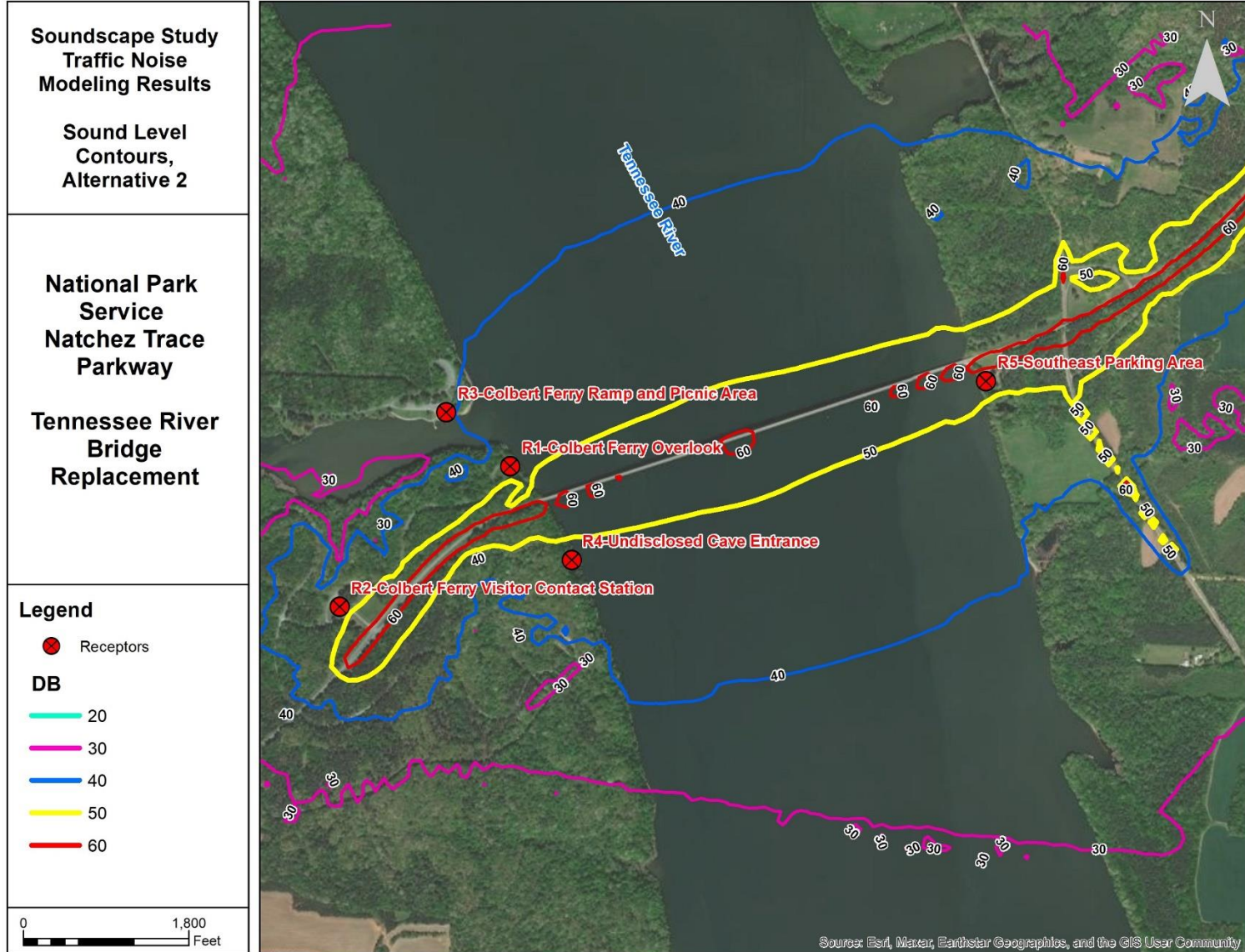


FIGURE 9. PREDICTED NOISE CONTOURS FOR ALTERNATIVE 2, FUTURE TRAFFIC

8.4 COMPARISON OF IMPACTS FROM ALTERNATIVES 1 AND 2

Construction noise from bridge foundation and superstructure phases is predicted to be less impactful for Alternative 1 than for Alternative 2. As expected, demolition activities are predicted to generate similar impacts for both design alternatives. Additionally, the duration of construction for Alternative 1 would be one year shorter than for Alternative 2, resulting in a reduced duration of exposure. Construction activities related to Alternative 2 would be physically closer to the Undisclosed Cave Entrance and forested land southwest of the bridge; thus, Alternative 2 is predicted to have greater impacts on wildlife southwest of the bridge. The extent of tree removal required for Alternative 2 is also comparatively extensive.

Future traffic noise impacts for both alternatives (measured as L_{A50}) would be similar with subtle differences due to the relative proximity to the receptors. Alternative 2 has an advantage of 2 decibels (dBA) at Colbert Ferry Overlook (R1) over Alternative 1. Meanwhile, with RPS, noise impacts are predicted to be 1 dB higher at the receptors compared to average pavement without RPS. Construction and operational noise impacts are compared and summarized in **Tables 59** and **60**.

TABLE 59. COMPARISON OF CONSTRUCTION NOISE IMPACTS FROM ALTERNATIVES 1 AND 2

Receptors	Bridge Foundation, L_{Aeq} , (dB)			Bridge Superstructure, L_{Aeq} , (dB)			Demolition, L_{Aeq} , (dB)		
	Alt. 1	Alt.2	Diff.	Alt. 1	Alt. 2	Diff.	Alt. 1	Alt. 2	Diff.
R1, Colbert Ferry Overlook	72	76	-4	69	60	9	71	71	0
R2, Colbert Ferry Visitor Station	53	58	-5	45	41	4	49	49	0
R3, Colbert Ferry Boat Ramp and Picnic Area	63	67	-4	54	52	2	60	60	0
R4, Undisclosed Cave Entrance	74	74	0	59	60	-1	73	73	0

Notes: Alt. = Alternative; Diff. = Difference

TABLE 60. COMPARISON OF OPERATIONAL NOISE IMPACTS FROM ALTERNATIVES 1 AND 2

Receptors	Alternative 1, $L_{A50}^{a,b}$ dB	Alternative 2, $L_{A50}^{a,b}$ dB	Difference a,b dB
R1, Colbert Ferry Overlook	47 (46)	45 (45)	2 (1)
R2, Colbert Ferry Visitor Station	43 (42)	43 (43)	0 (1)
R3, Colbert Ferry Boat Ramp and Picnic Area	39 (38)	38 (37)	1 (1)
R4, Undisclosed Cave Entrance	44 (43)	45 (44)	1 (1)
R5, Southeast Parking	48 (47)	47 (46)	1 (1)

Notes:

^a Noise levels are for average pavement with RPS and rounded to nearest zero.

^b Noise levels in brackets are for average pavement without RPS.

9.0 POTENTIAL MITIGATION MEASURES

9.1 BRIDGE CONSTRUCTION AND DEMOLITION

Construction noise is temporary and occurs predominantly during the daytime. Trucks and machinery that are used for construction produce noise, which may impact receptors and the soundscape of the Parkway. Specific noise control measures would be incorporated in the noise mitigation plan(s) required according to 23 Code of Federal Regulations Part 772 (23 Code of Federal Regulations 772). These measures could include a variety of source and path controls, including the following:

- The project would reduce the noise level at the source during the most time-sensitive period.
- Appropriate usage and maintenance procedures would be followed for all construction equipment to limit noise emissions. These procedures include:
 - Employ the manufacturer's appropriate noise reduction measures to maintain all equipment and mufflers.
 - Close engine housing doors during operation.
 - Cover portable noise-generating equipment with noise-insulating fabric and blankets where feasible and practicable.
- Where feasible and practicable, construction sites would be configured to minimize backup alarm noise. Broadband backup alarms are effective alternatives to minimize the impacts.
- All trucks, other than trucks loading, unloading, or processing material (e.g., concrete mixing trucks), would not be allowed to idle for extended periods at the construction site.
- Where logistics allow, path controls (such as portable barriers or shrouds around specific equipment) would be considered during the development of the construction noise mitigation plan.
- Where logistics allow, noisy equipment, such as cranes, concrete pumps, concrete trucks, and delivery trucks, would be located away from and shielded from sensitive receptor locations.
- As early in the construction period as logistics permit, diesel- or gas-powered equipment would be replaced with electrical-powered equipment, such as welders, water pumps, bench saws, and table saws, to the extent feasible and practicable.
- Noise barriers constructed from plywood or other materials would be used to provide shielding (e.g., the construction sites would have a minimum 8-foot barrier).
- Where logistics allow, truck deliveries would take place behind the noise barriers.

9.2 TRAFFIC OPERATIONS

The peak hour traffic noise is predicted to exceed the natural ambient sound level in most of the noise study area and exceed the existing L_{A50} ambient level during nighttime. The analysis predicted future traffic noise levels for average and porous pavement on the noise study area. The overall average reduction in A--weighted traffic noise levels with porous pavement compared to average pavement is predicted to be 0.8 dB at the receptors, based on FHWA TNM v3.2 modeling. Other potential traffic noise control options may include (1) prevention of heavy trucks on the Parkway, (2) reduction in speed limits, and (3) construction of barriers where feasible.

10.0 CONCLUSIONS

10.1 CONSTRUCTION NOISE

The study's findings indicate that the total noise exposure during construction activities is expected to reach peak levels of 74 dBA and 76 dBA for Alternative 1 and Alternative 2, respectively. These levels are well above the measured L_{Aeq} and L_{Anat} in the noise study area. During daytime hours, the predicted effects of these impacts could be speech interference in the vicinity of the construction equipment and audible construction noise at a considerable distance.

Construction noise impacts under Alternative 1 are expected to be less compared to Alternative 2 for the following reasons:

- The duration of construction is one year longer for Alternative 2, which would generate prolonged impacts.
- Alternative 2 would require more tree removal, with a commensurate use of chainsaws and associated equipment.
- Alternative 2 construction activities are physically closer to Undisclosed Cave Entrance and the forested area on the southwest side of the bridge, resulting in higher noise impacts on wildlife.

10.2 FUTURE OPERATIONAL NOISE

The study findings indicate that peak hour future traffic noise levels would exceed the existing measured ambient L_{A50} level and predicted no-build traffic noise levels for daytime, and would exceed measured ambient nighttime L_{A50} and L_{Anat} levels.

Future Build traffic noise levels are expected to reach a peak level of 48 dB L_{A50} for Alternative 1 and 47 dB L_{A50} for Alternative 2 at the closest receptor (Southeast Parking). These noise levels are below the speech interference threshold of 52 dB $L_{Aeq,1s}$. Traffic noise is not expected to adversely affect the ability for hikers or visitors to speak at normal conversational volumes at the receptors. With the exception of R3-Colbert Ferry Ramp and Picnic Area, traffic noise levels for Alternatives 1 and 2 would exceed the 40 dB L_{Aeq} threshold for noise-induced responses for terrestrial wildlife. Predicted existing and future no-build traffic noise levels also exceed 40 dB L_{Aeq} .

In general, a change of 3 dB is the minimum difference in levels that is perceived by a normal human being. Therefore, park visitors would not perceive the difference of 1 to 2 dBA between Alternative 1 and Alternative 2.

10.3 IMPACTS ON WILDLIFE

The amount of information available for assessing the effects of noise on wildlife is not as extensive as the information available for humans (Shannon et al. 2016). It was previously understood that an overall noise level guideline of 60 dBA for continuous noise was considered an appropriate level to limit sound masking for wildlife (Caltrans 2016). To the human ear, this 60-dBA threshold would be similar to hearing the sound of a normal conversation or a dishwasher running (Decibel Pro n.d.). More recent studies have documented functional responses in terrestrial wildlife starting at an overall sound level of 40 dB (Shannon et al. 2016; NPS 2020). Disturbance thresholds vary by species, especially among birds (Patón et al. 2012). The effects of noise on aquatic bivalves have not been studied extensively; however, a recent study found that a marine mussel species responded to underwater noise in a laboratory setting by closing its valves during exposure. The response decreased with repeated exposure (Hubert et al. 2022).

The demolition and replacement of the Tennessee River Bridge would generate several changes to the acoustic environment during its construction. The anticipated duration of construction is approximately five and a half years for Alternative 1 and six and a half years for Alternative 2, and the predicted construction noise impacts would exceed both the measured daytime median existing ambient sound level

and the median natural ambient sound level. The major noise-generating activities include demolition of the existing bridge and construction of the bridge foundations and superstructure. These activities are predicted to generate overall sound level noise impacts in the range of 45 dB to 74 dB at the sensitive receptors. The maximum impact predicted at the Undisclosed Cave entrance would be an overall sound level of 74 dB during the bridge foundation phase. These levels are well above the measured overall sound level and median natural ambient sound level in the noise study area and would have short-term, adverse impacts on the soundscape of the noise study area. However, Alternative 1 would be physically farther from the Undisclosed Cave and the forested area to the southwest side of the bridge, resulting in lower noise impacts on wildlife, primarily bats, other mammals, and birds.

Future Build overall sound levels are predicted to be in the range of 37 dB and 48 dB at the sensitive receptors. The maximum impact due to build traffic noise at the Undisclosed Cave entrance is predicted to be 45 dBA L_{A50} for Alternative 1, which is 1 dB above the no-build conditions, and 44 dBA L_{A50} for Alternative 2. These levels are above the 40 dBA sound level threshold for noise-induced responses to terrestrial wildlife. Due to topography, rock mass, and depth of the cave below ground, noise impacts would be further reduced within the cave portal. As a result, hibernating bats are not likely to be disturbed during the construction or from future operational traffic noise impacts (WSP 2021).

11.0 ACRONYMS AND ABBREVIATIONS

dB	decibels
dBA	A-weighted decibel
DGAC	Dense Graded Asphalt Concrete
FHWA	Federal Highway Administration
Hz	hertz
kHz	kilohertz
L _A	A-weighted sound pressure levels
L _{Aeq}	Energy Equivalent A-weighted Sound Level
L _{Amax}	Maximum A-weighted Sound levels
L _{Anat}	A-weighted Natural Ambient Sound Level
L _{dn}	Day-Night Average Sound Level
L _w	Sound Power Level
NFI	Noise-Free Interval
NPS	National Park Service
NSNSD	Natural Sounds and Night Skies Division
OGAC	open-graded asphalt concrete
Parkway	Natchez Trace Parkway
PCC	Portland Cement Concrete
project	John Coffee Memorial Bridge replacement project
RCNM	Roadway Construction Noise Model
RPS	Raised Profile Strips
SEL	Sound Exposure Level
TNM	Traffic Noise Model
WSP	WSP USA Inc.

12.0 REFERENCES

Alabama Department of Transportation (ALDOT)

- 2021 *Highway Traffic Noise Analysis and Abatement*. July 2021.

California Department of Transportation (Caltrans)

- 2016 *Technical Guidance for Assessment and Mitigation of the Effects of Road Noise and Road Construction Noise on Birds*. Division of Environmental Analysis. June (Contract 43A0306). Sacramento, CA. Prepared by ICF International, Sacramento, CA, Robert Dooling, Gaithersburg, MD, and Arthur Popper, Silver Spring, MD. <https://dot.ca.gov/-/media/dot-media/programs/environmental-analysis/documents/env/noise-effects-on-birds-jun-2016-ally.pdf>

Decibel Pro

- n.d. Decibel Sound Level Meter. <https://decibelpro.app/blog/decibel-chart-of-common-sound-sources/>

Federal Highway Administration (FHWA)

- 2010 *Highway Traffic Noise: Analysis and Abatement Guidance*. FHWA-HEP-10-025. July 2010.

Finch, D., H. Schofield, and F. Mathews

- 2020 “Traffic noise playback reduces the activity and feeding behaviour of free-living bats.” *Environmental Pollution*, Volume 263, Part B, 114405.

Haralabidis, A., S., K. Dimakopoulou, F. Vigna-Taglianti, M. Giampaolo, A. Borgini, M. L. Dudley, G. Pershagen, G. Bluhm, D. Houthuijs, W. Babisch, M. Velonakis, K. Katsouyanni, and L. Jarup

- 2008 “Acute effects of night-time noise exposure on blood pressure in populations living near airports.” *European Heart Journal Advance Access* 29(5): 658–64.

Hubert, J., E. Booms, R. Witbaard, and H. Slabbekoorn

- 2022 “Responsiveness and habituation to repeated sound exposures and pulse trains in blue mussels.” *Journal of Experimental Marine Biology and Ecology* 547:1–10.

NPS (National Park Service)

- 2006 *NPS Management Policies 2006*. Washington, DC: US Department of the Interior, National Park Service.
- 2020 *Acoustic Inventory 2015–2017. Katmai National Park and Preserve, Alaska*. Natural Resource Report NPS/NRSS/NSNSD/NRR—2020/2080.

Patón, D., R. Francisco, J. Cuenca, and J. C. Escudero

- 2012 “Tolerance to noise in 91 bird species from 27 urban gardens of Iberian Peninsula.” *Landscape and Urban Planning* 104:1–8.

Shannon G., M. F. McKenna, L. M. Angeloni, K. R. Crooks, K. M. Fristrup, E. Brown, K. A. Warner, M. D. Nelson, C. White, J. Briggs, S. McFarland, and G. Wittemyer

- 2016 “A synthesis of two decades of research documenting the effects of noise on wildlife.” *Biological Reviews* 91(4): 982–1005.

USEPA (US Environmental Protection Agency)

- 1974 *Information on Levels of Noise Requisite to Protect the Public Health and Welfare with an Adequate Margin of Safety*. Prepared by the US Environmental Protection Agency, Office of Noise Abatement and Control.

US Geological Survey (USGS)

- 2021 USGS Elevation TIFF data products. The National Map Download Client.
<https://apps.nationalmap.gov/downloader/#/>

WSP USA Inc. (WSP)

- 2021 “Biological Assessment for the acquisition of site for development of an underground safety research program facility for the Center for Disease Control and Prevention/National Institute for Occupational Safety and Health (CDC/NIOSH).”
- 2023 “Replacement of the Tennessee River Bridge Traffic Study.” September 2023.

**APPENDIX G:
TRAFFIC STUDY**

**United States Department of the Interior
National Park Service**

Natchez Trace Parkway

**Replacement of the John Coffee Memorial Bridge
(Tennessee River Bridge)**

Traffic Study

September 2023

CONTENTS

1.0	INTRODUCTION.....	1
2.0	BACKGROUND	1
3.0	ALTERNATIVES	2
3.1	ALTERNATIVE ELEMENTS	2
3.2	ALTERNATIVES DESCRIPTIONS	2
4.0	PLANNING CONTEXT.....	5
4.1	NORTHWEST ALABAMA COUNCIL OF LOCAL GOVERNMENTS	5
4.2	NATIONAL PARK SERVICE/PARKWAY-SPECIFIC PLANS	5
5.0	PARKWAY VISITATION.....	5
6.0	TRANSPORTATION NETWORK.....	6
6.1	TRAFFIC STUDY AREA DESCRIPTION	6
6.2	ROADWAY DESCRIPTION	6
6.3	DATA COLLECTION	8
7.0	ANALYSIS METHODOLOGY	16
7.1	INTERSECTION OPERATIONS	16
7.2	QUEUEING ANALYSIS.....	17
8.0	EXISTING CONDITIONS	17
8.1	INTERSECTION OPERATIONS	17
8.2	INTERSECTION QUEUEING.....	17
9.0	DEVELOPMENT OF THE NO BUILD.....	22
9.1	BACKGROUND GROWTH	22
9.2	ROADWAY IMPROVEMENTS	22
9.3	DEVELOPMENT OF NO BUILD TRAFFIC VOLUMES.....	22
10.0	ANALYSIS OF THE ALTERNATIVES	28
10.1	ALTERNATIVE 1.....	28
10.1.1	Description.....	28
10.1.2	Development of Traffic Forecasts.....	28
10.2	ALTERNATIVE 2.....	34
10.2.1	Description.....	34
10.2.2	Development of Traffic Forecasts.....	34
10.3	COMPARATIVE ANALYSIS	40
10.3.1	Intersection Operations	40
10.3.2	Queue Analysis	43
11.0	MITIGATION MEASURES.....	46
11.1	NO BUILD	46
11.2	ALTERNATIVES 1 AND 2	46
12.0	CONCLUSIONS	46
12.1	EXISTING CONDITIONS	46
12.2	NO BUILD	46

12.3	ALTERNATIVE 1.....	46
12.4	ALTERNATIVE 2.....	46
13.0	ACRONYMS AND ABBREVIATIONS.....	47
14.0	REFERENCES.....	47
15.0	LIST OF PREPARERS.....	48

LIST OF TABLES

Table ES-1.	No Build and Alternatives 1 and 2 Operations Analyses Summary	ix
Table 6-1.	ALDOT Traffic Volumes, 2013–2021	15
Table 6-2.	NPS Historic Traffic Volumes Near Cherokee, Alabama	15
Table 7-1.	Unsignalized Intersection Control Delay and LOS Thresholds—HCM 6th Edition Method.....	17
Table 8-1.	2022 Existing Condition Operations Analyses	20
Table 8-2.	2022 Existing Condition Queue Analysis.....	21
Table 9-1.	2045 No Build Operations Analyses.....	27
Table 10-1.	2045 Alternative 1 Operations Analyses	33
Table 10-2.	2045 Alternative 2 Operations Analyses	39
Table 10-3.	2045 No Build and Alternative 1 Operations Analyses.....	41
Table 10-4.	2045 No Build and Alternative 2 Operations Analyses.....	42
Table 10-5.	2045 No Build and Alternative 1 Queue Analysis.....	44
Table 10-6.	2045 No Build and Alternative 2 Queue Analysis.....	45

LIST OF FIGURES

Figure ES-1.	Horizontal Alignment for Alternative 1	v
Figure ES-2.	Horizontal Alignment for Alternative 2	vi
Figure 3-1.	Horizontal Alignment for Alternative 1	3
Figure 3-2.	Horizontal Alignment for Alternative 2.....	4
Figure 6-1.	Traffic Analysis Study Area.....	7
Figure 6-2.	Non-Peak Season 2022 Existing Volumes.....	9
Figure 6-3.	Peak Season 2022 Existing Volumes	10
Figure 6-4.	2022 Existing Geometry.....	11
Figure 6-5.	Collected Traffic Count Locations	13
Figure 6-6.	ALDOT Traffic Count Locations.....	14
Figure 7-1.	Level of Service Diagram	16
Figure 8-1.	Non-Peak Season 2022 Existing Level of Service.....	18
Figure 8-2.	Peak Season 2022 Existing Level of Service	19
Figure 9-1.	Non-Peak Season 2045 No Build Volumes	23
Figure 9-2.	Peak Season 2045 No Build Volumes.....	24

Figure 9-3. Non-Peak Season 2045 No Build Level of Service	25
Figure 9-4. Peak Season 2045 No Build Level of Service.....	26
Figure 10-1. Non-Peak Season 2045 Alternative 1 Volumes	29
Figure 10-2. Peak Season 2045 Alternative 1 Volumes	30
Figure 10-3. Non-Peak Season 2045 Alternative 1 Level of Service	31
Figure 10-4. Peak Season 2045 Alternative 1 Level of Service	32
Figure 10-5. Non-Peak Season 2045 Alternative 2 Volumes	35
Figure 10-6. Peak Season 2045 Alternative 2 Volumes	36
Figure 10-7. Non-Peak Season 2045 Alternative 2 Level of Service	37
Figure 10-8. Peak Season 2045 Alternative 2 Level of Service	38

APPENDICES

Appendix A	Alternative Plans
Appendix B	Traffic Counts
Appendix C	Historic Counts
Appendix D	Capacity Analysis Worksheets
Appendix E	Volume Development

EXECUTIVE SUMMARY

The National Park Service (NPS) is proposing to reconstruct the John Coffee Memorial Bridge (Tennessee River Bridge) along the Natchez Trace Parkway, spanning the Tennessee River between Colbert County, Alabama, and Lauderdale County, Alabama. The current bridge opened to traffic in 1964, providing a connection for Natchez Trace Parkway visitors and local traffic in northwest Alabama between Tennessee and Mississippi while preserving the movement of barge traffic in the Tennessee River. The Tennessee River Bridge has exceeded its design life, and the NPS is proposing replacing the bridge as a strategy to maintain safe movement on the Natchez Trace Parkway across the Tennessee River. This traffic study evaluates traffic operations based on two alternatives for bridge replacement.

The study analyzes vehicular traffic under existing conditions, future conditions without a bridge replacement (representing the No Build condition), and future conditions comparing two bridge alignment alternatives (alternatives 1 and 2, representing the alternatives condition). This traffic study will be used to inform the elements and alternatives under consideration, identify projected traffic operational issues, and provide information to support the planning process.

ALTERNATIVES

Alternative Elements

The preliminary conceptual design for the Tennessee River Bridge includes:

- Bridge length of 5,050 feet (approximately 0.95 miles) replacing the existing Tennessee River Bridge and providing a crossing over the Tennessee River between Colbert County, Alabama, and Lauderdale County, Alabama.
- Bridge width of 43 feet, which includes a two-lane roadway and shoulders having a combined width of 34 feet; a 5-foot walkway; and curbs, gutters, and barriers.
- Reconstructing a portion of the Natchez Trace Parkway to tie back into the existing alignment approximately between Colbert Ferry Road and Lauderdale County Route 2.

Alternatives Descriptions

The alternatives analyzed in this traffic study include:

- Alternative 1 – Replaces the Tennessee River Bridge, with the abutment on the west end of the bridge in the approximate position of the bridge's existing western abutment, angles the bridge spans southeastward heading in the eastbound direction with respect to the existing bridge alignment, and locates the abutment on the east end of the bridge to the south of the bridge's existing eastern abutment.
- Alternative 2 – Replaces the Tennessee River Bridge, locating the abutments on both ends of the bridge and the bridge spans to the south of the existing bridge alignment.

Figure ES-1 presents the horizontal alignment of Alternative 1 in the context of nearby intersections within the traffic study area, and Figure ES-2 presents the horizontal alignment of Alternative 2.

Natchez Trace Parkway: Tennessee River Bridge Replacement Alternative 1

National Park Service
US Department of the Interior

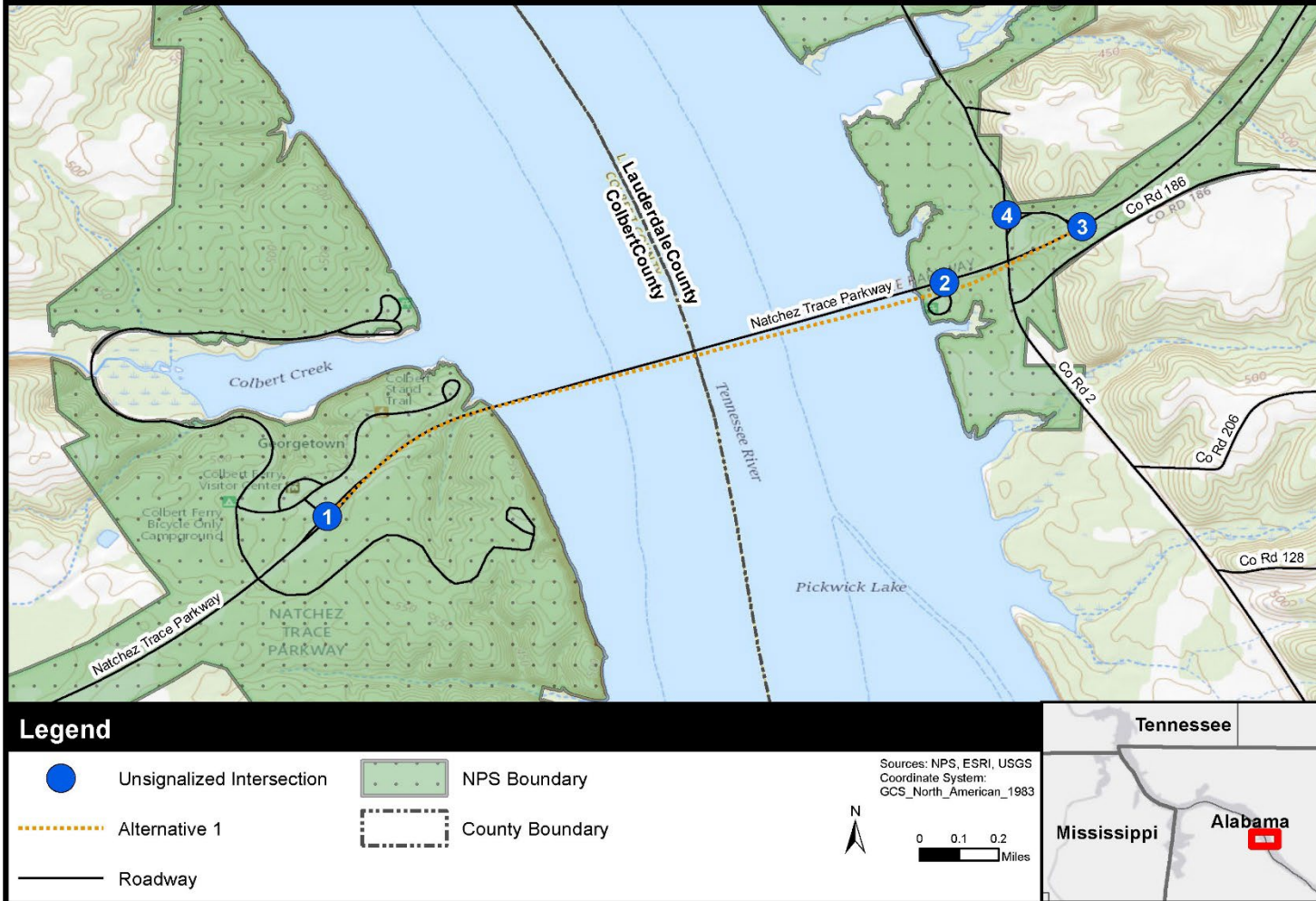


FIGURE ES-1. HORIZONTAL ALIGNMENT FOR ALTERNATIVE 1

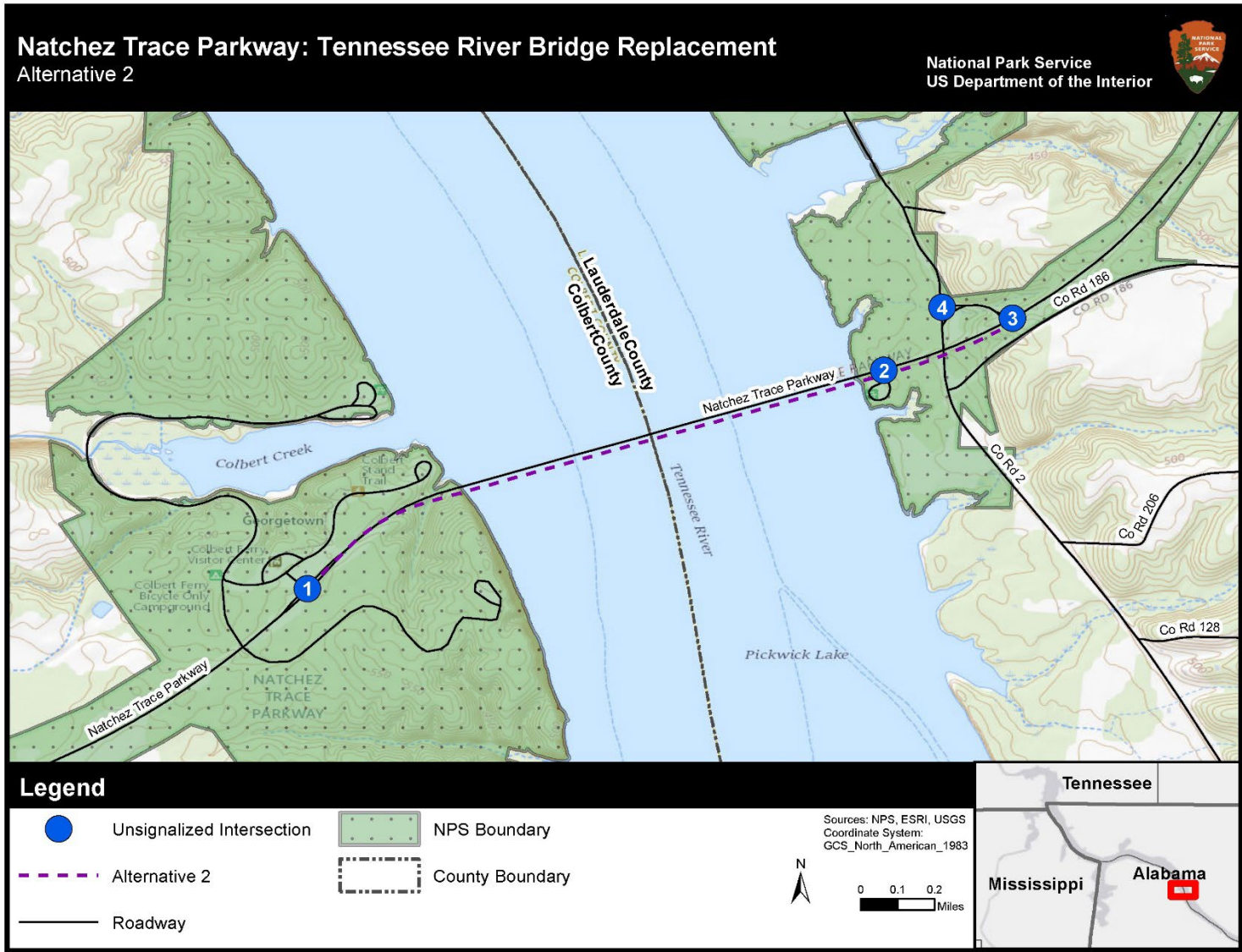


FIGURE ES-2. HORIZONTAL ALIGNMENT FOR ALTERNATIVE 2

TRANSPORTATION NETWORK

Traffic Study Area Description

The traffic study area for this traffic study includes four existing intersections, which are all unsignalized. The intersections evaluated in the existing conditions analysis are shown with their numeric designations in Figures ES-1 and ES-2 and include the following:

- (1) Natchez Trace Parkway/Colbert Ferry Road
- (2) Natchez Trace Parkway/Parking Area (Water Route Overlook)
- (3) Natchez Trace Parkway/Lauderdale County Route 2 Ramp
- (4) Lauderdale County Route 2/Lauderdale County Route 2 Ramp

Data Collection

At each of the four intersections in the traffic study area, the project team collected intersection turning movement counts (TMC) for days representing the non-peak season on Tuesday, August 23, 2022; Saturday, August 27, 2022; and Sunday, August 28, 2022. The project team also collected TMCs for the four traffic study intersections for days representing the peak season on Wednesday, November 9, 2022; Saturday, November 12, 2022; and Sunday, November 13, 2022. Four periods of daily intersection peak hours of traffic were determined: the weekday peaks of (1) the 2022 non-peak season, and (2) the 2022 peak season; and the weekend peaks (the greater volume of either Saturday or Sunday) of (3) the 2022 non-peak season and (4) the 2022 peak season.

Counts of roadway volume, vehicle classification, and speed (VSC) data using automatic traffic recorders were collected by the transportation team over continuous 7-day periods from Monday, August 22, 2022, to Sunday, August 28, 2022 (representing the non-peak season), and from Wednesday, November 9, 2022, to Tuesday, November 15, 2022 (representing the peak season).

In addition to VSC data collected, the project team reviewed roadway volume data obtained from the Alabama Department of Transportation and monthly traffic volumes from a NPS counter on Natchez Trace Parkway near Cherokee, Alabama.

ANALYSIS METHODOLOGY

The analysis of the traffic study area intersections was prepared using Synchro™ Traffic Signal Coordination Software Version 11.1 (Build 2, Revision 9). Two analyses were performed for traffic using the Synchro™ software tool, including an intersection capacity analysis and an intersection queuing analysis.

Intersection Operations

Level of service (LOS) is the primary measure of traffic operations for intersections. LOS is a standard performance measure developed by the transportation profession to quantify driver perception for such elements as travel time, number of stops, total amount of stopped delay, and impediments caused by other vehicles. The LOS for unsignalized intersections (stop-controlled intersections) is based on the Highway Capacity Manual (HCM) 6th Edition.

Queuing Analysis

In addition to analyzing the vehicle delay, the vehicle queue lengths were calculated for the approaches of each study intersection. The study used Synchro™ to calculate the 95th percentile queue lengths for the four existing study intersections.

EXISTING CONDITIONS

Based on the Synchro™ analysis, all study intersections are operating at acceptable conditions (LOS A) and provide enough storage for the calculated 95th percentile queuing lengths.

DEVELOPMENT OF THE NO BUILD

The No Build evaluates future year 2045 traffic operation conditions on the existing bridge. That is, the No Build assumes the current Tennessee River Bridge location, alignment, and design and assumes no changes to the adjacent roadway system intersections, geometry, or traffic control to provide a direct comparison to future conditions that include replacement of the bridge. The No Build traffic study area matches existing conditions intersections, geometry, and traffic control. Traffic forecast volume estimates account for background growth traffic to evaluate 2045 traffic operations.

Based on review of area transportation plans and funded projects in the traffic study area, no major roadway or intersections improvements were identified or incorporated for traffic study area intersections in 2045.

ANALYSIS OF THE ALTERNATIVES

The traffic study area for alternatives 1 and 2 include the same four unsignalized intersections and geometry as the existing conditions analysis and No Build analysis. The Tennessee River Bridge project is not expected to modify the traffic forecast volume estimates, lane use or traffic controls of the study intersections.

Intersection Operations

The study used Synchro™ to calculate the vehicle delay, volume-to-capacity (v/c) ratio and LOS operation for the intersections under the No Build and the future 2045 conditions for alternatives 1 and 2. A summary of the worst intersection approach LOS grades for the two-way stop-controlled study intersections under the No Build, Alternative 1 and Alternative 2 is depicted in Table ES-1.

Queue Analysis

The study used Synchro™ to calculate 95th percentile queue lengths for the intersections under future 2045 conditions. The queue analysis was performed for the No Build and alternatives 1 and 2.

TABLE ES-1. NO BUILD AND ALTERNATIVES 1 AND 2 OPERATIONS ANALYSES SUMMARY

Intersection Number, Name, and Approach	Direction	Lane Group	2045 No Build			2045 Alternative 1			2045 Alternative 2		
			Non-Peak / Peak Season Weekday / Saturday Peak			Non-Peak / Peak Season Weekday / Saturday Peak			Non-Peak / Peak Season Weekday / Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC)^a											
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	7.4	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	9.2	0.01	A	9.2	0.01	A	9.2	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)											
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.3	0.00	A	9.3	0.00	A	9.3	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.5	0.00	A	7.5	0.00	A	7.5	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)											
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	9.0	0.04	A	9.0	0.04	A	9.0	0.04
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)											
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.8	0.04	A	8.8	0.04	A	8.8	0.04
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.2	0.00	A	7.2	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

MITIGATION MEASURES

The purpose of mitigation measures is to improve intersection operations from unacceptable operations (level of service [LOS] E or F) through a variety of strategies such as modifying the intersection roadway geometry. Based on the analysis of intersection operations and queuing for the No Build and alternatives 1 and 2, all four study intersections would operate with acceptable (LOS A) operations, and the available lane group storage capacity would accommodate the calculated 95th percentile queue lengths. No mitigation measures would be required for future conditions of traffic.

No Build

Because the four study intersections would operate with acceptable operations and the available lane group storage capacity would accommodate the calculated 95th percentile queue lengths, no mitigation measures would be required for the No Build.

Alternatives 1 and 2

Because the four study intersections would operate with acceptable operations and the available lane group storage capacity would accommodate the calculated 95th percentile queue lengths, no mitigation measures would be required for alternatives 1 and 2.

Conclusions

This traffic study evaluates traffic operations for existing conditions, the No Build, and two alignment alternatives for the Tennessee River Bridge project (alternatives 1 and 2) in the vicinity of Colbert Ferry, inclusive of Natchez Trace Parkway from Colbert Ferry Road to Lauderdale County Route 2 and adjacent intersecting roadways. Based on projected traffic forecasts and the capacity analysis, key traffic operation findings of this traffic study are provided below.

Existing Conditions

Under existing conditions, the four existing study intersections operate with LOS A, which is considered an acceptable LOS grade for traffic operations. The available storage capacity of the lane groups at the four study intersections are adequately accommodating 95th percentile queues lengths under existing conditions.

No Build

Under the No Build, the four study intersections would continue to operate with LOS A, which is considered an acceptable LOS grade for traffic operations. The available storage capacity of the lane groups at the four study intersections would continue to adequately accommodate 95th percentile queues lengths.

Alternative 1

Under Alternative 1, the four study intersections would continue to operate with LOS A. The available storage capacity of the lane groups at the four study intersections would continue to adequately accommodate 95th percentile queues lengths.

Alternative 2

Under Alternative 2, the four study intersections would continue to operate with LOS A. The available storage capacity of the lane groups at the four study intersections would continue to adequately accommodate 95th percentile queues lengths.

1.0 INTRODUCTION

The NPS is proposing to reconstruct to the John Coffee Memorial Bridge (Tennessee River Bridge) along the Natchez Trace Parkway, spanning the Tennessee River between Colbert County, Alabama, and Lauderdale County, Alabama. The current bridge opened to traffic in 1964, providing a connection for Natchez Trace Parkway visitors and local traffic in northwest Alabama between Tennessee and Mississippi, while preserving the movement of barge traffic in the Tennessee River. The Tennessee River has exceeded its design life, and the NPS is proposing replacing the bridge as a strategy to maintain safe movement on the Natchez Trace Parkway across the Tennessee River. This traffic study evaluates traffic operations based on two alternatives for bridge replacement.

This traffic study analyzes vehicular traffic under existing conditions, future conditions without a bridge replacement (representing the No Build condition), and future conditions comparing two bridge alignment alternatives (alternatives 1 and 2, representing the alternatives condition). Existing conditions refer to weekday and weekend traffic operations during the non-peak and peak seasons of the Natchez Trace Parkway in August 2022 and November 2022, respectively. No Build and alternatives' conditions evaluate future projected traffic operations during the non-peak and peak seasons in future year 2045. This traffic study will be used to inform the elements and alternatives under consideration, identify projected traffic operational issues, and provide information to support the planning process.

2.0 BACKGROUND

The existing Tennessee River Bridge is a nearly 1-mile long girder bridge that spans the Tennessee River and is supported by two concrete abutments on either end of the river and 36 concrete piers that support the bridge's 38 spans. The bridge is 34 feet wide, including two 11-foot travel lanes (one for each direction of traffic); 3-foot paved shoulders in each direction; and 3-foot curbs, gutters, and barriers on either side of the bridge. The Tennessee River Bridge is part of the Natchez Trace Parkway, which provides recreational, scenic, and cultural and historical learning opportunities to the millions of travelers of the Natchez Trace Parkway each year. Natchez Trace Parkway extends 444 miles between Natchez, Mississippi, and Pasquo, Tennessee. In the vicinity of the Tennessee River Bridge, the nearest US and state routes are US Route 72, approximately 7 miles southwest of the bridge, and Alabama State Route 20, approximately 8 miles northeast of the bridge. Near the Tennessee River Bridge, the Natchez Trace Parkway also connects with several local county roads, providing access to the Natchez Trace Parkway and improving connectivity between local communities. The presence of the bridge enables direct travel between northeast Mississippi and south-central Tennessee, further facilitating movement between local communities. The nearest crossings over the Tennessee River apart from the Tennessee River Bridge are otherwise via US Route 72 between Muscle Shoals, Alabama, and Florence, Alabama (15 miles to the southeast), or via Tennessee State Route 128 along Pickwick Dam (24 miles to the northwest).

The bridge has not undergone major rehabilitation since it opened in 1964 and has exceeded its 50-year design life. Several alignment alternatives for bridge replacement are under consideration that would address the long-term structural concerns of the existing bridge. Two alternatives, alternatives 1 and 2, are specifically analyzed in this traffic study. Each of these alternatives would preserve the movement of traffic along Natchez Trace Parkway over the Tennessee River. The positioning of the alternative alignments would be similar to the existing bridge alignment and maintain one lane of travel each for the eastbound and westbound approach. As a result, vehicular capacity and travel patterns with the new bridge are expected to be similar to current conditions. However, replacing the bridge with either of the alternatives would restore the roadway facility to a state of good repair by addressing the structural deficiencies of the current bridge. The following sections of this traffic study demonstrate the process for and results of the analysis of the two alignment alternatives with a specific focus on vehicular operations.

3.0 ALTERNATIVES

3.1 ALTERNATIVE ELEMENTS

The preliminary conceptual design for the Tennessee River Bridge includes:

- Bridge length of 5,050 feet (approximately 0.95 miles) replacing the existing Tennessee River Bridge and providing a crossing over the Tennessee River between Colbert County, Alabama, and Lauderdale County, Alabama.
- Bridge width of 43 feet, which includes a two-lane roadway and shoulders having a combined width of 34 feet; a 5-foot walkway; and curbs, gutters, and barriers.
- Reconstructing a portion of the Natchez Trace Parkway to tie back into the existing alignment approximately between Colbert Ferry Road and Lauderdale County Route 2.

3.2 ALTERNATIVES DESCRIPTIONS

The alternatives analyzed in this traffic study include:

- Alternative 1 – Replaces the Tennessee River Bridge, with the abutment on the west end of the bridge in the approximate position of the bridge's existing western abutment, angles the bridge spans southeastward heading in the eastbound direction with respect to the existing bridge alignment, and locates the abutment on the east end of the bridge to the south of the bridge's existing eastern abutment.
- Alternative 2 – Replaces the Tennessee River Bridge, locating the abutments on both ends of the bridge and the bridge spans to the south of the existing bridge alignment.

Figure 3-1 presents the horizontal alignment of Alternative 1 in the context of nearby intersections within the traffic study area, and Figure 3-2 presents the horizontal alignment of Alternative 2. Appendix A includes preliminary conceptual design plans for Alternative 1 and 2.

Natchez Trace Parkway: Tennessee River Bridge Replacement Alternative 1

National Park Service
US Department of the Interior

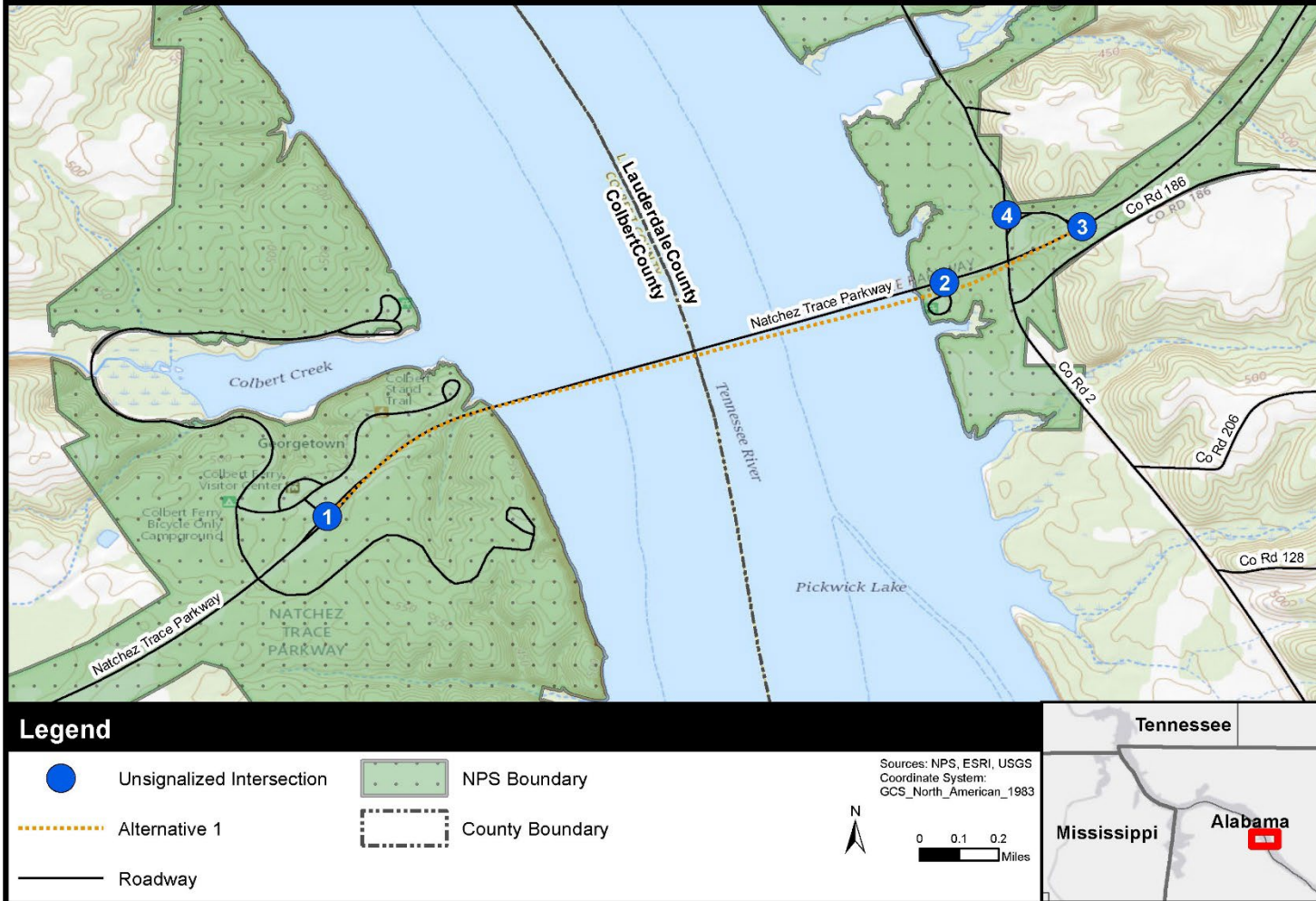


FIGURE 3-1. HORIZONTAL ALIGNMENT FOR ALTERNATIVE 1

Natchez Trace Parkway: Tennessee River Bridge Replacement Alternative 2

National Park Service
US Department of the Interior

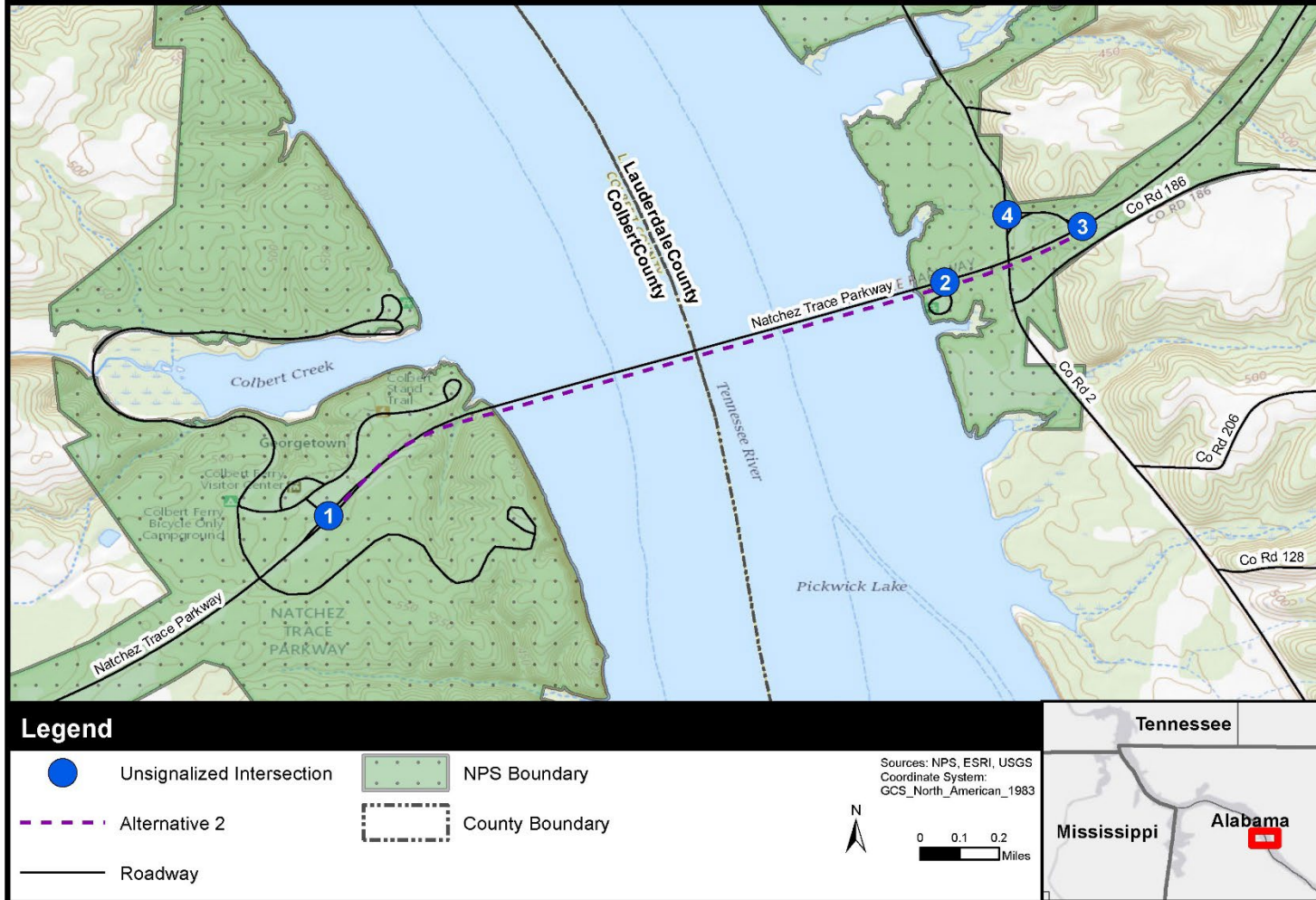


FIGURE 3-2. HORIZONTAL ALIGNMENT FOR ALTERNATIVE 2

4.0 PLANNING CONTEXT

4.1 NORTHWEST ALABAMA COUNCIL OF LOCAL GOVERNMENTS

The Northwest Alabama Council of Local Governments (NACOLG) oversees the regional priorities of five counties in northwest Alabama, including the two counties (Colbert and Lauderdale) that encompass the study area. The NACOLG's Department of Planning and Transportation administers and manages the Shoals Area Metropolitan Planning Organization and the Northwest Alabama Rural Planning Organization (RPO). The purpose of the RPO is to engage local officials of the non-urbanized areas within northwest Alabama in the development and implementation of Alabama's statewide transportation plans and associated projects. Functions of the RPO include engaging local populations through meetings to discuss transportation issues, documenting safety and maintenance issues throughout northwest Alabama and then disseminating the findings to the Alabama Department of Transportation (ALDOT), and organizing meetings between local officials and ALDOT or other stakeholders.

The NACOLG's RPO Work Program for Fiscal Year 2023 describes work to be performed for the non-urbanized areas of the five counties in northwest Alabama that the RPO oversees (NACOLG 2022). The Work Program is used to organize the Rural Transportation Planning Cooperative Process, which conducts a rural transportation planning process in the form of a one-year program. As part of the Work Program, the RPO prepares, documents, and distributes key information, including traffic counts, functional highway classification maps, project location maps, and a project information database for northwest Alabama. As part of the Work Program, the RPO also documents transportation needs in response to natural disasters and coordinates with local, state, and federal governments on the activities of the Americans with Disabilities Act Transition Plan. The Work Program does not provide specific tasks related to the Natchez Trace Parkway study area.

4.2 NATIONAL PARK SERVICE/PARKWAY-SPECIFIC PLANS

Southeast Region Draft Long-Range Transportation Plan (September 2016)

The Southeast Region of the NPS includes parks across nine states, Puerto Rico, and the US Virgin Islands. Natchez Trace Parkway is included among the parks that the Southeast Region encompasses. The *Southeast Region Long-Range Transportation Plan* (NPS 2016) helps this region better understand existing and forecasted needs and provides a framework for making more effective transportation decisions and strategic program investments. The plan establishes regional goals, objectives, and performance measures; defines existing conditions and transportation needs; identifies safety, congestion, and capital improvement needs; and identifies sustainable strategies that protect resources while maintaining quality visitor experiences. The next steps for the *Southeast Region Long-Range Transportation Plan* entail implementation, monitoring and performance tracking, and updates.

5.0 PARKWAY VISITATION

Information about visitation to Natchez Trace Parkway is available from the NPS Visitor Use Statistics and includes summaries of total recreational visitors to the Natchez Trace Parkway and monthly traffic counts at various locations along the Natchez Trace Parkway (NPS 2023a). These data were used in this traffic study to develop an understanding of trends in visitations and their potential impact on future traffic.

Based on reported recreational visitor data, in 2022, there were 6.5 million recreation visits to the entire Natchez Trace Parkway, the most since 1989 and a continuation of a long-term increase since a low of 5.3 million recreation visits in 1994. Based on reported traffic counts of the Natchez Trace Parkway by the NPS, nearly 300,000 vehicles traveled Natchez Trace Parkway near Cherokee, Alabama, in 2022, with annual traffic counts at this same location varying modestly year-to-year since 1991.

6.0 TRANSPORTATION NETWORK

6.1 TRAFFIC STUDY AREA DESCRIPTION

The traffic study area for this traffic study includes four existing intersections, which are all unsignalized. The intersections evaluated in the existing conditions analysis are shown with their numeric designations in Figure 6-1 and include the following:

- (1) Natchez Trace Parkway/Colbert Ferry Road
- (2) Natchez Trace Parkway/Parking Area (Water Route Overlook)
- (3) Natchez Trace Parkway/Lauderdale County Route 2 Ramp
- (4) Lauderdale County Route 2/Lauderdale County Route 2 Ramp

6.2 ROADWAY DESCRIPTION

The following section describes the roadways in the traffic study area and provides the ALDOT roadway functional classification, the number of lanes in each direction, and any noteworthy characteristics such as the roadway's role within the transportation network. The information was collected from the ALDOT Highway Functional Classification Maps for Colbert and Lauderdale Counties (ALDOT 2021, 2008), and aerial imagery.

- **Natchez Trace Parkway** extends southwest to northeast for 444 miles from Natchez, Mississippi, to Pasquo, Tennessee, approximately following the historic corridor known as “Old Natchez Trace.” Natchez Trace Parkway provides recreational, scenic, and cultural and historical learning opportunities to its users, and as noted above, visitation data from the NPS indicates there were 6.5 million recreation visits to the entire Natchez Trace Parkway in 2022 (NPS 2023a). ALDOT defines this roadway as a minor arterial within Colbert and Lauderdale Counties, which contain the Natchez Trace Parkway's entire passage through Alabama. On Natchez Trace Parkway immediately east of the Tennessee River, from 2013 to 2021, ALDOT reports an annual average daily traffic (AADT) of 1,440 in 2013; a peak AADT of 1,598 in 2018; and an AADT of 1,048 in 2021 during the COVID-19 pandemic (ALDOT 2023a). Within the traffic study area, Natchez Trace Parkway is generally a two-lane, undivided roadway that is 22 feet wide with 11-foot lanes. At-grade sections of Natchez Trace Parkway do not have paved shoulders and instead have grass shoulders on either side. The Tennessee River Bridge section of Natchez Trace Parkway is approximately 34 feet wide, which includes 11-foot travel lanes, 3-foot paved shoulders, and 3-foot curbs, gutters, and barriers on either side of the bridge. Natchez Trace Parkway has a statutory speed limit of 50 miles per hour.
- **Lauderdale County Route 2** extends from its northern terminus at the intersection with Lauderdale County Route 14 in Gravelly Springs to its southeastern terminus at the intersection with Alabama State Route 20 near Florence. ALDOT does not define this roadway with a specific functional classification within the traffic study area, nor has it reported historic AADTs for this roadway within the traffic study area. Lauderdale County Route 2 is generally a two-lane, undivided roadway that provides on-ramp and off-ramp access to Natchez Trace Parkway. Within the traffic study area, Lauderdale County Route 2 is approximately 20 feet wide and has grass shoulders on either side in lieu of paved shoulders. Lauderdale County Route 2 has a statutory speed limit of 45 miles per hour (Alabama Law Enforcement Agency 2023).

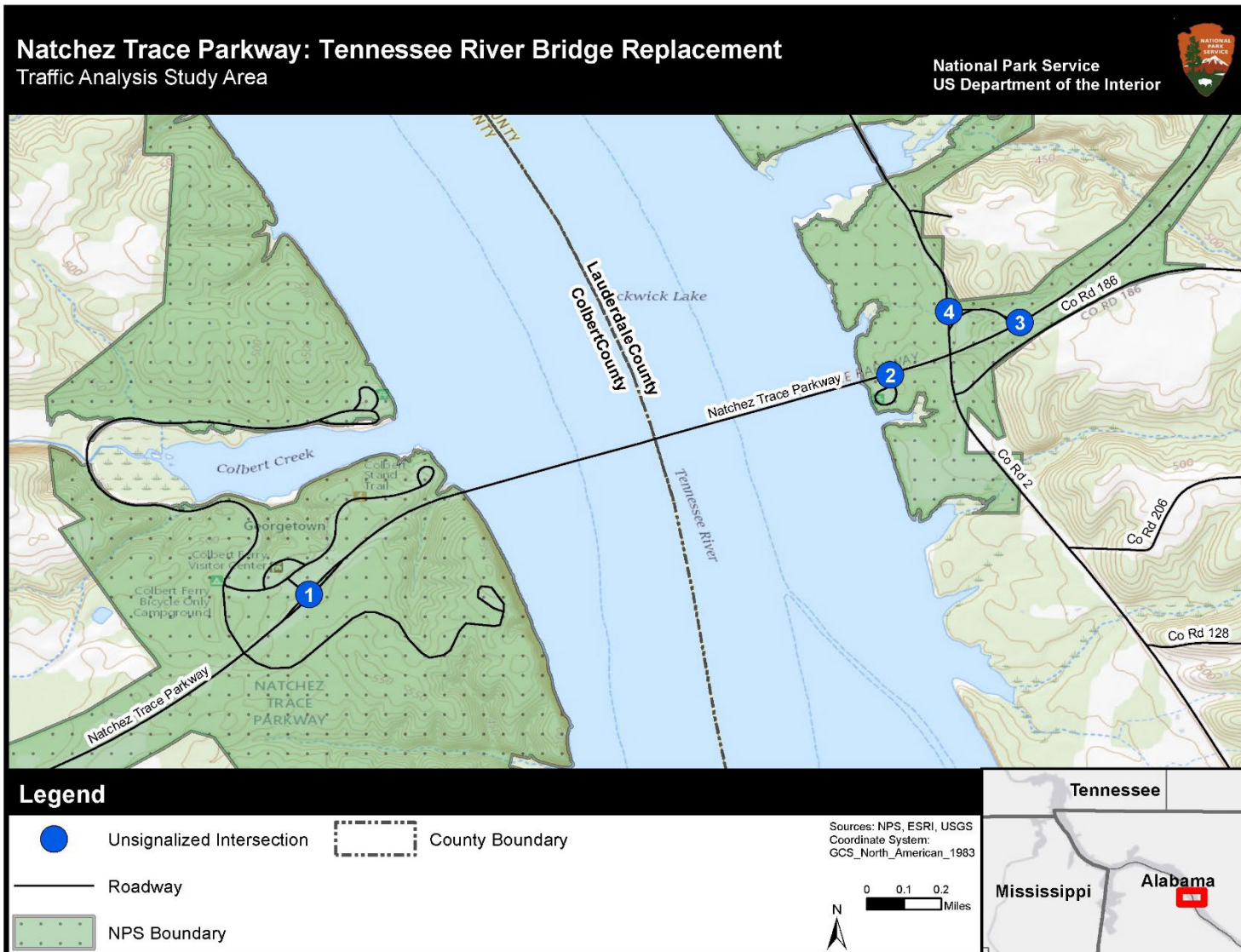


FIGURE 6-1. TRAFFIC ANALYSIS STUDY AREA

6.3 DATA COLLECTION

Four intersection locations along Natchez Trace Parkway and Lauderdale County Route 2 comprise the traffic study area. The traffic study area is depicted in Figure 6-1 and includes the following study intersections:

- Natchez Trace Parkway/Colbert Ferry Road (Intersection #1)
- Natchez Trace Parkway/Parking Area (Water Route Overlook) (Intersection #2)
- Natchez Trace Parkway/Lauderdale County Route 2 Ramp (Intersection #3)
- Lauderdale County Route 2/Lauderdale County Route 2 Ramp (Intersection #4)

At each of the four intersections in the traffic study area, the project team collected intersection turning movement counts (TMC) for days representing the non-peak season on Tuesday, August 23, 2022; Saturday, August 27, 2022; and Sunday, August 28, 2022. The project team also collected TMCs for the four traffic study intersections for days representing the peak season on Wednesday, November 9, 2022; Saturday, November 12, 2022; and Sunday, November 13, 2022.

Four periods of daily intersection peak hours of traffic were determined from the TMCs: the weekday peaks of (1) the 2022 non-peak season and (2) the 2022 peak season, and the weekend peaks (the greater volume of either Saturday or Sunday) of (3) the 2022 non-peak season and (4) the 2022 peak season. The following individual intersection daily peak hours were observed and evaluated for the traffic analysis:

- Weekday 2022 Non-Peak Season, from 4:30 p.m. to 5:30 p.m. for intersections along Natchez Trace Parkway (Intersections #1 through 3) and from 5:00 p.m. to 6:00 p.m. in the case of Lauderdale County Route 2/Lauderdale County Route 2 Ramp (Intersection #4).
- Saturday 2022 Non-Peak Season, from 10:30 a.m. to 11:30 a.m. in the case of Lauderdale County Route 2/Lauderdale County Route 2 Ramp (Intersection #4); from 11:30 a.m. to 12:30 p.m. in the case of Natchez Trace Parkway/Colbert Ferry Road (Intersection #1); and from 2:30 p.m. to 3:30 p.m. in the cases of Natchez Trace Parkway/Parking Area (Water Route Overlook) (Intersection #2) and Natchez Trace Parkway/Lauderdale County Route 2 Ramp (Intersection #3).
- Weekday 2022 Peak Season, from 3:30 p.m. to 4:00 p.m. in the cases of Natchez Trace Parkway/Colbert Ferry Road (Intersection #1), Natchez Trace Parkway/Lauderdale County Route 2 Ramp (Intersection #3), and Lauderdale County Route 2/Lauderdale County Route 2 Ramp (Intersection #4); and from 3:45 p.m. to 4:45 p.m. in the case of Natchez Trace Parkway/Parking Area (Water Route Overlook) (Intersection #2).
- Saturday 2022 Peak Season, from 10:30 a.m. to 11:30 a.m. in the case of Lauderdale County Route 2/Lauderdale County Route 2 Ramp (Intersection #4); from 10:45 a.m. to 11:45 a.m. in the cases of Natchez Trace Parkway/Colbert Ferry Road (Intersection #1) and Natchez Trace Parkway/Parking Area (Water Route Overlook) (Intersection #2); and from 1:00 p.m. to 2:00 p.m. in the case of Natchez Trace Parkway/Lauderdale County Route 2 Ramp (Intersection #3).

Figures 6-2 and 6-3 present the daily peak hour intersection turning movement volumes for each study intersection for 2022 non-peak season and 2022 peak season, respectively. Appendix B presents the TMC data. The existing intersection lane use geometry and traffic controls were determined by studying aerial imagery. Based on this information, the existing lane geometry and traffic control type are shown in Figure 6-4.

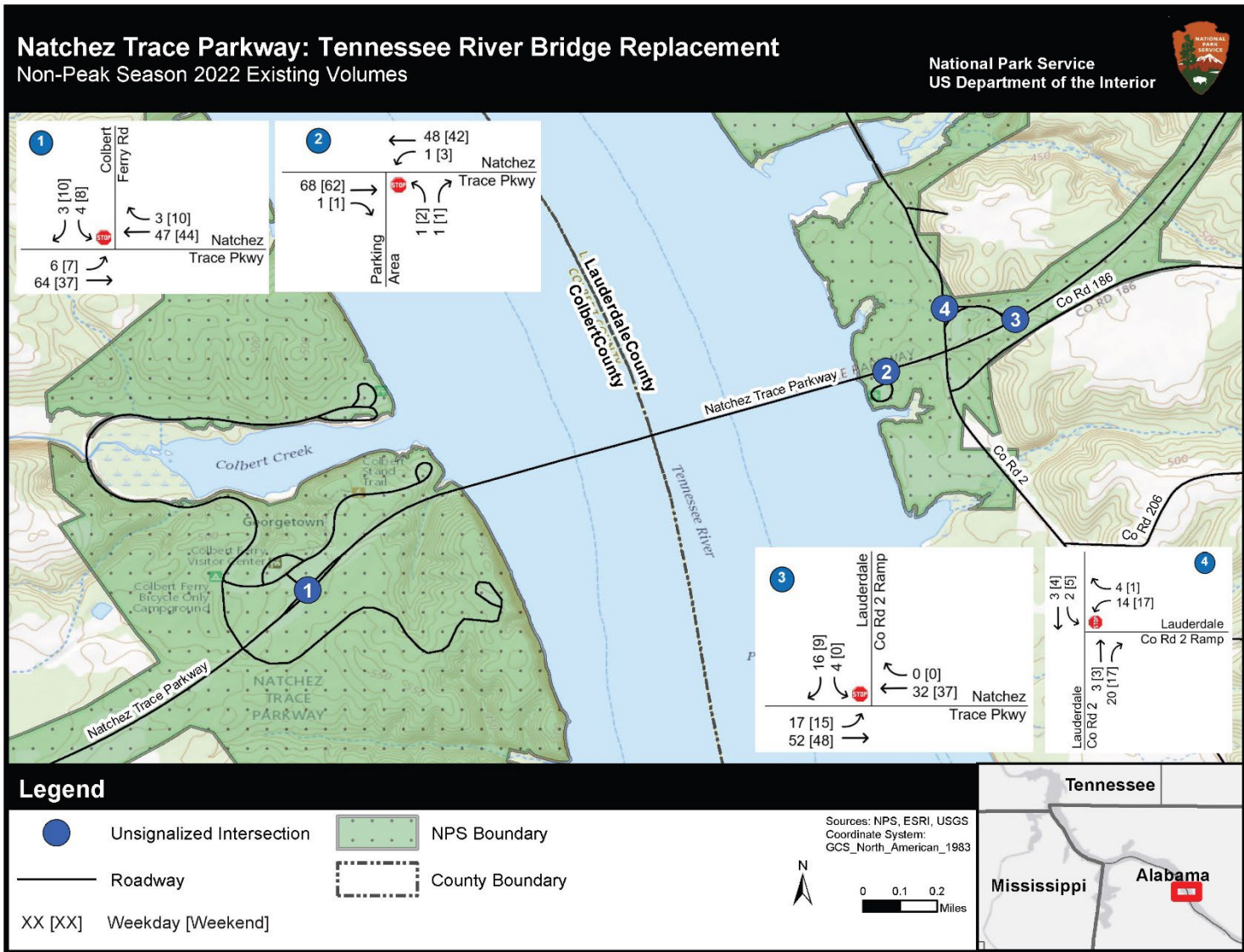


FIGURE 6-2. NON-PEAK SEASON 2022 EXISTING VOLUMES

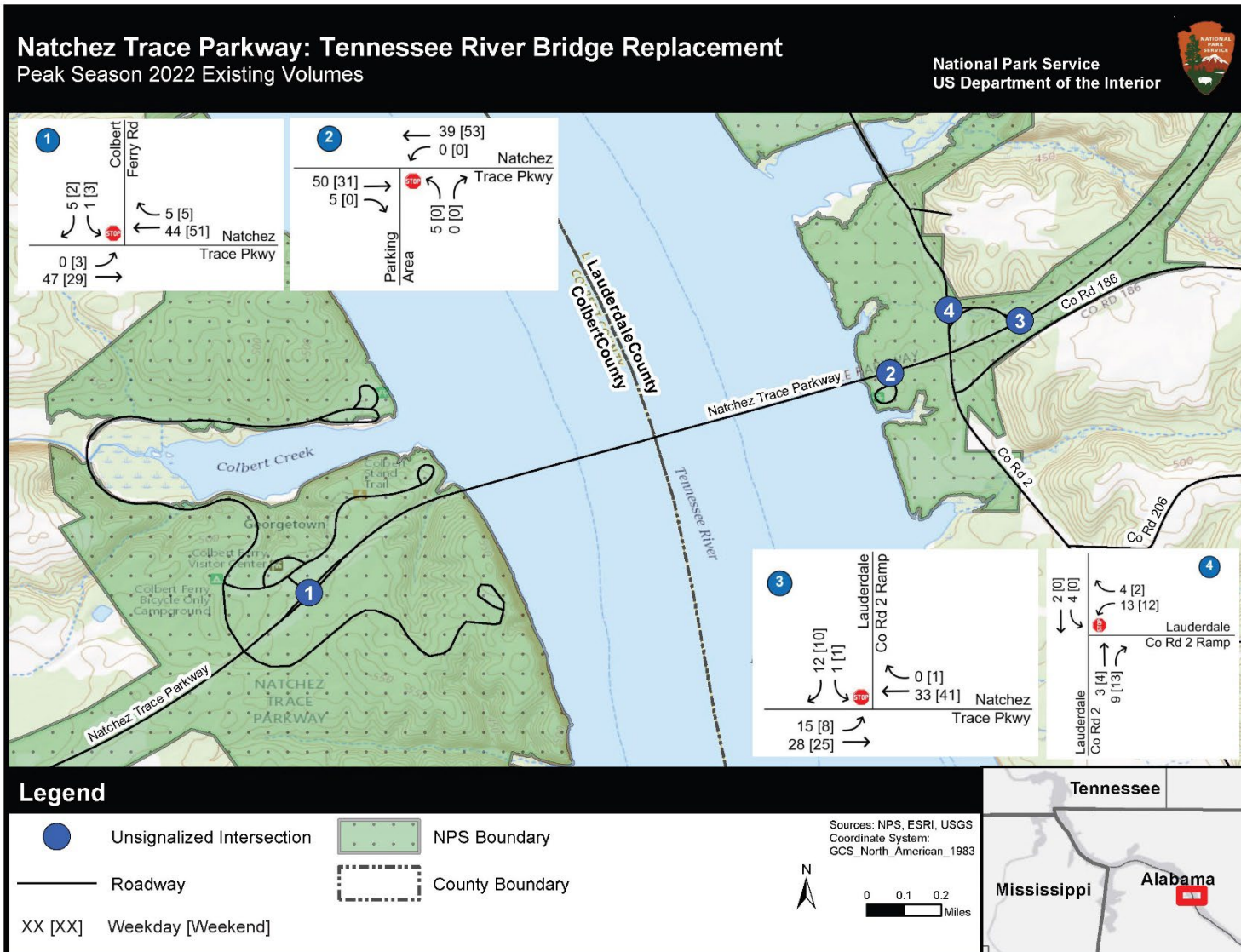


FIGURE 6-3. PEAK SEASON 2022 EXISTING VOLUMES

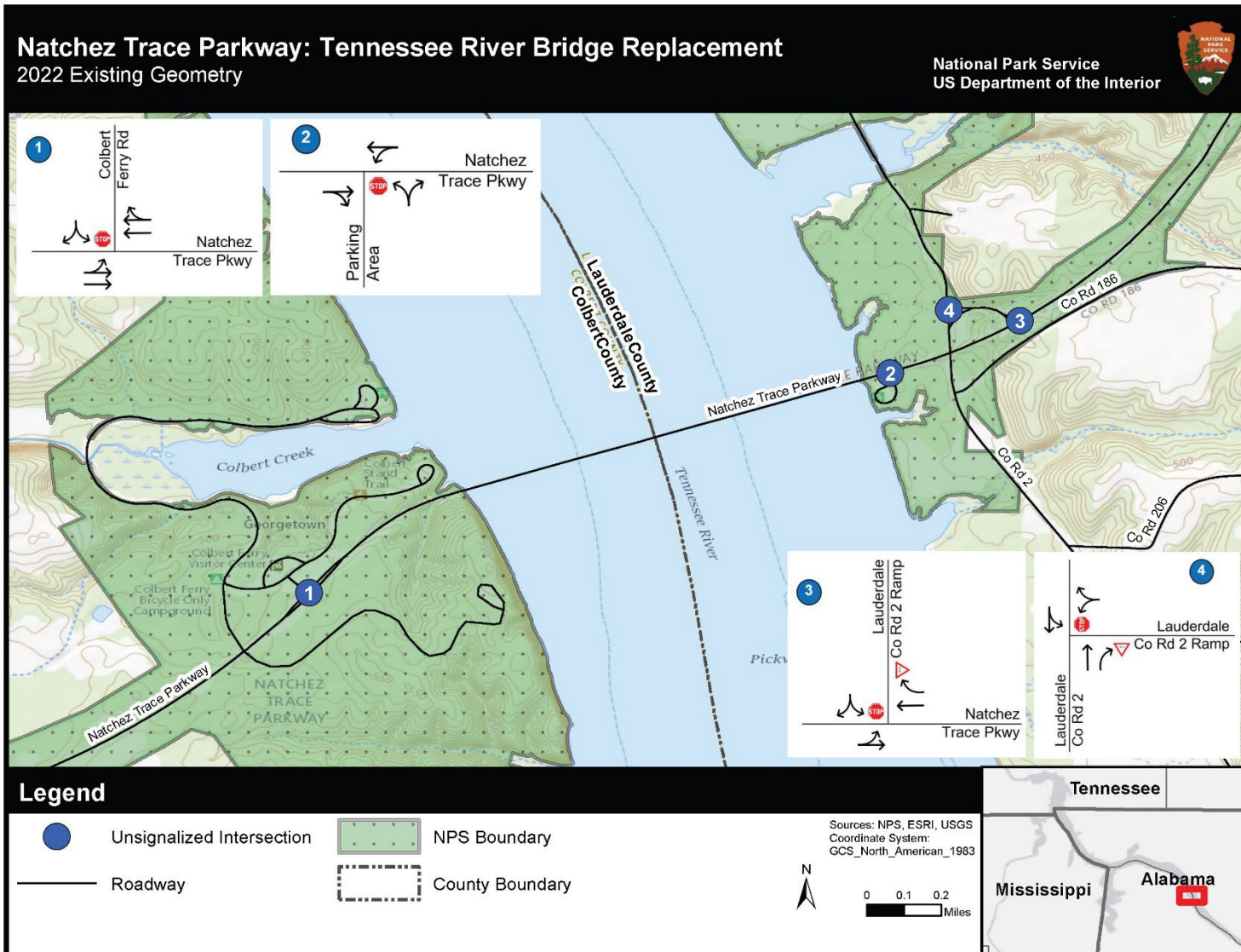


FIGURE 6-4. 2022 EXISTING GEOMETRY

Counts of roadway volume, vehicle classification, and speed (VSC) data using automatic traffic recorders were collected by the transportation team over continuous 7-day periods from Monday, August 22, 2022 to Sunday, August 28, 2022 (representing the non-peak season) and from Wednesday, November 9, 2022 to Tuesday, November 15, 2022 (representing the peak season). Automatic traffic recorder counts were conducted on the Tennessee River Bridge, the Comfort Station Parking Area, the Overlook Road Parking Area, and the Colbert Ferry Boat Ramp. Appendix B presents the roadway VSC data.

In addition to VSC data collected, the project team reviewed roadway volume data obtained from ALDOT and monthly traffic volumes from a NPS counter on Natchez Trace Parkway near Cherokee, Alabama (NPS 2023b). The roadway volume from ALDOT includes AADT and TADT (the latter defined as trucks with three or more axles as a percentage of total daily traffic) collected from portable or virtual counters (ALDOT 2023b). Portable counters include short duration (often for either 48 continuous hours or 168 continuous hours) collections of vehicle classification and volume data on a three-year cycle and virtual counters are based on data that was collected and transferred from an adjacent point of the specified roadway (ALDOT 2020). Figure 6-5 depicts the locations of the intersection TMC and roadway VSC data collection in the traffic study area. Figure 6-6 depicts locations of select ALDOT and NPS counters in the project vicinity. Table 6-1 summarizes AADT of select roadways from 2013 to 2021 based on ALDOT data and Table 6-2 depicts historic volumes from the NPS count location near Cherokee. Appendix C presents NPS counts of historic traffic volumes on Natchez Trace Parkway.

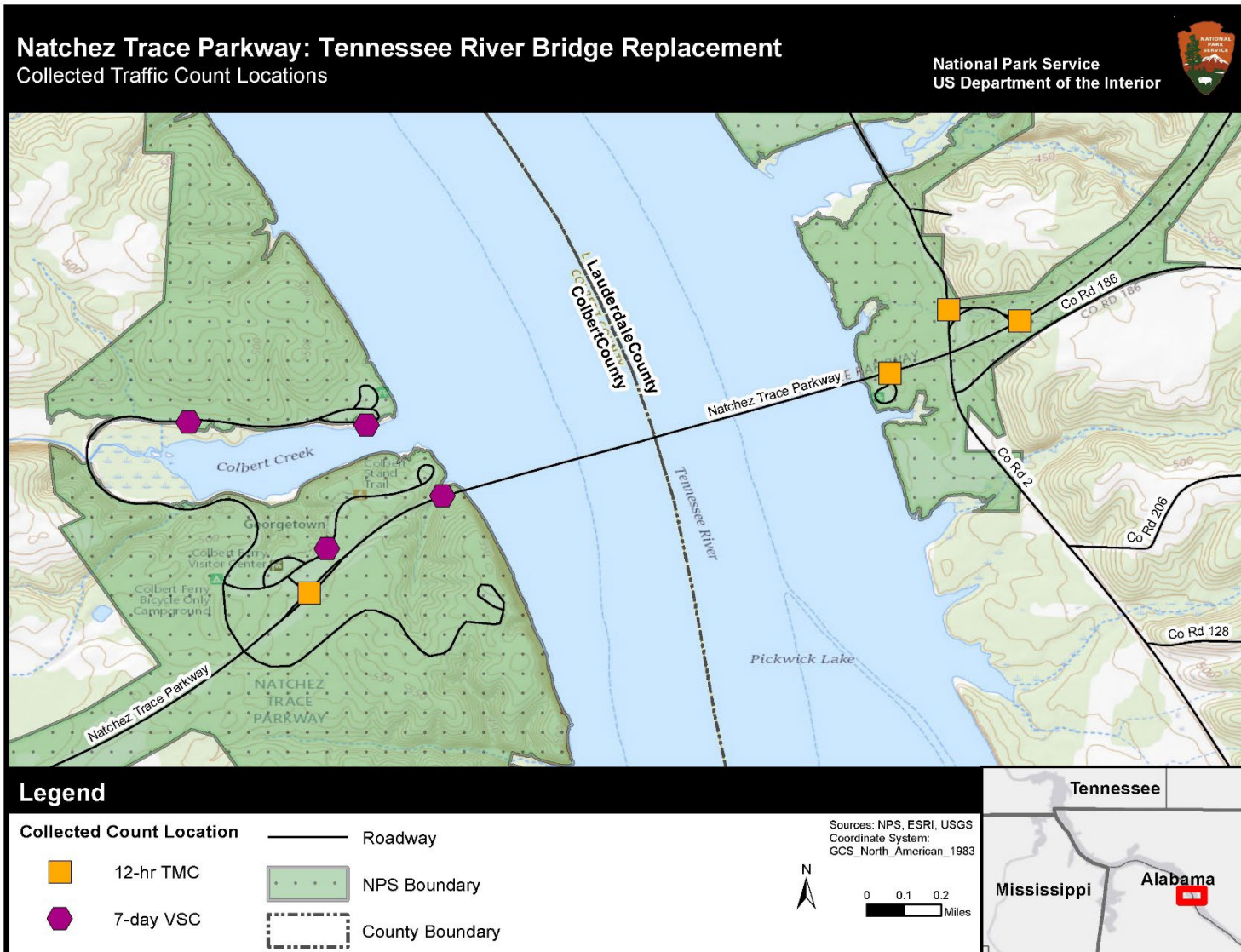


FIGURE 6-5. COLLECTED TRAFFIC COUNT LOCATIONS

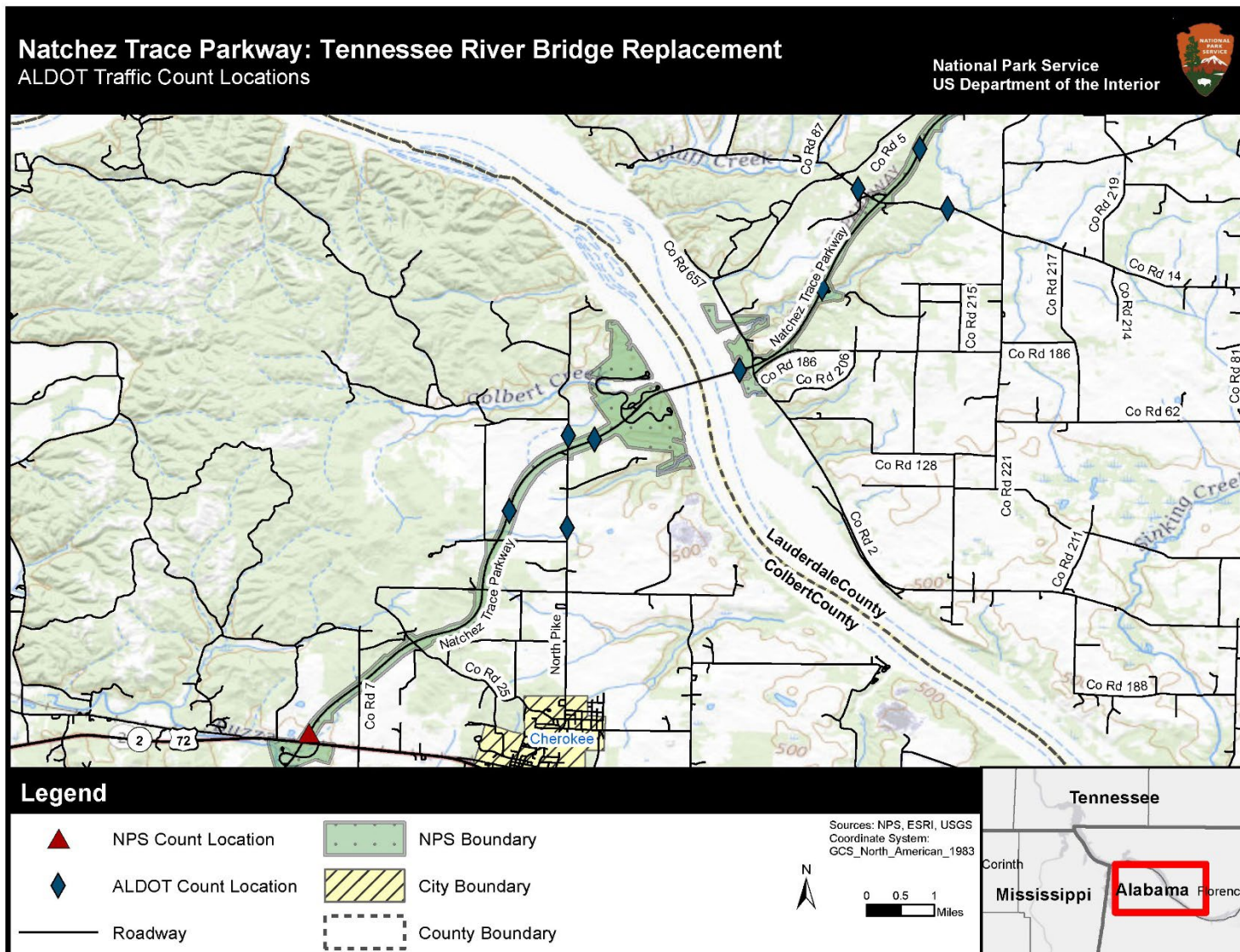


FIGURE 6-6. ALDOT TRAFFIC COUNT LOCATIONS

TABLE 6-1. ALDOT TRAFFIC VOLUMES, 2013–2021

ROUTE	COUNTY and STATION NUMBER	LOCATION	2013	2014	2015	2016	2017	2018	2019	2020	2021
Natchez Trace Parkway	Lauderdale (636)	NORTH of Lauderdale County Route 14	770	780	800	740	740	955	720	688	683
Natchez Trace Parkway	Lauderdale (637)	NORTH of Colbert Creek	940	950	970	980	980	1,242	1,022	948	985
Natchez Trace Parkway	Lauderdale (638)	EAST of Tennessee River	1,440	1,450	1,480	1,410	1,410	1,598	1,451	1,348	1,048
Lauderdale County Route 14	Lauderdale (963)	EAST of Natchez Trace Parkway	1,340	1,350	1,380	1,080	910	951	1,516	1,376	1,420
Lauderdale County Route 14	Lauderdale (906)	WEST of Natchez Trace Parkway	1,340	1,350	1,380	1,040	890	930	1,308	1,153	1,215
Natchez Trace Parkway	Colbert (8)	WEST of Colbert County Route 21	800	720	730	690	690	721	714	653	698
Natchez Trace Parkway	Colbert (502)	EAST of Colbert County Route 21	800	720	730	690	690	721	714	653	698
Colbert County Route 21	Colbert (951)	SOUTH of Natchez Trace Parkway	890	880	900	880	880	900	795	812	881
Colbert County Route 21	Colbert (952)	NORTH of Natchez Trace Parkway	880	210	220	170	170	170	170	200	197

TABLE 6-2. NPS HISTORIC TRAFFIC VOLUMES NEAR CHEROKEE, ALABAMA

YEAR	1991	1992	1993	1994	1995	1996	1997	1998	1999	2000	2001	2002	2003	2004	2005	2006
ANNUAL TOTAL VOLUME	282,420	282,580	281,100	243,822	281,880	282,580	281,880	281,880	281,880	283,280	258,480	279,990	281,100	282,580	281,880	281,880
YEAR	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
ANNUAL TOTAL VOLUME	283,380	282,580	281,880	281,880	281,880	282,580	281,880	281,880	281,880	282,580	281,880	281,880	281,880	282,580	283,480	297,258

Source: NPS (2023a)

7.0 ANALYSIS METHODOLOGY

The analysis of the traffic study area intersections was prepared using Synchro™ Traffic Signal Coordination Software Version 11.1 (Build 2, Revision 9). Two analyses were performed for traffic using the Synchro™ software tool, including an intersection capacity analysis and an intersection queuing analysis. The intersection capacity analysis used the Synchro™ software tool and various input values as described in the following sections to determine a driver's perception of an intersection's operation. The intersection queuing analysis used the Synchro™ tool to determine different levels of queuing or the length that vehicles may back up at an intersection.

7.1 INTERSECTION OPERATIONS

Level of service (LOS) is the primary measure of traffic operations for intersections. LOS is a standard performance measure developed by the transportation profession to quantify driver perception for such elements as travel time, number of stops, total amount of stopped delay, and impediments caused by other vehicles. LOS provides a scale that is intended to match motorists' perception of how a transportation facility operates and to compare different facilities. Detailed LOS descriptions are presented in Figure 7-1.

Unsignalized Intersection Level of Service

The LOS for unsignalized intersections (stop-controlled intersections) is based on the Highway Capacity Manual (HCM) 6th Edition method and requires several inputs to determine an accurate LOS, including vehicular volumes, pedestrian volumes, roadway geometry, speed limits, truck percentages, and peak hour factor.

The average vehicle control delay (seconds per vehicle) is calculated using these parameters with the HCM 6th Edition procedures. The average vehicle control delay in this case represents average delay caused by the presence of a stop sign or roundabout and includes time required to decelerate, stop, and accelerate. Control delay and volume-to-capacity (v/c) ratio are used to characterize LOS for an intersection, lane group, or individual movement.

Table 7-1 shows the average control delay and corresponding LOS for unsignalized intersections. It should be noted that the worst LOS at one-way, STOP-controlled and two-way, STOP-controlled (TWSC) intersections represents the delay for the minor approach only. Using the HCM 6th Edition unsignalized intersection method, a 50-second delay or LOS F constitutes failing operations.

Level of Service



LOS A
Represents the best operating condition, where traffic stream is considered free-flow.



LOS B
Represents reasonably free-flow conditions. The ability to maneuver is only slightly restricted. Effects of minor incidents are still easily absorbed.



LOS C
Represents speeds at or near free-flow conditions. The freedom to maneuver is noticeably restricted. Queues may form.



LOS D
Represents traffic operations approaching unstable flow. Speeds decline slightly with increasing flows. Road density increases more quickly. The freedom to maneuver is more noticeably limited. Minor incidents cause queuing.



LOS E
Represents operation that is near or at capacity. There are no usable gaps in the traffic stream. Operations are extremely volatile. Any disruption causes queuing.



LOS F
Represents a breakdown in flow. Queues form behind breakdown points. The demand is greater than capacity.

Source: TRB (2000)

FIGURE 7-1. LEVEL OF SERVICE DIAGRAM

TABLE 7-1. UNSIGNALIZED INTERSECTION CONTROL DELAY AND LOS THRESHOLDS—HCM 6TH EDITION METHOD

LOS	Average Control Delay (seconds/vehicle)	Description
A	Less than or equal to 10	Stable conditions
B	>10-15	
C	>15-25	
D	>25-35	
E	>35-50	Unstable conditions
F	More than 50	Above capacity and unstable conditions

Source: TRB (2016)

7.2 QUEUING ANALYSIS

In addition to analyzing the vehicle delay, the vehicle queue lengths were calculated for the approaches of each study intersection. The 95th percentile queue length is the worst-case scenario, calculated as the queue that has a 5% probability of being exceeded. A failing queue length is determined by a queue length exceeding the intersection approach storage capacity. Because the available storage for each intersection approach differs, these values reflect whether the existing storage provides enough space for vehicles waiting to pass through the intersection without blocking another lane or another intersection. The study used Synchro™ to calculate the 95th percentile queue lengths for the four existing study intersections.

8.0 EXISTING CONDITIONS

8.1 INTERSECTION OPERATIONS

Synchro™ was used to calculate the vehicle delay and LOS operation based on the HCM 6th Edition method. The ALDOT *Access Management Manual* describes a LOS E or F as “typically considered unacceptable” (ALDOT 2021). This standard is used as a reference when comparing future year 2045 no-action and action alternatives.

Based on the Synchro™ analysis, all study intersections are operating at acceptable conditions (LOS A) according to the LOS definitions in Table 7-1.

The intersection approach LOS grades for the unsignalized, TWSC study intersections are depicted in Figure 8-1 for the non-peak season weekday and Saturday peak hours and Figure 8-2 for the peak season weekday and Saturday peak hours. Table 8-1 shows the results of the LOS capacity analysis, the intersection vehicle delays, and v/c ratios for the existing condition during the peak season and non-peak season weekday and Saturday peak hours. Appendix D presents capacity analysis worksheets.

8.2 INTERSECTION QUEUING

Based on the Synchro™ intersection analysis results, the four study intersections in the traffic study area provide enough storage for the calculated 95th percentile queuing lengths. Table 8-2 contains the queuing results and lane groups.

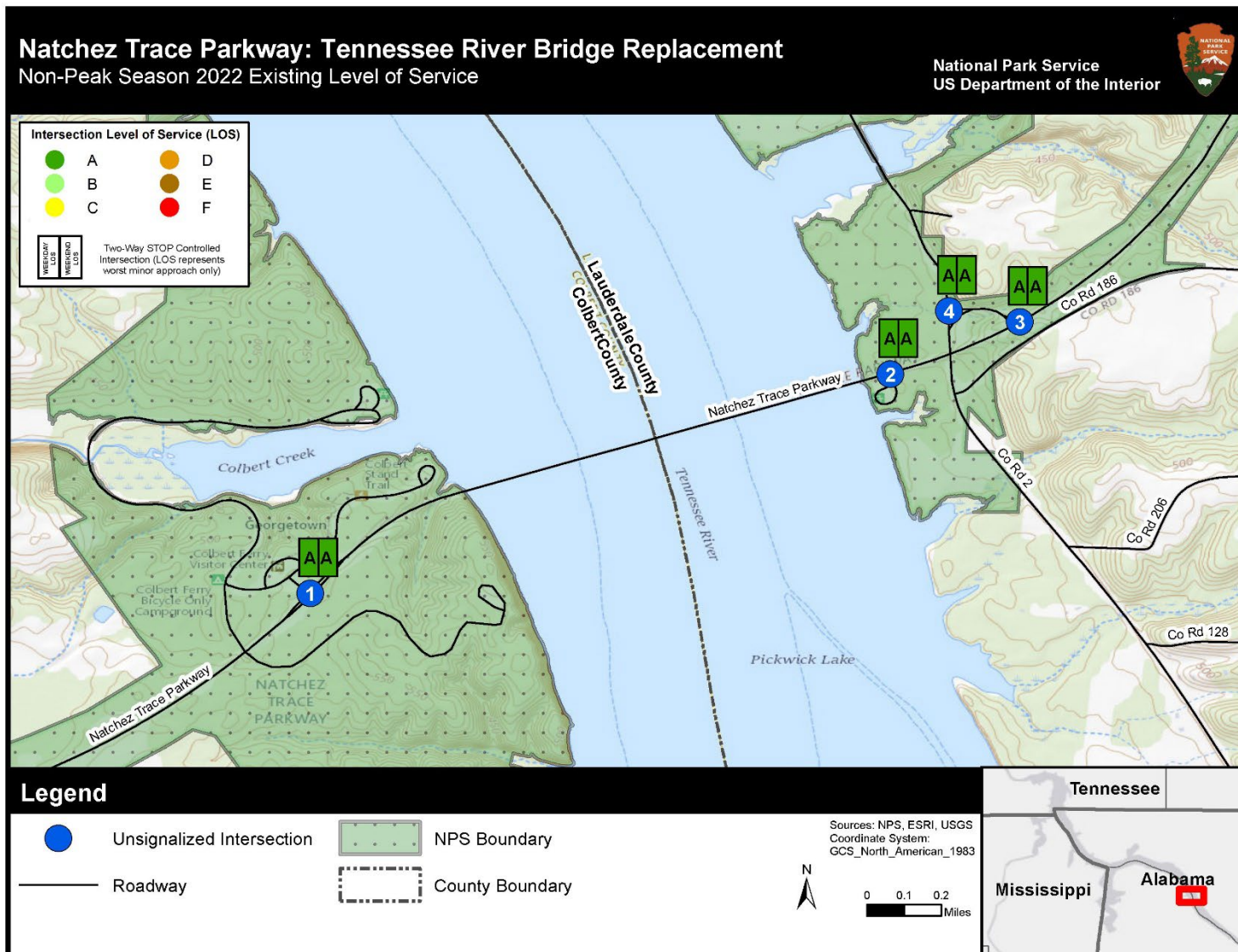


FIGURE 8-1. NON-PEAK SEASON 2022 EXISTING LEVEL OF SERVICE

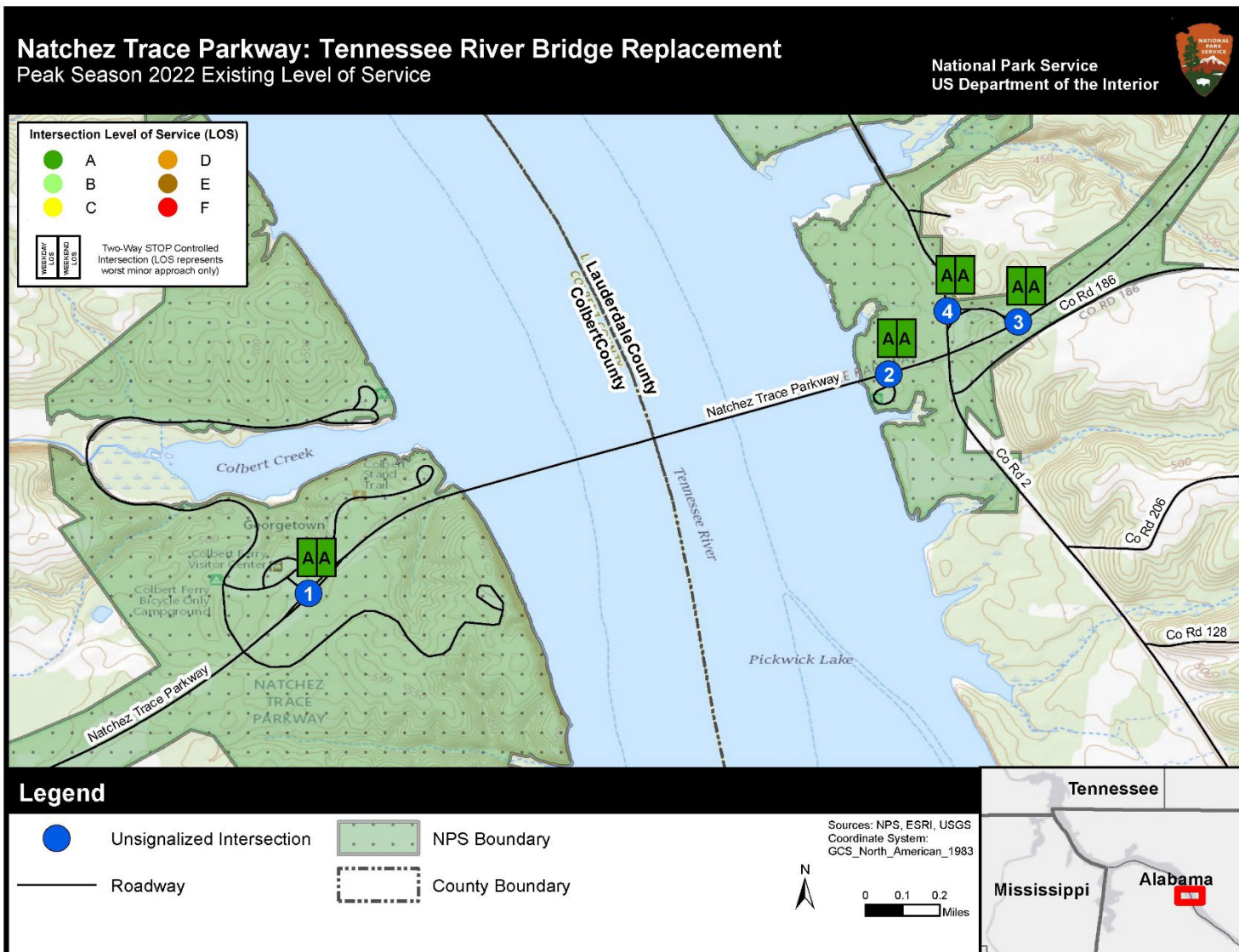


FIGURE 8-2. PEAK SEASON 2022 EXISTING LEVEL OF SERVICE

TABLE 8-1. 2022 EXISTING CONDITION OPERATIONS ANALYSES

Intersection Number, Name, and Approach	Direction	Lane Group	2022 Existing Conditions											
			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.4	0.00
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	8.9	0.01	A	8.8	0.02	A	8.6	0.01	A	9.0	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)														
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.0	0.00	A	9.1	0.01	A	9.1	0.01	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.4	0.00	A	7.4	0.00	A	0.0	0.00	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)														
EB (Natchez Trace Parkway)	EB	L	A	7.3	0.01	A	7.4	0.00	A	7.3	0.00	A	7.4	0.00
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.01	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	8.8	0.03	A	8.6	0.00	A	8.6	0.00	A	8.7	0.00
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)														
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.7	0.03	A	8.8	0.00	A	8.7	0.00	A	8.7	0.00
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.2	0.00	A	7.2	0.00	A	0.0	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

TABLE 8-2. 2022 EXISTING CONDITION QUEUE ANALYSIS

Intersection Number, Name, and Approach	Direction	Lane Group	Turning Bay/Link Length (feet)	2022 Existing Conditions			
				Non-Peak Season Weekday Peak 95th Percentile Queue (feet)	Non-Peak Season Saturday Peak 95th Percentile Queue (feet)	Peak Season Weekday Peak 95th Percentile Queue (feet)	Peak Season Saturday Peak 95th Percentile Queue (feet)
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC)^a							
EB (Natchez Trace Parkway)	EB	LT	>5000	0	0	0	0
SB (Colbert Ferry Road)	SB	LR	270	0	3	0	0
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)							
NB (Parking Area (Water Route Overlook))	NB	LR	1110	0	0	0	0
WB (Natchez Trace Parkway)	WB	LT	1540	0	0	0	0
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)							
EB (Natchez Trace Parkway)	EB	LT	1540	0	0	0	0
SB (Lauderdale County Route 2 Ramp)	SB	LR	510	3	0	0	3
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)							
WB (Lauderdale County Route 2 Ramp)	WB	LR	820	3	3	3	3
SB (Lauderdale County Route 2)	SB	LT	>5000	0	0	0	0

Notes:

EB = eastbound; SB = southbound; NB = northbound; WB = westbound

^a HCM 6th Edition queue values are reported in units of vehicles, but an average vehicle length of 25 feet is assumed to convert the queue value into units of feet.

9.0 DEVELOPMENT OF THE NO BUILD

The No Build evaluates future year 2045 traffic operation conditions on the existing bridge. That is, the No Build assumes the current Tennessee River Bridge location, alignment, and design and assumes no changes to the adjacent roadway system intersections, geometry, or traffic control to provide a direct comparison to future conditions that include replacement of the bridge. The No Build traffic study area matches existing conditions intersections, geometry, and traffic control. Traffic forecast volume estimates account for background growth traffic to evaluate 2045 traffic operations.

9.1 BACKGROUND GROWTH

Background annual growth rates for traffic study area facilities were developed based on a review of ALDOT historical AADT counts for roadway segments in the vicinity of the traffic study area. Proximate to the location of the Tennessee River Bridge project, on Natchez Trace Parkway immediately east of the Tennessee River, ALDOT reported an AADT of 1,440 in 2013, a peak AADT of 1,598 in 2018, and an AADT of 1,048 in 2021 during the COVID-19 pandemic. In coordination with the NPS after a review of these historic counts, a 1.75% compounded annual growth rate is applied to 2022 non-peak and peak season traffic volumes to the future (design) year of 2045. Appendix E presents information about the background growth assumptions.

9.2 ROADWAY IMPROVEMENTS

Based on review of area transportation plans and funded projects in the traffic study area, no major roadway or intersections improvements were identified or incorporated for traffic study area intersections in 2045. Therefore, the existing lane geometry and traffic control type previously shown in Figure 6-4 would continue under the No Build.

9.3 DEVELOPMENT OF NO BUILD TRAFFIC VOLUMES

The No Build analysis assumes the addition of trips to the roadway network from annual growth through 2045. The No Build traffic volumes without the Tennessee River Bridge project are the sum of the existing peak hour traffic volumes plus traffic due to background growth. The non-peak season weekday and Saturday peak hour No Build traffic volumes are presented in Figure 9-1, and the peak season 2045 weekday and Saturday peak hour No Build traffic volumes are presented in Figure 9-2. The non-peak season and peak season No Build weekday and Saturday intersection LOS are provided in Figures 9-3 and 9-4, respectively. A summary of the intersection LOS, delay, and v/c ratio results for the No Build is presented in Table 9-1. Appendix D presents capacity analysis worksheets.

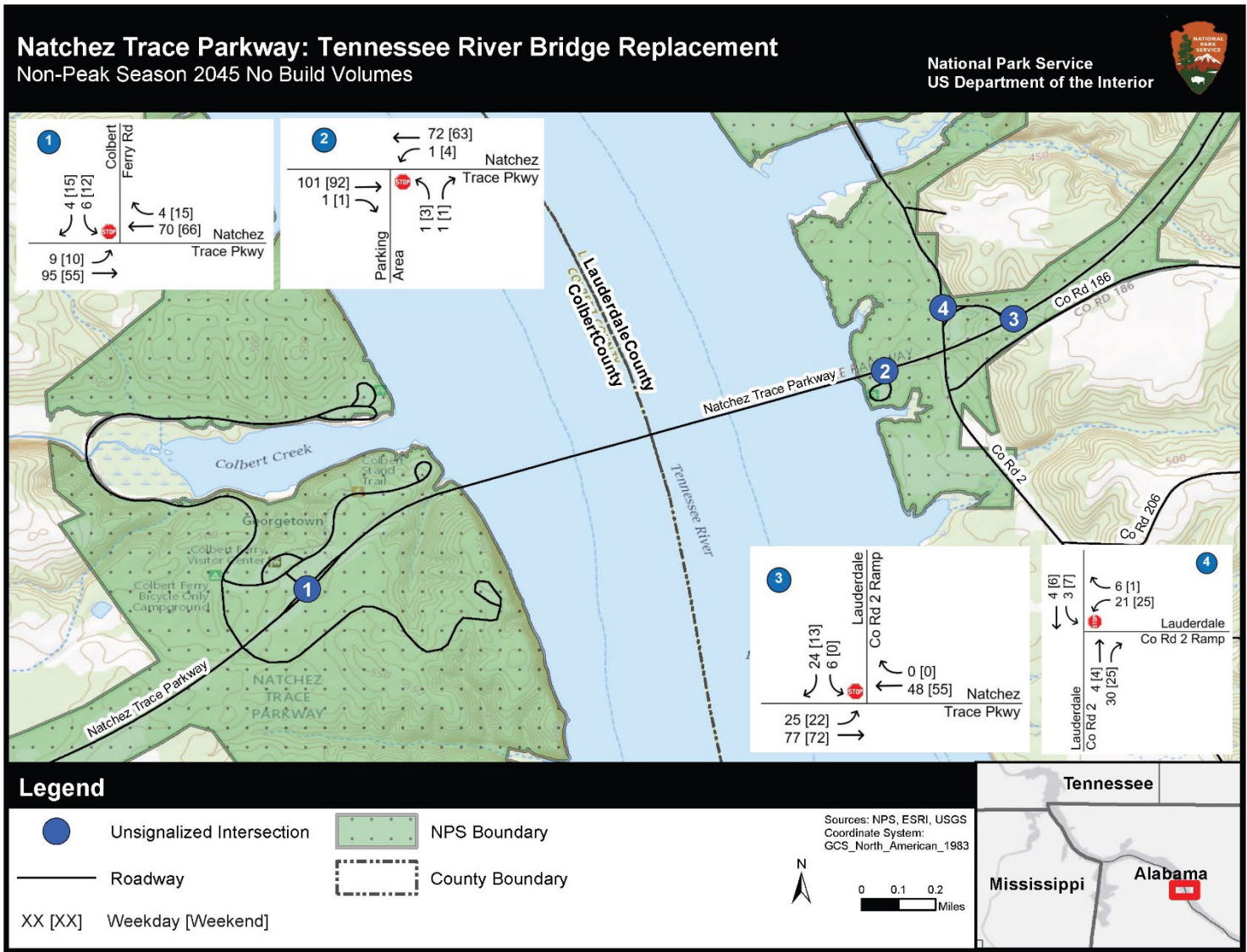


FIGURE 9-1. NON-PEAK SEASON 2045 NO BUILD VOLUMES

Natchez Trace Parkway: Tennessee River Bridge Replacement Peak Season 2045 No Build Volumes

National Park Service
US Department of the Interior

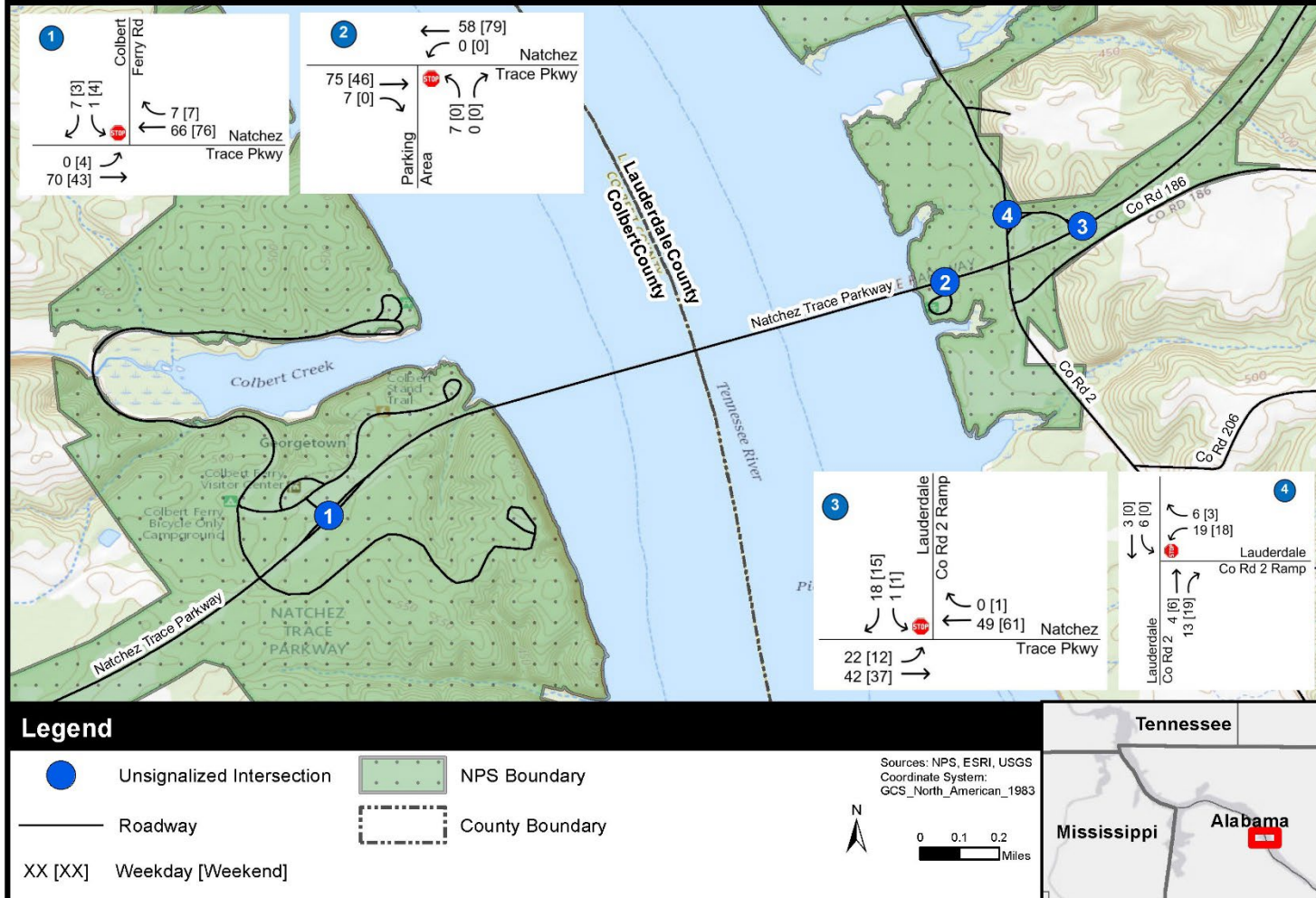


FIGURE 9-2. PEAK SEASON 2045 NO BUILD VOLUMES

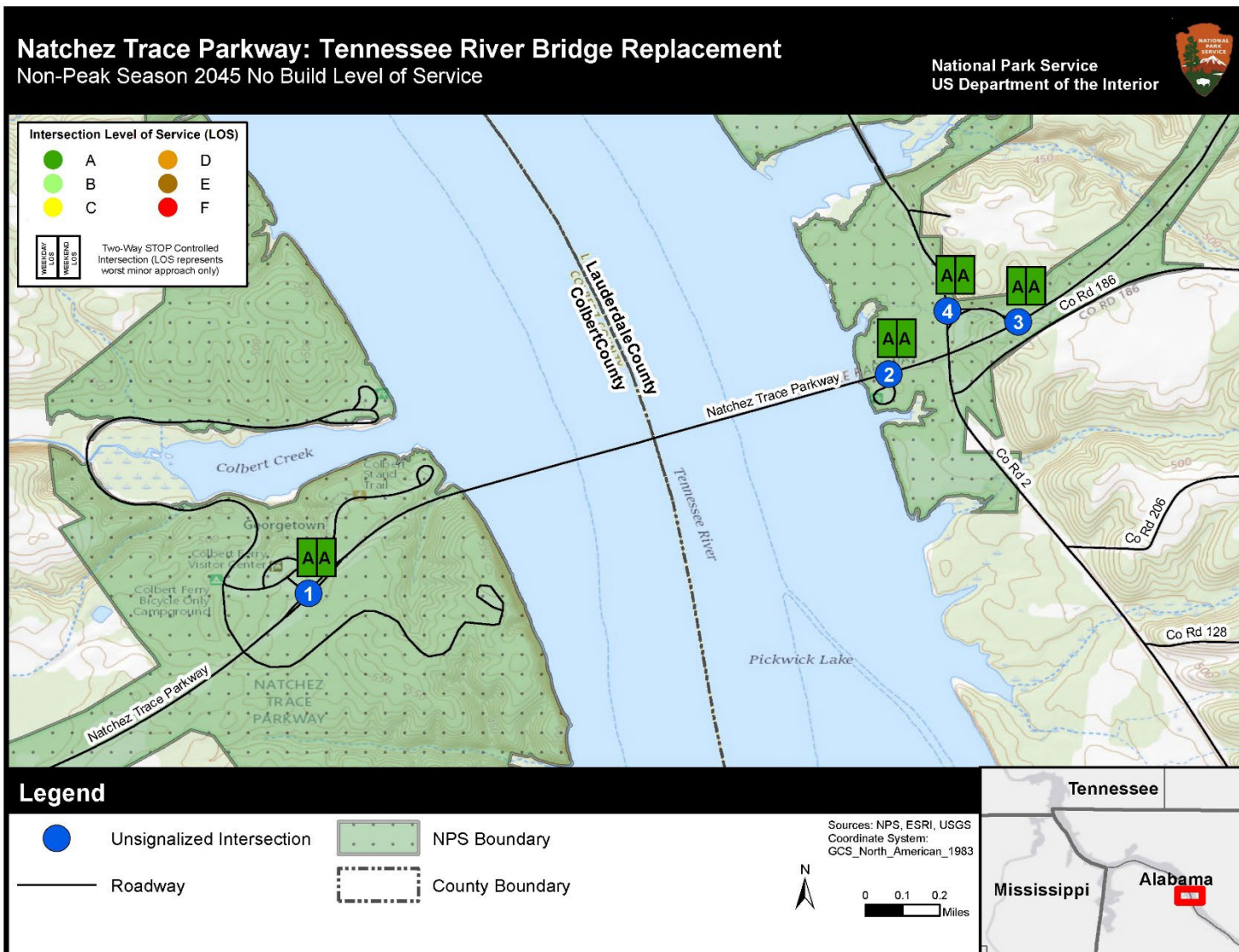


FIGURE 9-3. NON-PEAK SEASON 2045 NO BUILD LEVEL OF SERVICE

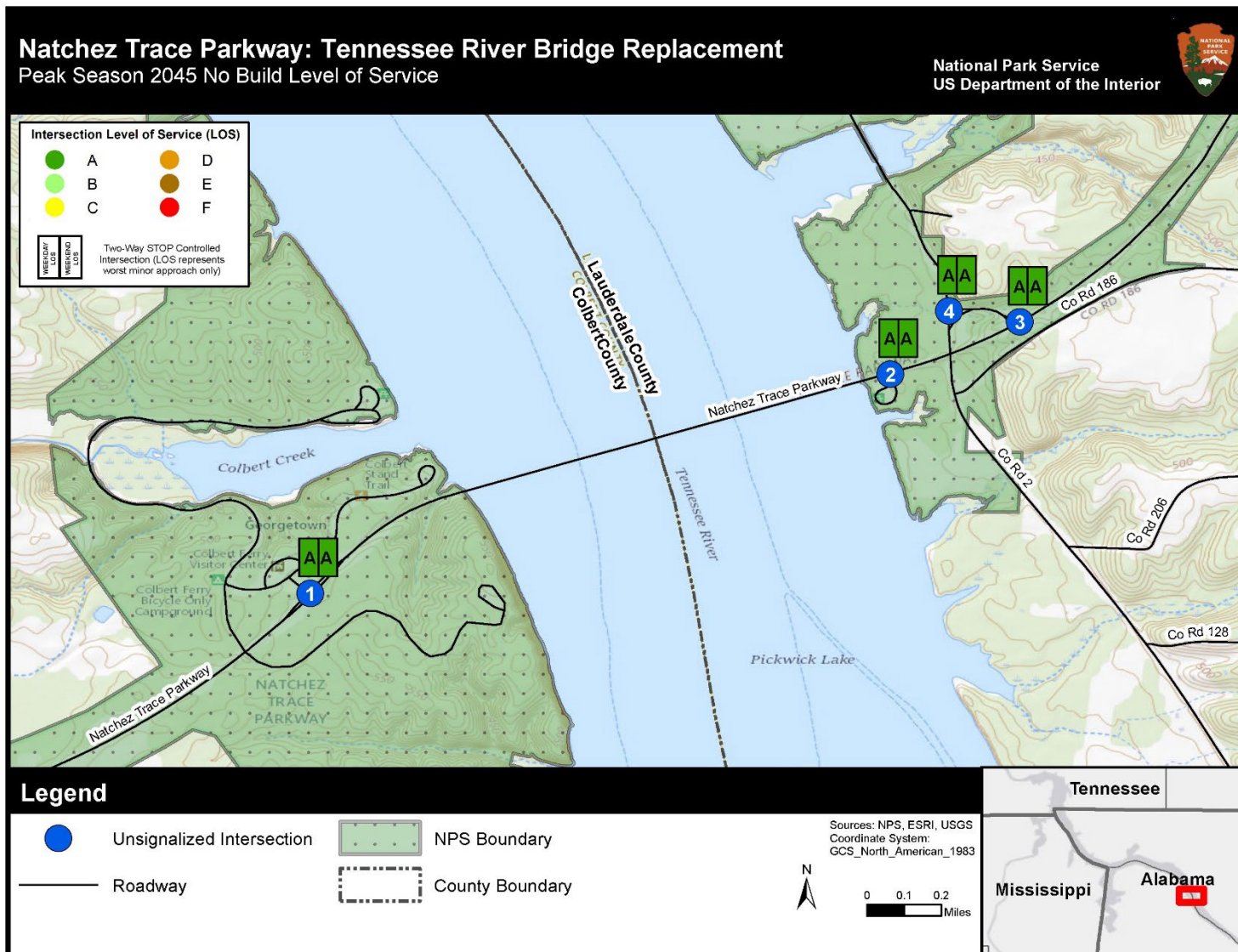


FIGURE 9-4. PEAK SEASON 2045 NO BUILD LEVEL OF SERVICE

TABLE 9-1. 2045 NO BUILD OPERATIONS ANALYSES

Intersection Number, Name, and Approach	Direction	Lane Group	2045 No Build											
			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)														
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.02	A	7.4	0.00	A	7.4	0.00	A	7.4	0.00
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.02	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	9.0	0.04	A	8.7	0.00	A	8.7	0.00	A	8.9	0.00
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)														
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.8	0.04	A	9.0	0.00	A	8.7	0.00	A	8.8	0.00
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.3	0.00	A	7.2	0.00	A	0.0	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

10.0 ANALYSIS OF THE ALTERNATIVES

The traffic study area for alternatives 1 and 2 for this study include the same four unsignalized intersections as the existing conditions analysis and no build analysis. The intersections evaluated in the alternatives analyses include the following:

- (1) Natchez Trace Parkway/Colbert Ferry Road
- (2) Natchez Trace Parkway/Parking Area (Water Route Overlook)
- (3) Natchez Trace Parkway/Lauderdale County Route 2 Ramp
- (4) Lauderdale County Route 2/Lauderdale County Route 2 Ramp

No major roadway or intersections improvements were identified or incorporated for traffic study area intersections in 2045. The Tennessee River Bridge project is not expected to modify the lane use or traffic controls of the study intersections. Therefore, the existing lane geometry and traffic control type previously shown in Figure 6-4 would continue under alternatives 1 and 2.

10.1 ALTERNATIVE 1

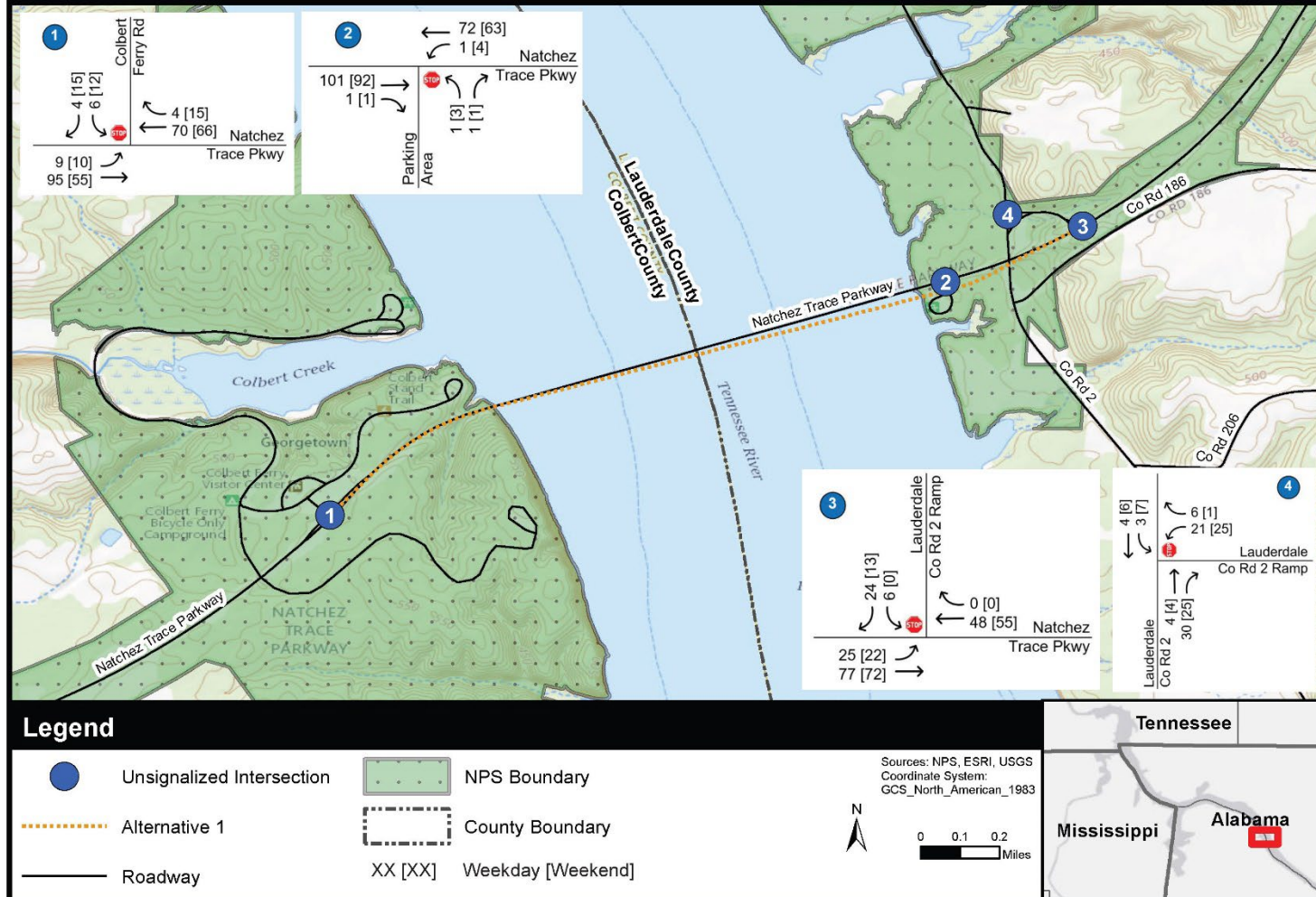
10.1.1 Description

The Alternative 1 future year 2045 traffic forecasts and analysis include the Tennessee River Bridge replacement alignment wherein the reconstructed bridge would be aligned with Natchez Trace Parkway's existing alignment west of the Tennessee River and would be shifted to the south with respect to Natchez Trace Parkway's existing alignment east of the Tennessee River. The southward shift of Natchez Trace Parkway east of the Tennessee River would continue for several hundred feet before ultimately tying in with the prevailing Natchez Trace Parkway east of the traffic study area. This alignment would not change the roadway connectivity or capacity of the traffic study area.

10.1.2 Development of Traffic Forecasts

The Tennessee River Bridge project would not generate new vehicle trips and is not anticipated to alter travel patterns that would occur under the No Build because the reconstructed bridge would retain the overall roadway system's existing connectivity and would not add or remove capacity. Traffic forecasts for Alternative 1 are therefore assumed to be identical to the forecasts developed for the No Build. The non-peak season weekday and Saturday peak hour Alternative 1 traffic volumes are presented in Figure 10-1, and the peak season 2045 weekday and Saturday peak hour Alternative 1 traffic volumes are presented in Figure 10-2. The non-peak season and peak season Alternative 1 weekday and Saturday intersection LOS are provided in Figures 10-3 and 10-4, respectively. A summary of the intersection LOS, delay, and v/c ratio results for Alternative 1 is presented in Table 10-1. Appendix D provides the capacity analysis worksheets.

**National Park Service
US Department of the Interior**



Natchez Trace Parkway Traffic Study
September 2023

Natchez Trace Parkway: Tennessee River Bridge Replacement Peak Season 2045 Alternative 1 Volumes

National Park Service
US Department of the Interior

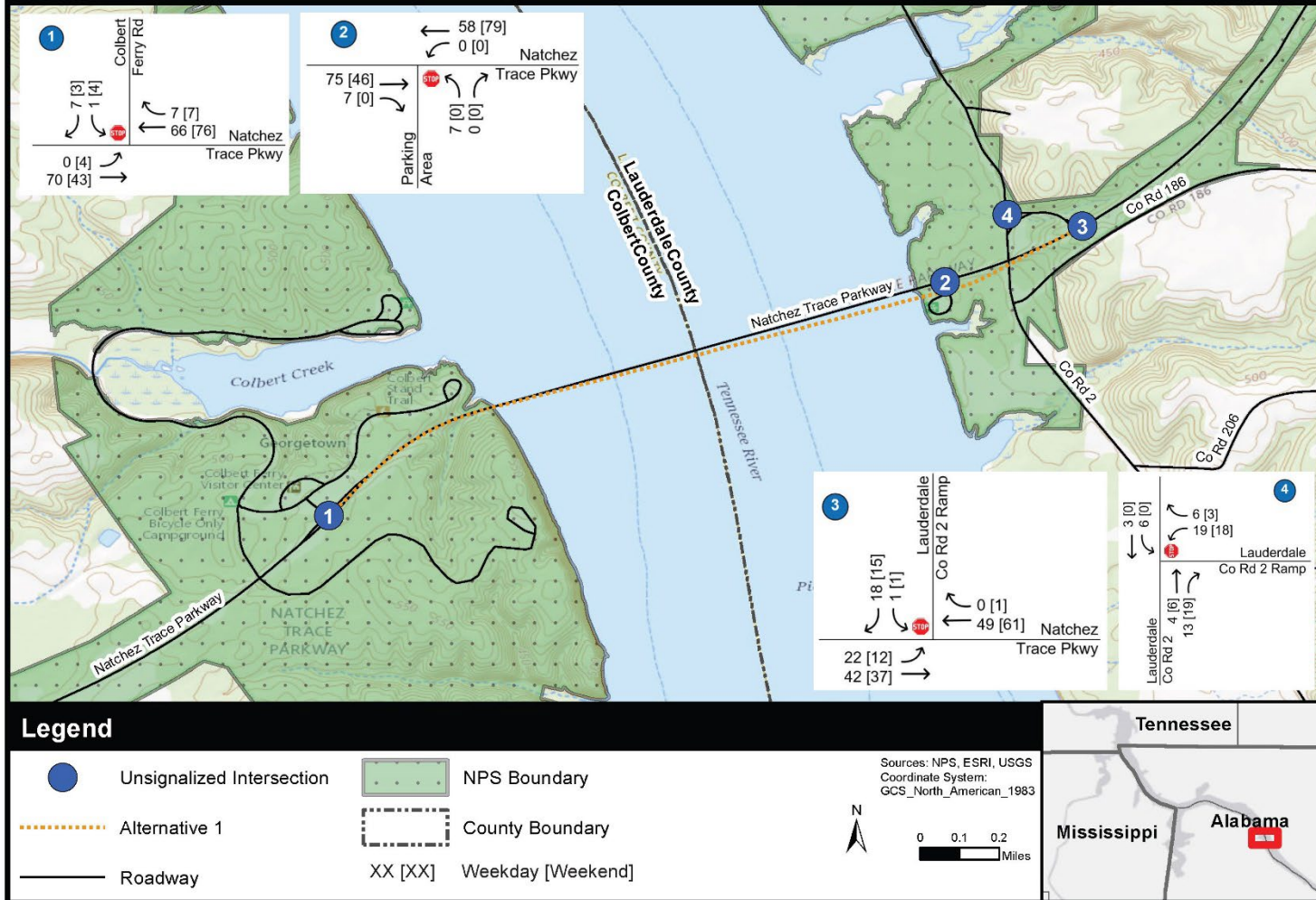


FIGURE 10-2. PEAK SEASON 2045 ALTERNATIVE 1 VOLUMES

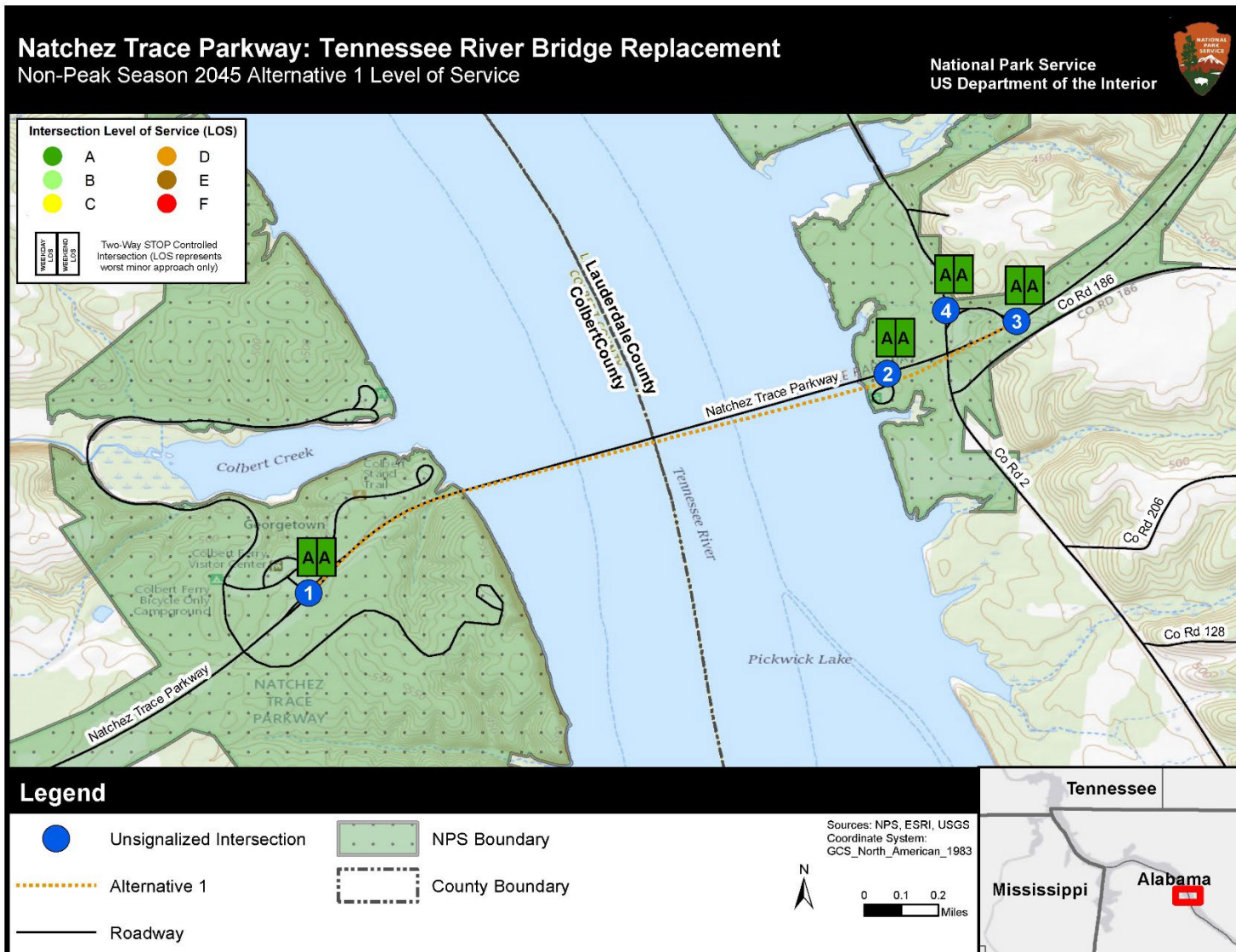


FIGURE 10-3. NON-PEAK SEASON 2045 ALTERNATIVE 1 LEVEL OF SERVICE

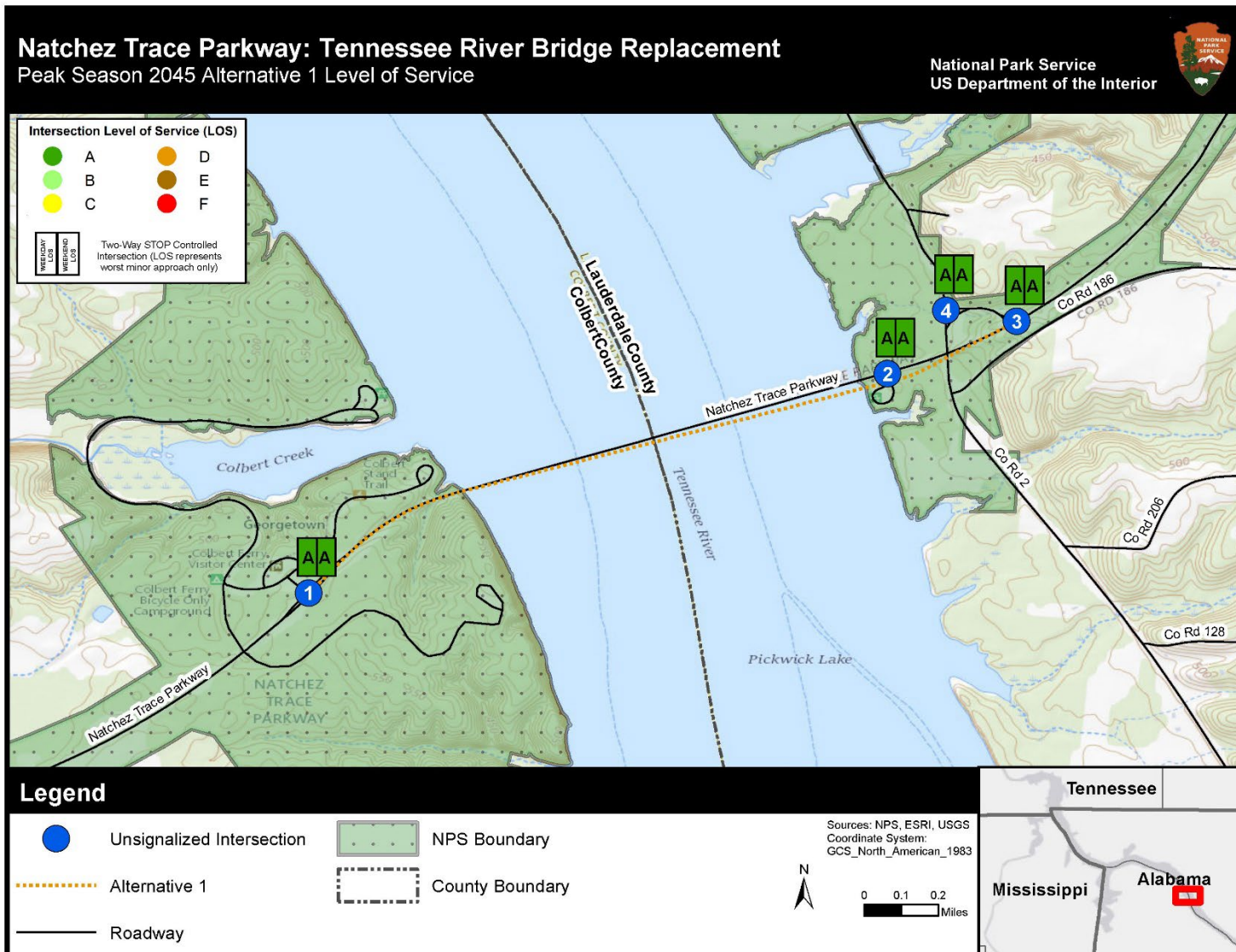


FIGURE 10-4. PEAK SEASON 2045 ALTERNATIVE 1 LEVEL OF SERVICE

TABLE 10-1. 2045 ALTERNATIVE 1 OPERATIONS ANALYSES

Intersection Number, Name, and Approach	Direction	Lane Group	2045 Alternative 1											
			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)														
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02	A	7.4	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	9.0	0.04	A	8.7	0.02	A	8.7	0.02	A	8.9	0.03
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)														
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.8	0.04	A	9.0	0.04	A	8.7	0.03	A	8.8	0.04
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.3	0.01	A	7.2	0.01	A	0.0	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

10.2 ALTERNATIVE 2

10.2.1 Description

The Alternative 2 future year 2045 traffic forecasts and analysis include the Tennessee River Bridge replacement alignment wherein the reconstructed bridge would be shifted to the south with respect to Natchez Trace Parkway's existing alignment on both sides of the Tennessee River. The southward shift of Natchez Trace Parkway on either side of the Tennessee River would continue for several hundred feet before ultimately tying in with the prevailing Natchez Trace Parkway. This alignment would not change the roadway connectivity or capacity of the traffic study area.

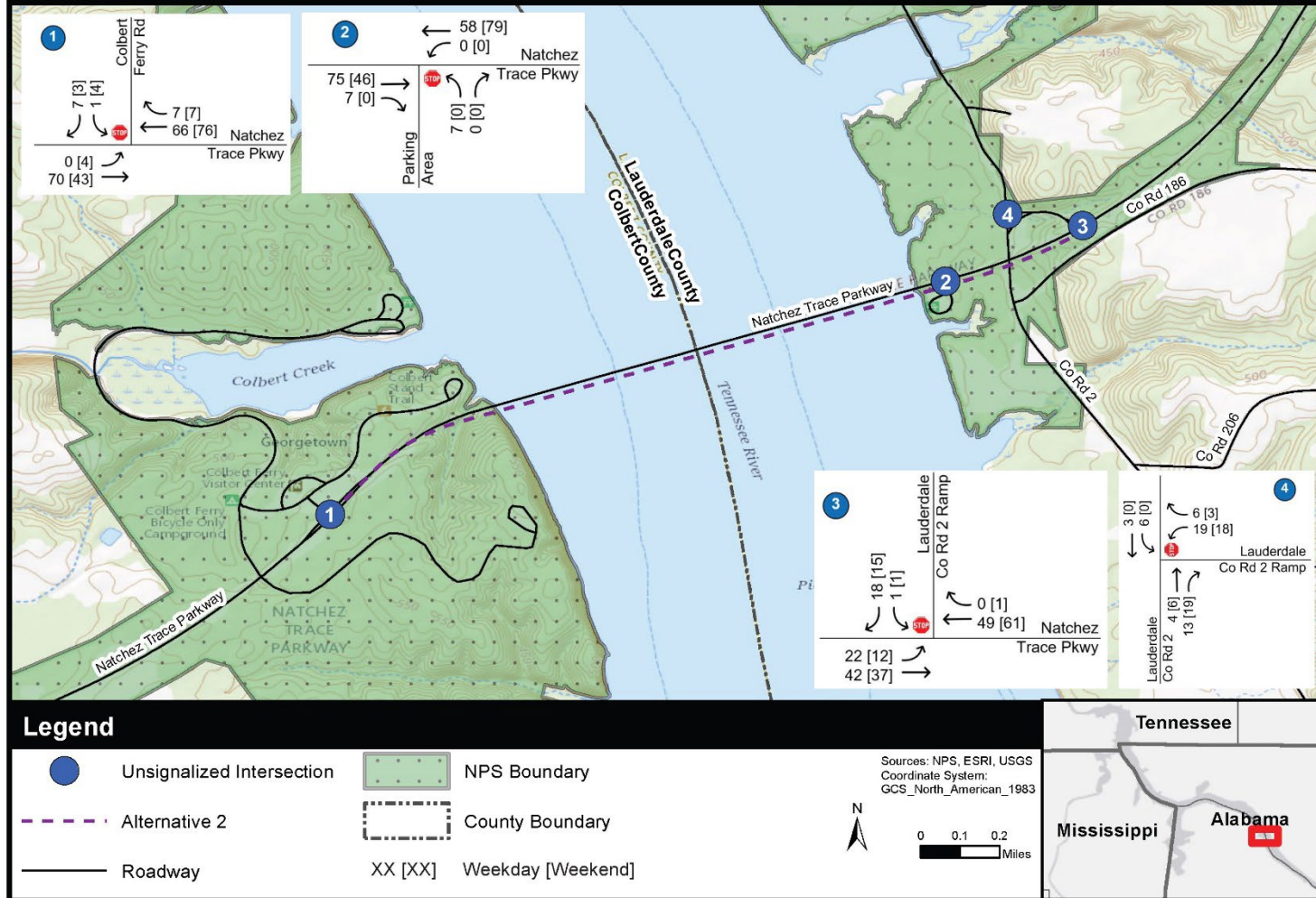
10.2.2 Development of Traffic Forecasts

The Tennessee River Bridge project would not generate new vehicle trips and is not anticipated to alter travel patterns that would occur under the No Build because the reconstructed bridge would retain the overall roadway system's existing connectivity and would not add or remove capacity. Traffic forecasts for Alternative 2 are therefore assumed to be identical to the forecasts developed for the No Build. The non-peak season weekday and Saturday peak hour Alternative 2 traffic volumes are presented in Figure 10-5, and the peak season 2045 weekday and Saturday peak hour Alternative 2 traffic volumes are presented in Figure 10-6. The non-peak season and peak season Alternative 2 weekday and Saturday intersection LOS are provided in Figures 10-7 and 10-8, respectively. A summary of the intersection LOS, delay, and v/c ratio results for Alternative 2 is presented in Table 10-2. Appendix D provides the capacity analysis worksheets.

National Park Service
US Department of the Interior



National Park Service
US Department of the Interior



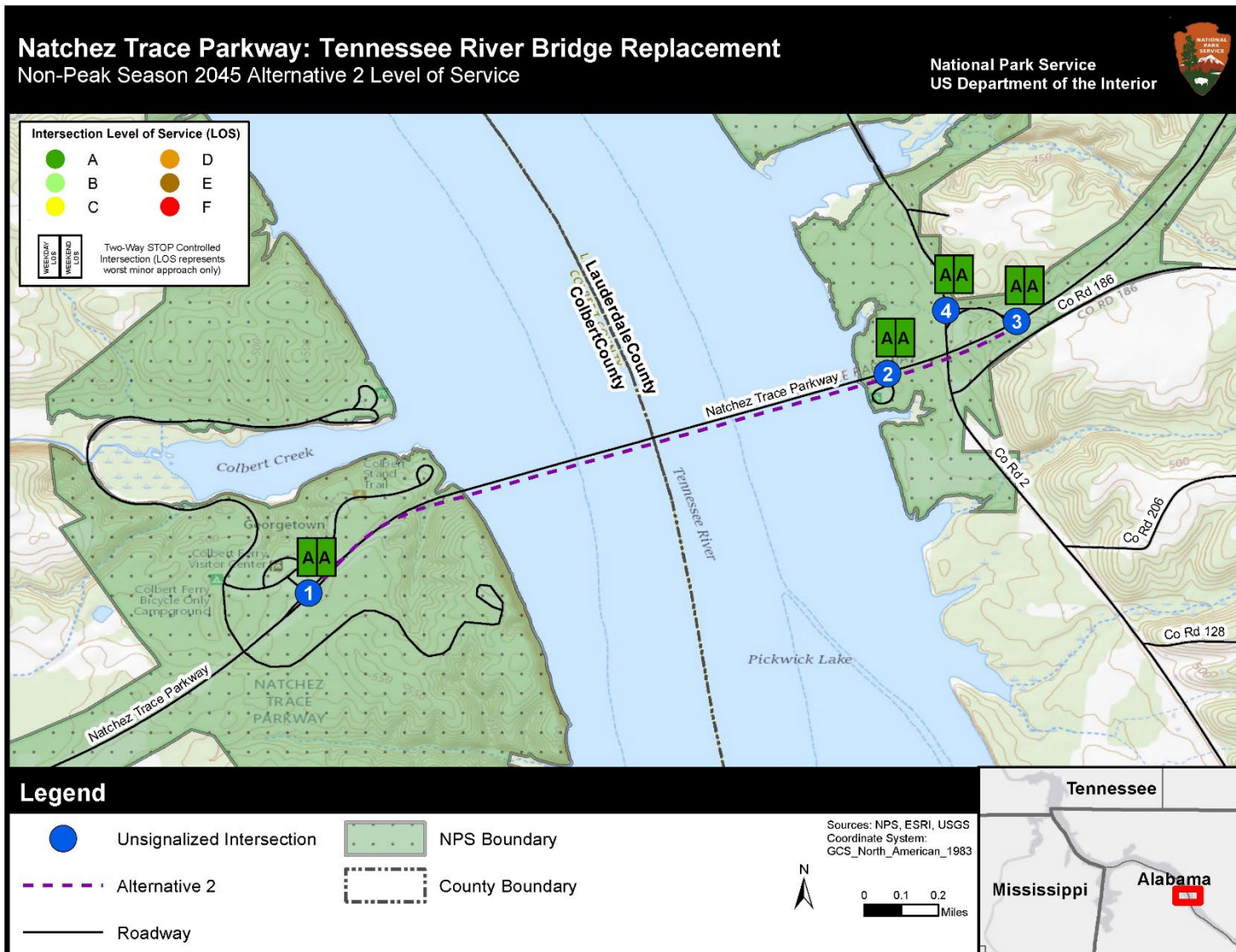


FIGURE 10-7. NON-PEAK SEASON 2045 ALTERNATIVE 2 LEVEL OF SERVICE

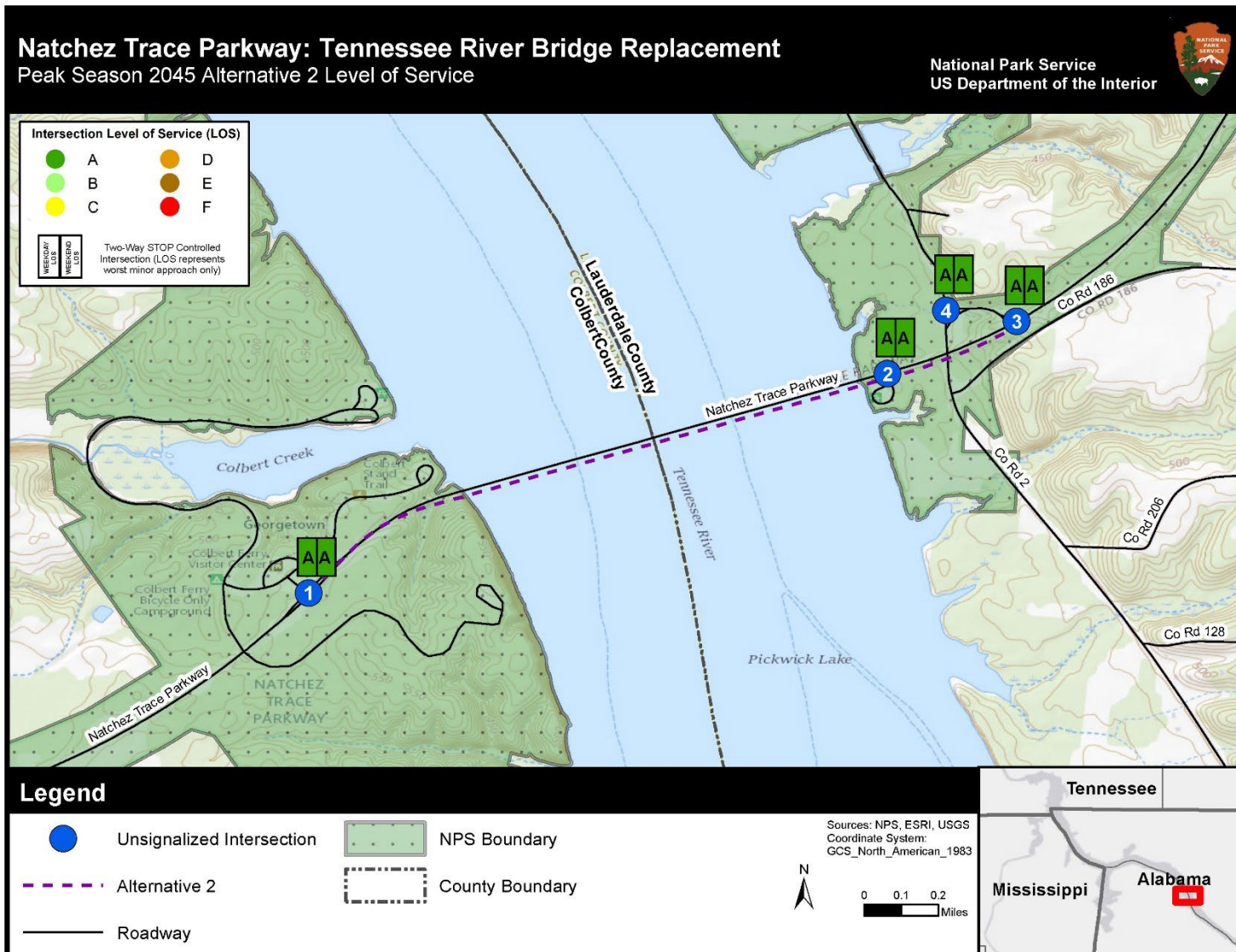


FIGURE 10-8. PEAK SEASON 2045 ALTERNATIVE 2 LEVEL OF SERVICE

TABLE 10-2. 2045 ALTERNATIVE 2 OPERATIONS ANALYSES

Intersection Number, Name, and Approach	Direction	Lane Group	2045 Alternative 2											
			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)														
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)														
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02	A	7.4	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	9.0	0.04	A	8.7	0.02	A	8.7	0.02	A	8.9	0.03
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)														
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.8	0.04	A	9.0	0.04	A	8.7	0.03	A	8.8	0.04
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.3	0.01	A	7.2	0.01	A	0.0	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

10.3 COMPARATIVE ANALYSIS

10.3.1 Intersection Operations

The study used Synchro™ to calculate the vehicle delay, v/c ratio, and LOS operation for the intersections under the No Build and the future 2045 conditions for alternatives 1 and 2. A summary of the intersection approach LOS grades for the two-way stop-controlled study intersections under the No Build and Alternative 1 is depicted in Table 10-3. A similar summary comparing the No Build and Alternative 2 is depicted in Table 10-4.

No Build

Based on the Synchro™ analysis, all study intersections would operate at acceptable conditions (LOS A) under the No Build.

Alternative 1

Based on the Synchro™ analysis, all study intersections would operate at acceptable conditions (LOS A) under Alternative 1, and there would be an imperceptible change in intersection operations when comparing Alternative 1 with the No Build.

Alternative 2

Based on the Synchro™ analysis, all study intersections would operate at acceptable conditions (LOS A) under Alternative 2, and there would be an imperceptible change in intersection operations when comparing Alternative 2 with the No Build.

TABLE 10-3. 2045 NO BUILD AND ALTERNATIVE 1 OPERATIONS ANALYSES

Intersection Number, Name, and Approach	Direction	Lane Group	2045 No Build												2045 Alternative 1											
			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a																										
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)																										
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)																										
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02	A	7.4	0.01	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02	A	7.4	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	9.0	0.04	A	8.7	0.02	A	8.7	0.02	A	8.9	0.03	A	9.0	0.04	A	8.7	0.02	A	8.7	0.02	A	8.9	0.03
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)																										
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.8	0.04	A	9.0	0.04	A	8.7	0.03	A	8.8	0.04	A	8.8	0.04	A	9.0	0.04	A	8.7	0.03	A	8.8	0.04
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.3	0.01	A	7.2	0.01	A	0.0	0.00	A	7.2	0.00	A	7.3	0.01	A	7.2	0.01	A	0.0	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

TABLE 10-4. 2045 NO BUILD AND ALTERNATIVE 2 OPERATIONS ANALYSES

Intersection Number, Name, and Approach	Direction	Lane Group	2045 No Build												2045 Alternative 2											
			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak			Non-Peak Season Weekday Peak			Non-Peak Season Saturday Peak			Peak Season Weekday Peak			Peak Season Saturday Peak		
			LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio	LOS	Delay (sec/ veh)	V/C ratio
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a																										
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01	A	7.4	0.01	A	7.4	0.01	A	0.0	0.00	A	7.5	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Colbert Ferry Road)	SB	LR	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01	A	9.2	0.01	A	9.0	0.03	A	8.7	0.01	A	9.3	0.01
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)																										
NB (Parking Area (Water Route Overlook))	NB	LR	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00	A	9.3	0.00	A	9.6	0.01	A	9.4	0.01	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	L	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00	A	7.5	0.00	A	7.5	0.00	A	0.0	0.00	A	0.0	0.00
WB (Natchez Trace Parkway)	WB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)																										
EB (Natchez Trace Parkway)	EB	L	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02	A	7.4	0.01	A	7.4	0.02	A	7.4	0.02	A	7.4	0.02	A	7.4	0.01
EB (Natchez Trace Parkway)	EB	T	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00	A	0.0	0.00
SB (Lauderdale County Route 2 Ramp)	SB	LR	A	9.0	0.04	A	8.7	0.02	A	8.7	0.02	A	8.9	0.03	A	9.0	0.04	A	8.7	0.02	A	8.7	0.02	A	8.9	0.03
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)																										
WB (Lauderdale County Route 2 Ramp)	WB	LR	A	8.8	0.04	A	9.0	0.04	A	8.7	0.03	A	8.8	0.04	A	8.8	0.04	A	9.0	0.04	A	8.7	0.03	A	8.8	0.04
SB (Lauderdale County Route 2)	SB	L	A	7.2	0.00	A	7.3	0.01	A	7.2	0.01	A	0.0	0.00	A	7.2	0.00	A	7.3	0.01	A	7.2	0.01	A	0.0	0.00

Notes:
TWSC = Two-way STOP-Controlled unsignalized intersection (TWSC intersections do not have an overall LOS)
EB = eastbound; SB = southbound; NB = northbound; WB = westbound
Delay is measured in seconds per vehicle.

10.3.2 Queue Analysis

The study used Synchro™ to calculate 95th percentile queue lengths for the intersections under future 2045 conditions. The queue analysis was performed for the No Build and alternatives 1 and 2. Table 10-5 depicts the 95th percentile queue lengths and available storage capacity of study intersection lane groups under the No Build and Alternative 1. A similar summary comparing the No Build and Alternative 2 is depicted in Table 10-6.

No Build

Based on the Synchro™ intersection analysis results, the four study intersections in the traffic study area would provide enough storage for the calculated 95th percentile queuing lengths under the No Build.

Alternative 1

Based on the Synchro™ intersection analysis results, the four study intersections in the traffic study area would provide enough storage for the calculated 95th percentile queuing lengths under Alternative 1. There would be an imperceptible change in queue lengths when comparing Alternative 1 with the No Build.

Alternative 2

Based on the Synchro™ intersection analysis results, the four study intersections in the traffic study area would provide enough storage for the calculated 95th percentile queuing lengths under Alternative 2. There would be an imperceptible change in queue lengths when comparing Alternative 2 with the No Build.

TABLE 10-5. 2045 NO BUILD AND ALTERNATIVE 1 QUEUE ANALYSIS

Intersection Number, Name, and Approach	Direction	Lane Group	Turning Bay/Link Length (feet)	2045 No Build				2045 Alternative 1			
				Non-Peak Season Weekday Peak 95th Percentile Queue (feet)	Non-Peak Season Saturday Peak 95th Percentile Queue (feet)	Peak Season Weekday Peak 95th Percentile Queue (feet)	Peak Season Saturday Peak 95th Percentile Queue (feet)	Non-Peak Season Weekday Peak 95th Percentile Queue (feet)	Non-Peak Season Saturday Peak 95th Percentile Queue (feet)	Peak Season Weekday Peak 95th Percentile Queue (feet)	Peak Season Saturday Peak 95th Percentile Queue (feet)
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a											
EB (Natchez Trace Parkway)	EB	LT	>5000	0	0	0	0	0	0	0	0
SB (Colbert Ferry Road)	SB	LR	270	0	3	0	0	0	3	0	0
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)											
NB (Parking Area (Water Route Overlook))	NB	LR	1110	0	0	0	0	0	0	0	0
WB (Natchez Trace Parkway)	WB	LT	1540	0	0	0	0	0	0	0	0
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)											
EB (Natchez Trace Parkway)	EB	LT	1540	3	3	3	0	3	3	3	0
SB (Lauderdale County Route 2 Ramp)	SB	LR	510	3	3	3	3	3	3	3	3
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)											
WB (Lauderdale County Route 2 Ramp)	WB	LR	820	3	3	3	3	3	3	3	3
SB (Lauderdale County Route 2)	SB	LT	>5000	0	0	0	0	0	0	0	0

Notes:

EB = eastbound; SB = southbound; NB = northbound; WB = westbound

^a HCM 6th Edition queue values are reported in units of vehicles, but an average vehicle length of 25 feet is assumed to convert the queue value into units of feet.

TABLE 10-6. 2045 NO BUILD AND ALTERNATIVE 2 QUEUE ANALYSIS

Intersection Number, Name, and Approach	Direction	Lane Group	Turning Bay/Link Length (feet)	2045 No Build				2045 Alternative 2			
				Non-Peak Season Weekday Peak 95th Percentile Queue (feet)	Non-Peak Season Saturday Peak 95th Percentile Queue (feet)	Peak Season Weekday Peak 95th Percentile Queue (feet)	Peak Season Saturday Peak 95th Percentile Queue (feet)	Non-Peak Season Weekday Peak 95th Percentile Queue (feet)	Non-Peak Season Saturday Peak 95th Percentile Queue (feet)	Peak Season Weekday Peak 95th Percentile Queue (feet)	Peak Season Saturday Peak 95th Percentile Queue (feet)
1. Natchez Trace Parkway/Colbert Ferry Road (TWSC) ^a											
EB (Natchez Trace Parkway)	EB	LT	>5000	0	0	0	0	0	0	0	0
SB (Colbert Ferry Road)	SB	LR	270	0	3	0	0	0	3	0	0
2. Natchez Trace Parkway/Parking Area (Water Route Overlook) (TWSC)											
NB (Parking Area (Water Route Overlook))	NB	LR	1110	0	0	0	0	0	0	0	0
WB (Natchez Trace Parkway)	WB	LT	1540	0	0	0	0	0	0	0	0
3. Natchez Trace Parkway/Lauderdale County Route 2 Ramp (TWSC)											
EB (Natchez Trace Parkway)	EB	LT	1540	3	3	3	0	3	3	3	0
SB (Lauderdale County Route 2 Ramp)	SB	LR	510	3	3	3	3	3	3	3	3
4. Lauderdale County Route 2/Lauderdale County Route 2 Ramp (TWSC)											
WB (Lauderdale County Route 2 Ramp)	WB	LR	820	3	3	3	3	3	3	3	3
SB (Lauderdale County Route 2)	SB	LT	>5000	0	0	0	0	0	0	0	0

Notes:

EB = eastbound; SB = southbound; NB = northbound; WB = westbound

^a HCM 6th Edition queue values are reported in units of vehicles, but an average vehicle length of 25 feet is assumed to convert the queue value into units of feet.

11.0 MITIGATION MEASURES

The purpose of mitigation measures is to improve intersection operations from unacceptable operations (LOS E or F) through a variety of strategies such as modifying the intersection roadway geometry. Based on the analysis of intersection operations and queuing for the No Build and alternatives 1 and 2, all four study intersections would operate with acceptable (LOS A) operations, and the available lane group storage capacity would accommodate the calculated 95th percentile queue lengths. No mitigation measures would be required for future conditions of traffic.

11.1 NO BUILD

Because the four study intersections would operate with acceptable operations and the available lane group storage capacity would accommodate the calculated 95th percentile queue lengths, no mitigation measures would be required for the No Build.

11.2 ALTERNATIVES 1 AND 2

Because the four study intersections would operate with acceptable operations and the available lane group storage capacity would accommodate the calculated 95th percentile queue lengths, no mitigation measures would be required for alternatives 1 and 2.

12.0 CONCLUSIONS

This traffic study evaluates traffic operations for existing conditions, the No Build, and two alignment alternatives for the Tennessee River Bridge project (alternatives 1 and 2) in the vicinity of Colbert Ferry, including Natchez Trace Parkway from Colbert Ferry Road to Lauderdale County Route 2 and adjacent intersecting roadways. Based on projected traffic forecasts and the capacity analysis, key traffic operation findings of this traffic study are provided below.

12.1 EXISTING CONDITIONS

Under existing conditions, the four existing study intersections operate with LOS A, which is considered an acceptable LOS grade for traffic operations. The available storage capacity of the lane groups at the four study intersections are adequately accommodating 95th percentile queues lengths under existing conditions.

12.2 NO BUILD

Under the No Build, the four study intersections would continue to operate with LOS A, which is considered an acceptable LOS grade for traffic operations. The available storage capacity of the lane groups at the four study intersections would continue to adequately accommodate 95th percentile queues lengths.

12.3 ALTERNATIVE 1

Under Alternative 1, the four study intersections would continue to operate with LOS A. The available storage capacity of the lane groups at the four study intersections would continue to adequately accommodate 95th percentile queues lengths.

12.4 ALTERNATIVE 2

Under Alternative 2, the four study intersections would continue to operate with LOS A. The available storage capacity of the lane groups at the four study intersections would continue to adequately accommodate 95th percentile queues lengths.

13.0 ACRONYMS AND ABBREVIATIONS

AADT	annual average daily traffic
ALDOT	Alabama Department of Transportation
EB	eastbound
HCM	Highway Capacity Manual
LOS	level of service
NACOLG	Northwest Alabama Council of Local Governments
NB	northbound
NPS	National Park Service
RPO	rural planning organization
SB	southbound
TMC	turning movement count
TRB	Transportation Research Board
TWSC	two-way, STOP-controlled
WB	westbound
v/c	volume-to-capacity
VSC	vehicle classification, and speed

14.0 REFERENCES

Alabama Department of Transportation (ALDOT)

- 2008 Colbert County Highway Functional Classification System. November 13, 2008. Accessed June 6, 2023. <https://www.dot.state.al.us/maps/pdf/surveyMapping/HFC/regno/39-Lauderdale.pdf>
- 2020 Traffic Data Manager Public Interface User Guide. October 20, 2020. <https://aldotgis.dot.state.al.us/TDMPublic/Content/TDMHelp.pdf>
- 2021 Colbert County Highway Functional Classification System. July 2021. Accessed June 6, 2023. <https://www.dot.state.al.us/maps/pdf/surveyMapping/HFC/regno/17-Colbert.pdf>
- 2023a Personal communication between D. Grant, ALDOT, and B. Reynolds, WSP, on January 23, 2023, providing link to Annual Average Daily Traffic counts between 2011 and 2021.
- 2023b Alabama Traffic Data. <https://aldotgis.dot.state.al.us/TDMPublic/>

Alabama Law Enforcement Agency

- 2023 Speed Limits. <https://www.alea.gov/dps/highway-patrol/speed-limits>

Northwest Alabama Council of Local Governments (NACOLG)

- 2022 *Northwest Alabama Rural Planning Organization (RPO) FY 2023 Work Program*. August 16, 2022. Accessed June 6, 2023. <https://www.nacolg.org/news/northwest-alabama-rural-planning-organization-rpo-fy-2022-work-program>

National Park Service (NPS)

- 2016 *Southeast Region Draft Long-Range Transportation Plan*. September 2016.
<https://parkplanning.nps.gov/document.cfm?parkID=423&projectID=57274&documentID=75534>
- 2023a NPS Stats. Park Reports. <https://irma.nps.gov/Stats/Reports/Park/NATR>
- 2023b NPS Stats. Natchez Trace Parkway.
<https://irma.nps.gov/Stats/SSRSReports/Park%20Specific%20Reports/Traffic%20Counts?Park=NATR>

Transportation Research Board (TRB)

- 2000 *Highway Capacity Manual*. Transportation Research Board for the National Academies of Science, Washington, DC.
- 2016 *Highway Capacity Manual (HCM)*. Transportation Research Board for the National Academies of Science, Washington, DC.

15.0 LIST OF PREPARERS

Bradley Reynolds PE, Traffic Manager—MBA, Appalachian State University; BS, Civil Engineering, North Carolina State University

Kevin Berger, Transportation Planner—BS, Civil and Environmental Engineering, University of Maryland

Lori Fox, Project Manager—MCP, Land Use and Environment, University of Maryland; BS, Environmental Policy, University of Michigan

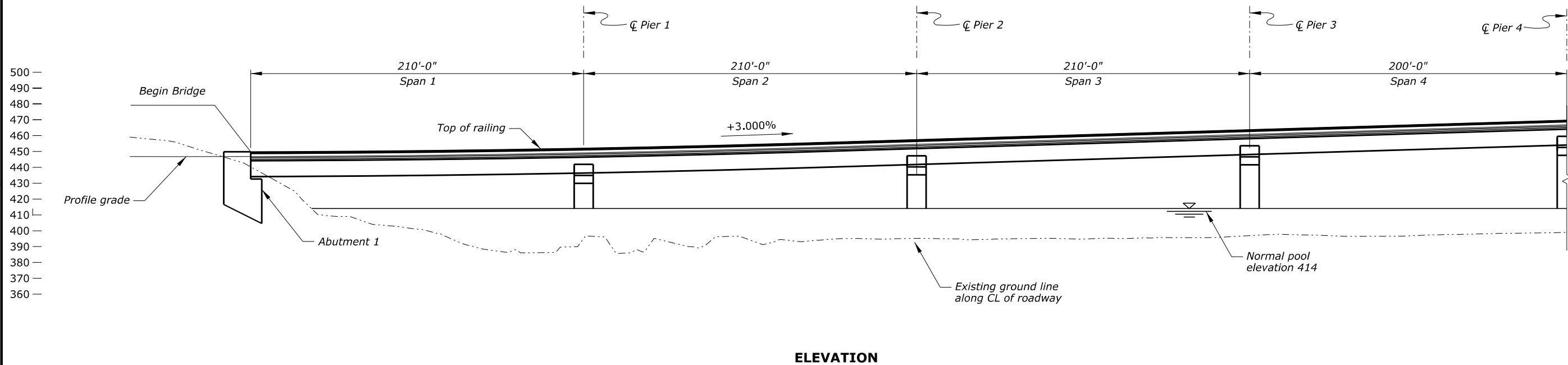
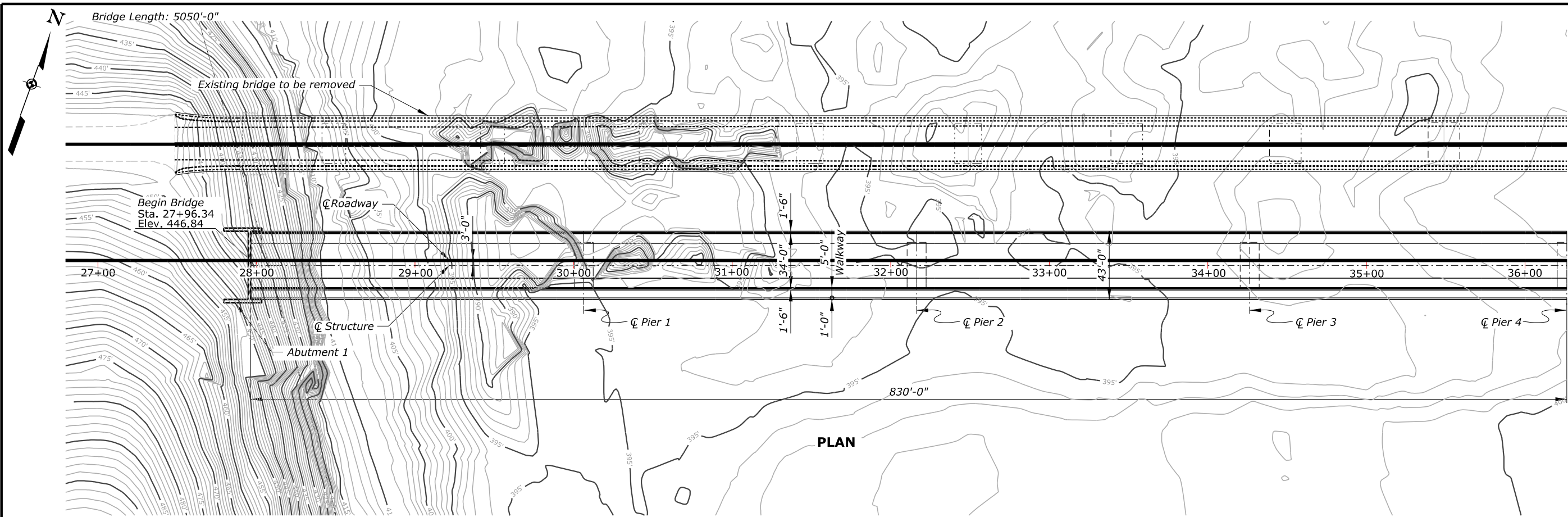
Alexandria Sentilles, Deputy Project Manager—MUEP, Urban and Environmental Planning, University of Virginia; BS, Sustainable Development, Appalachian State University

Deborah Mandell, Senior Technical Editor—MBA, Finance and Marketing, Northwestern University; BA Government, Wesleyan University

Appendix A:
Alternative Plans

M:\PROJECTS\028P\Bridges\Microstation\T S< S&L - Alt 1 - Segmental Concrete Box Girder .DGN

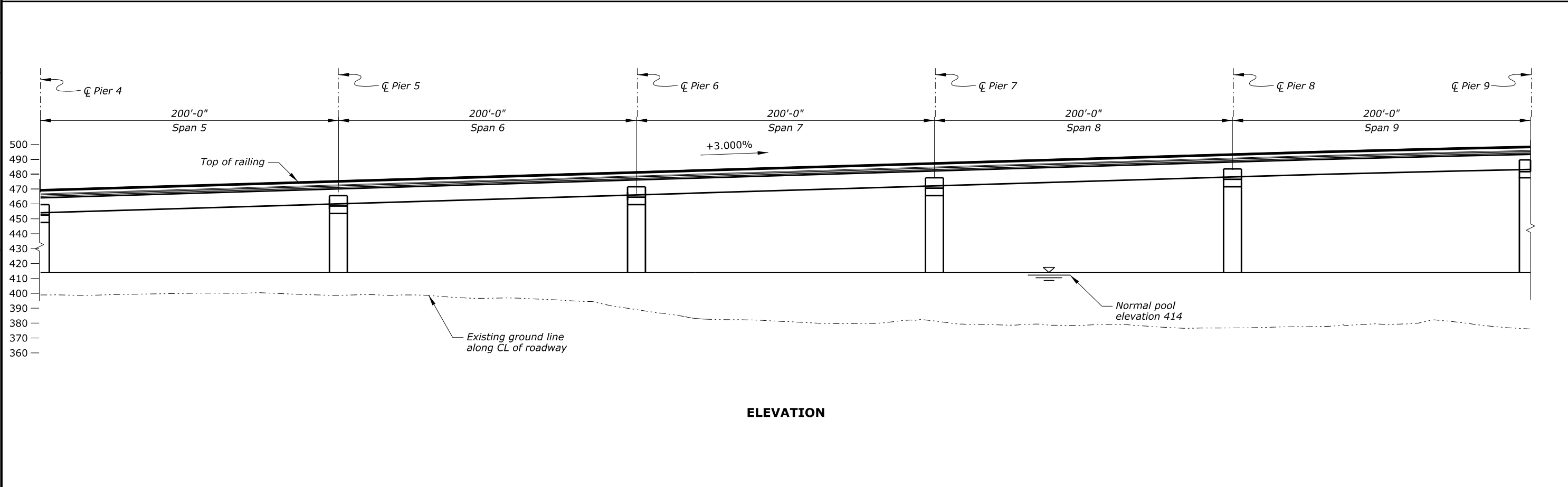
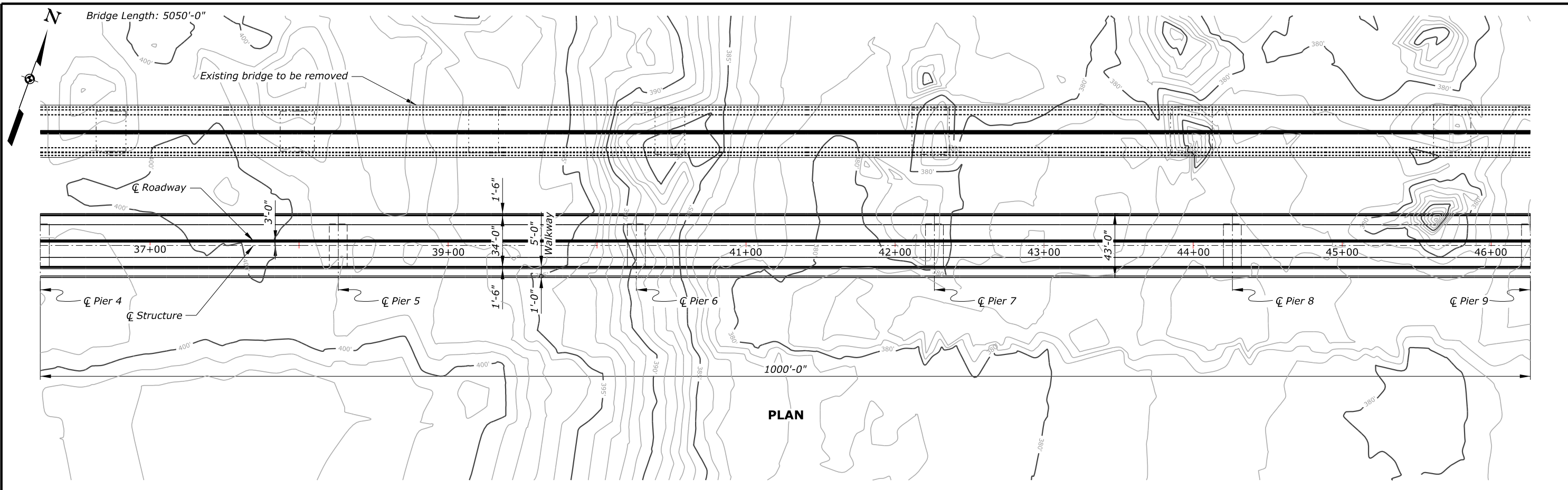
3/28/2023



ROUTE NAME AND NUMBER _____ Natchez Trace Parkway		FOREST/PARK/OTHER _____ Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SOUTH ALIGNMENT)
STATE _____ Alabama	COUNTY _____ Lauderdale	STREAM NAME _____ Tennessee River		
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____ \$494,172,000		
PREPARED BY _____ Carlos J. Peña-Marsh / Nicole Rojas		SCALE: _____ No Scale		
		DATE: _____ March 2023		
				PAGE 1 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< S&L - Alt 1 - Segmental Concrete Box Girder .DGN

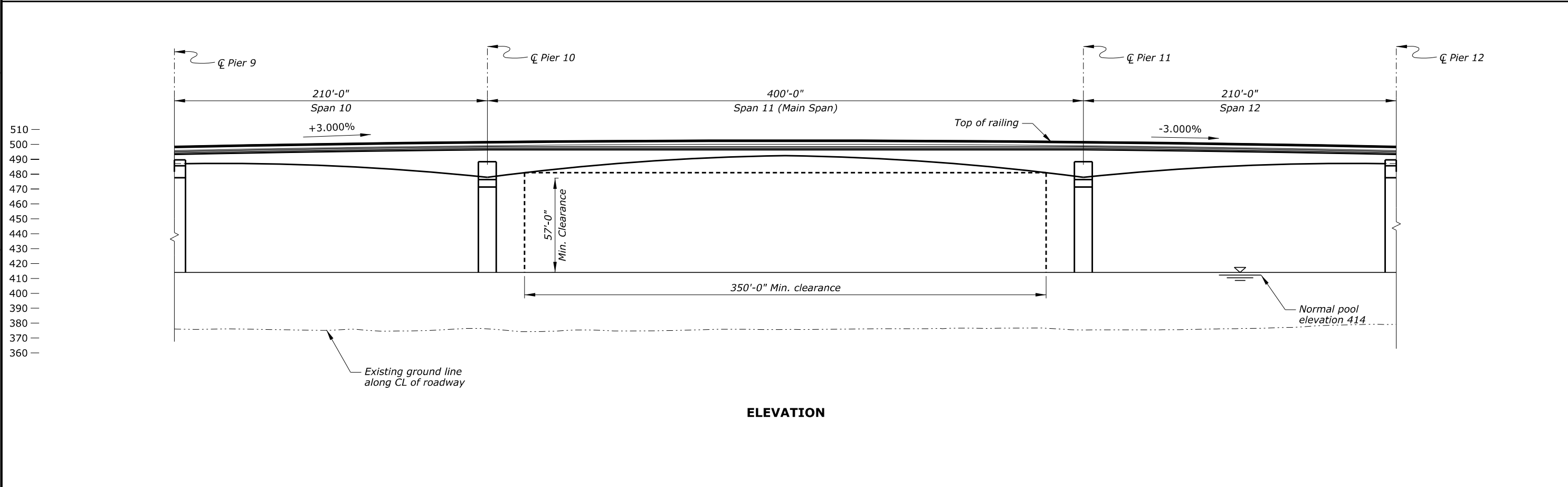
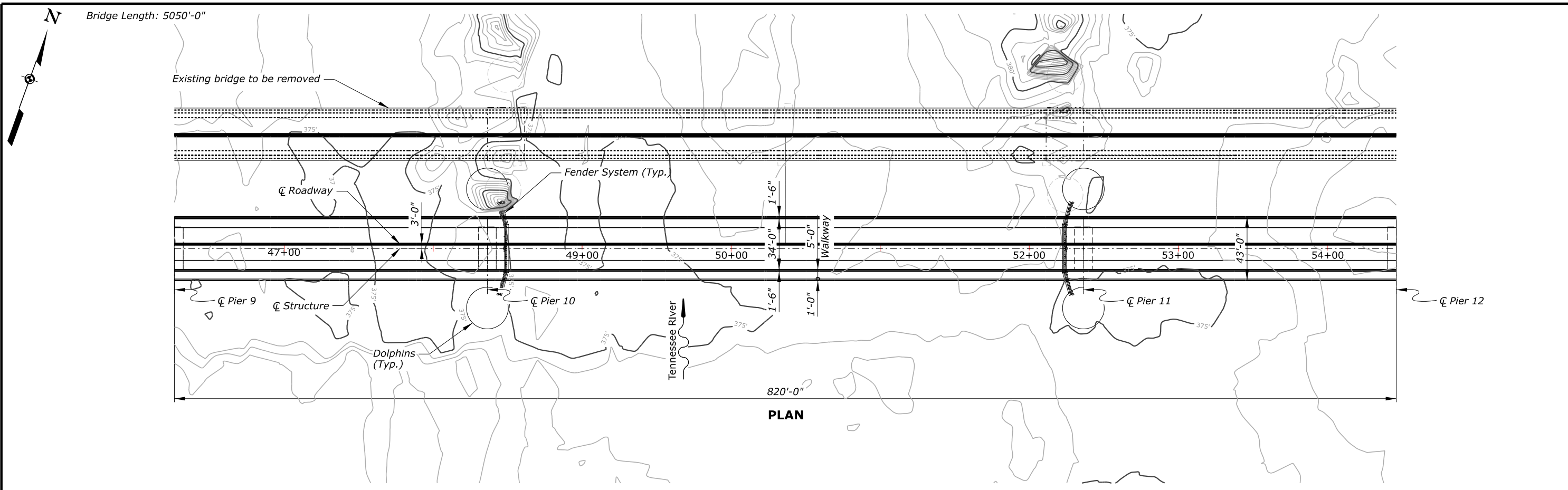
3/28/2023



ROUTE NAME AND NUMBER		Natchez Trace Parkway		FOREST/PARK/OTHER		Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SOUTH ALIGNMENT) PAGE 2 OF 9			
STATE		Alabama		COUNTY		Lauderdale					
ACCOUNT NO.				STREAM NAME		Tennessee River					
				ESTIMATED PROJECT COST		\$494,172,000					
PREPARED BY		Carlos J. Peña-Marsh / Nicole Rojas		SCALE:		No Scale		DATE:		March 2023	

M:\PROJECTS\028P\Bridges\Microstation\T S< S&L - Alt 1 - Segmental Concrete Box Girder .DGN

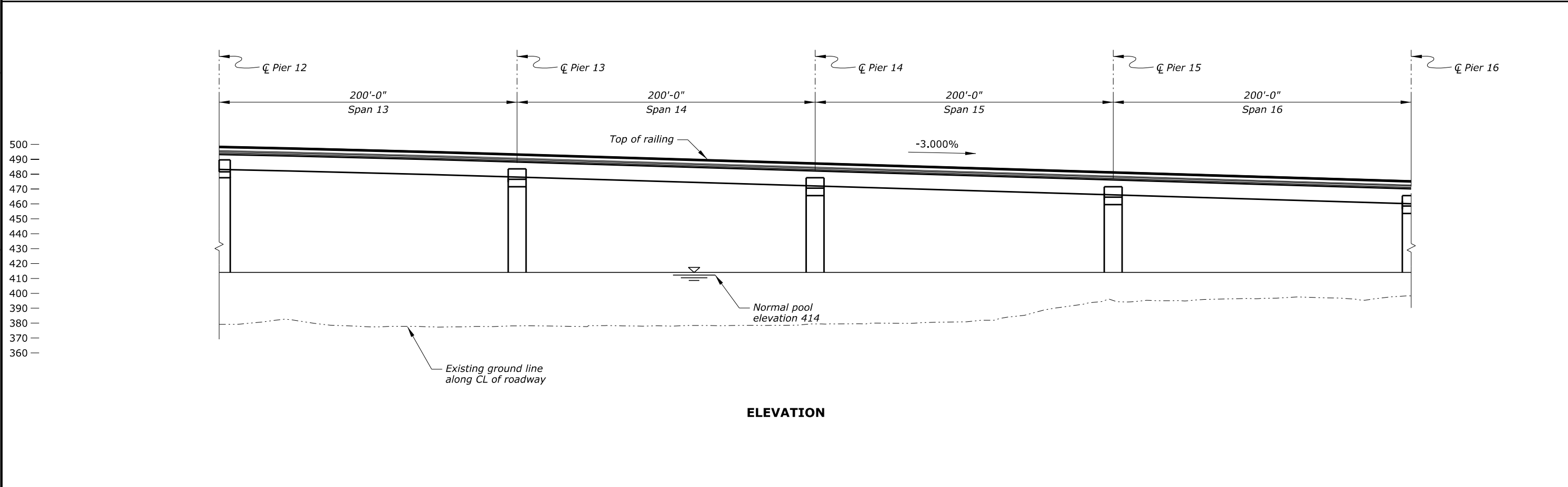
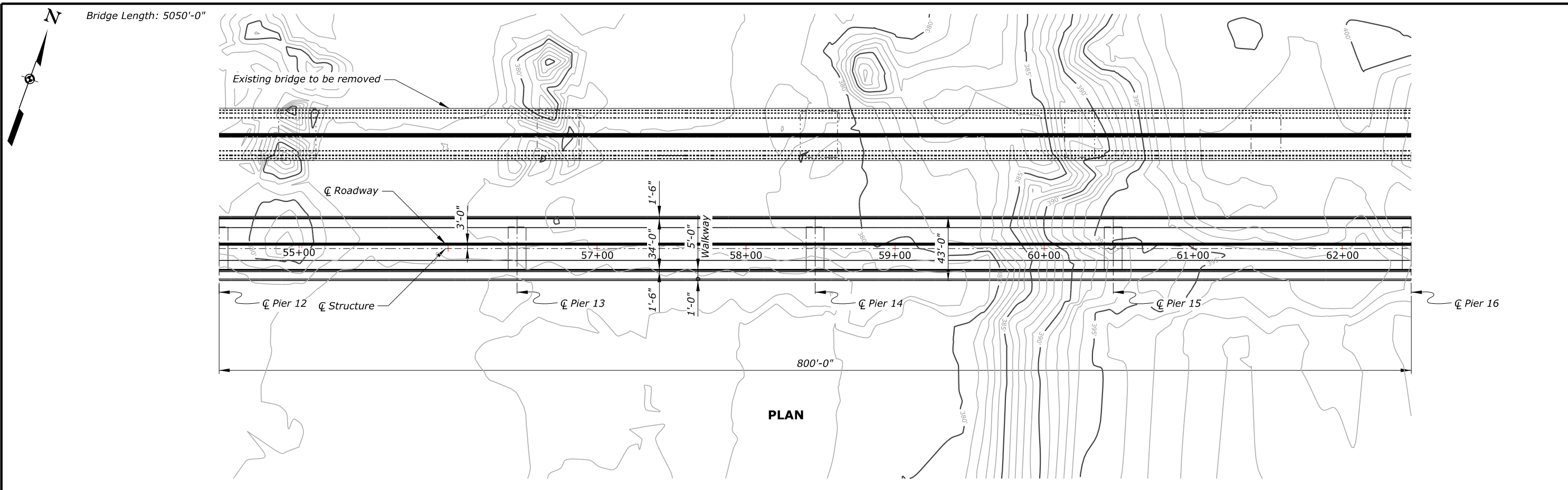
3/28/2023



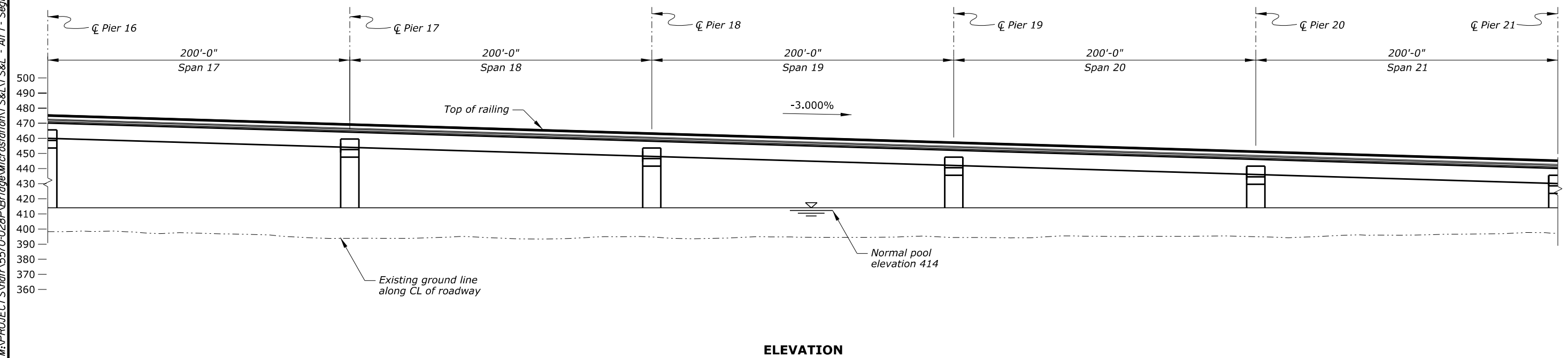
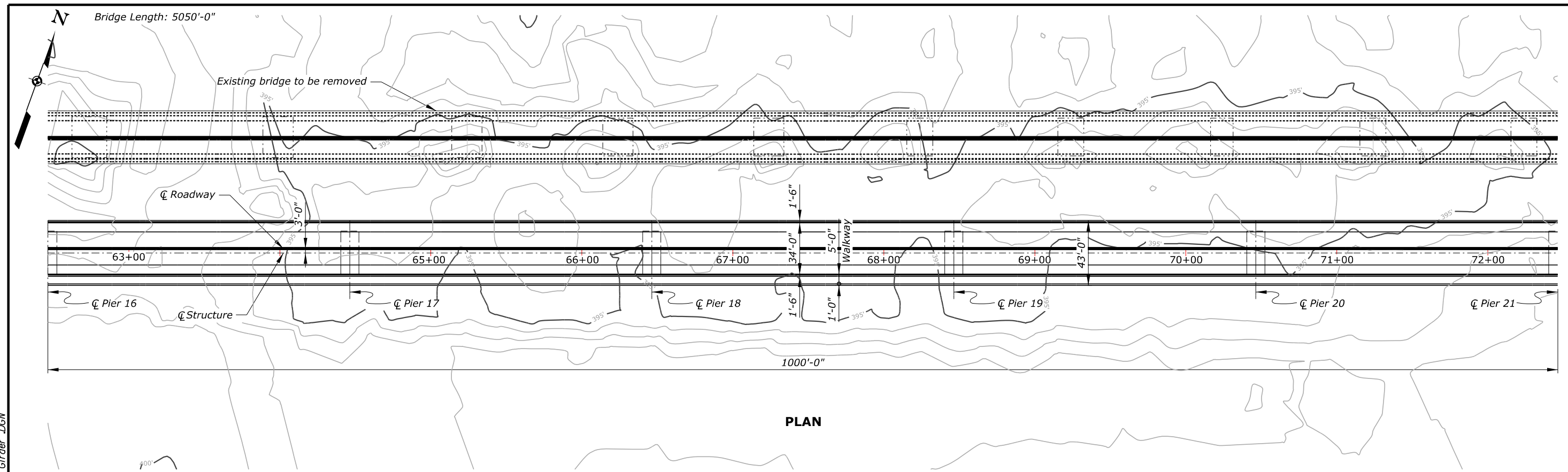
ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SOUTH ALIGNMENT) JOHN COFFEE MEMORIAL BRIDGE		
STATE	Alabama	COUNTY	Lauderdale			
ACCOUNT NO.		STREAM NAME	Tennessee River			
		ESTIMATED PROJECT COST	\$494,172,000			
PREPARED BY	Carlos J. Peña-Marsh / Nicole Rojas	SCALE:	No Scale	DATE:	March 2023	PAGE 3 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\TS<\S&L - Alt 1 - Segmental Concrete Box Girder .DGN

3/28/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SOUTH ALIGNMENT) JOHN COFFEE MEMORIAL BRIDGE		
STATE	Alabama	COUNTY	Lauderdale			
ACCOUNT NO.		STREAM NAME	Tennessee River			
		ESTIMATED PROJECT COST	\$494,172,000			
PREPARED BY	Carlos J. Peña-Marsh / Nicole Rojas	SCALE:	No Scale	DATE:	March 2023	PAGE 4 OF 9



ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER _____ Natchez Trace Parkway

STREAM NAME _____ Tennessee River

ESTIMATED PROJECT COST _____ \$494,172,000

SCALE: _____ No Scale DATE: _____ March 2023

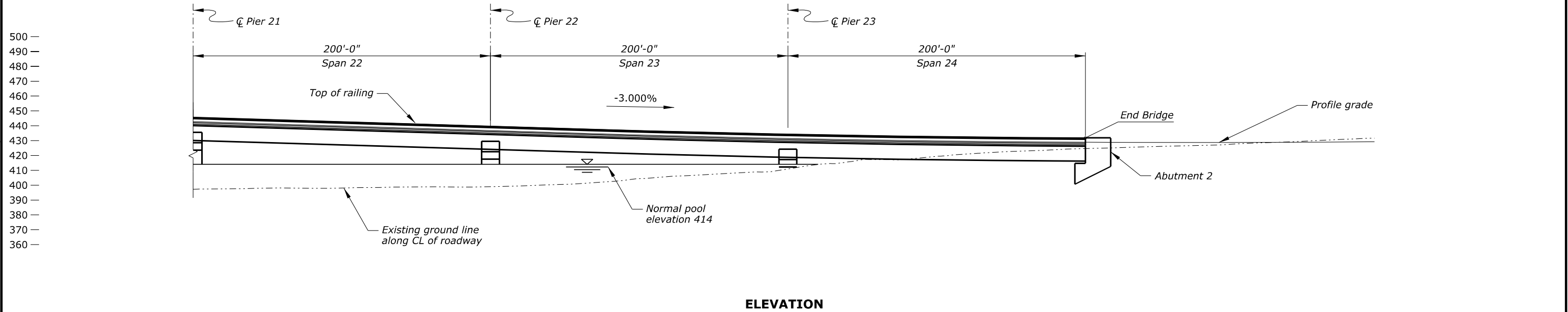
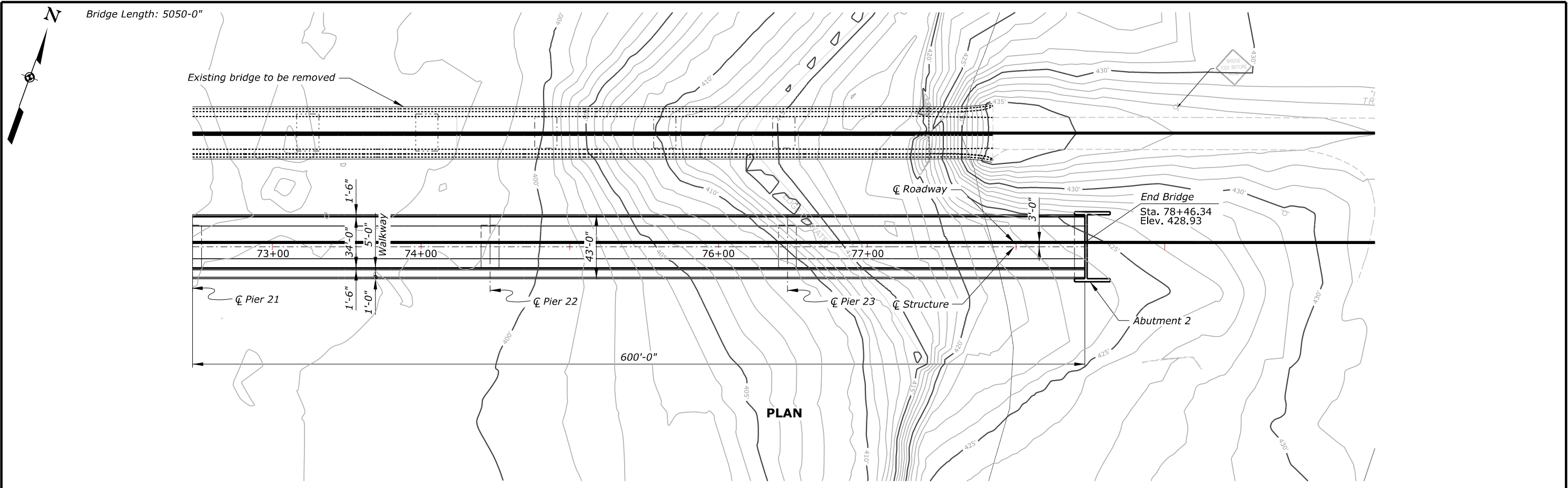
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SOUTH ALIGNMENT)

PAGE 5 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\TS<\S&L - Alt 1 - Segmental Concrete Box Girder .DGN

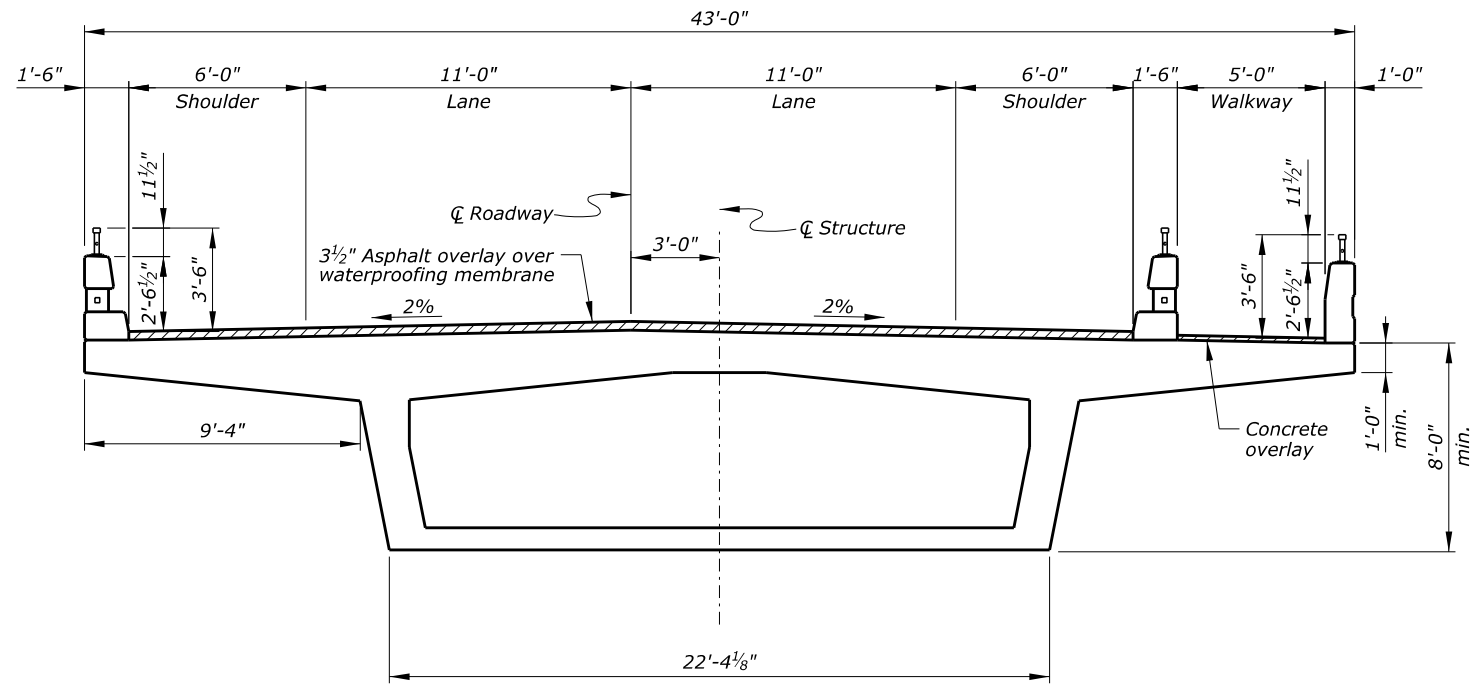
3/28/2023



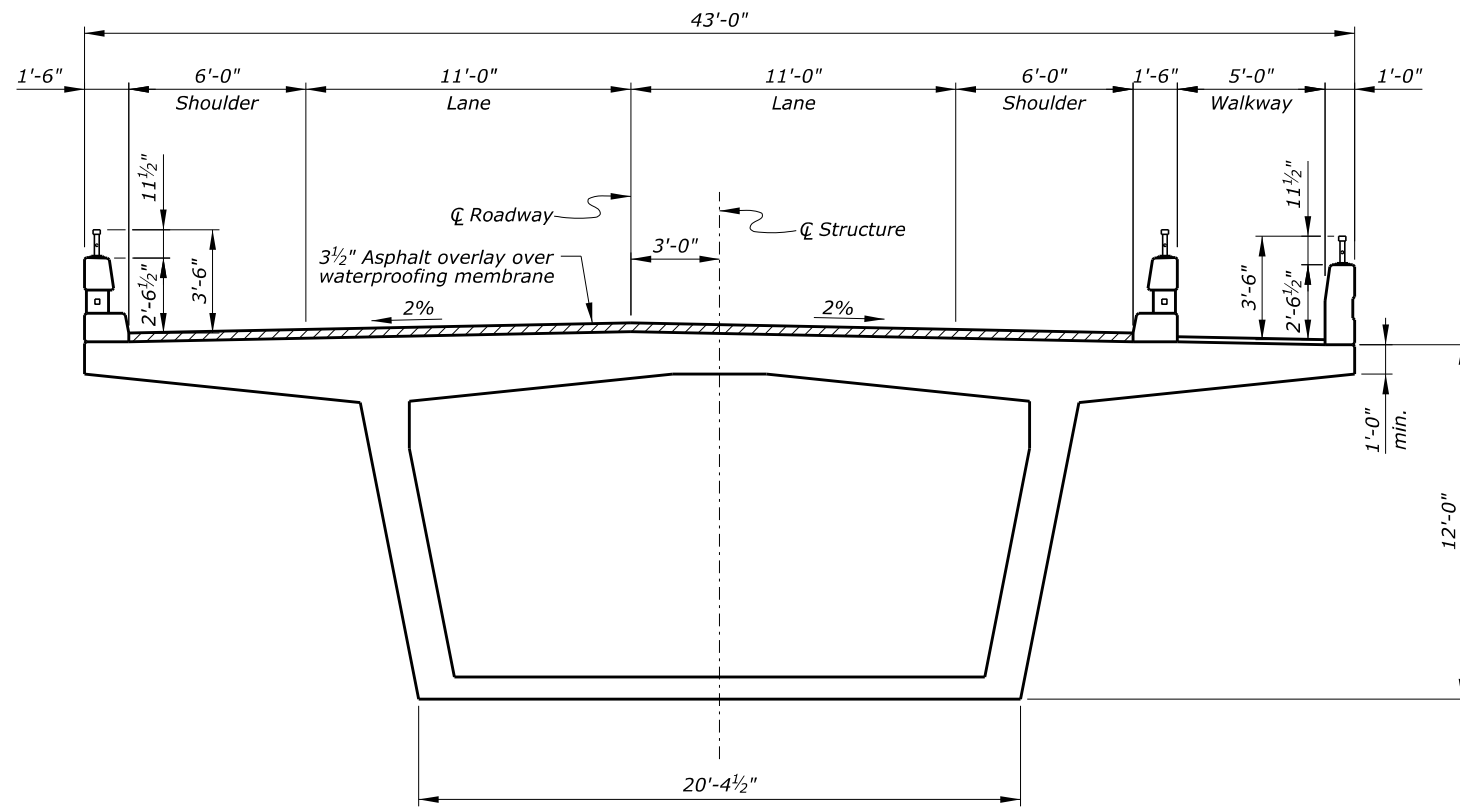
ROUTE NAME AND NUMBER _____		FOREST/PARK/OTHER _____		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SOUTH ALIGNMENT)	
STATE _____	COUNTY _____	STREAM NAME _____			
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____			
PREPARED BY _____		SCALE: _____			
_____		_____		DATE: _____	
_____		_____		_____	

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< S&L - Alt 1 - Segmental Concrete Box Girder .DGN

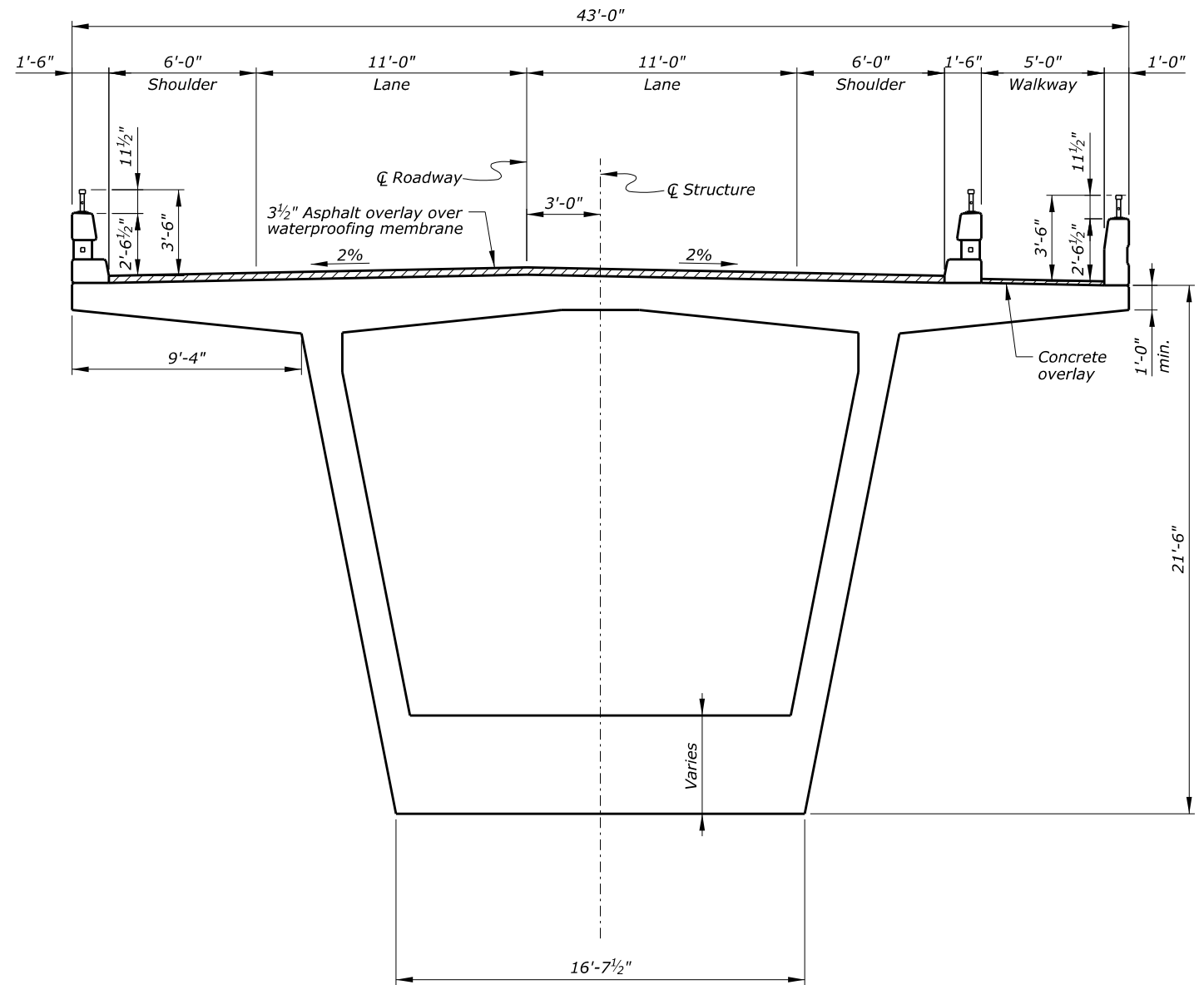
3/28/2023



TYPICAL SECTION
(Near midspan of span 10 and span 12)



TYPICAL SECTION
(Near midspan of spans 1 - 9 and 13 - 24)



TYPICAL SECTION
(Near pier 10 and 11)

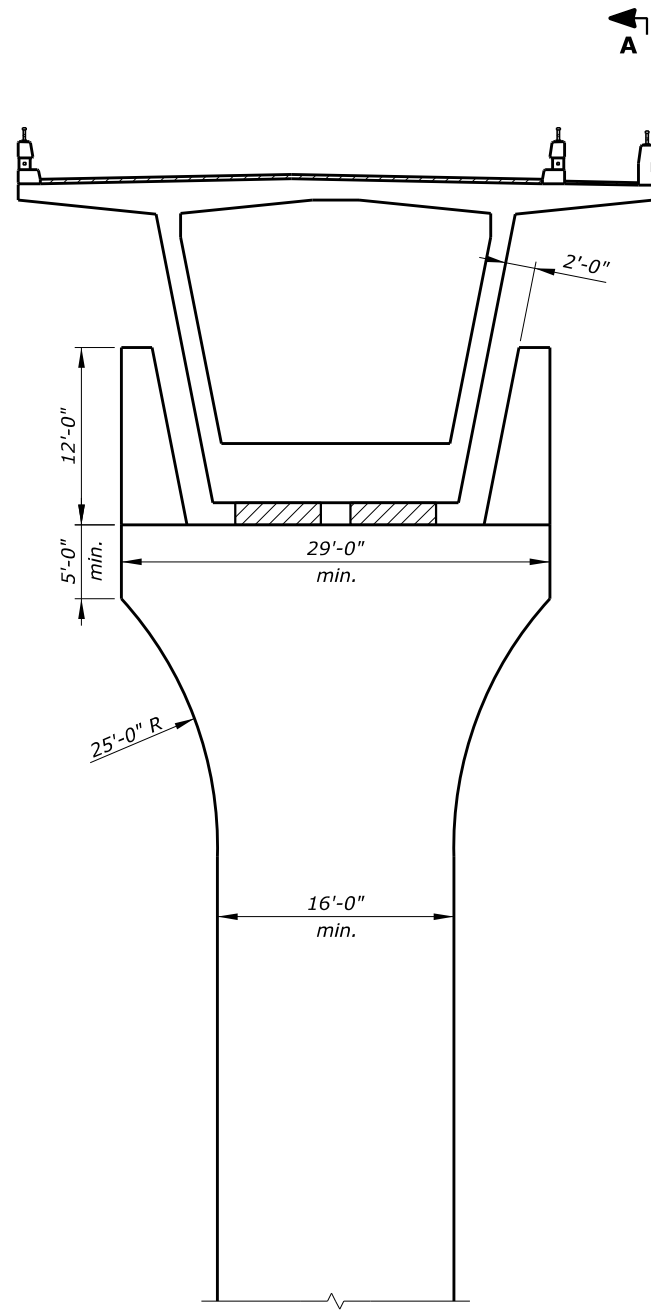
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$494,172,000
SCALE: No Scale DATE: March 2023

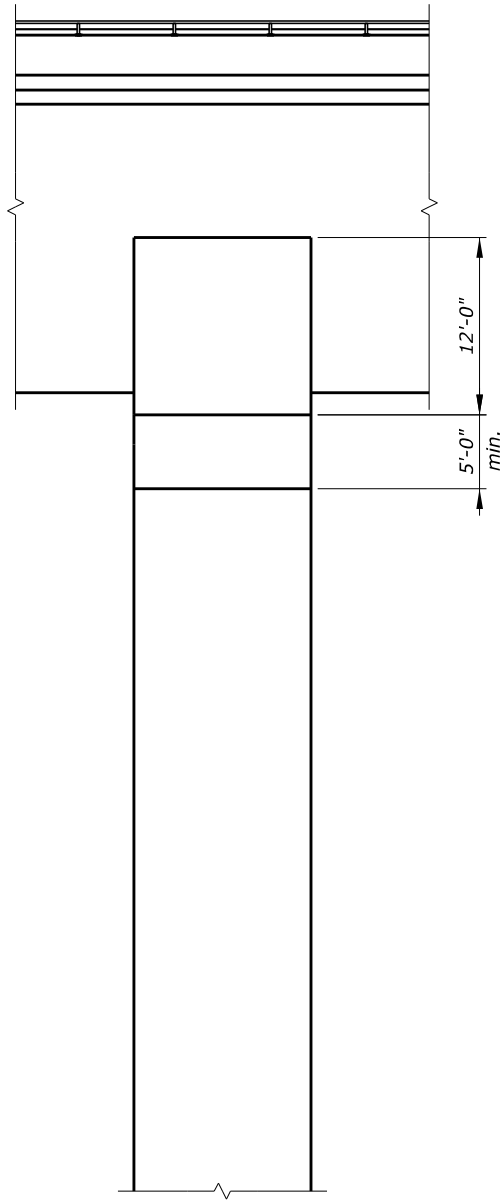
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SOUTH ALIGNMENT) PAGE 7 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 1 - Segmental Concrete Box Girder .DGN

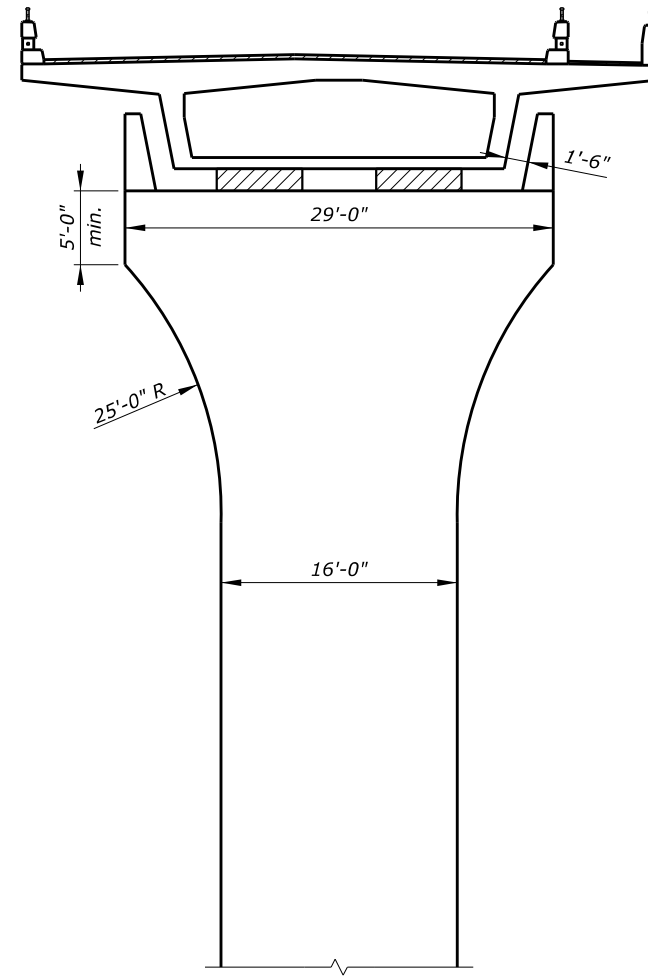
3/28/2023



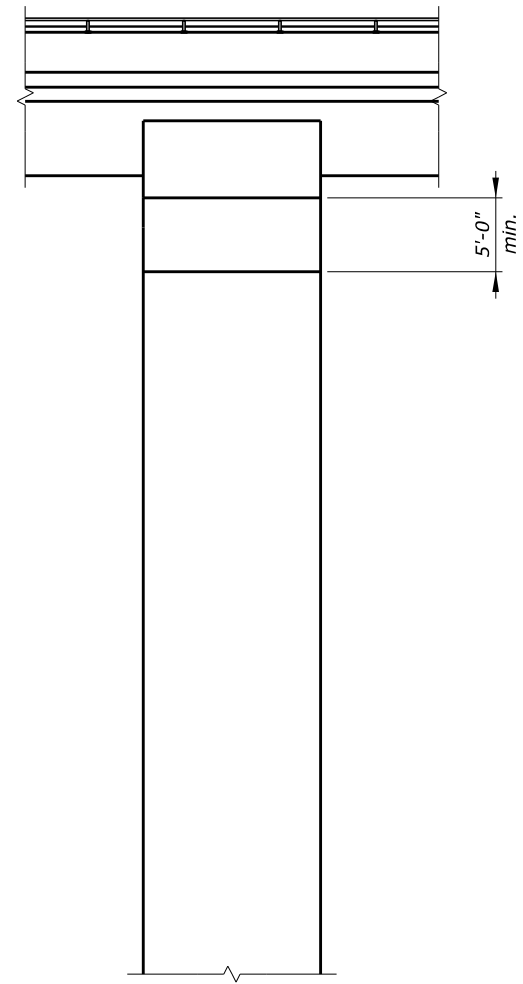
TYPICAL PIER SECTION
(Pier 10 and 11)
(Looking up station)



SECTION A-A



TYPICAL PIER SECTION
(Pier 9 and 12)
(Looking up station)



SECTION A-A

ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$494,172,000
SCALE: No Scale DATE: March 2023

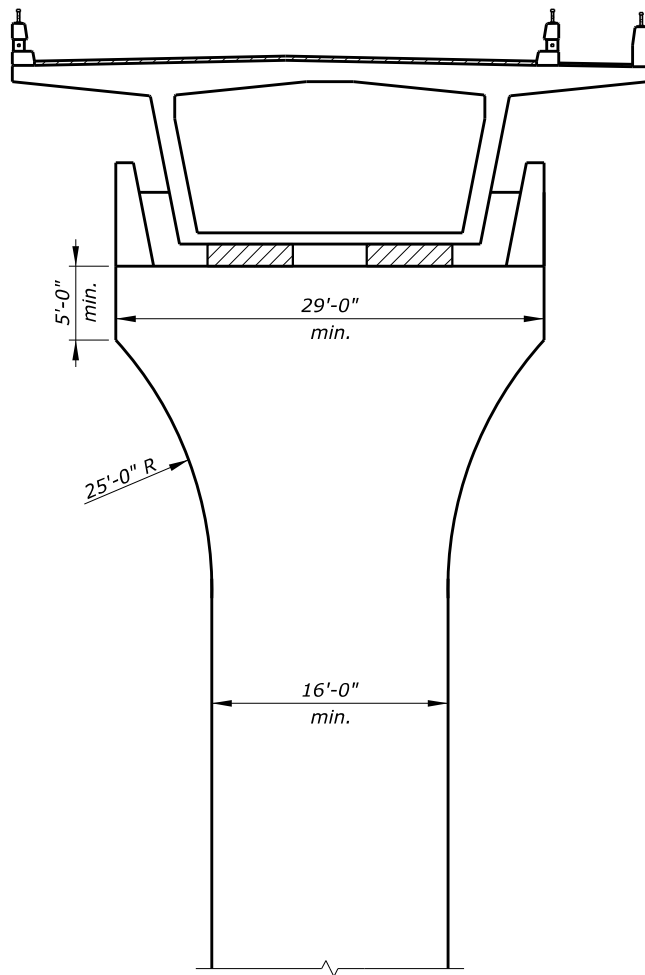
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SOUTH ALIGNMENT) PAGE 8 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 1 - Segmental Concrete Box Girder .DWG

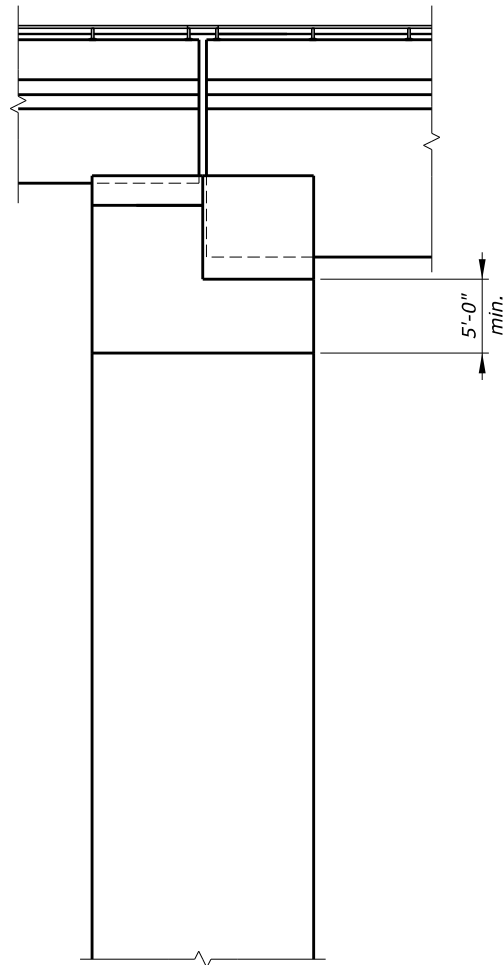
3/28/2023

A

A



TYPICAL PIER SECTION
(Pier 19 and 12)
(Looking up station)



SECTION A-A

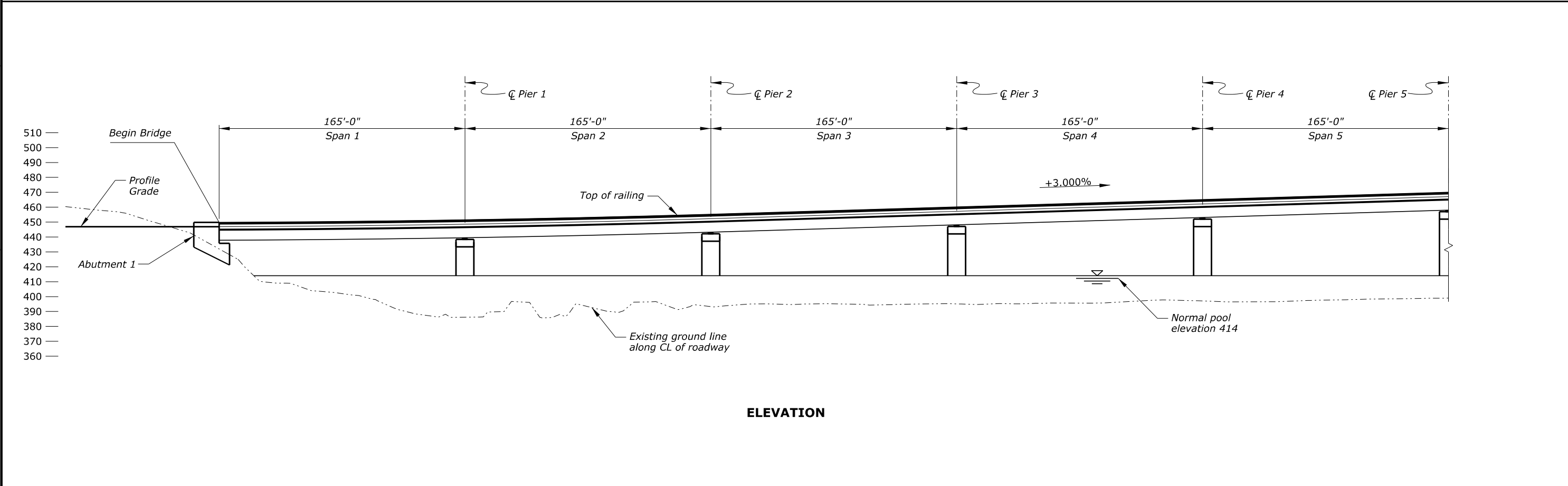
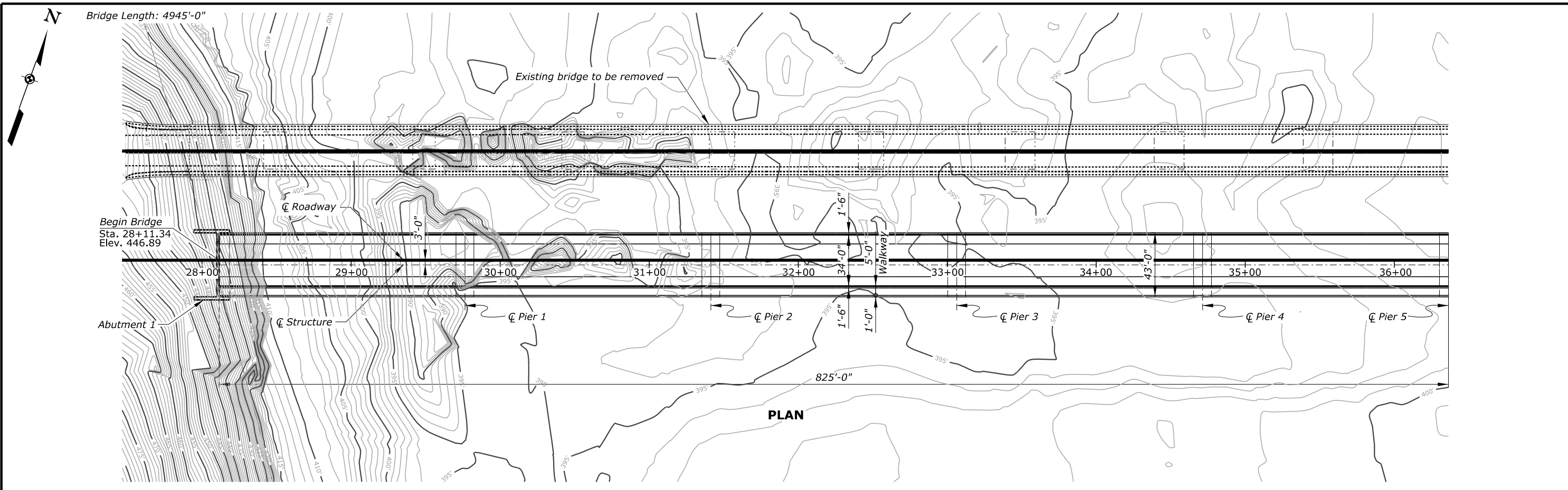
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$494,172,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SOUTH ALIGNMENT) PAGE 9 OF 9

M:\PROJECTS\5570-028P\Bridges\Microstation\T S< S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

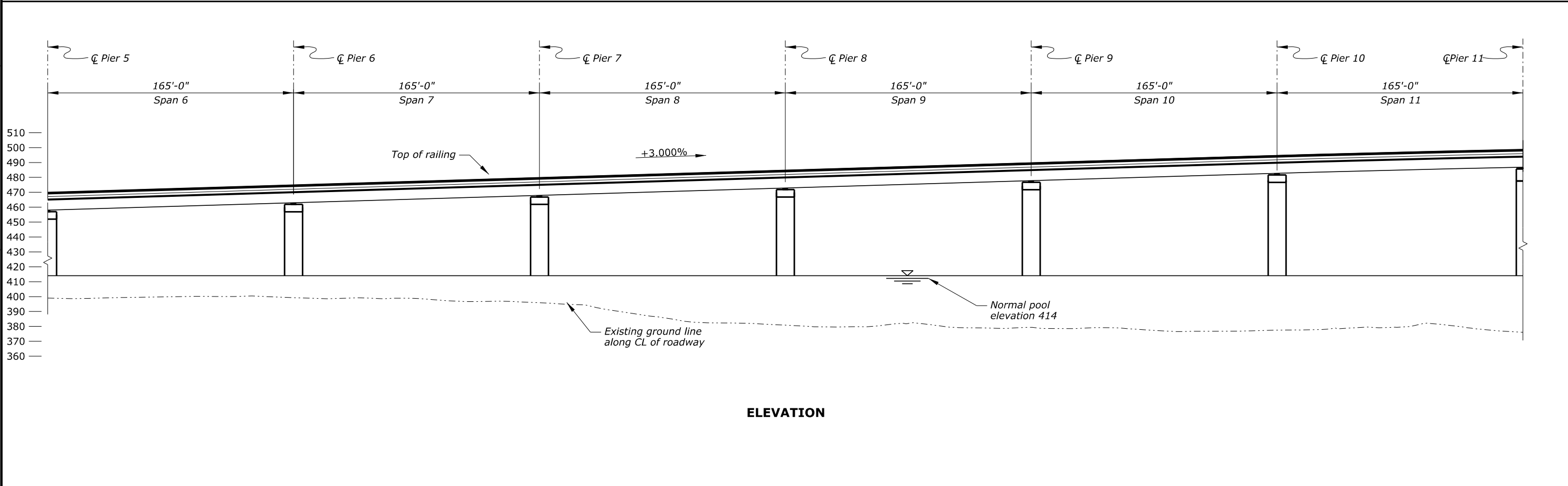
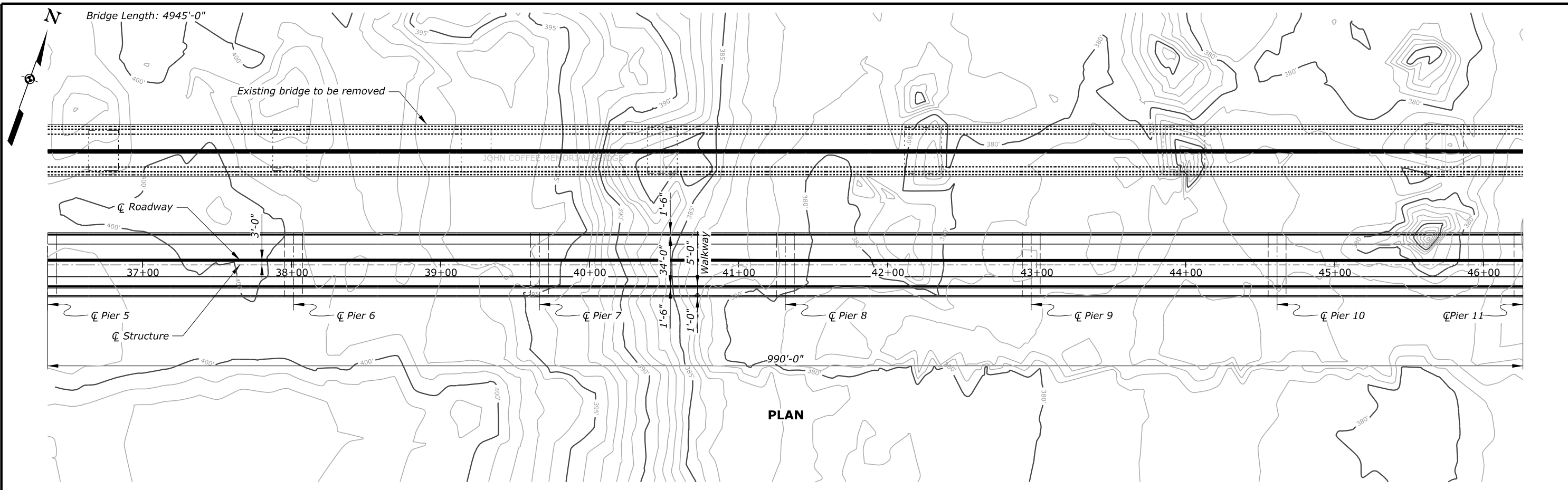
3/27/2023



ROUTE NAME AND NUMBER _____ Natchez Trace Parkway		FOREST/PARK/OTHER _____ Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION
STATE _____ Alabama	COUNTY _____ Lauderdale	STREAM NAME _____	Tennessee River	
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____	\$455,382,000	
PREPARED BY _____ Carlos J. Peña-Marsh / Nicole Rojas		SCALE: _____ No Scale	DATE: _____ March 2023	
				BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE
				ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT) PAGE 1 OF 9

M:\PROJECTS\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

3/27/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway
STATE	Alabama	COUNTY	Lauderdale
ACCOUNT NO.		STREAM NAME	Tennessee River
		ESTIMATED PROJECT COST	\$455,382,000
PREPARED BY	Carlos J. Peña-Marsh / Nicole Rojas	SCALE:	No Scale
		DATE:	March 2023

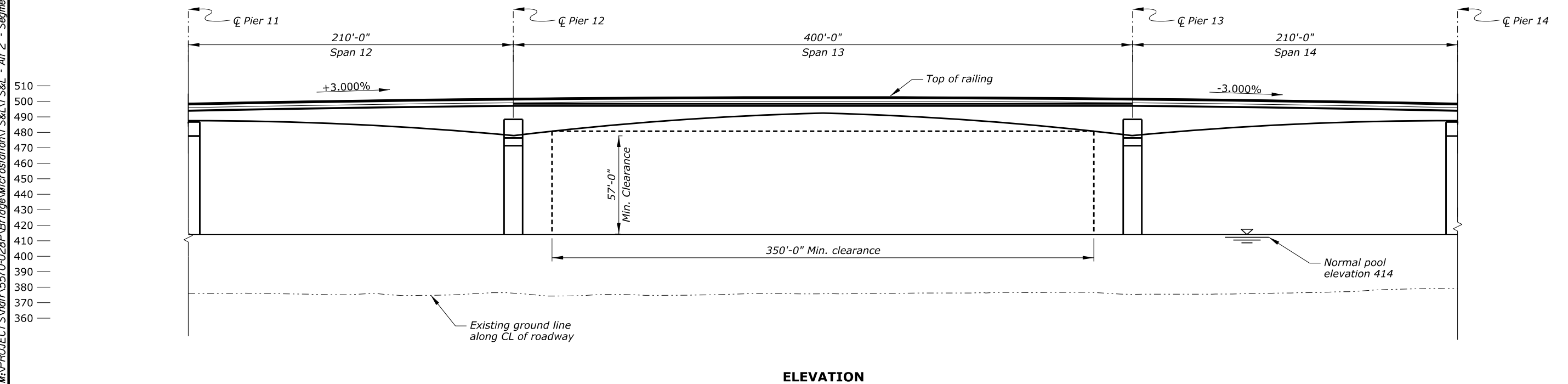
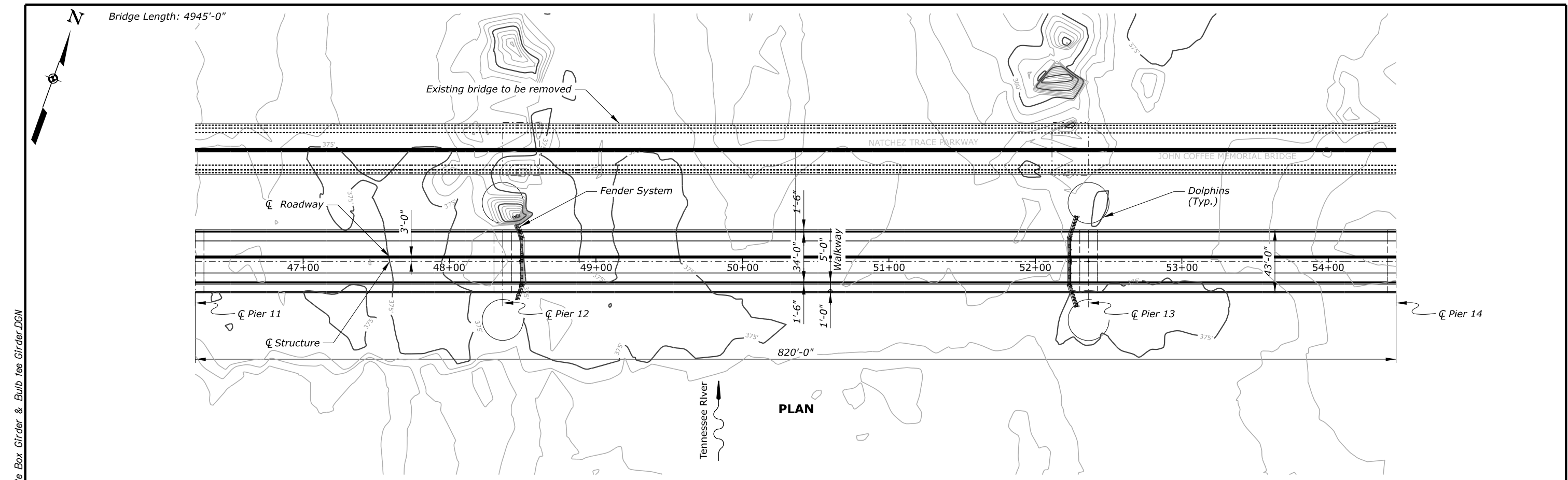
BRIDGE NAME

JOHN COFFEE MEMORIAL BRIDGE

ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

PAGE 2 OF 9



ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER _____ Natchez Trace Parkway

STREAM NAME _____ Tennessee River

ESTIMATED PROJECT COST _____ \$455,382,000

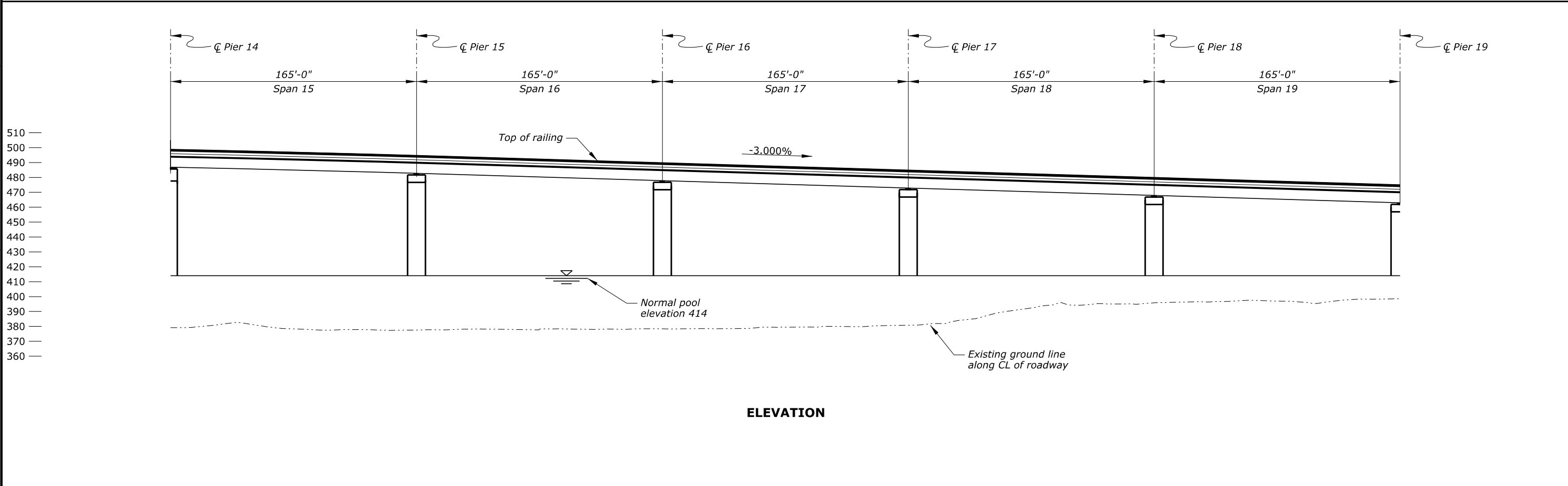
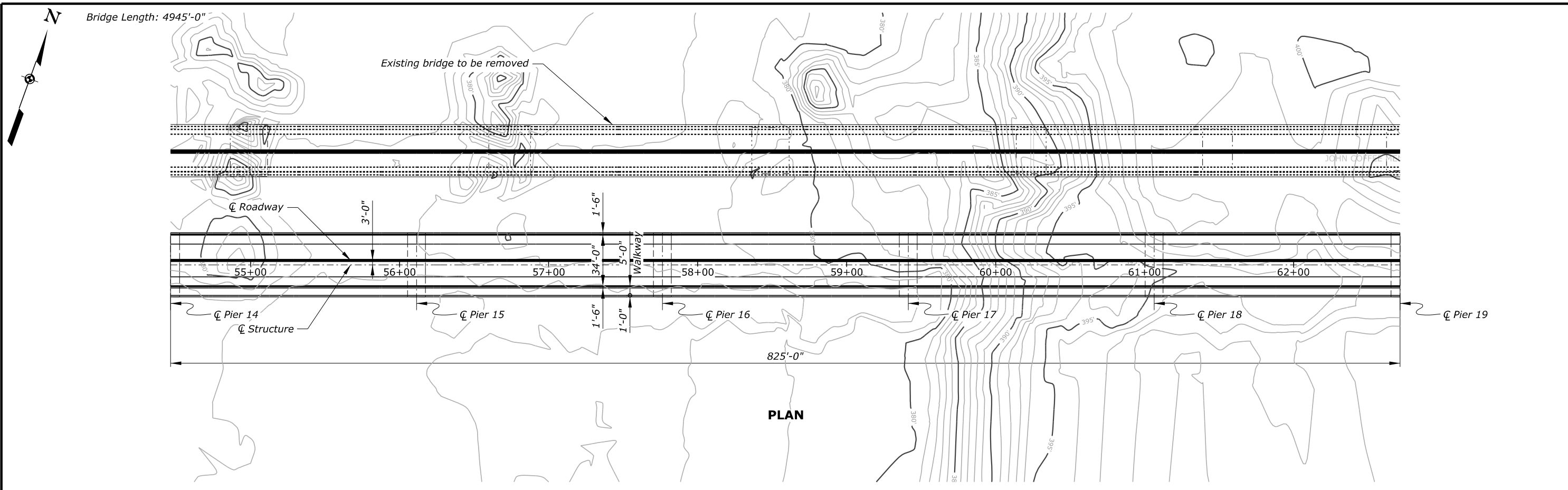
SCALE: _____ No Scale DATE: _____ March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)** PAGE 3 OF 9

M:\PROJECTS\15570-028P\Bridges\Microstation\T S&L\T S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

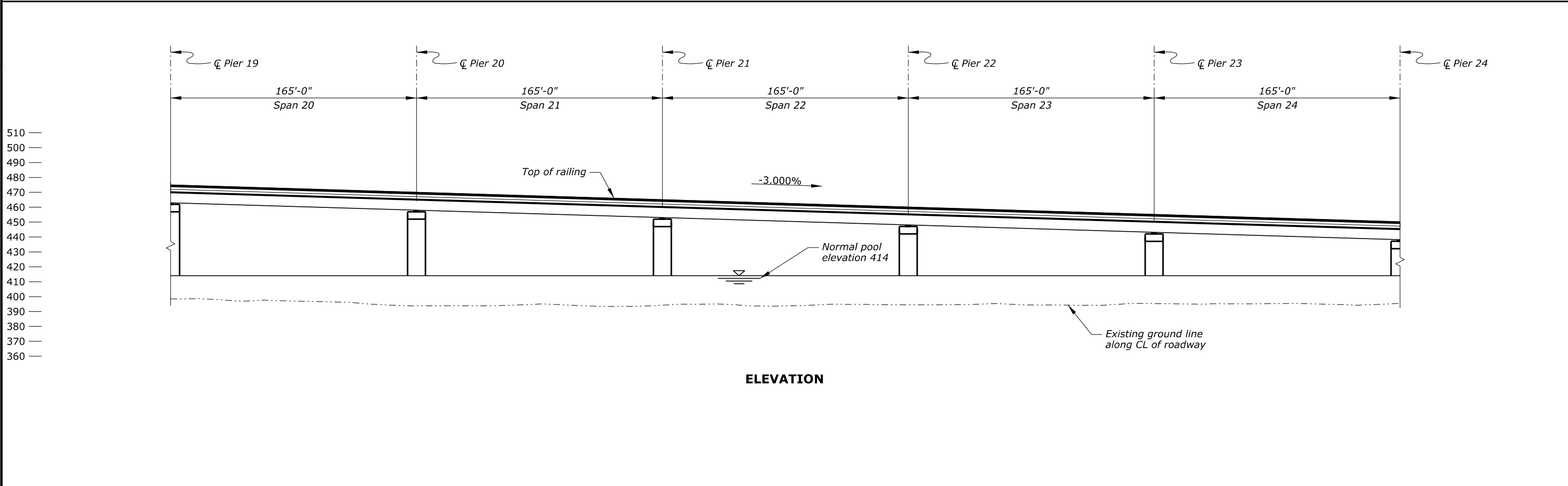
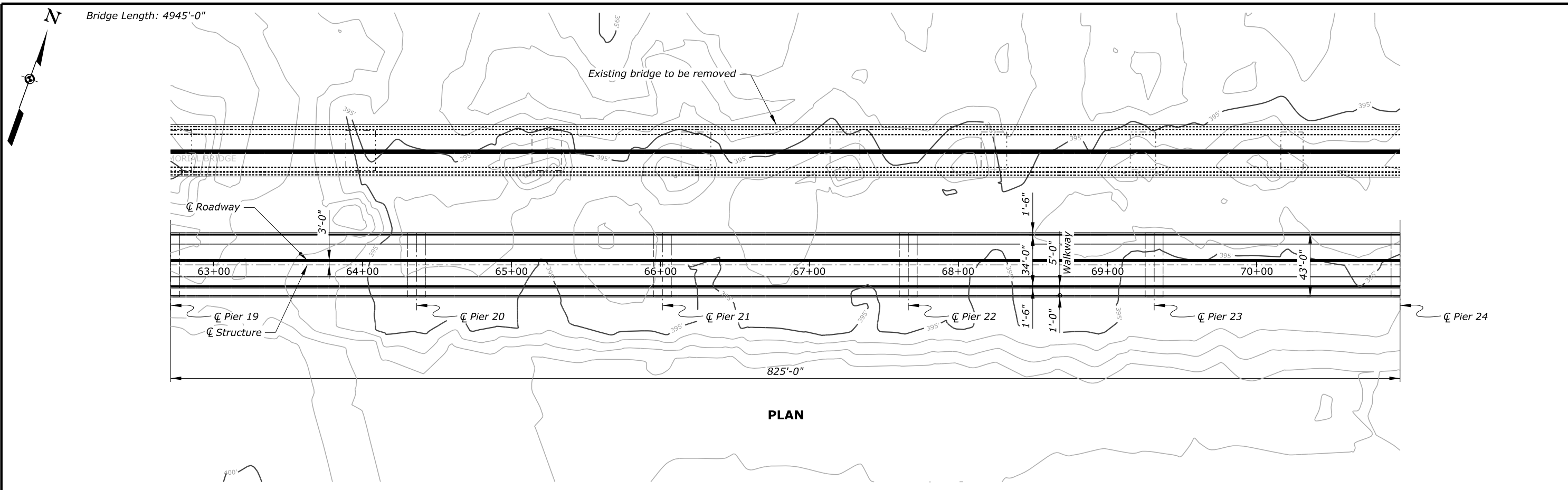
3/27/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)
STATE	Alabama	COUNTY	Lauderdale	
ACCOUNT NO.		ESTIMATED PROJECT COST	\$455,382,000	
PREPARED BY	Carlos J. Peña-Marsh / Nicole Rojas	SCALE:	No Scale	
		DATE:	March 2023	PAGE 4 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

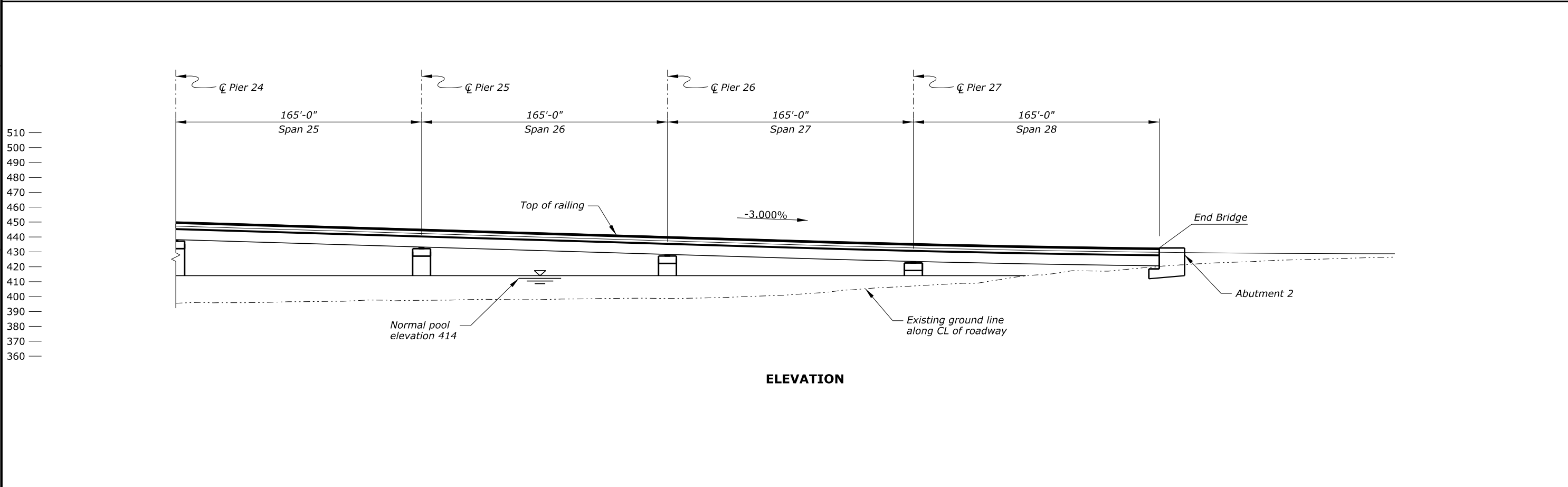
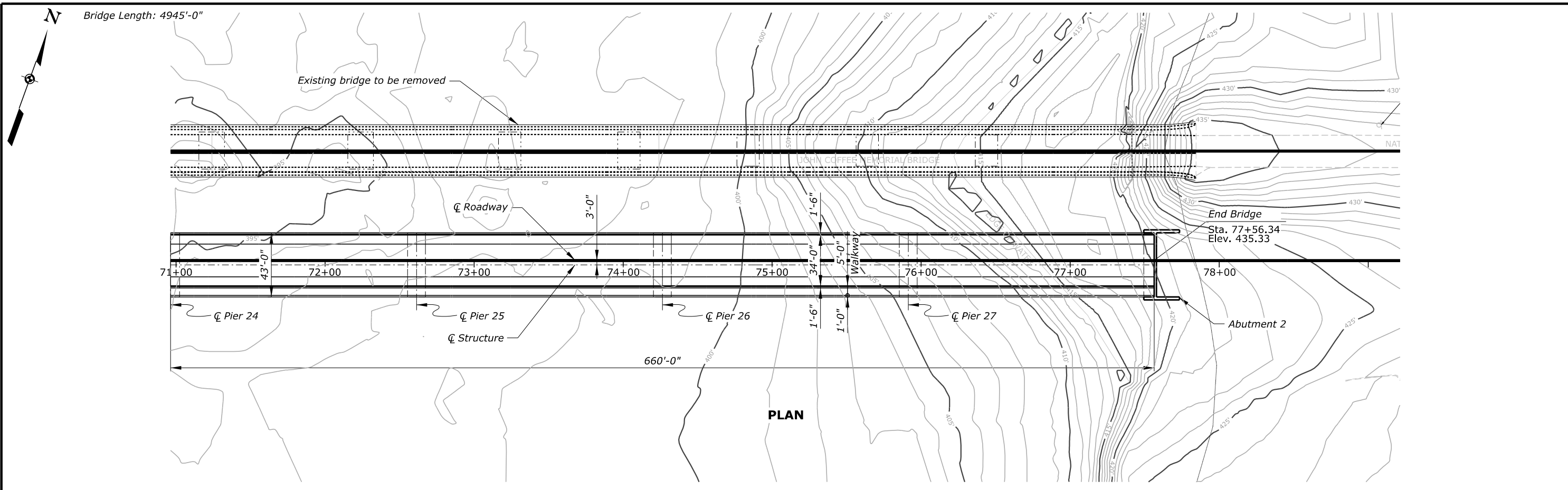
3/27/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT) PAGE 5 OF 9
STATE	Alabama	COUNTY	Lauderdale	
ACCOUNT NO.				
PREPARED BY	Carlos J. Peña-Marsh / Nicole Rojas	SCALE:	No Scale	
		DATE:	March 2023	

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

3/27/2023



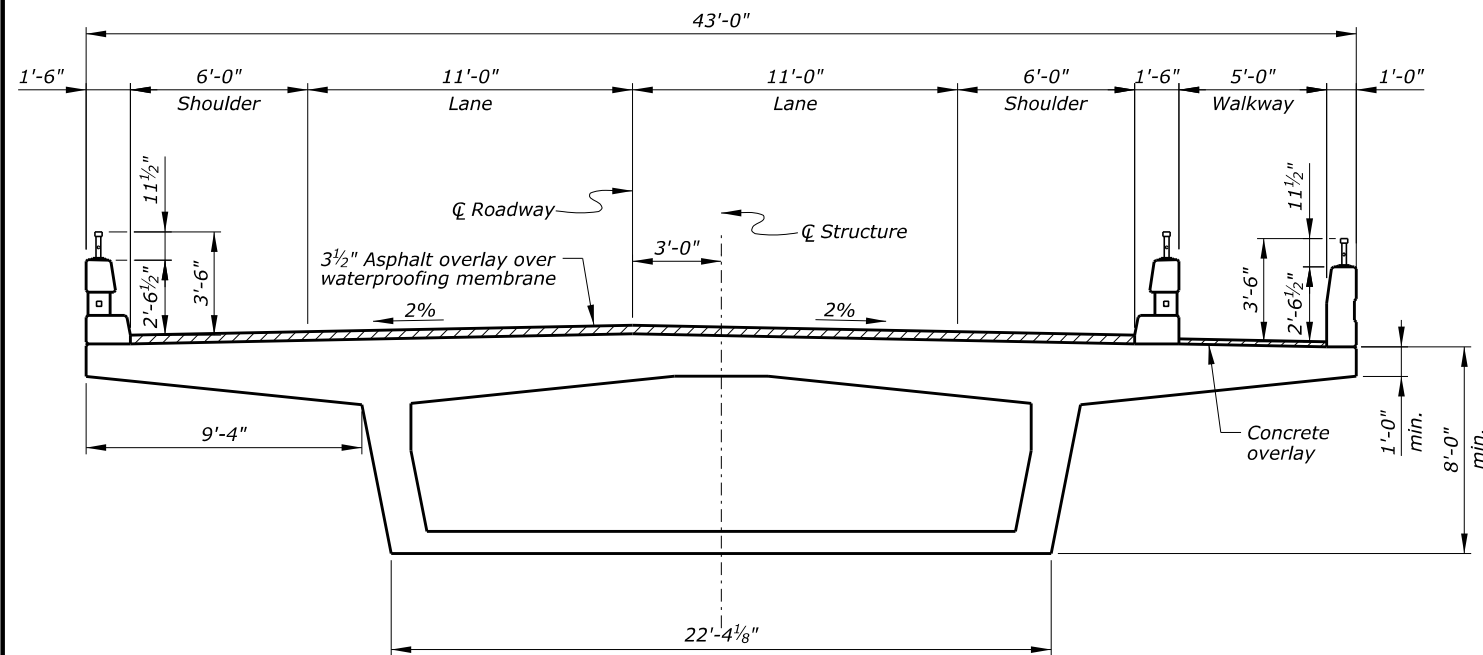
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$455,382,000
SCALE: No Scale DATE: March 2023

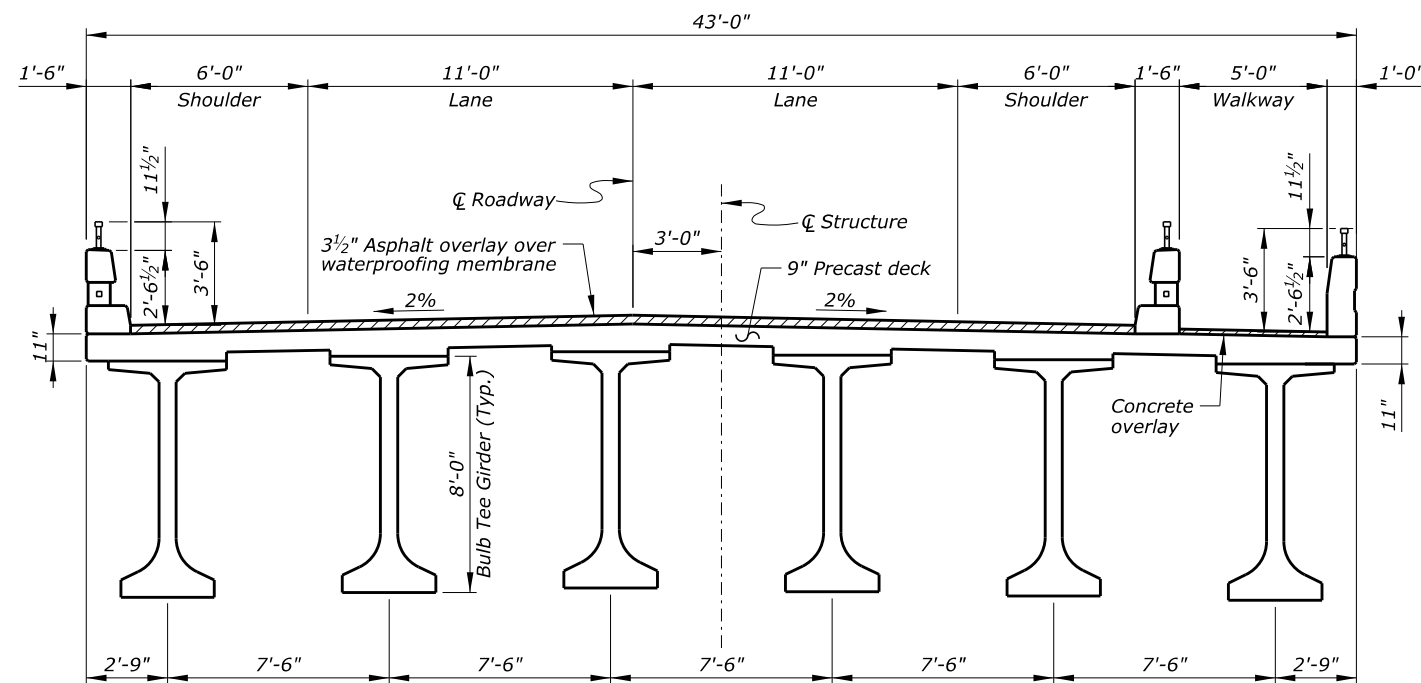
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)** PAGE 6 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\T S< S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

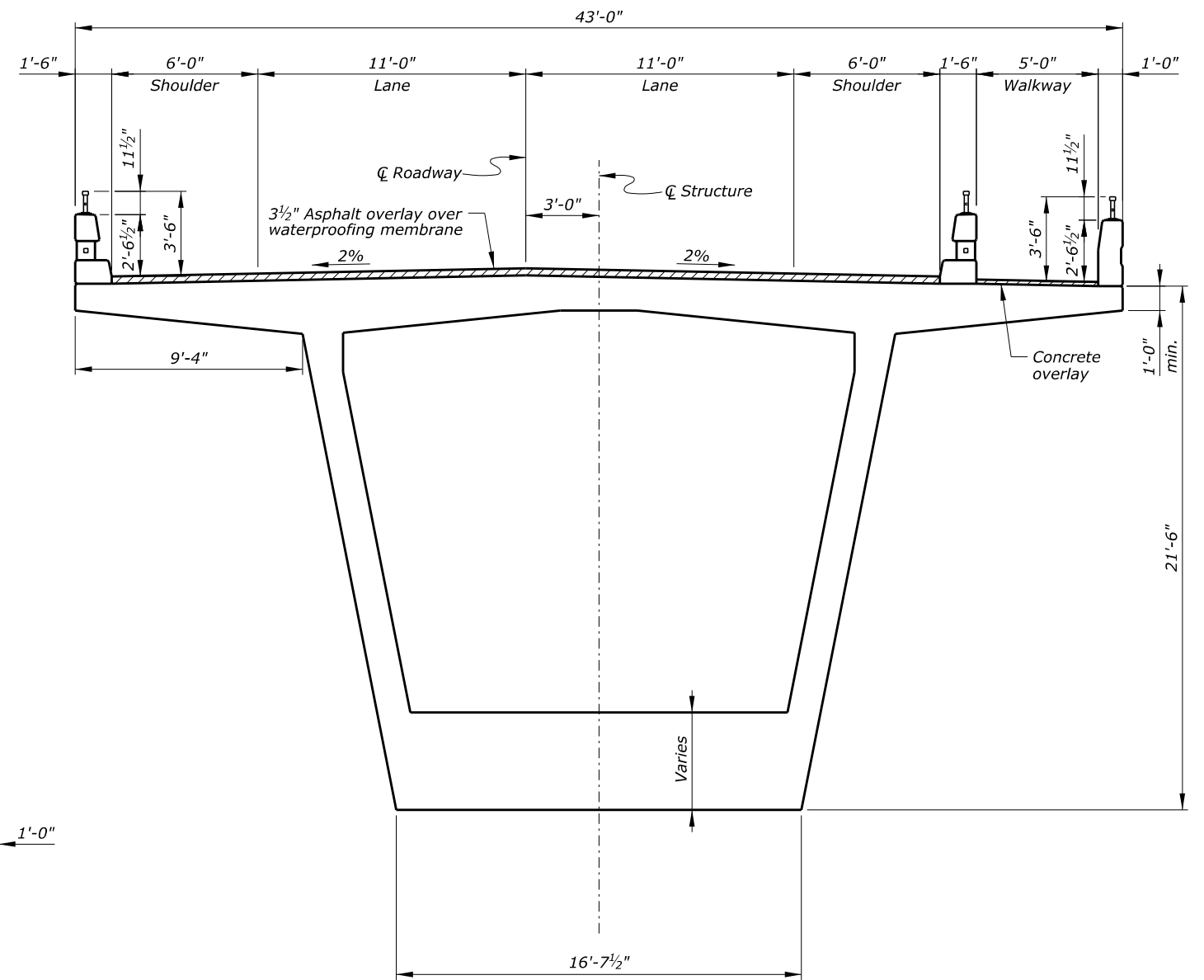
3/27/2023



TYPICAL SECTION
(Near midspan of span 13; Near pier 11 and 14)



TYPICAL SECTION
(Spans 1 - 11 and Spans 15 - 28)



TYPICAL SECTION
(Near pier 12 and 13)

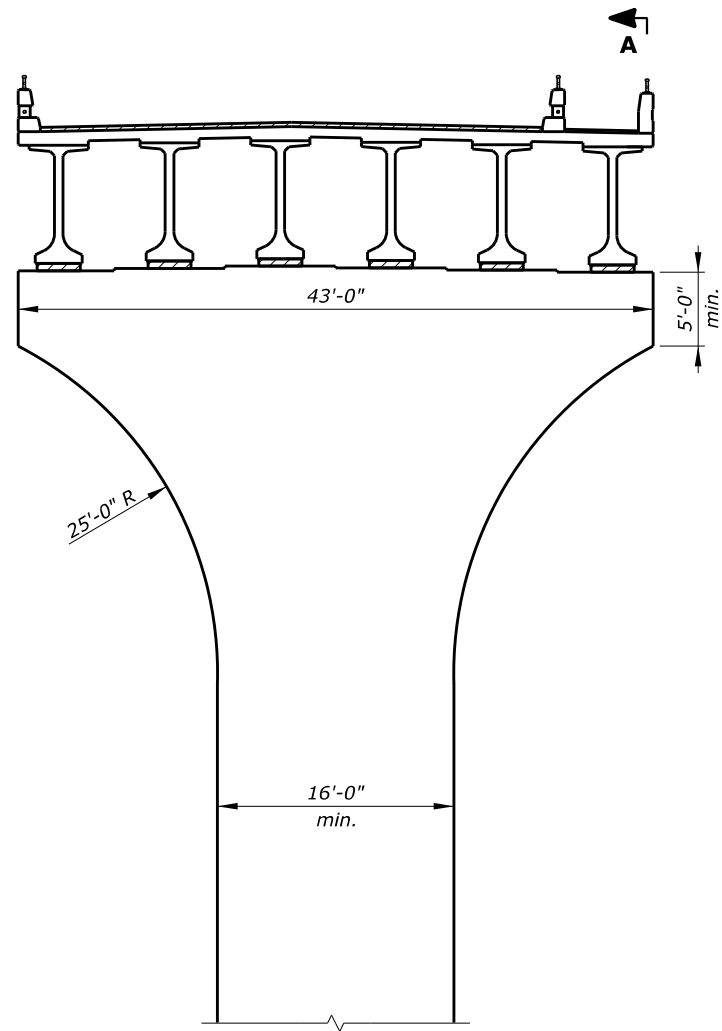
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$455,382,000
SCALE: No Scale DATE: March 2023

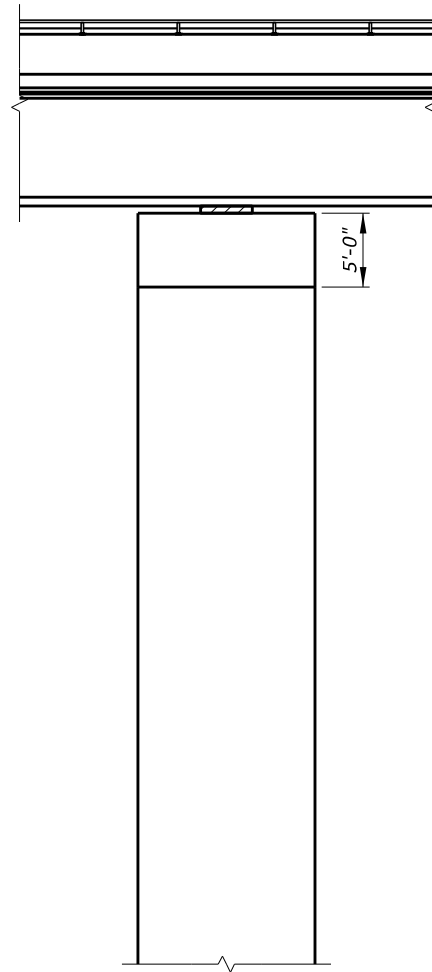
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)** PAGE 7 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\T S< S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN

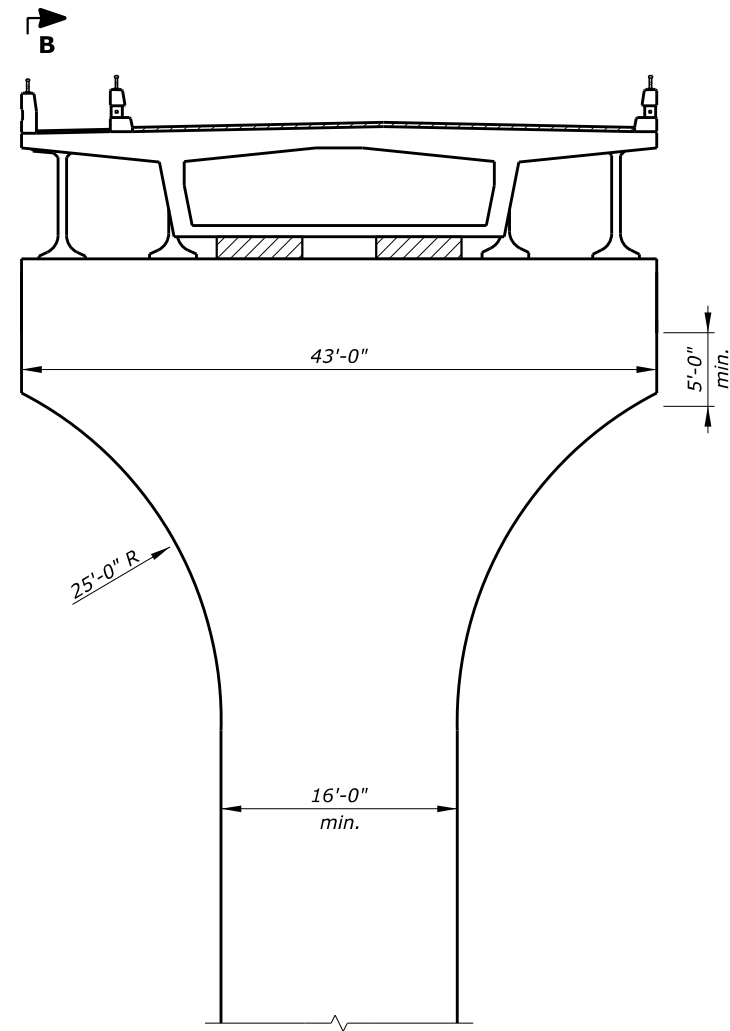
3/27/2023



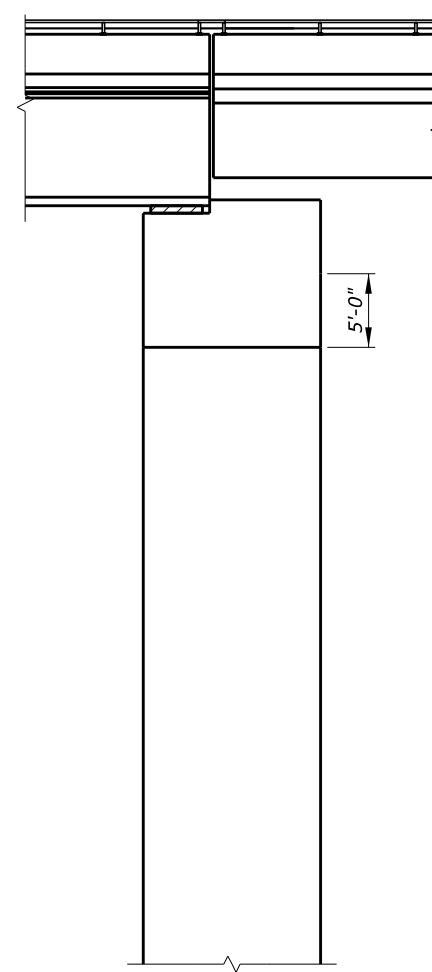
TYPICAL PIER SECTION
(Pier 1 - 10 and 15 - 27)
(Looking up station)



SECTION A-A



TYPICAL PIER SECTION
(Pier 11)
(Looking down station)
(Pier 14 Similar)



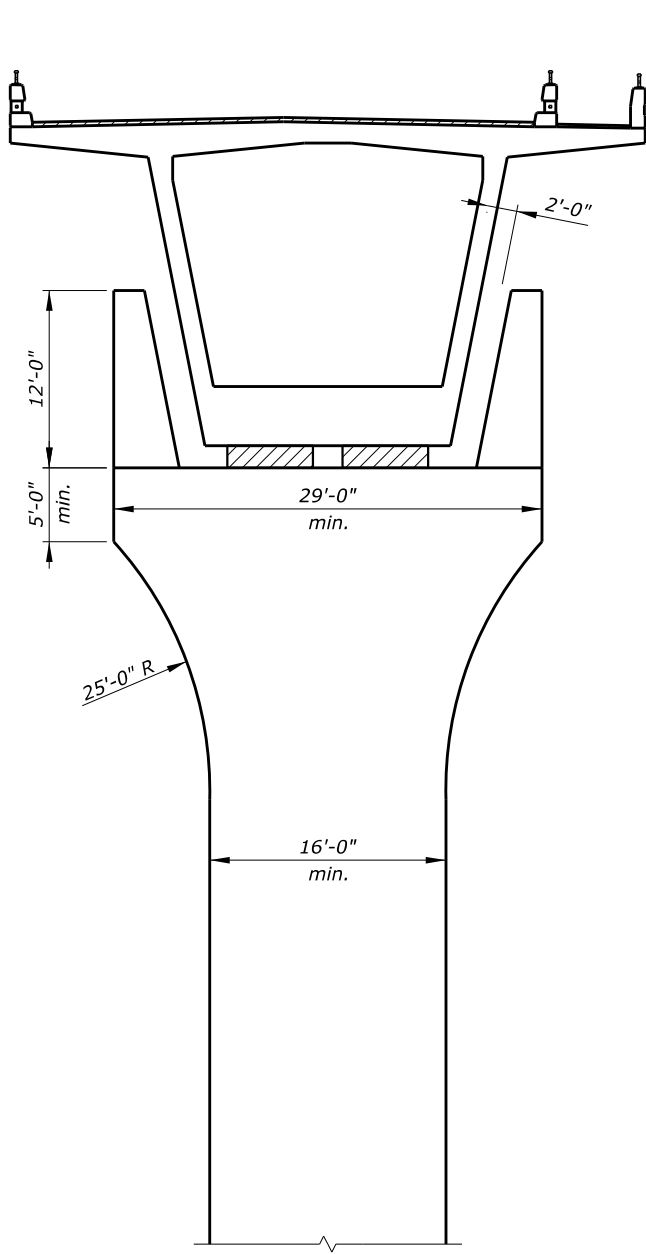
SECTION B-B

ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

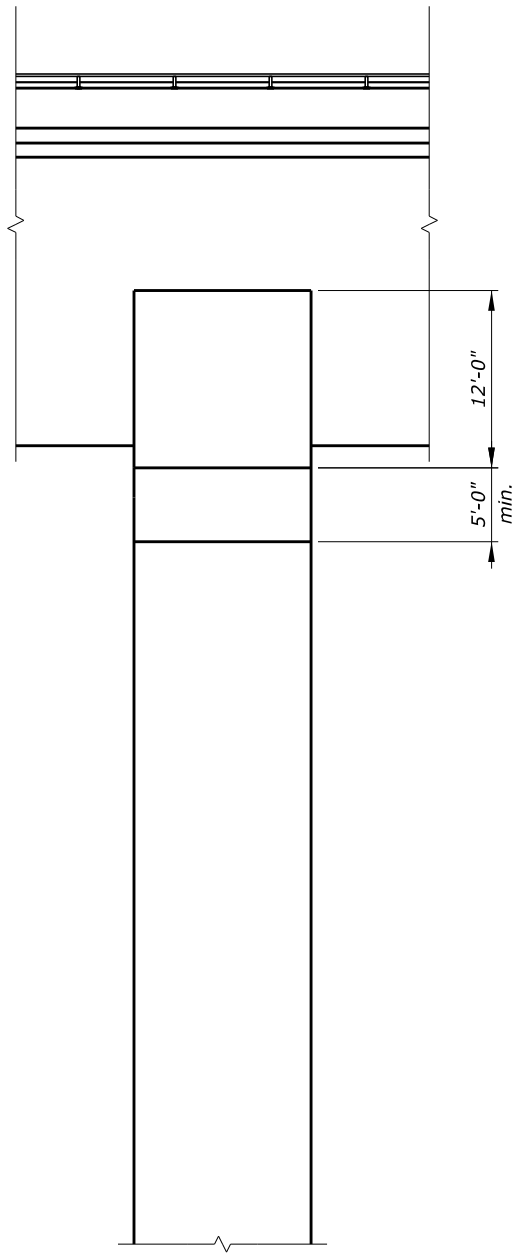
FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$455,382,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)** PAGE 8 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 2 - Segmental Concrete Box Girder & Bulb Tee Girder.DGN



TYPICAL PIER SECTION
(Pier 12 and 13)
(Looking up station)



SECTION A-A

3/27/2023

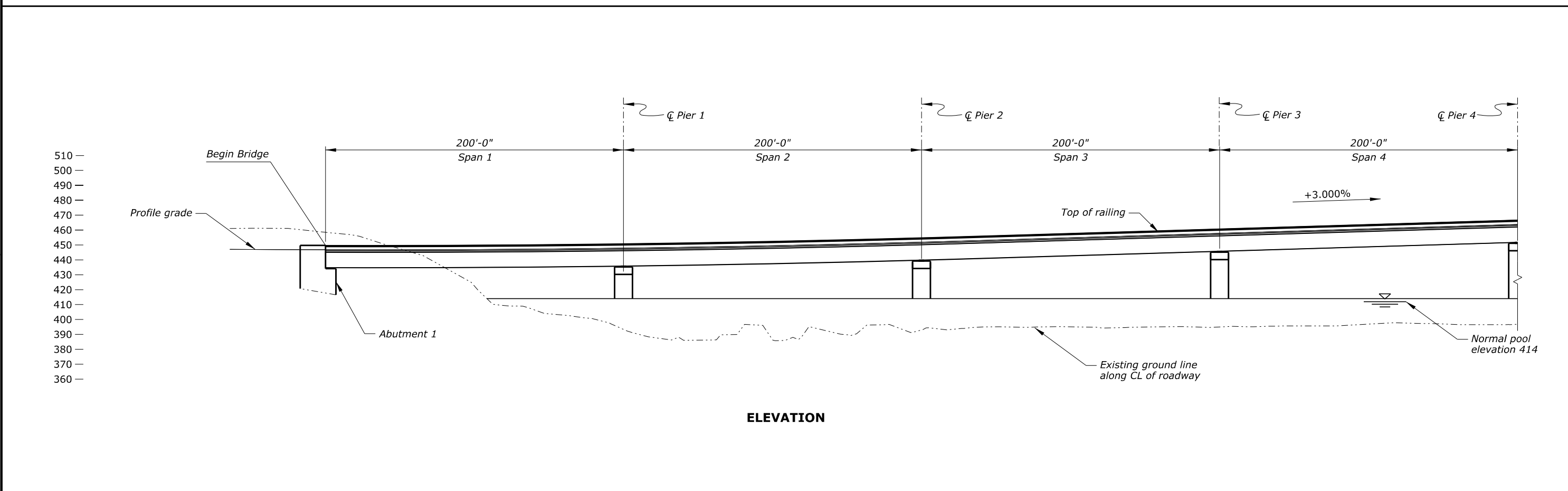
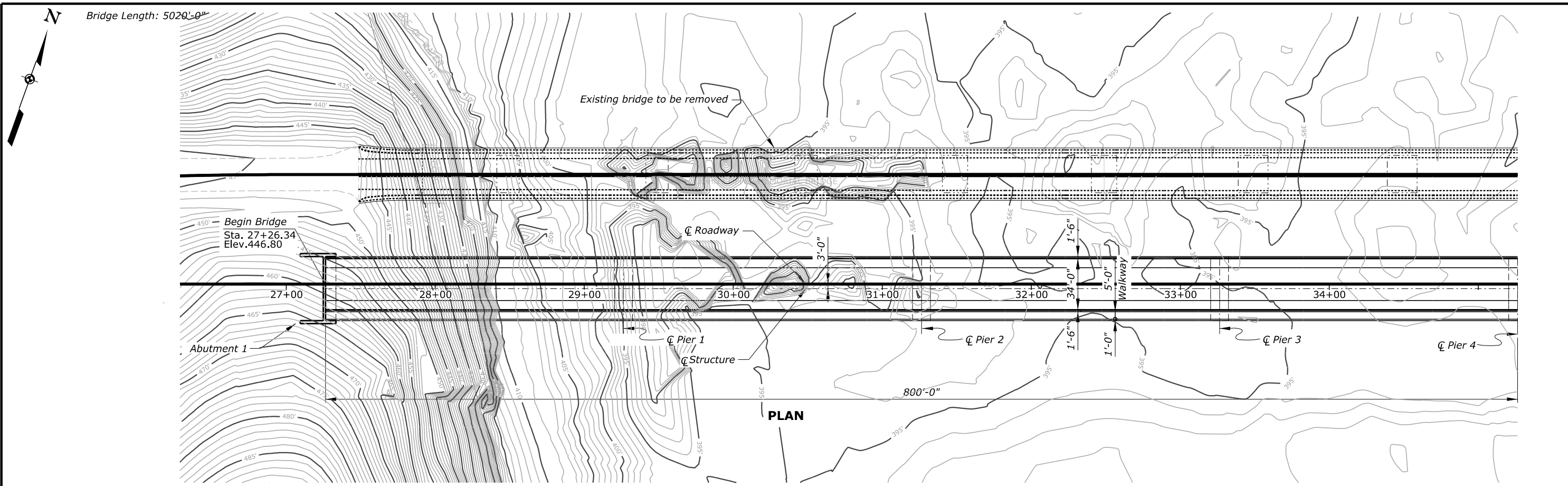
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$455,382,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SOUTH ALIGNMENT)** PAGE 9 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\T S< S&L - Alt 3 - Steel Plate Girder .DGN

3/27/2023

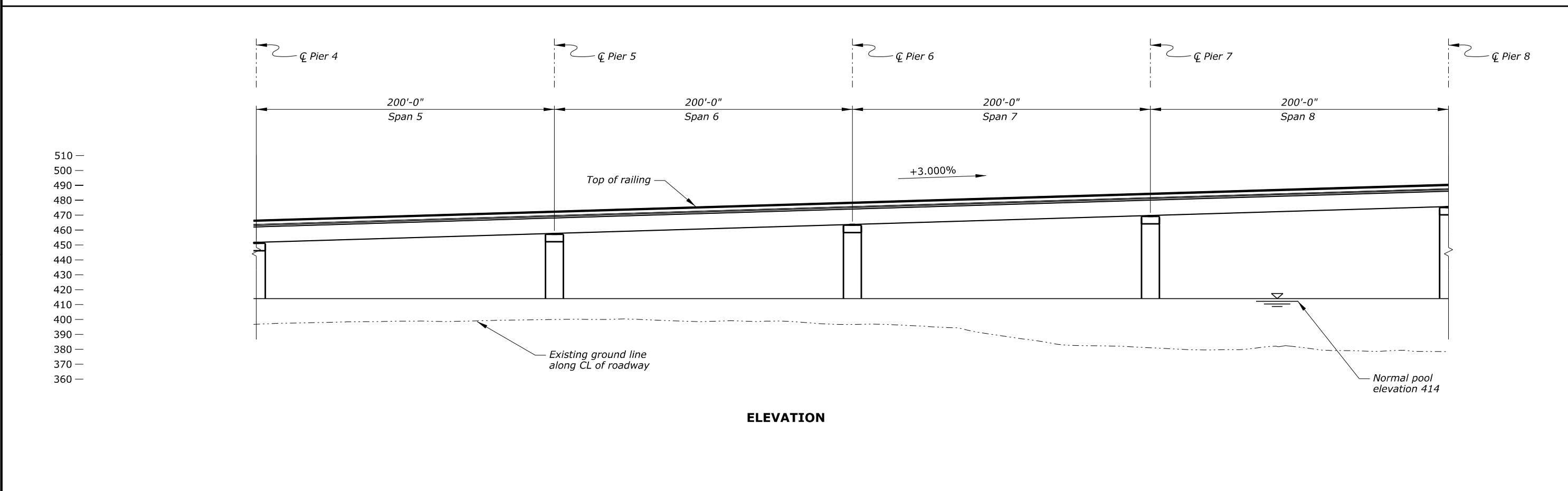
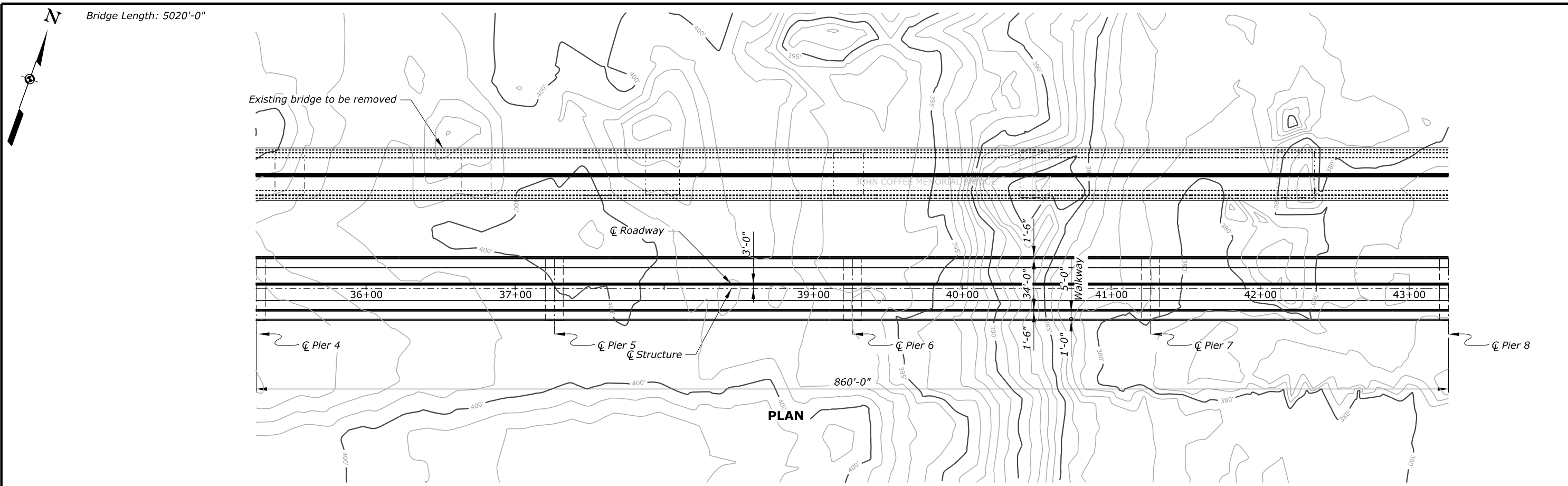


ROUTE NAME AND NUMBER _____ Natchez Trace Parkway		FOREST/PARK/OTHER _____ Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 3 STEEL PLATE GIRDER (SOUTH ALIGNMENT)	
STATE _____ Alabama	COUNTY _____ Lauderdale	STREAM NAME _____	Tennessee River		
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____ \$572,022,000			
PREPARED BY _____ Carlos J. Peña-Marsh / Nicole Rojas		SCALE: _____ No Scale	DATE: _____ March 2023		

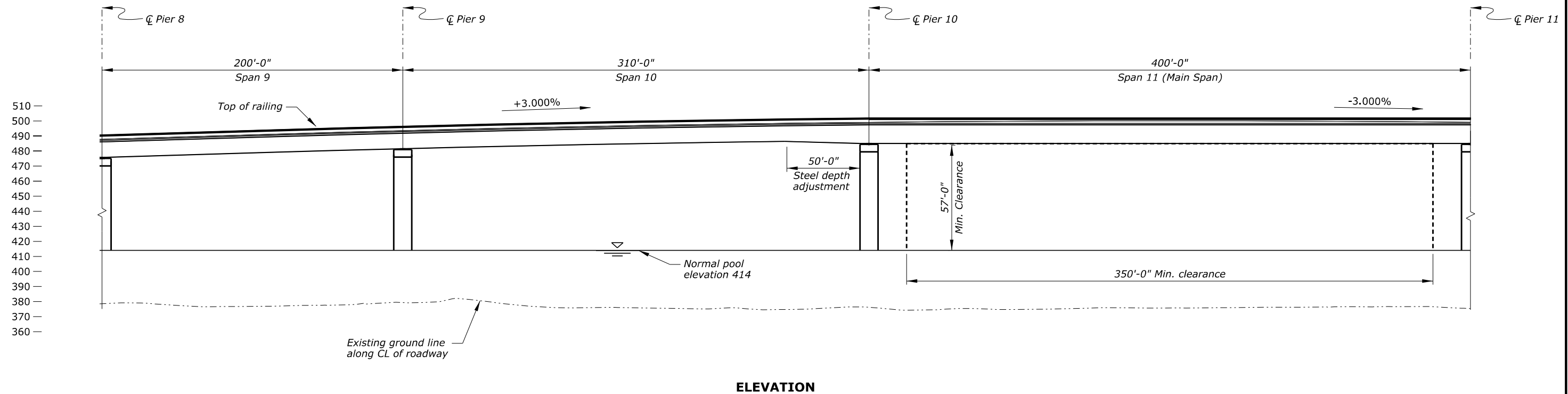
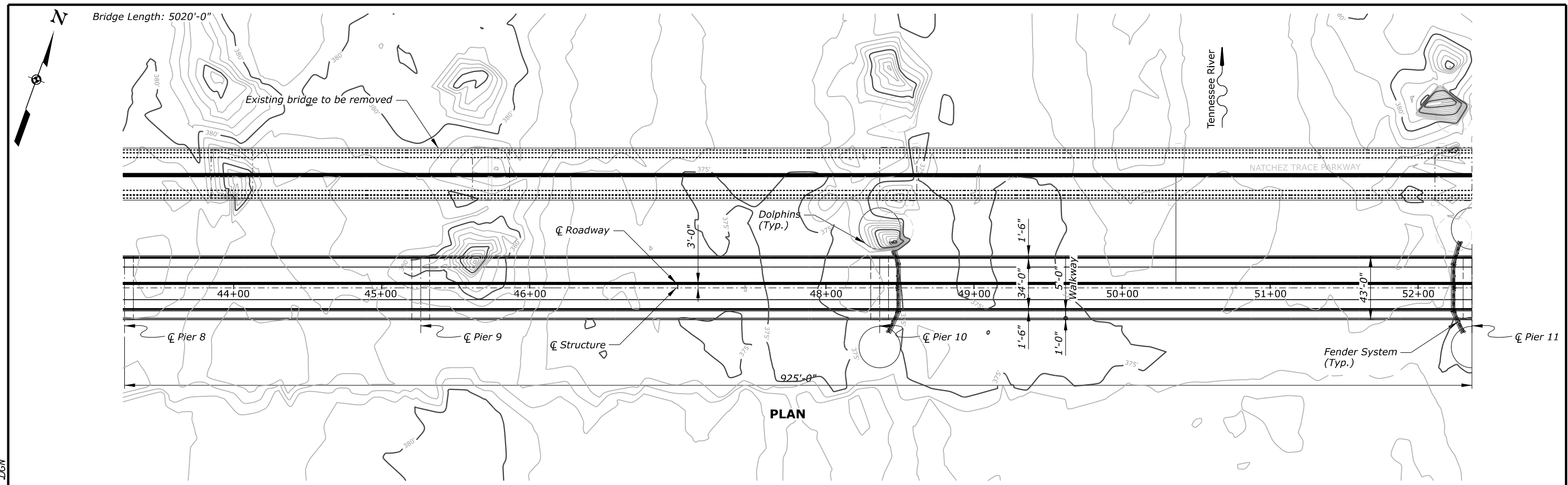
PAGE 1 OF 8

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 3 - Steel Plate Girder .DGN

3/27/2023



ROUTE NAME AND NUMBER STATE <u>Alabama</u> COUNTY <u>Lauderdale</u> ACCOUNT NO. _____ PREPARED BY <u>Carlos J. Peña-Marsh / Nicole Rojas</u>	FOREST/PARK/OTHER <u>Natchez Trace Parkway</u> STREAM NAME <u>Tennessee River</u> ESTIMATED PROJECT COST <u>\$572,022,000</u> SCALE: <u>No Scale</u>	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME <u>JOHN COFFEE MEMORIAL BRIDGE</u> ALTERNATIVE 3 STEEL PLATE GIRDER (SOUTH ALIGNMENT) PAGE 2 OF 8
---	---	---



ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway

STREAM NAME Tennessee River

ESTIMATED PROJECT COST \$572,022,000

SCALE: No Scale DATE: March 2023

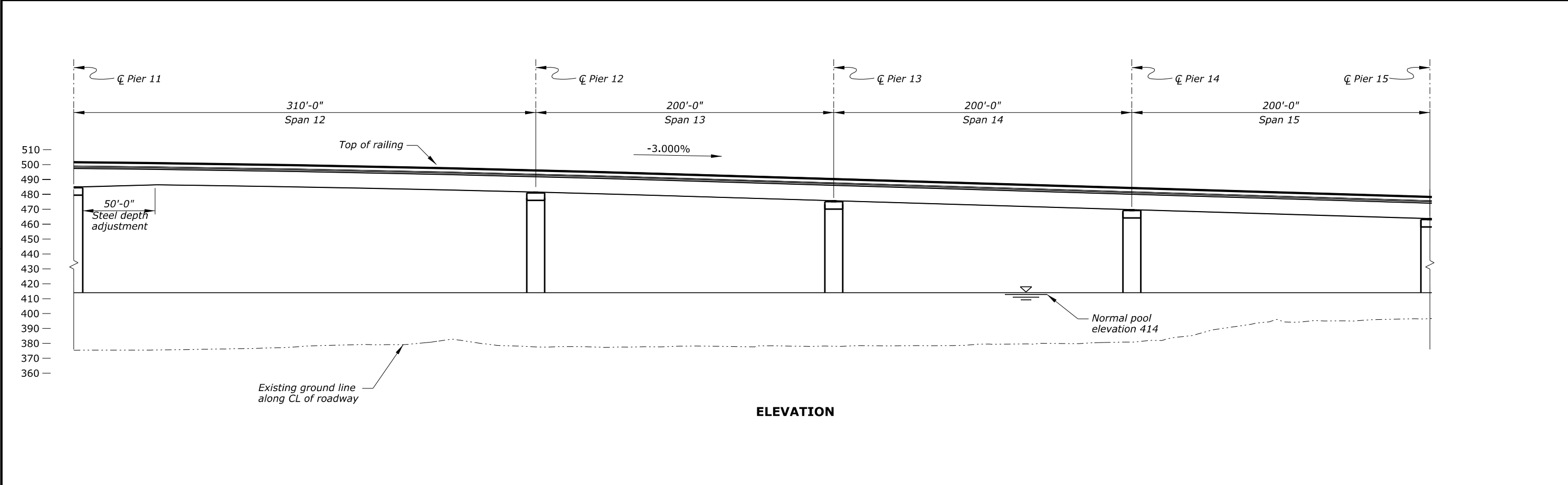
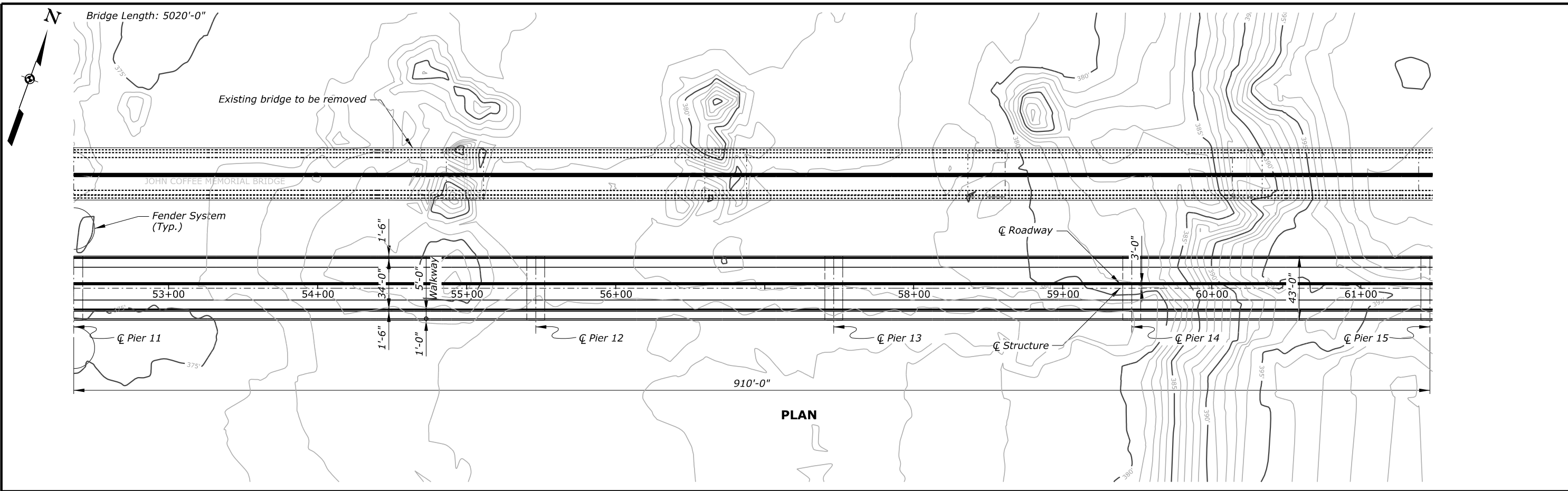
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 3 STEEL PLATE GIRDER
(SOUTH ALIGNMENT)**

PAGE 3 OF 8

M:\PROJECTS\5570-028P\Bridges\Microstation\T S< S&L - Alt 3 - Steel Plate Girder .DGN

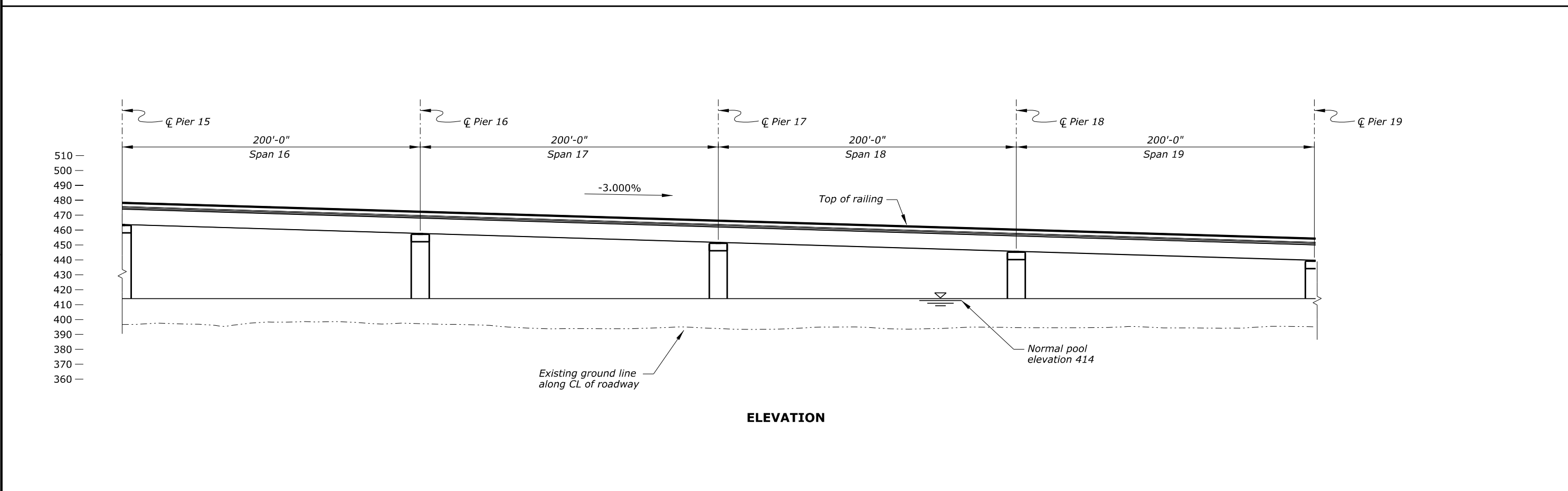
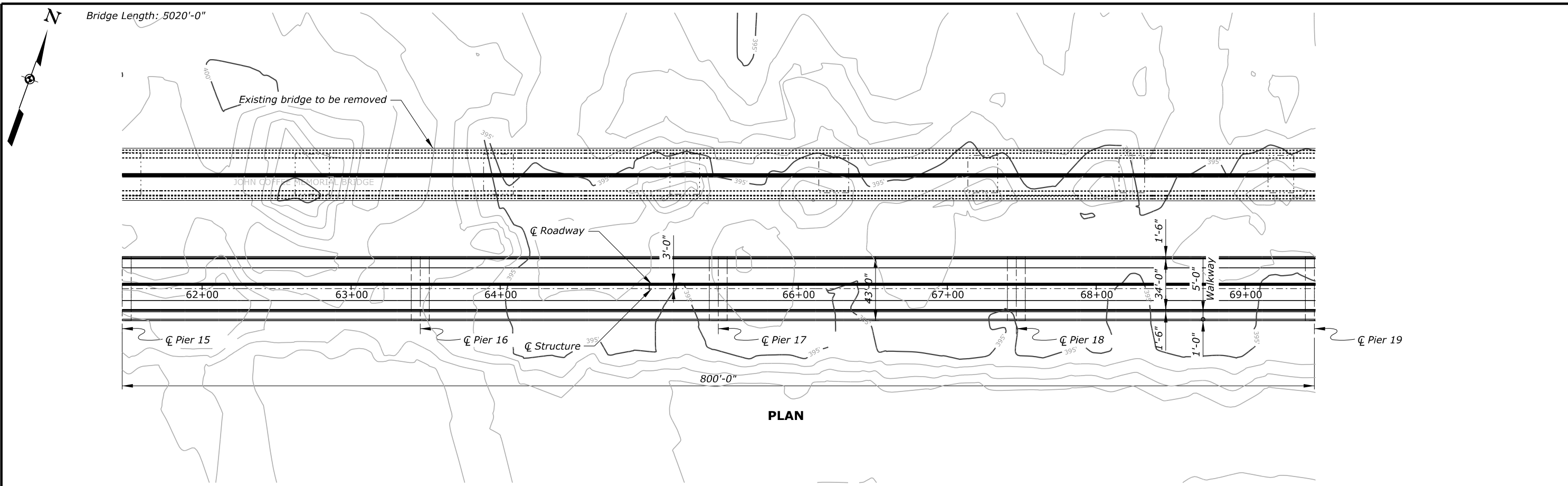
3/27/2023



ROUTE NAME AND NUMBER STATE <u>Alabama</u> COUNTY <u>Lauderdale</u>	FOREST/PARK/OTHER <u>Natchez Trace Parkway</u> STREAM NAME <u>Tennessee River</u> ESTIMATED PROJECT COST <u>\$572,022,000</u>	BRIDGE NAME <u>JOHN COFFEE MEMORIAL BRIDGE</u> ALTERNATIVE 3 STEEL PLATE GIRDER (SOUTH ALIGNMENT)
ACCOUNT NO. _____	SCALE: <u>No Scale</u> DATE: <u>March 2023</u>	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION PAGE 4 OF 8
PREPARED BY <u>Carlos J. Peña-Marsh / Nicole Rojas</u>		

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< S&L - Alt 3 - Steel Plate Girder .DGN

3/27/2023

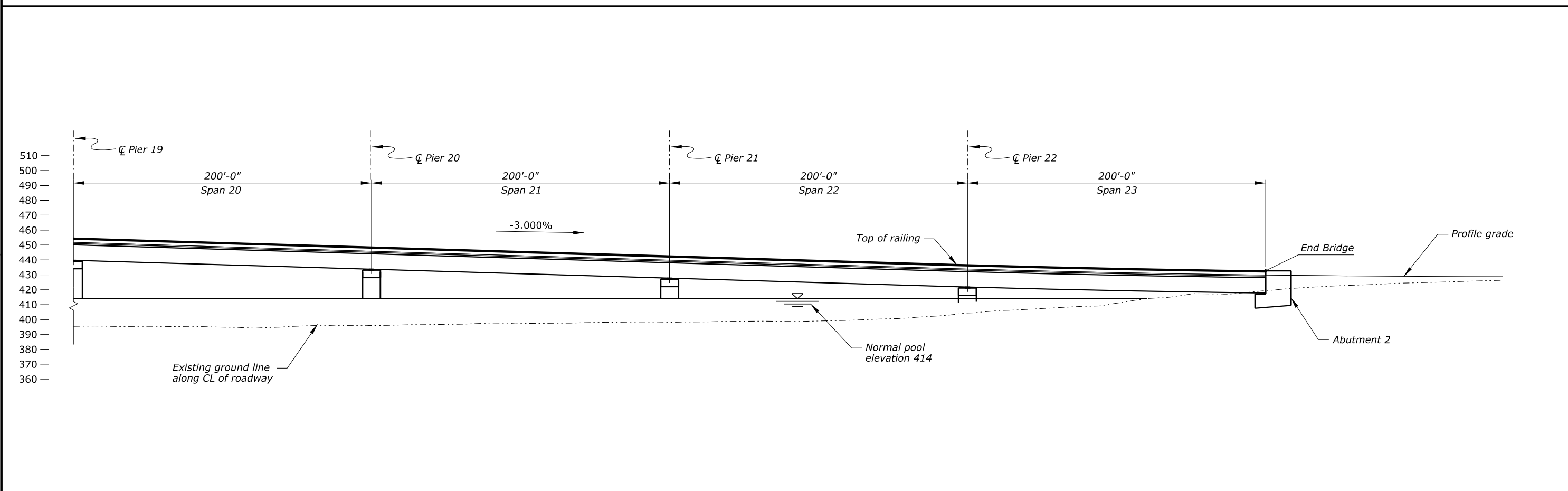
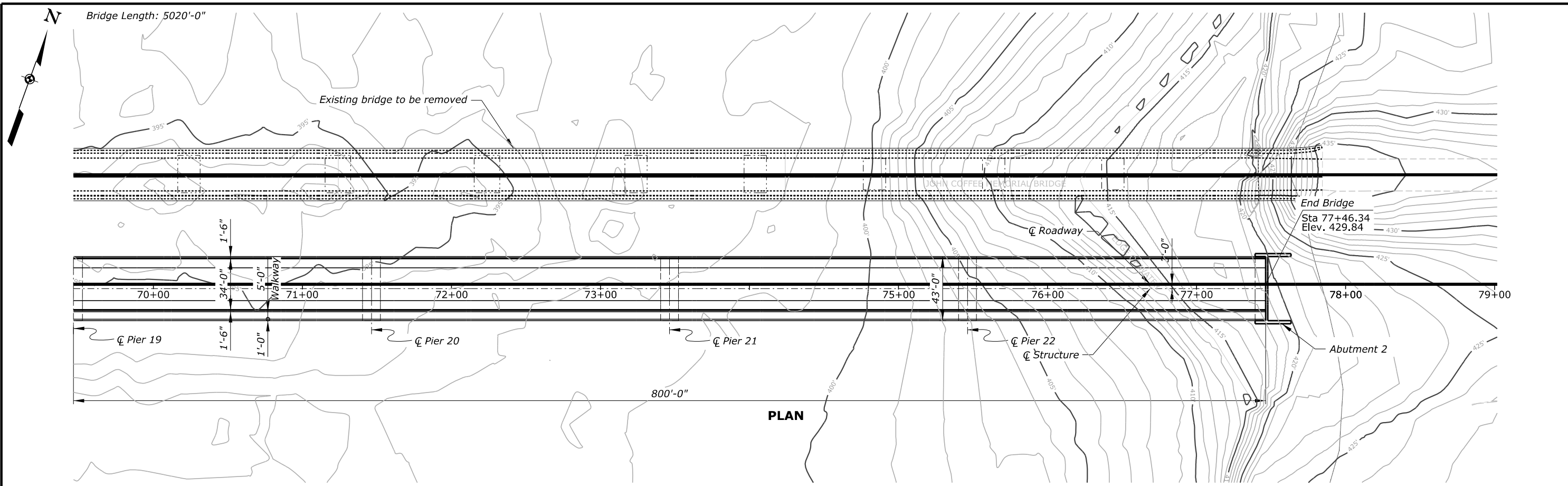


ROUTE NAME AND NUMBER <u>Natchez Trace Parkway</u>		FOREST/PARK/OTHER <u>Natchez Trace Parkway</u>	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME <u>JOHN COFFEE MEMORIAL BRIDGE</u> ALTERNATIVE 3 STEEL PLATE GIRDER (SOUTH ALIGNMENT)
STATE <u>Alabama</u> COUNTY <u>Lauderdale</u>		STREAM NAME <u>Tennessee River</u>	
ACCOUNT NO. _____		ESTIMATED PROJECT COST <u>\$572,022,000</u>	
PREPARED BY <u>Carlos J. Peña-Marsh / Nicole Rojas</u>		SCALE: <u>No Scale</u> DATE: <u>March 2023</u>	

PAGE 5 OF 8

M:\PROJECTS\5570-028P\Bridges\Microstation\TSS<\S&L - Alt 3 - Steel Plate Girder.dgn

3/27/2023

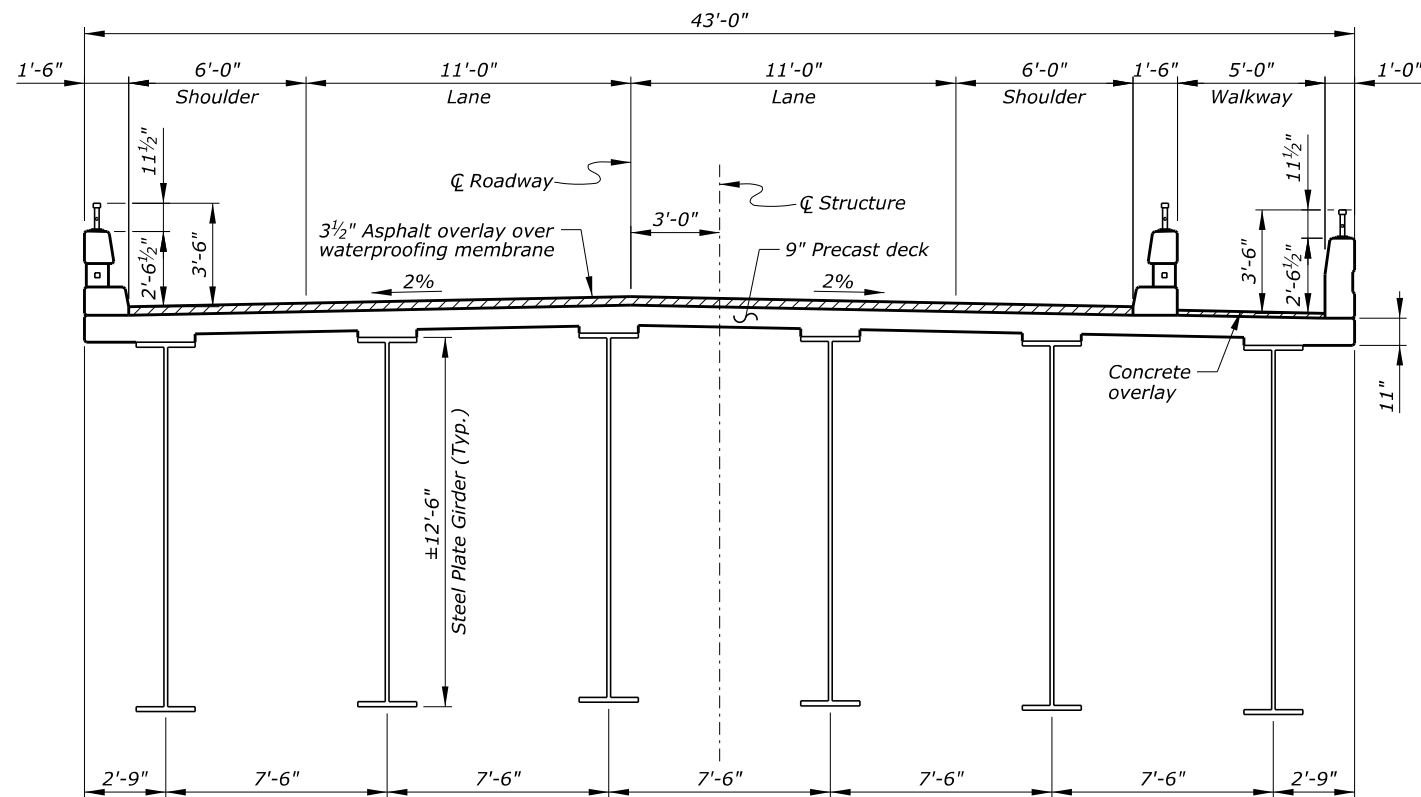


ROUTE NAME AND NUMBER _____ Natchez Trace Parkway		FOREST/PARK/OTHER _____ Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 3 STEEL PLATE GIRDER (SOUTH ALIGNMENT)	
STATE _____ Alabama	COUNTY _____ Lauderdale	STREAM NAME _____ Tennessee River			
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____ \$572,022,000			
PREPARED BY _____ Carlos J. Peña-Marsh / Nicole Rojas		SCALE: _____ No Scale			
		DATE: _____ March 2023			

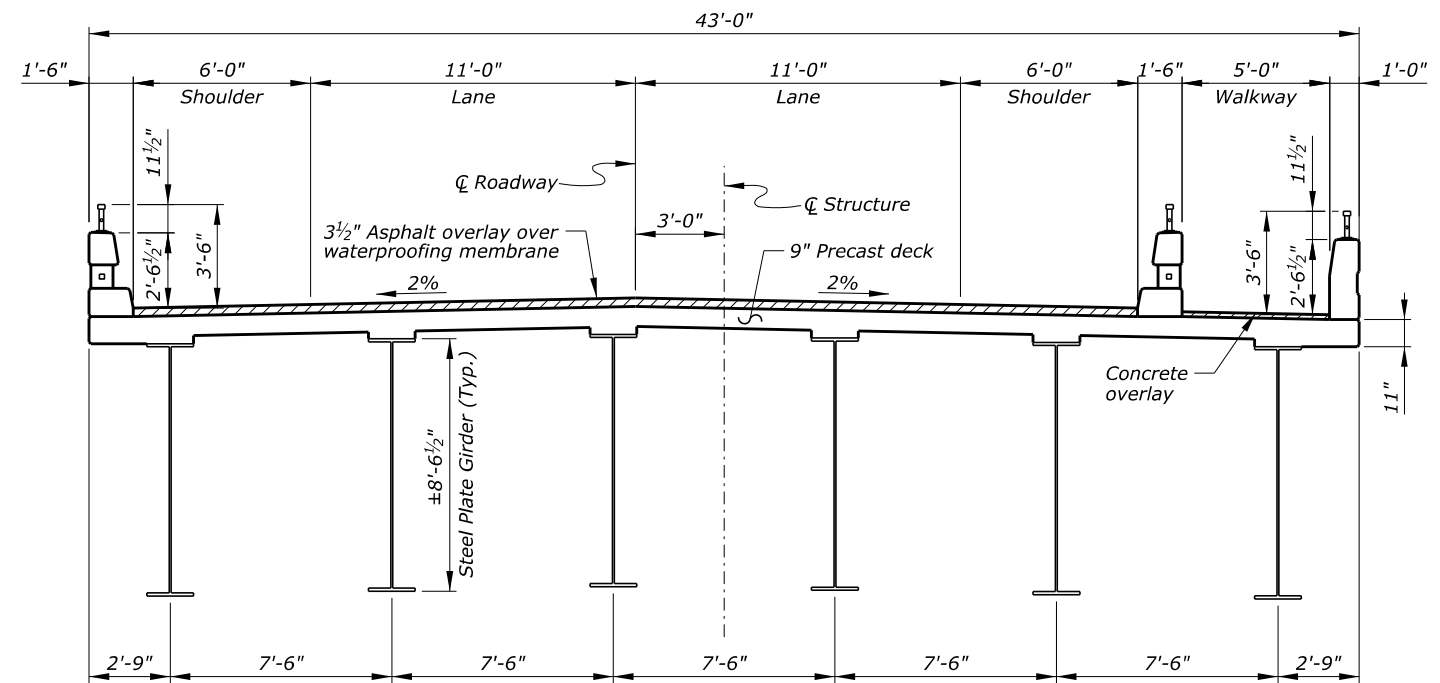
PAGE 6 OF 8

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< S&L - Alt 3 - Steel Plate Girder .DGN

3/27/2023



TYPICAL SECTION
(Span 11)



TYPICAL SECTION
(Spans 1 - 10 and Spans 12 - 23)

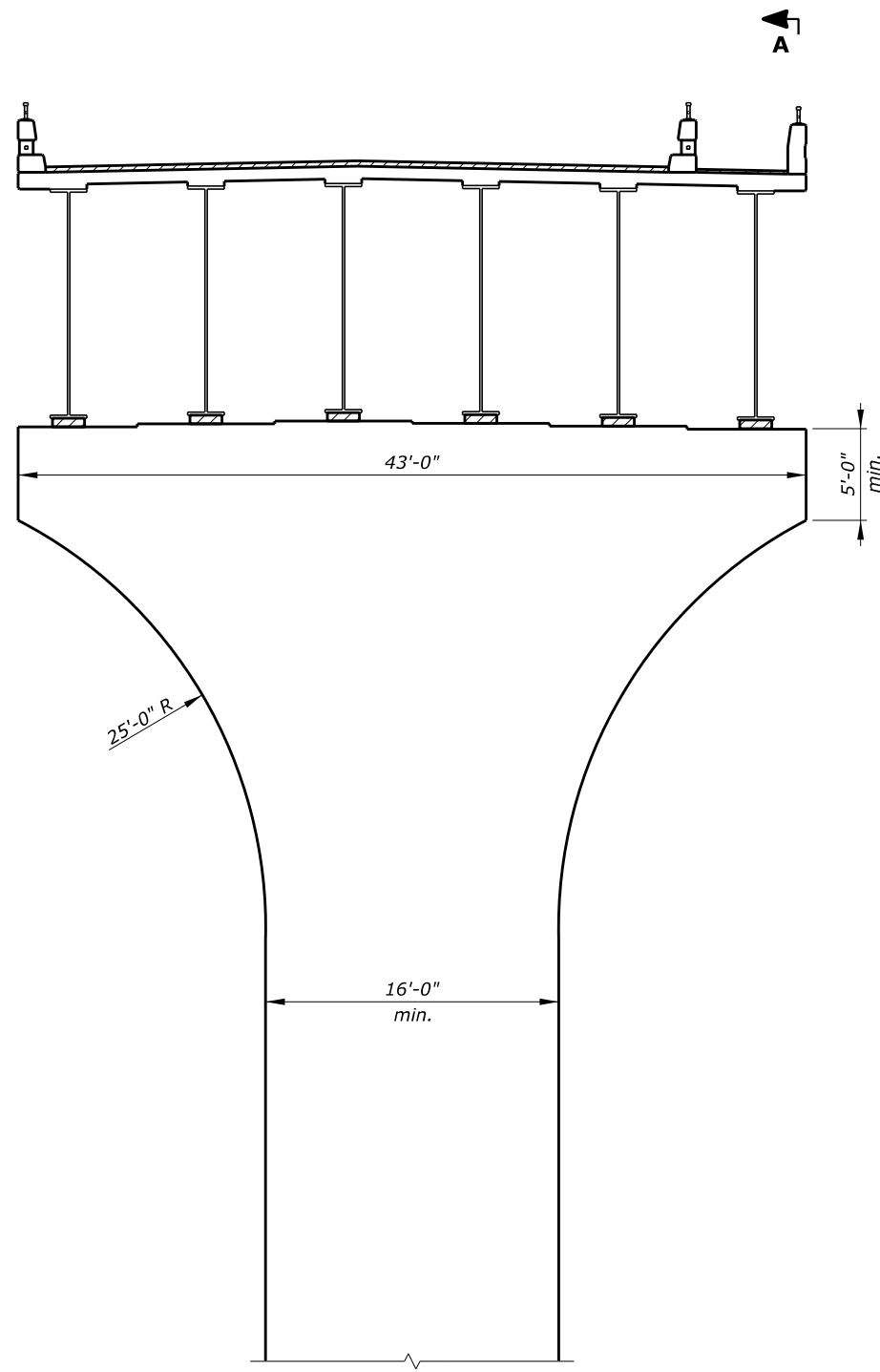
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$572,022,000
SCALE: No Scale DATE: March 2023

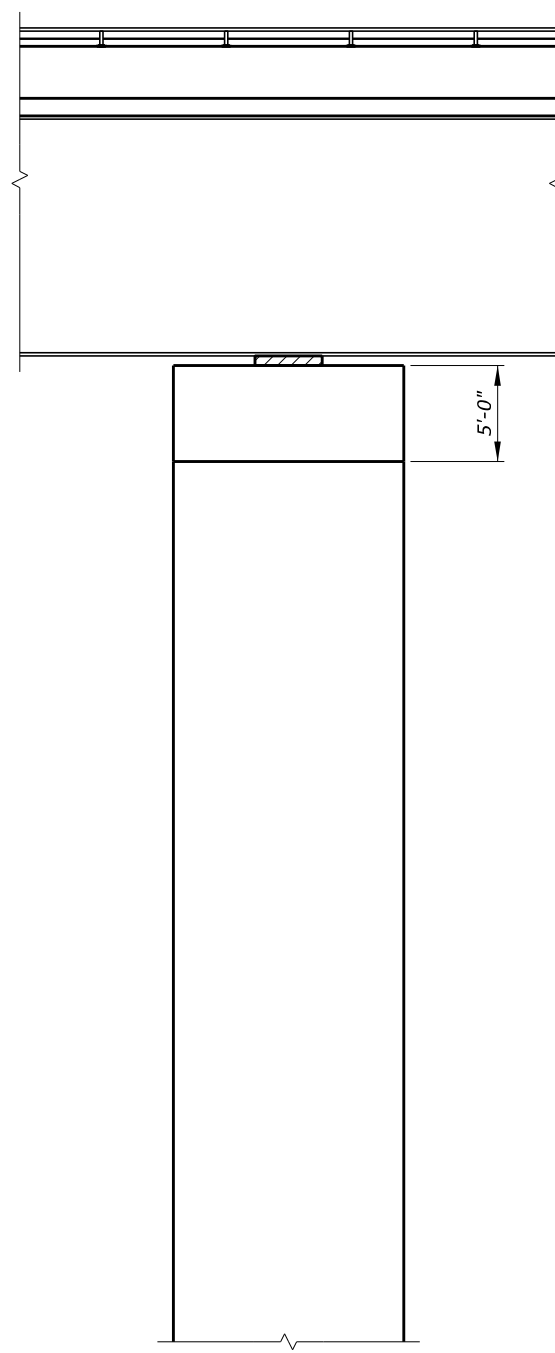
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 3 STEEL PLATE GIRDER
(SOUTH ALIGNMENT) PAGE 7 OF 8

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Alt 3 - Steel Plate Girder .DGN

3/27/2023



TYPICAL PIER SECTION
(Looking up station)



SECTION A-A

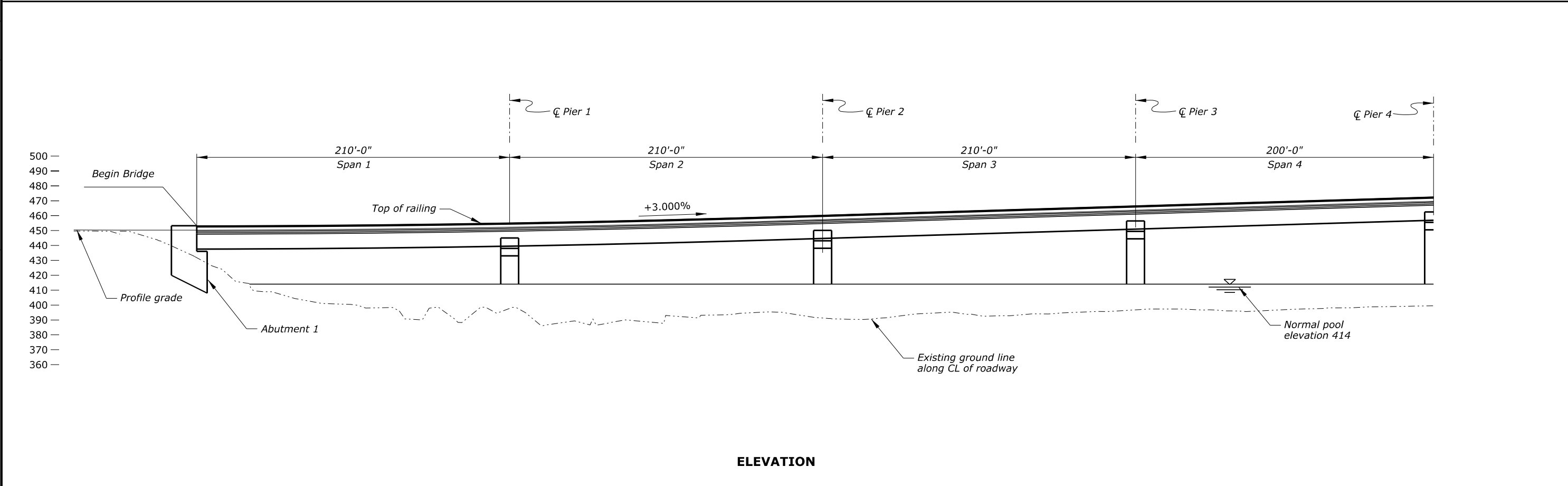
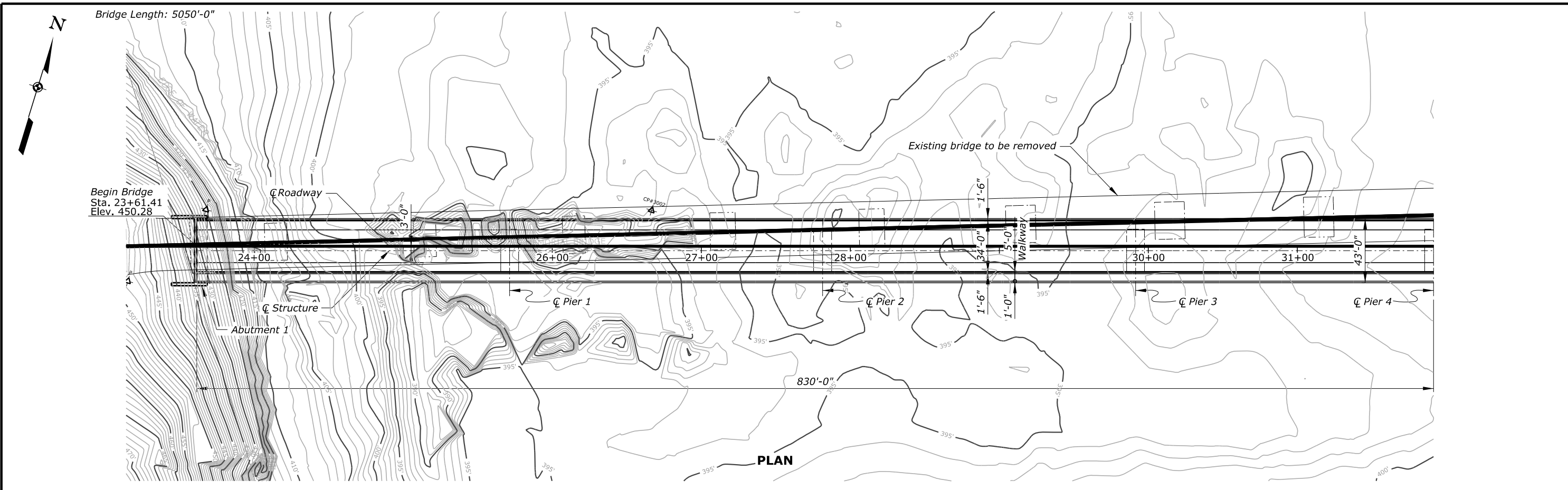
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Carlos J. Peña-Marsh / Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$572,022,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 3 STEEL PLATE GIRDER
(SOUTH ALIGNMENT) PAGE 8 OF 8

M:\PROJECTS\05570-028P\Bridge\Microstation\T&L\T&L - Fourth Alignment.dgn

3/28/2023

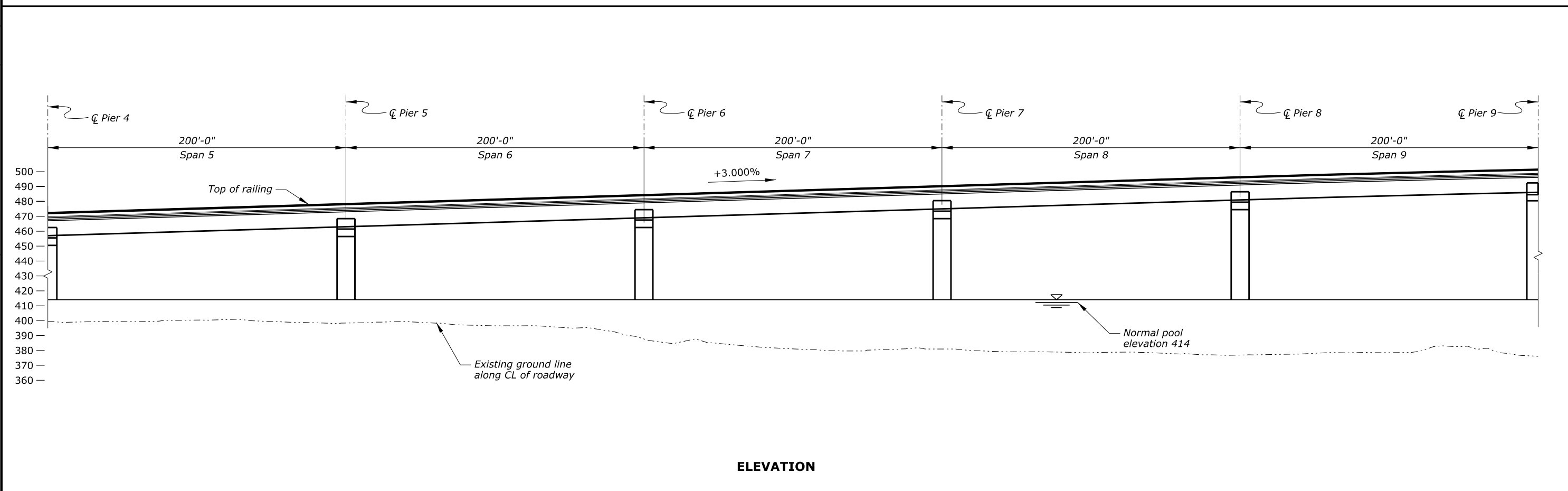
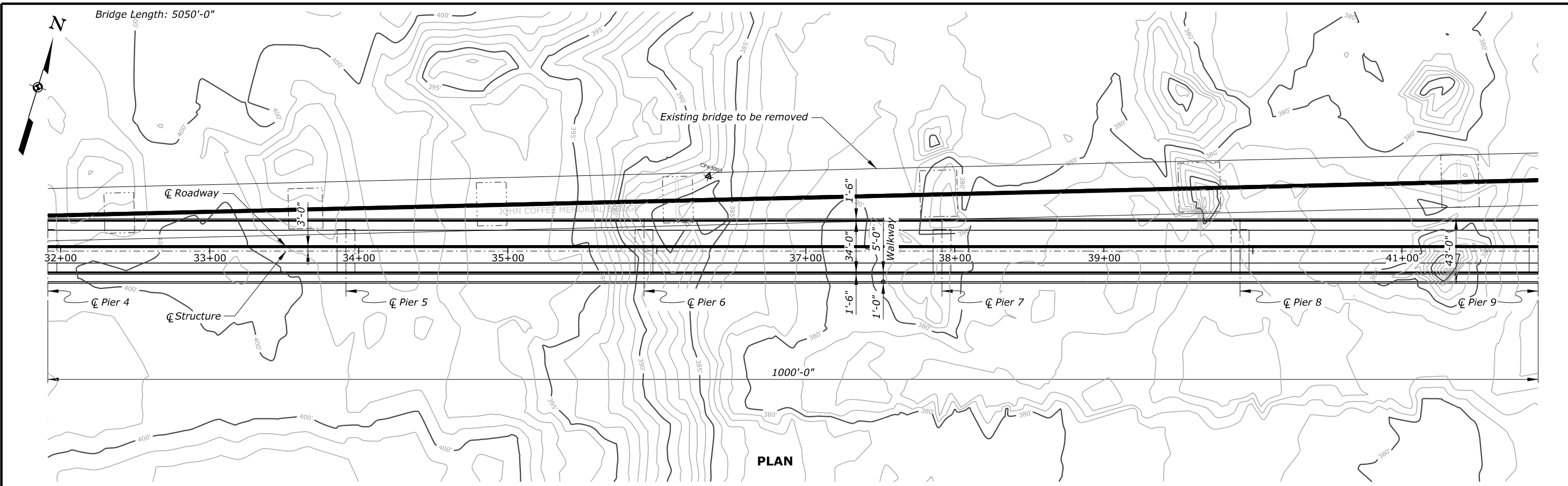


ROUTE NAME AND NUMBER _____ Natchez Trace Parkway		FOREST/PARK/OTHER _____ Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SKEWED ALIGNMENT)	
STATE _____ Alabama	COUNTY _____ Lauderdale	STREAM NAME _____	Tennessee River		
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____ \$478,140,000			
PREPARED BY _____ Nicole Rojas		SCALE: _____ No Scale	DATE: _____ March 2023		

PAGE 1 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\T&L\TSL&L - Fourth Alignment.dgn

3/28/2023

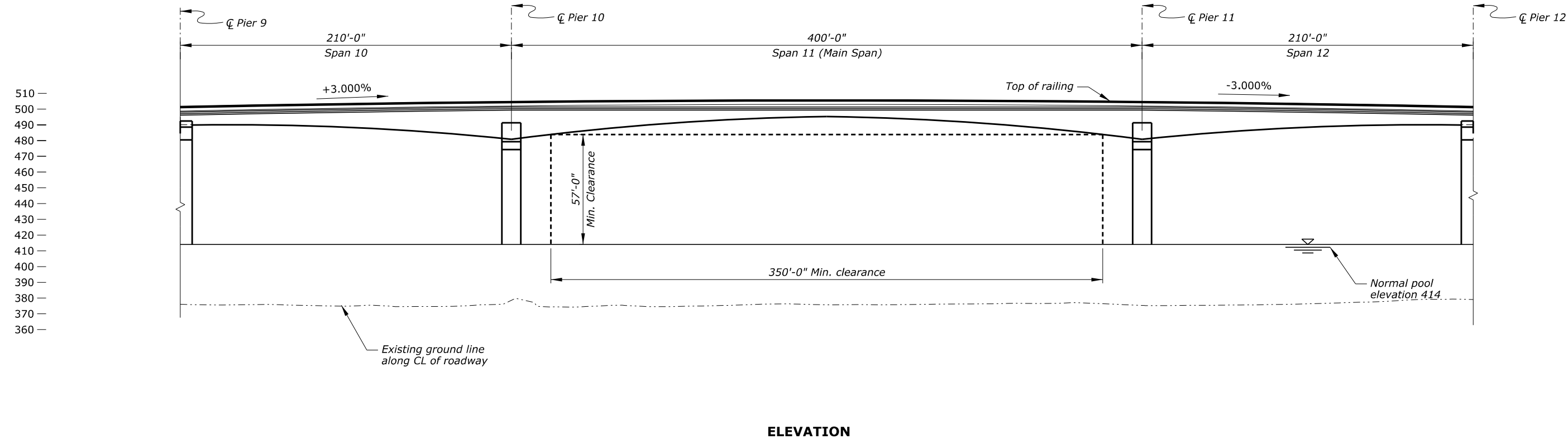
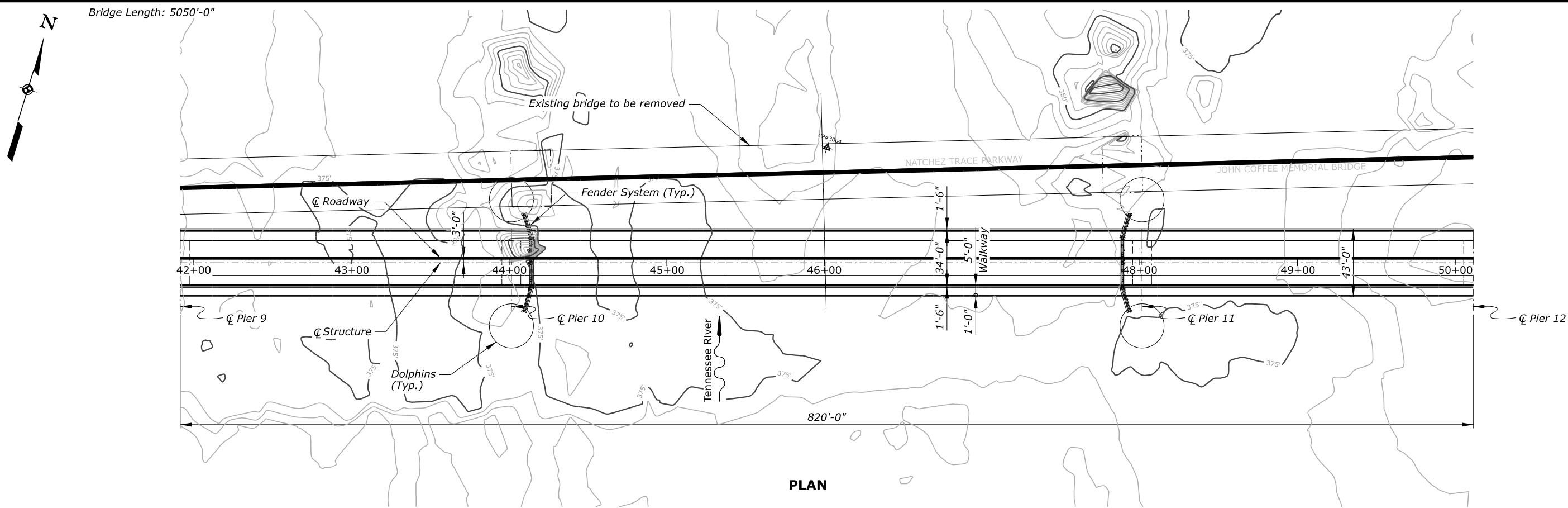


ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway
STATE	Alabama	COUNTY	Lauderdale
ACCOUNT NO.		STREAM NAME	Tennessee River
		ESTIMATED PROJECT COST	\$478,140,000
PREPARED BY	Nicole Rojas	SCALE:	No Scale
		DATE:	March 2023

BRIDGE NAME	JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SKEWED ALIGNMENT)	
PAGE 2 OF 9	

M:\PROJECTS\028P\Bridges\Microstation\TSS<\SL&L - Fourth Alignment.dgn

3/28/2023



ROUTE NAME AND NUMBER Natchez Trace Parkway

STATE Alabama COUNTY Lauderdale

ACCOUNT NO. _____

PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway

STREAM NAME Tennessee River

ESTIMATED PROJECT COST \$478,140,000

SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

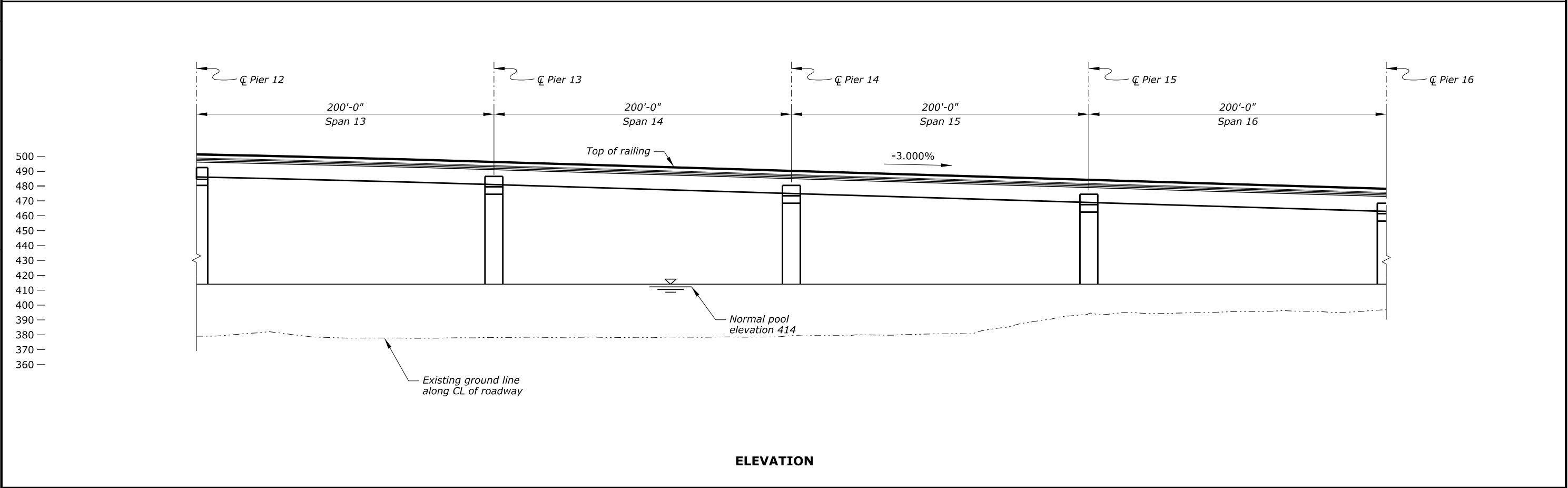
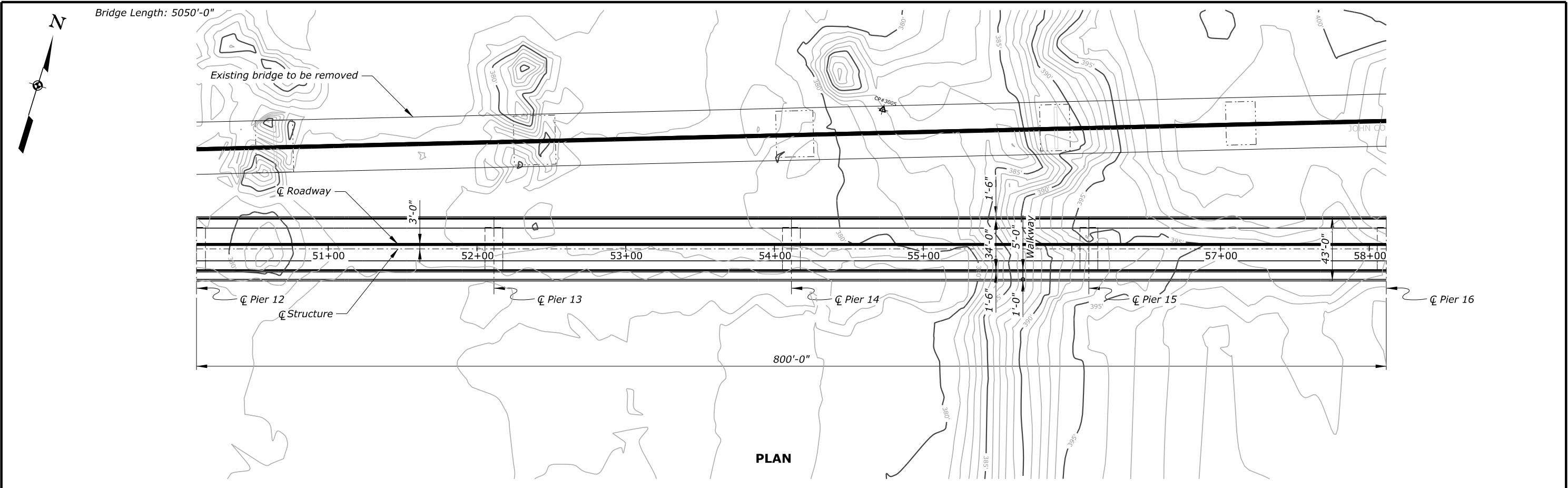
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE

**ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SKEWED ALIGNMENT)**

PAGE 3 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

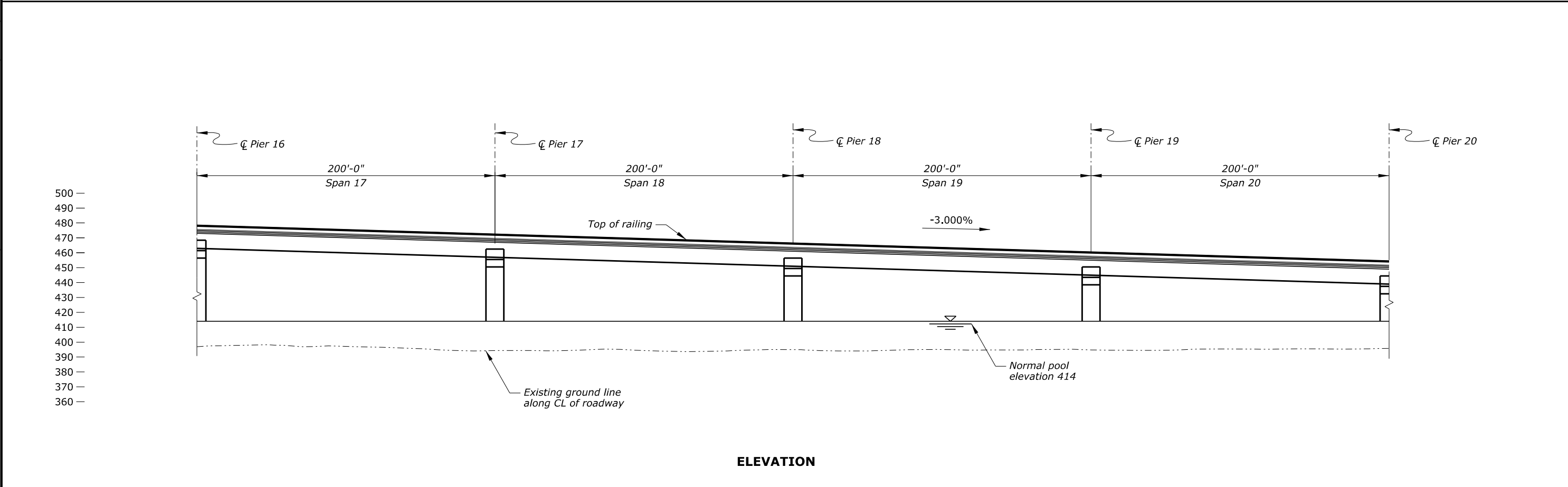
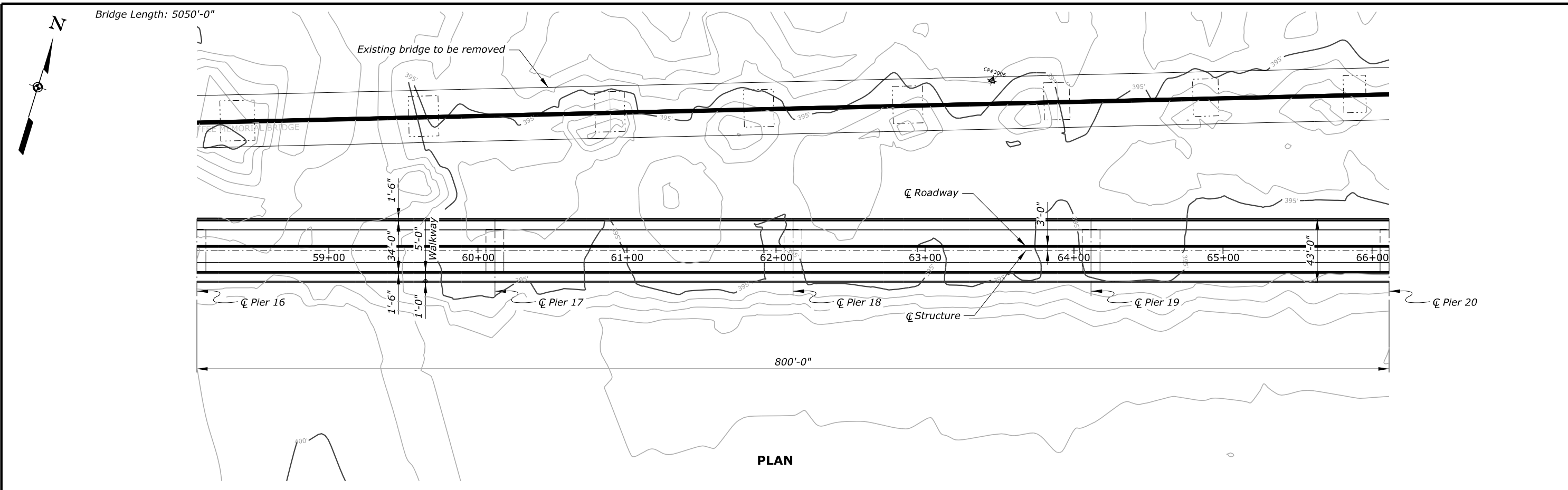
3/28/2023



ROUTE NAME AND NUMBER _____		Natchez Trace Parkway		FOREST/PARK/OTHER _____		Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SKEWED ALIGNMENT) PAGE 4 OF 9							
STATE _____		Alabama		COUNTY _____		Lauderdale						STREAM NAME _____		Tennessee River	
ACCOUNT NO. _____				ESTIMATED PROJECT COST _____								\$478,140,000			
PREPARED BY _____				Nicole Rojas								SCALE: _____		No Scale	

M:\PROJECTS\0415570-028P\Bridges\Microstation\TSS<\SL&L - Fourth Alignment.dgn

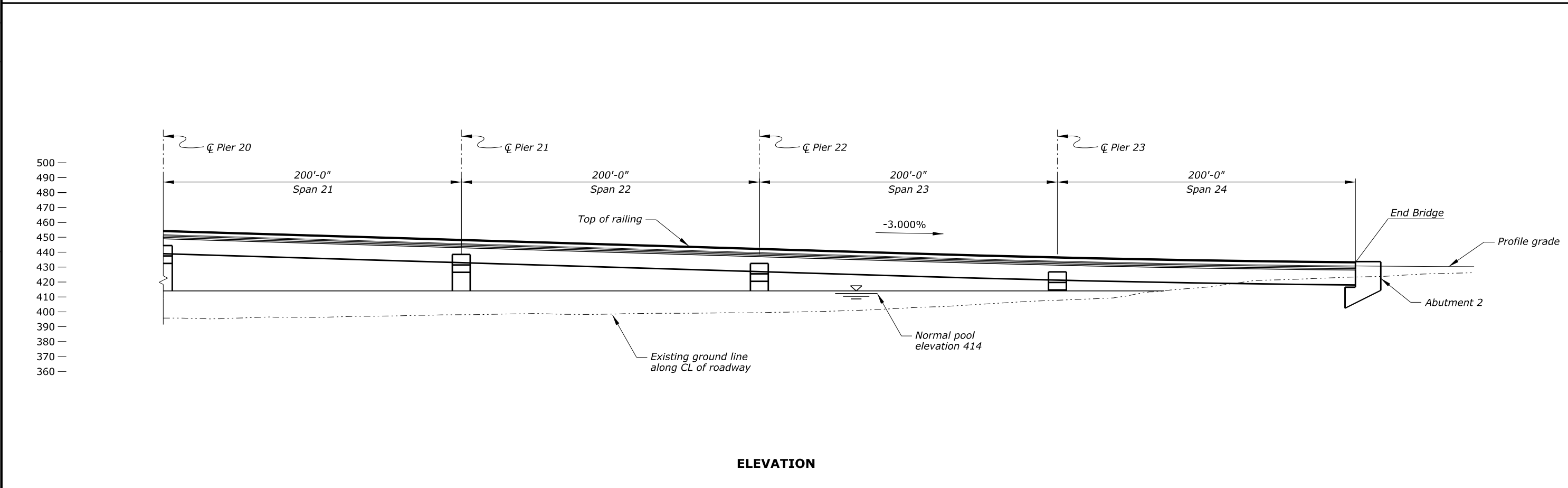
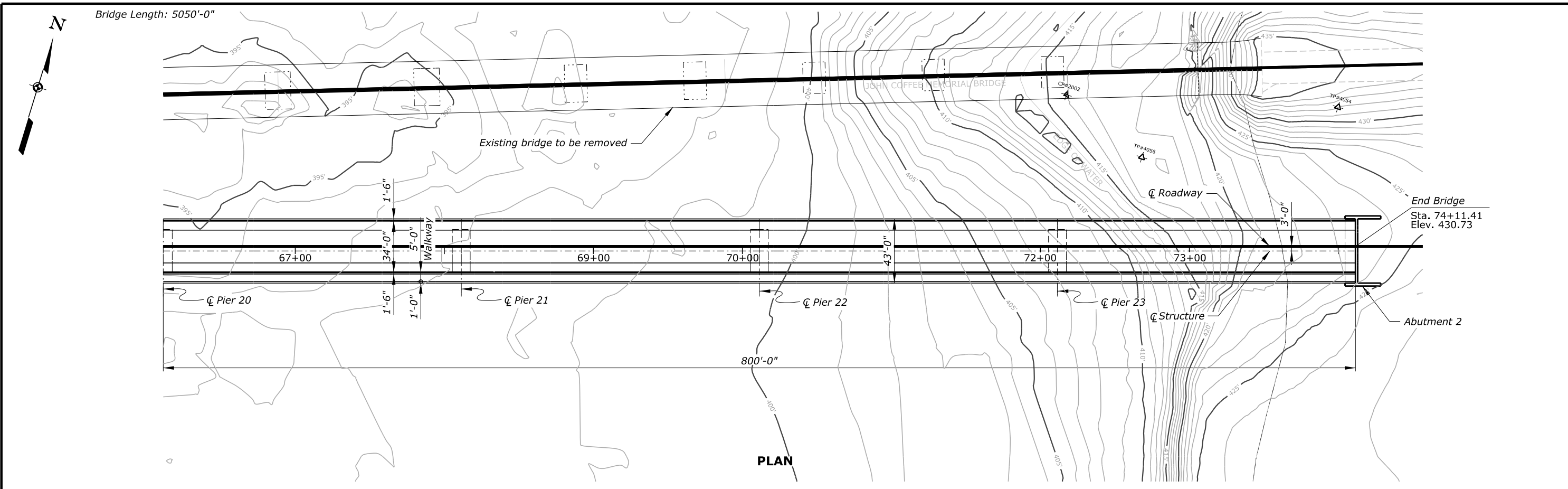
3/28/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SKEWED ALIGNMENT) PAGE 5 OF 9
STATE	Alabama	COUNTY	Lauderdale	
ACCOUNT NO.		STREAM NAME	Tennessee River	
ESTIMATED PROJECT COST	\$478,140,000	DATE:	March 2023	
PREPARED BY	Nicole Rojas	SCALE:	No Scale	

M:\PROJECTS\0415570-028P\Bridges\Microstation\T&L\T&L - Fourth Alignment.dgn

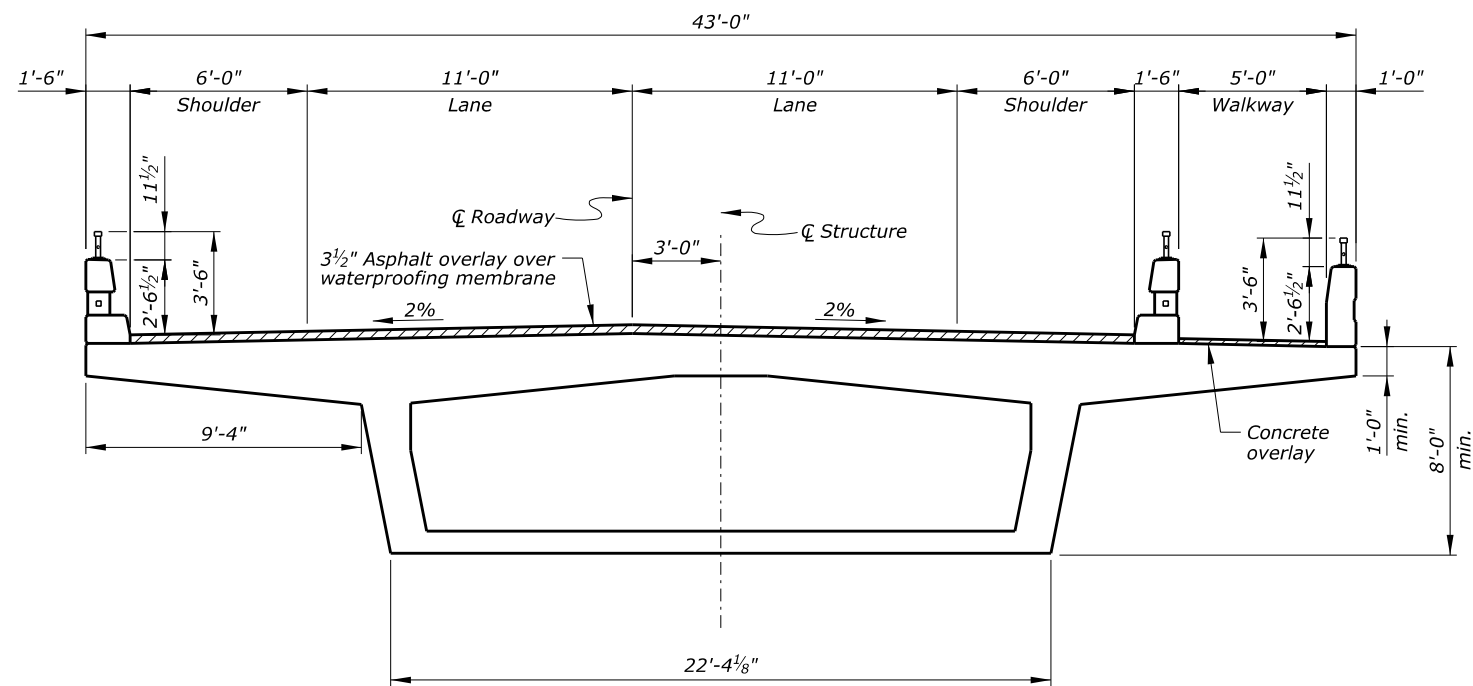
3/28/2023



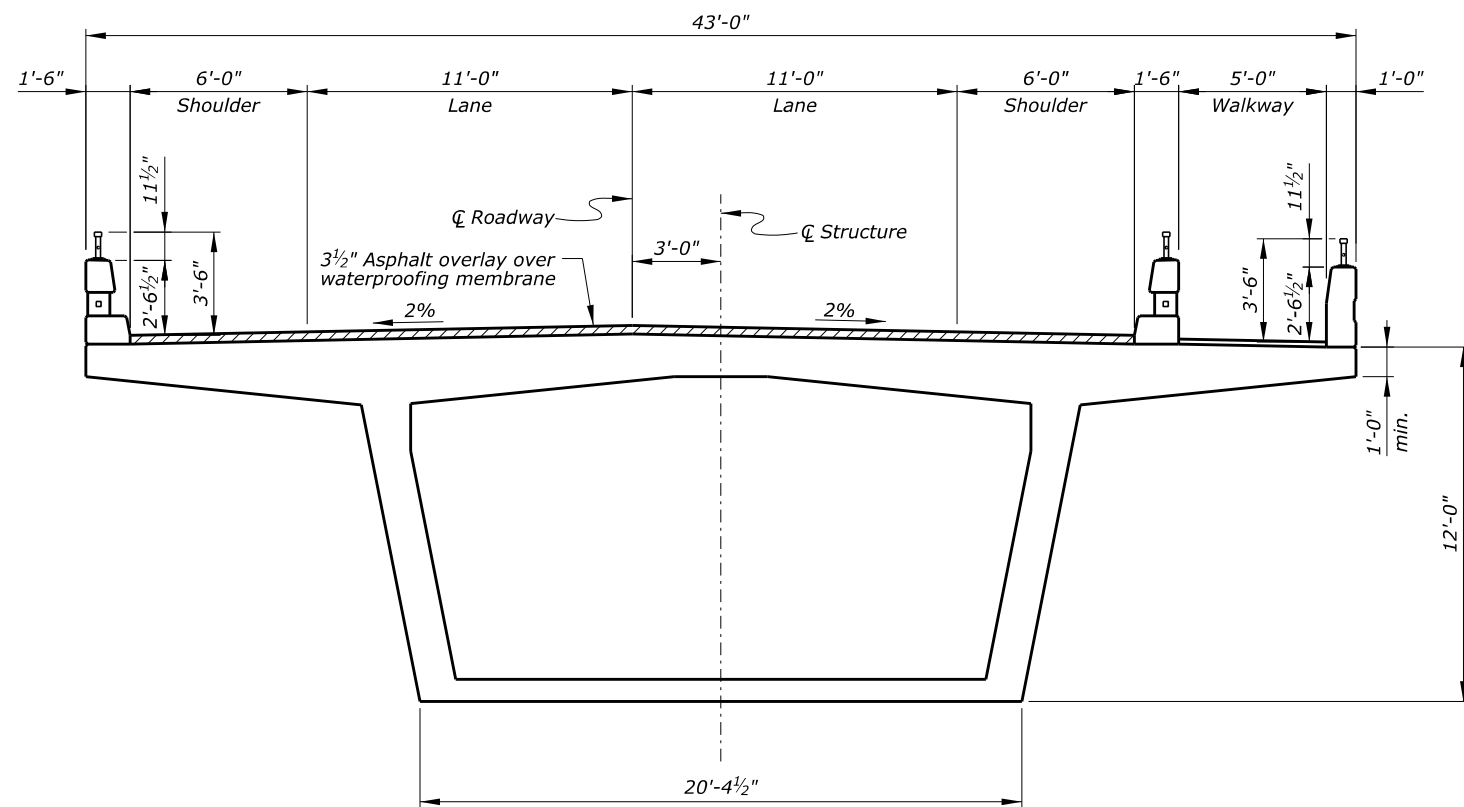
ROUTE NAME AND NUMBER		Natchez Trace Parkway		FOREST/PARK/OTHER		Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION			
STATE		Alabama		COUNTY		Lauderdale					
ACCOUNT NO.				STREAM NAME		Tennessee River					
				ESTIMATED PROJECT COST		\$478,140,000					
PREPARED BY		Nicole Rojas		SCALE:		No Scale		DATE:		March 2023	
BRIDGE NAME										JOHN COFFEE MEMORIAL BRIDGE	
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER (SKEWED ALIGNMENT)											
PAGE 6 OF 9											

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< SL&L - Fourth Alignment.dgn

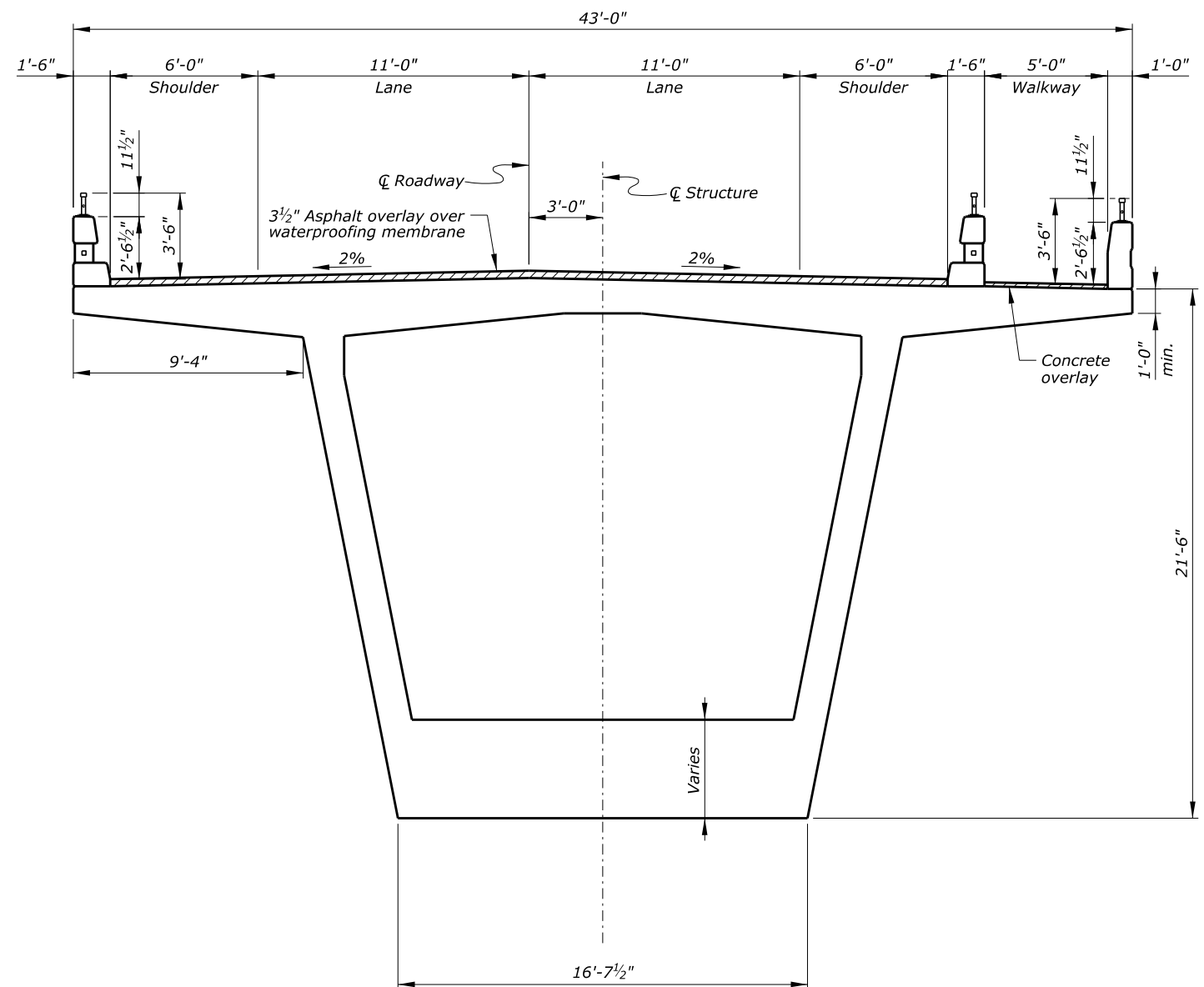
3/28/2023



TYPICAL SECTION
(Near midspan of span 10 and span 12)



TYPICAL SECTION
(Near midspan of spans 1 - 9 and 13 - 24)



TYPICAL SECTION
(Near pier 10 and 11)

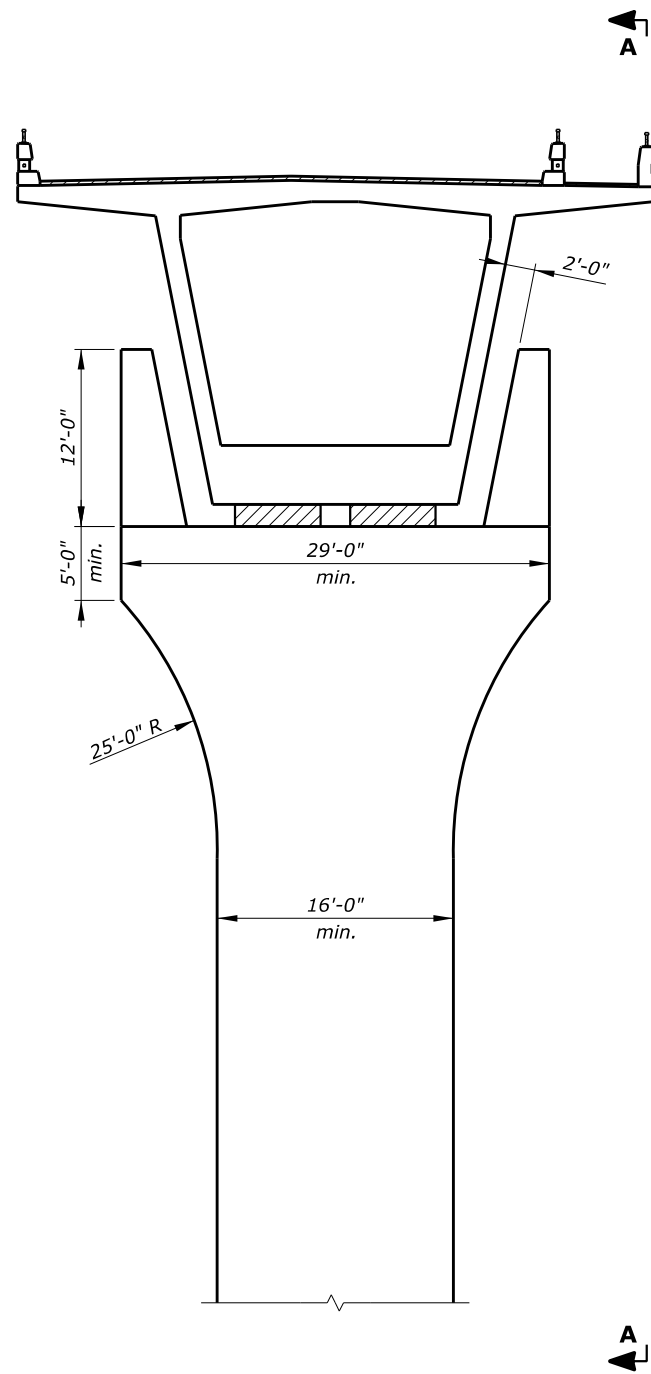
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$478,140,000
SCALE: No Scale DATE: March 2023

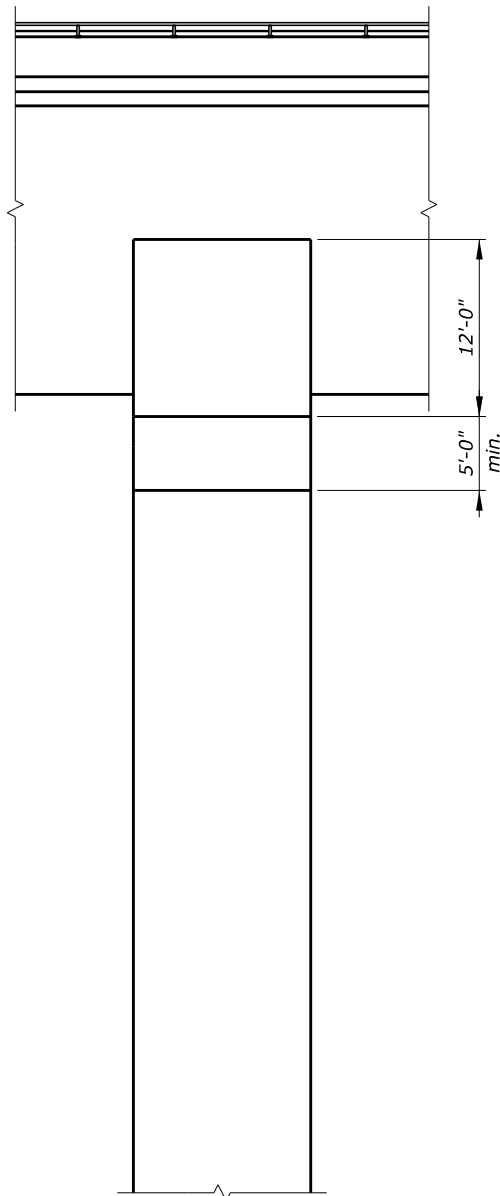
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SKEWED ALIGNMENT) PAGE 7 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< SL&L - Fourth Alignment.dgn

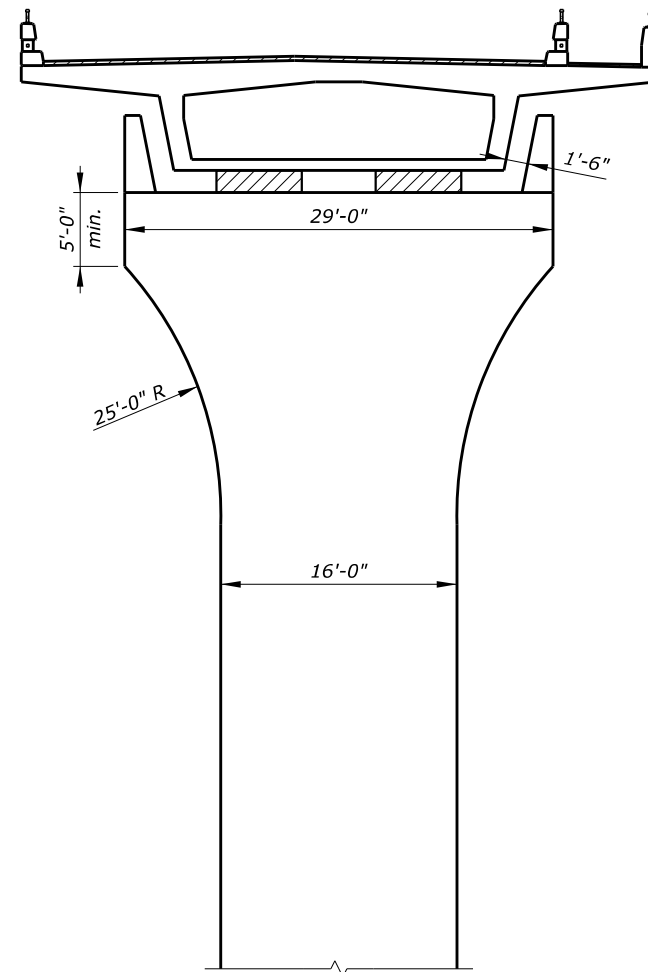
3/28/2023



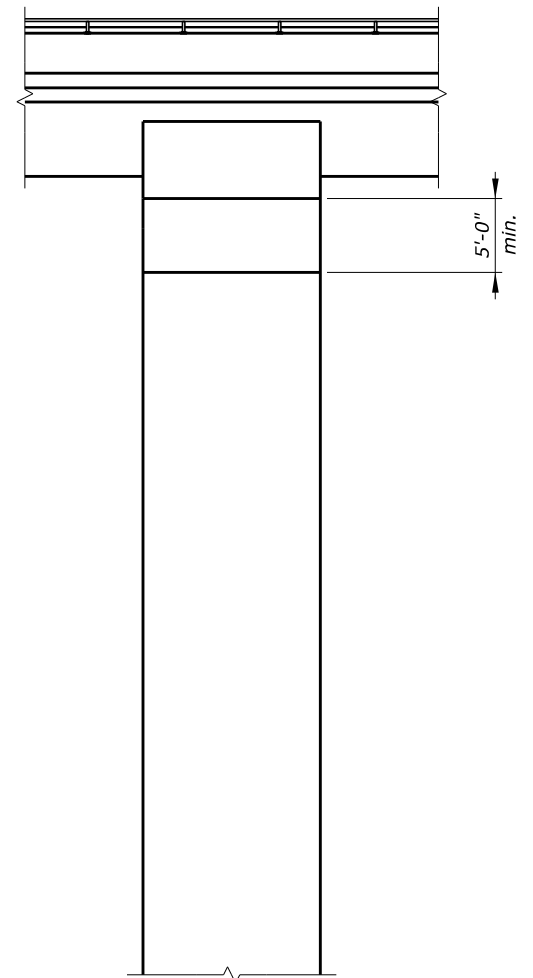
TYPICAL PIER SECTION
(Pier 10 and 11)
(Looking up station)



SECTION A-A



TYPICAL PIER SECTION
(Pier 9 and 12)
(Looking up station)



SECTION A-A

ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

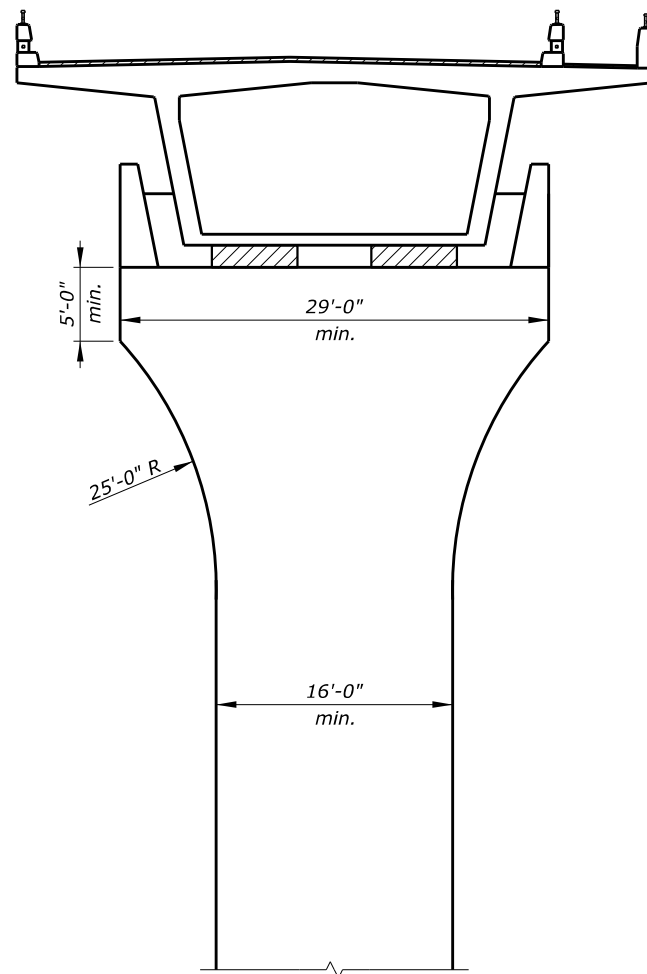
FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$478,140,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SKEWED ALIGNMENT) PAGE 8 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

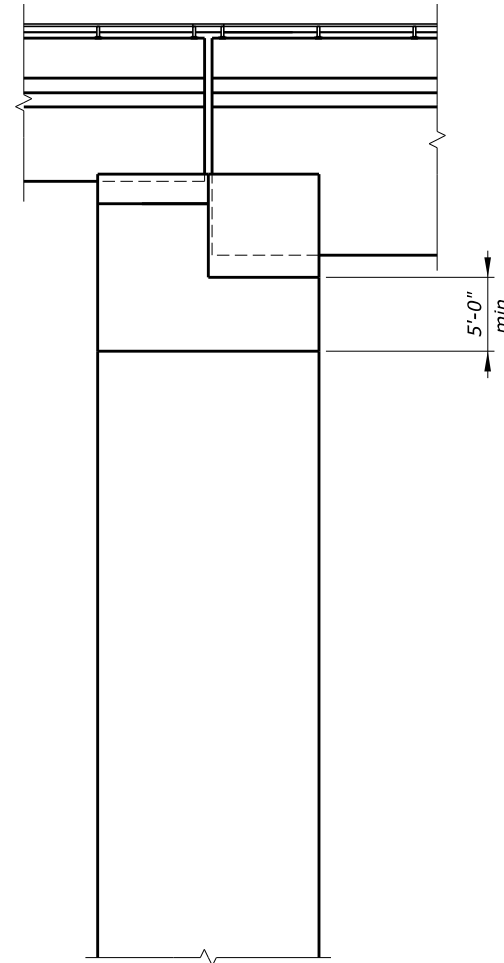
3/28/2023

A



A

TYPICAL PIER SECTION
(Pier 19 and 12)
(Looking up station)



SECTION A-A

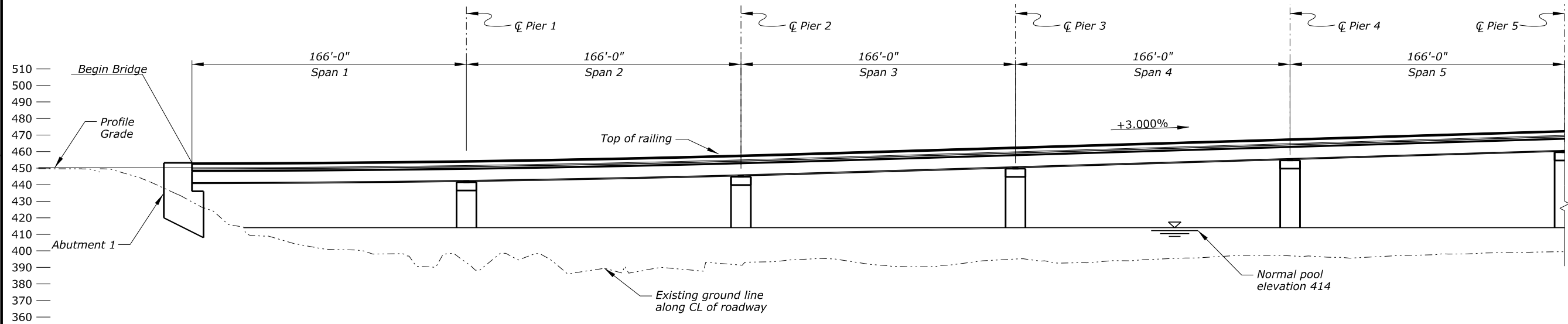
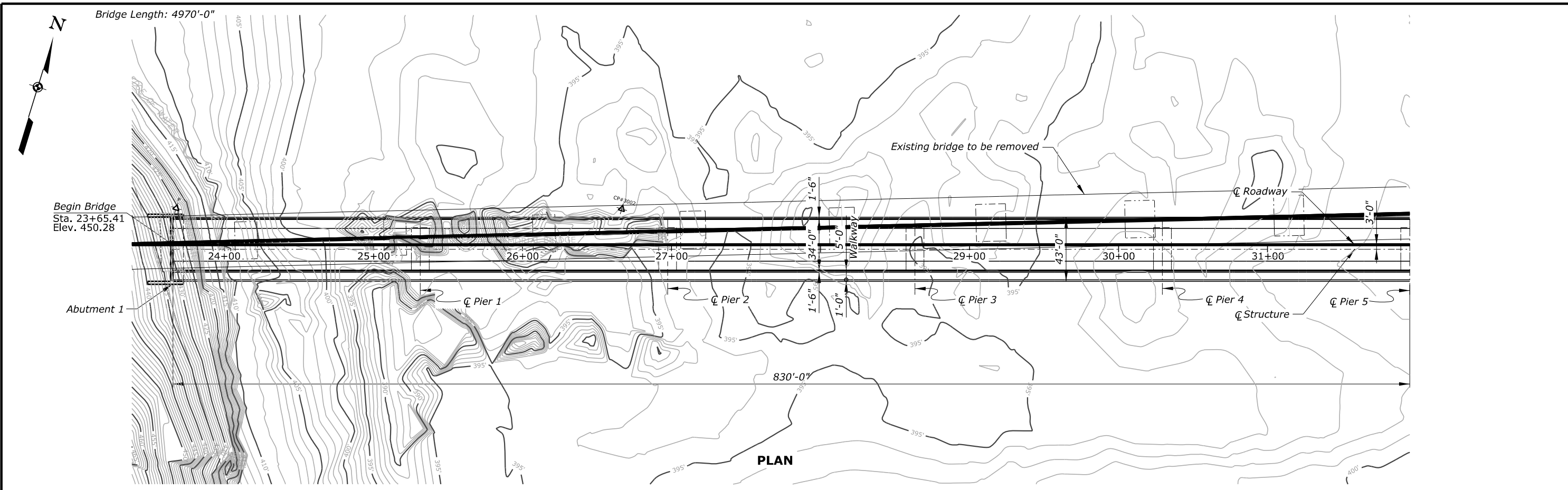
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$478,140,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 1 SEGMENTAL CONCRETE BOX GIRDER
(SKEWED ALIGNMENT) PAGE 9 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\T&L\T&L - Fourth Alignment.dgn

3/28/2023



ELEVATION

ROUTE NAME AND NUMBER Natchez Trace Parkway

STATE Alabama COUNTY Lauderdale

ACCOUNT NO. _____

PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway

STREAM NAME Tennessee River

ESTIMATED PROJECT COST \$437,088,000

SCALE: No Scale DATE: March 2023

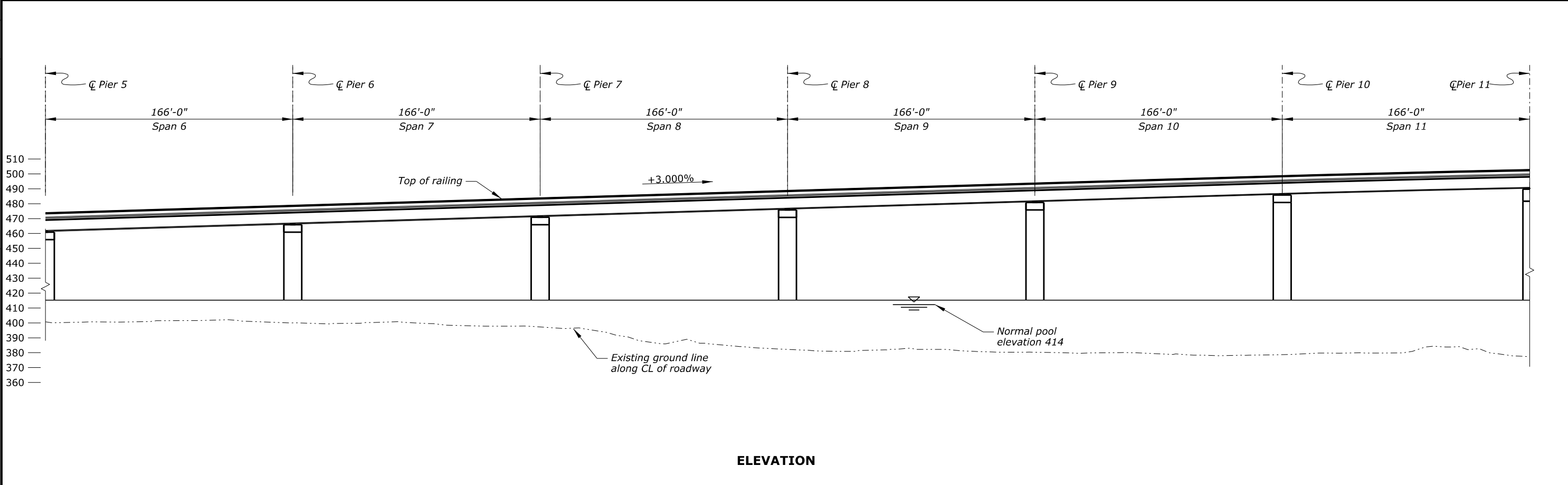
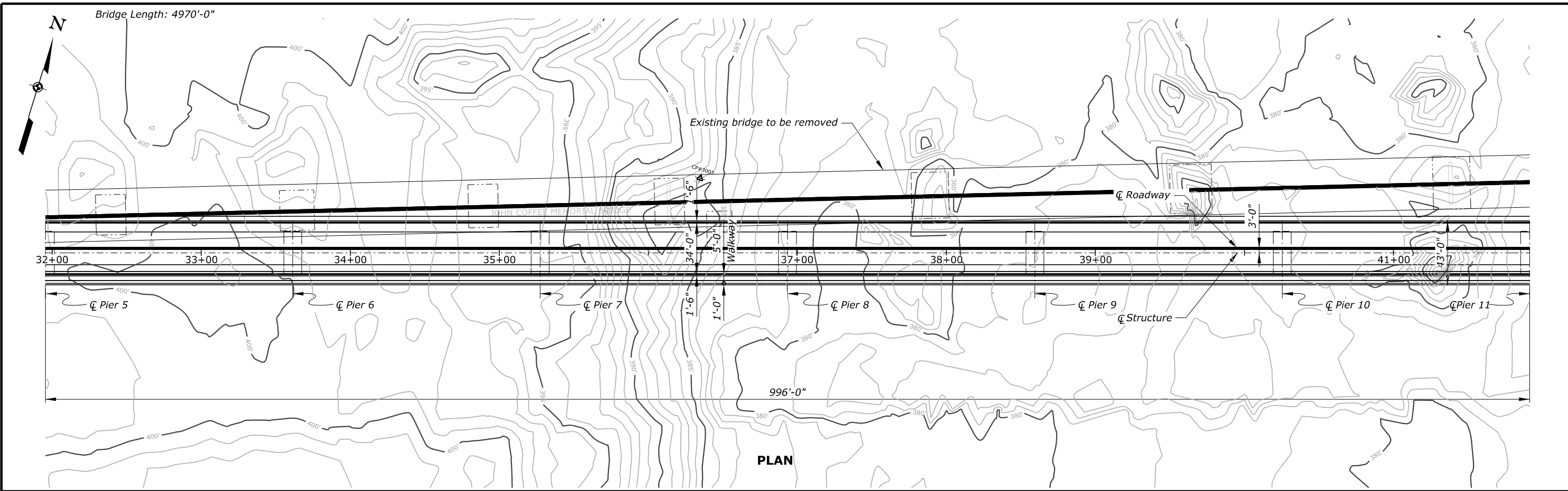
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE

**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)** PAGE 1 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

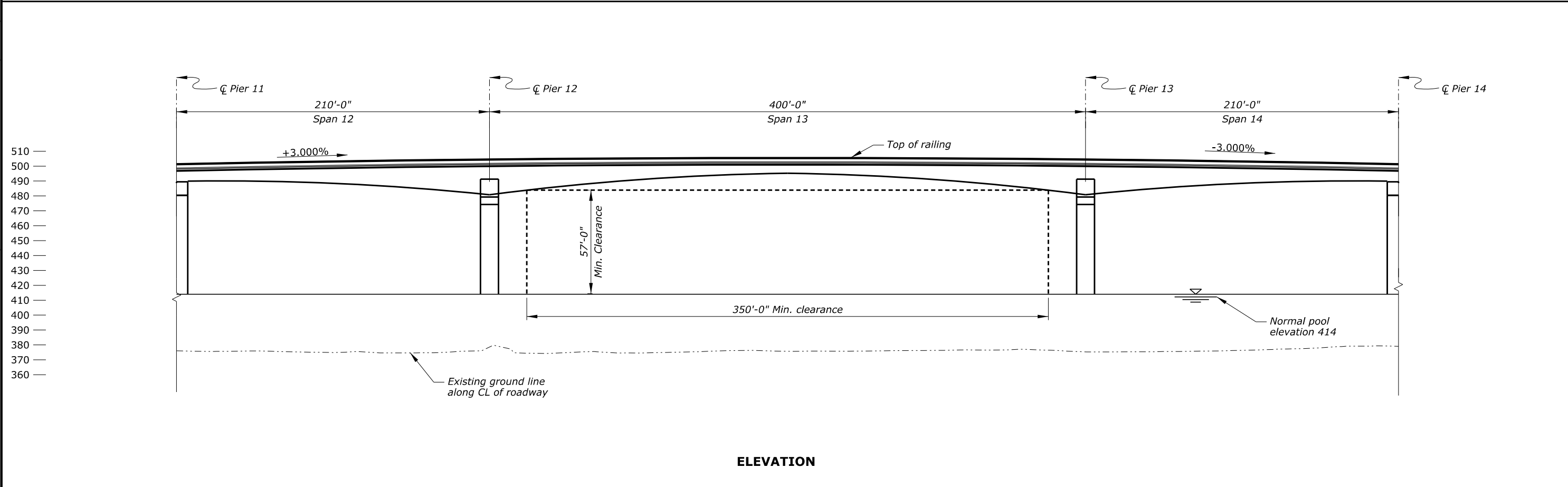
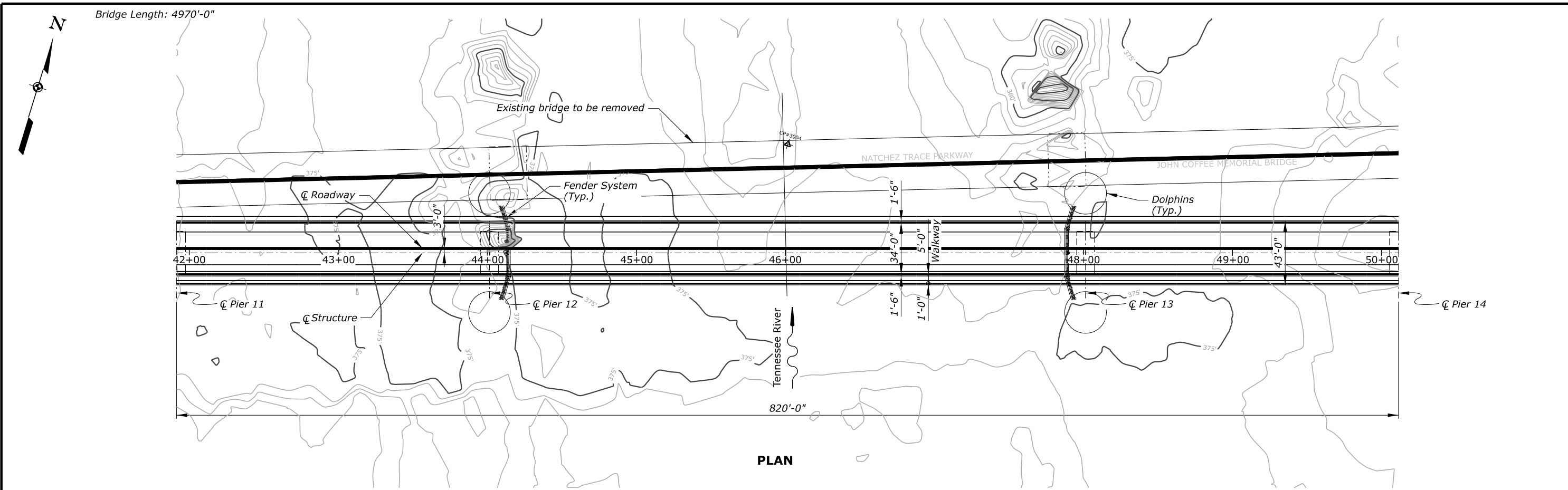
3/28/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION	
STATE	Alabama	COUNTY	Lauderdale		
ACCOUNT NO.		STREAM NAME	Tennessee River		
ESTIMATED PROJECT COST	\$437,088,000	DATE:	March 2023		
PREPARED BY	Nicole Rojas	SCALE:	No Scale	BRIDGE NAME	JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT) PAGE 2 OF 9					

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T&L\T&L - Fourth Alignment.dgn

3/28/2023

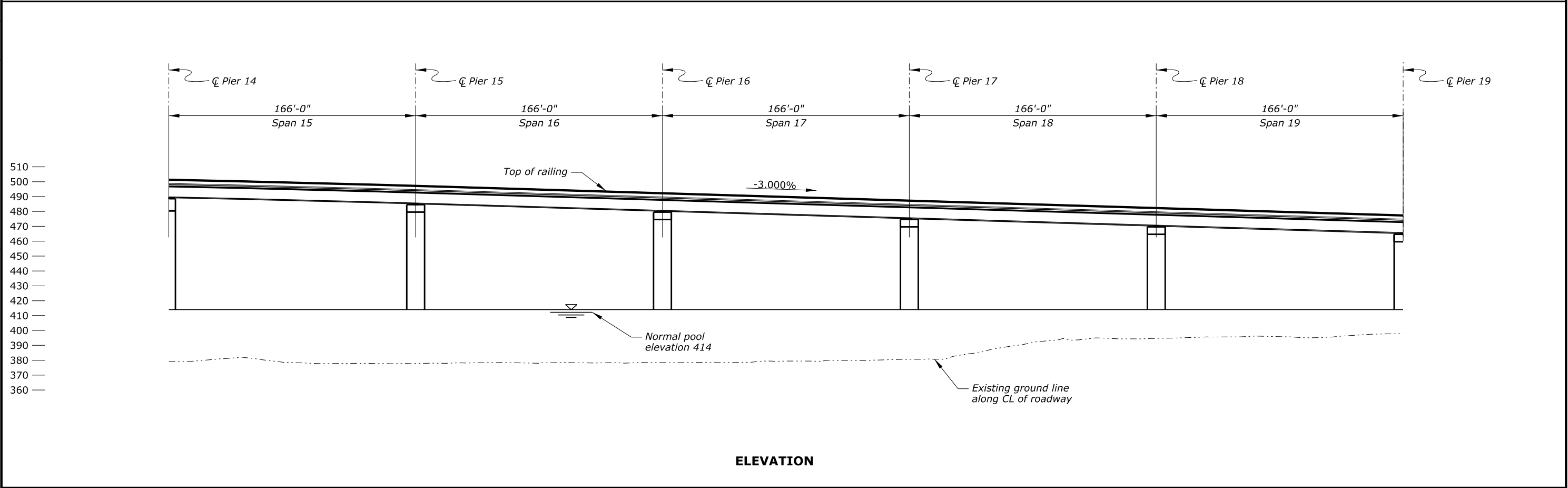
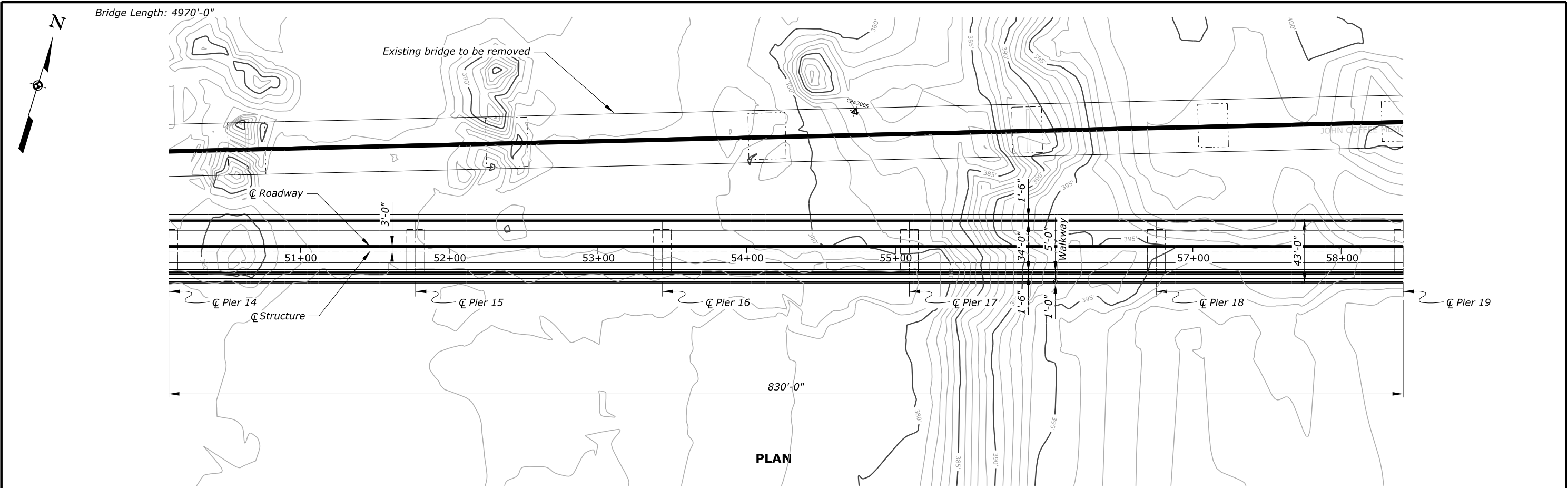


ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION		
STATE	Alabama	COUNTY	Lauderdale			
ACCOUNT NO.		STREAM NAME	Tennessee River			
		ESTIMATED PROJECT COST	\$437,088,000			
PREPARED BY	Nicole Rojas	SCALE:	No Scale	DATE:	March 2023	BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)

PAGE 3 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T8<\SL&L - Fourth Alignment.dgn

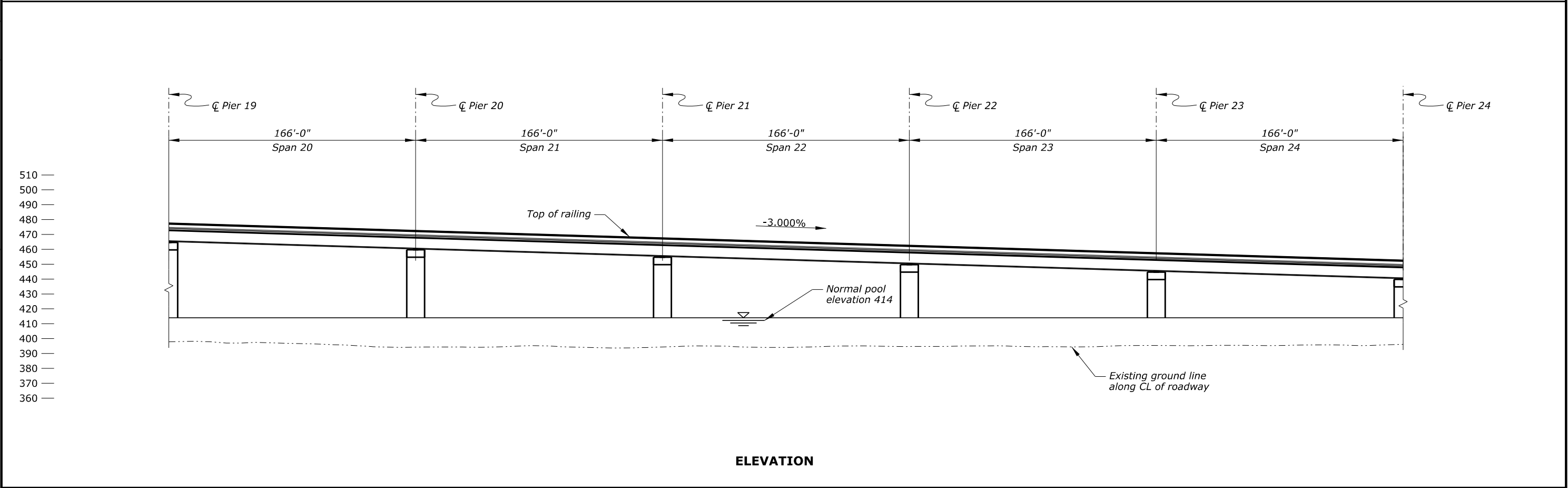
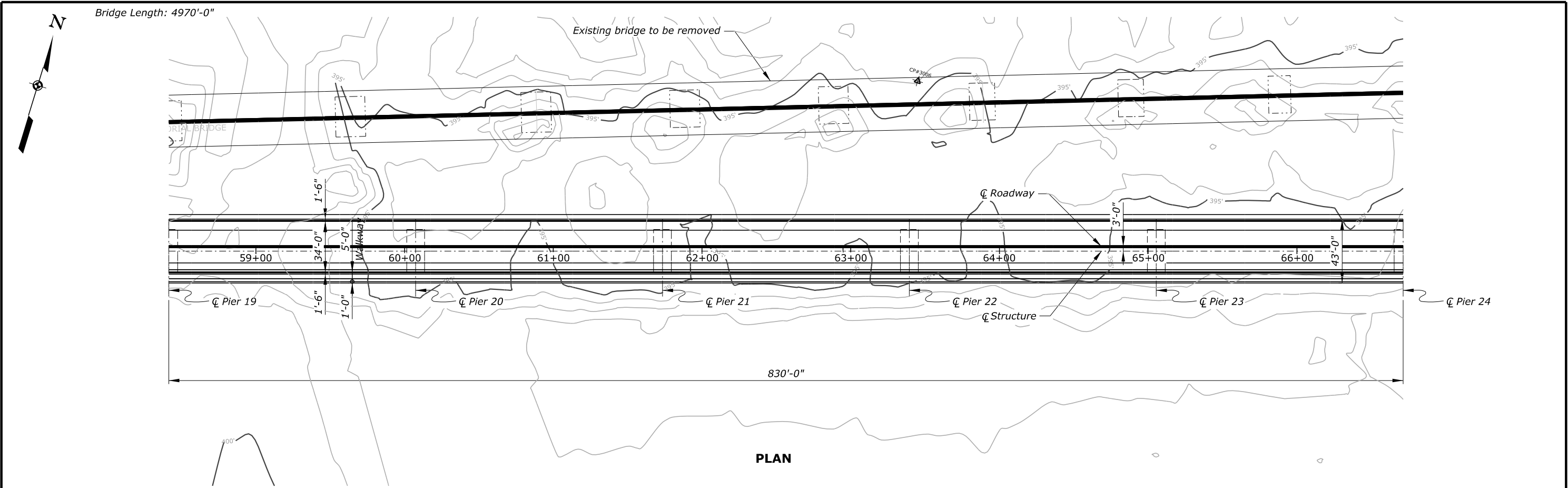
3/28/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION
STATE	Alabama	COUNTY	Lauderdale	BRIDGE NAME
ACCOUNT NO.		STREAM NAME	Tennessee River	JOHN COFFEE MEMORIAL BRIDGE
PREPARED BY	Nicole Rojas	ESTIMATED PROJECT COST	\$437,088,000	ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)
		SCALE:	No Scale	PAGE 4 OF 9
		DATE:	March 2023	

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T&L\T&L - Fourth Alignment.dgn

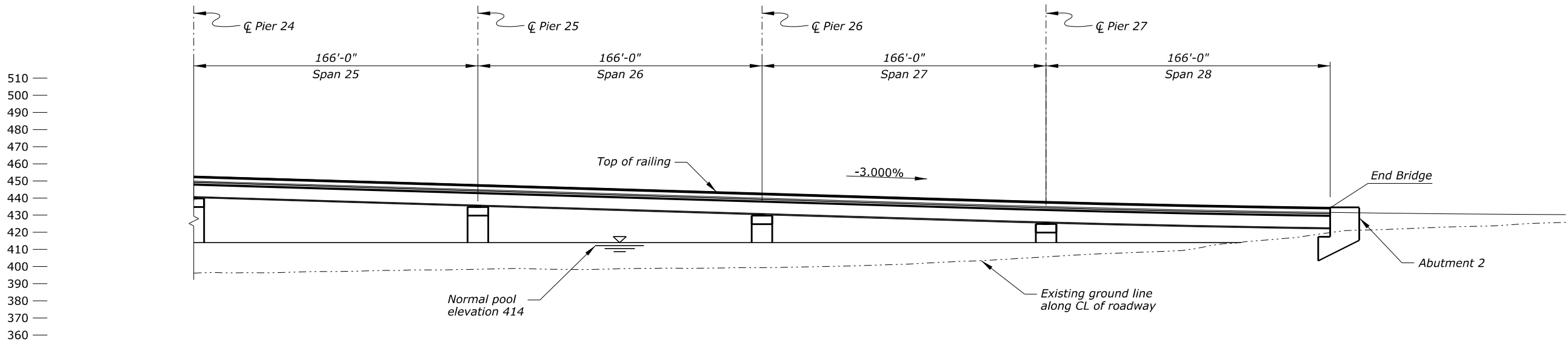
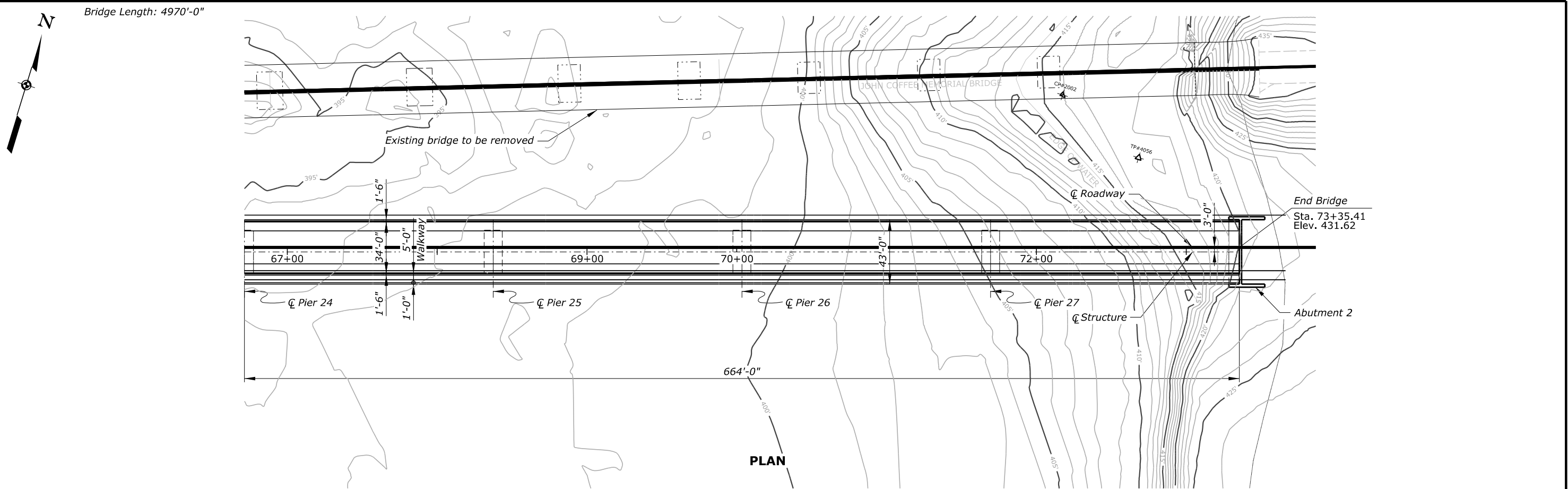
3/28/2023



ROUTE NAME AND NUMBER _____		Natchez Trace Parkway		FOREST/PARK/OTHER _____		Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION					
STATE _____		Alabama		COUNTY _____		Lauderdale				STREAM NAME _____		Tennessee River	
ACCOUNT NO. _____				ESTIMATED PROJECT COST _____		\$437,088,000							
PREPARED BY _____		Nicole Rojas		SCALE: _____		No Scale		DATE: _____		March 2023		BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 2 SEGMENTAL CONCRETE BOX GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT) PAGE 5 OF 9	

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

3/28/2023



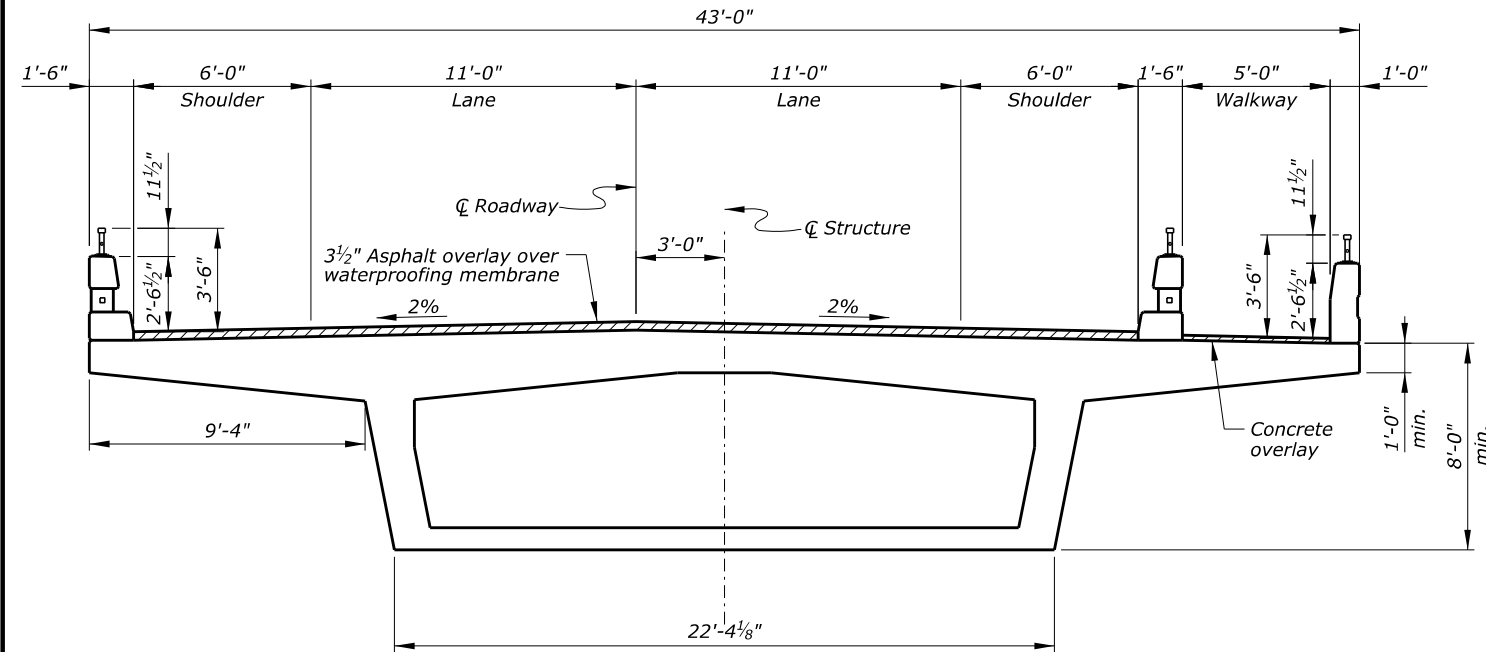
ROUTE NAME AND NUMBER _____ Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY _____ Nicole Rojas

FOREST/PARK/OTHER _____ Natchez Trace Parkway
STREAM NAME _____ Tennessee River
ESTIMATED PROJECT COST _____ \$437,088,000
SCALE: _____ No Scale
DATE: _____ March 2023

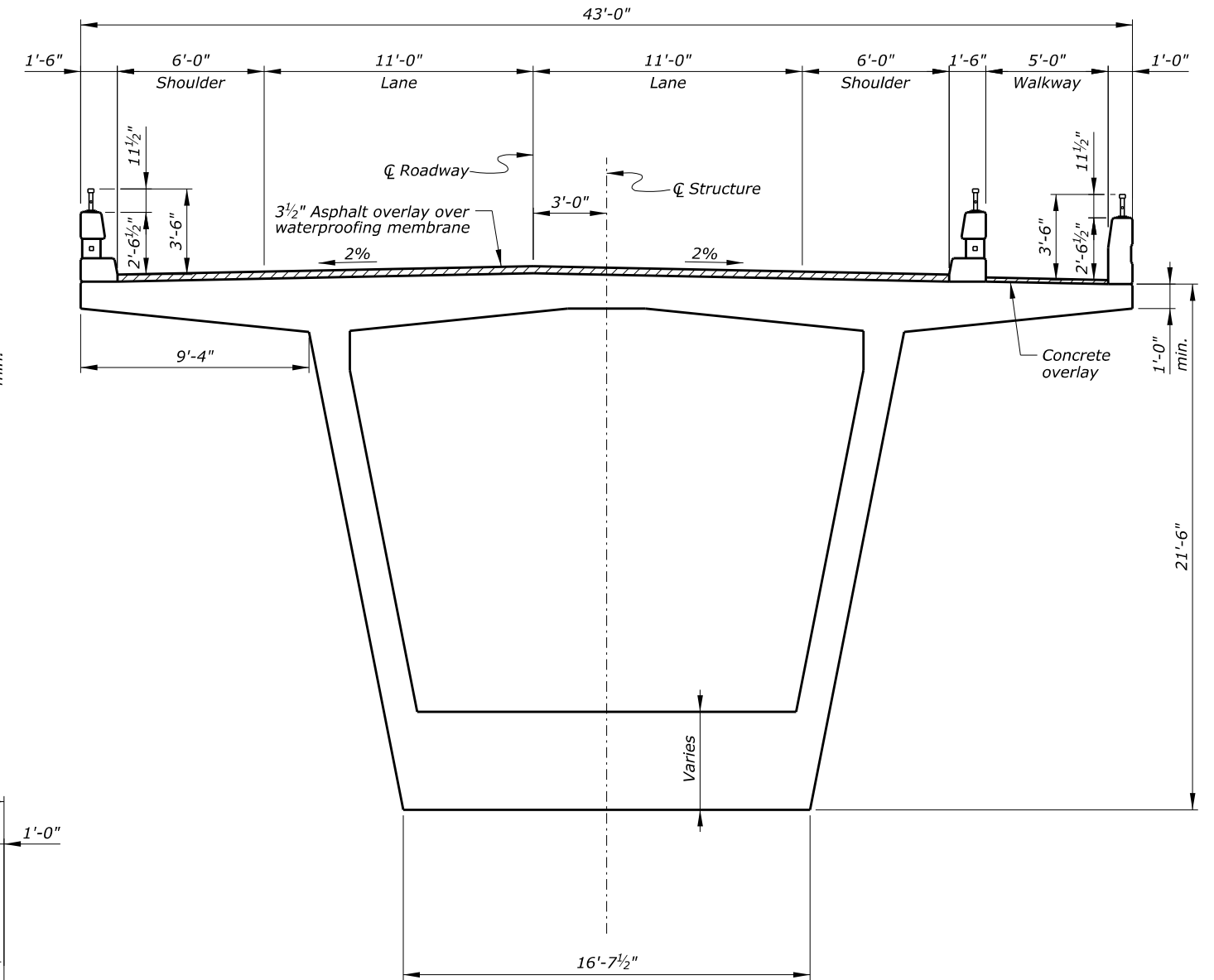
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)** PAGE 6 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< SL&L - Fourth Alignment.dgn

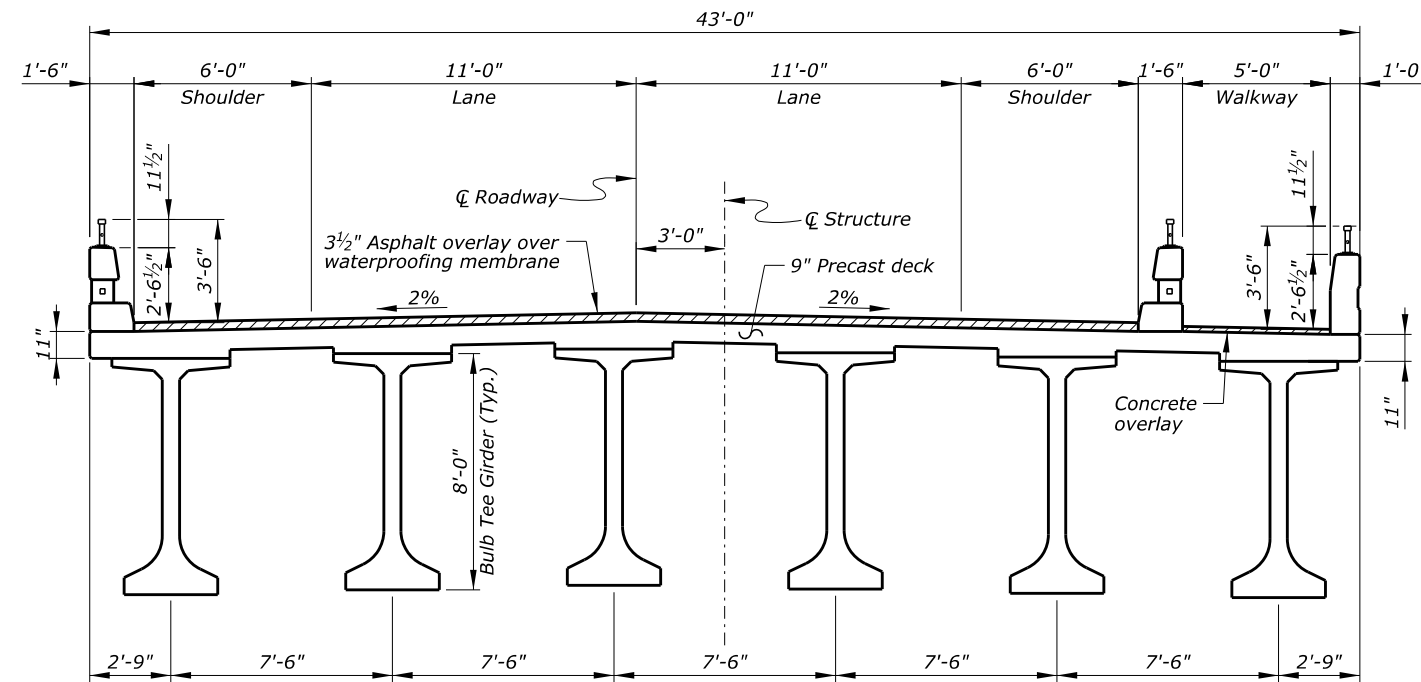
3/28/2023



TYPICAL SECTION
(Near midspan of span 13; Near pier 11 and 14)



TYPICAL SECTION
(Near pier 12 and 13)



TYPICAL SECTION
(Spans 1 - 11 and Spans 16 - 28)

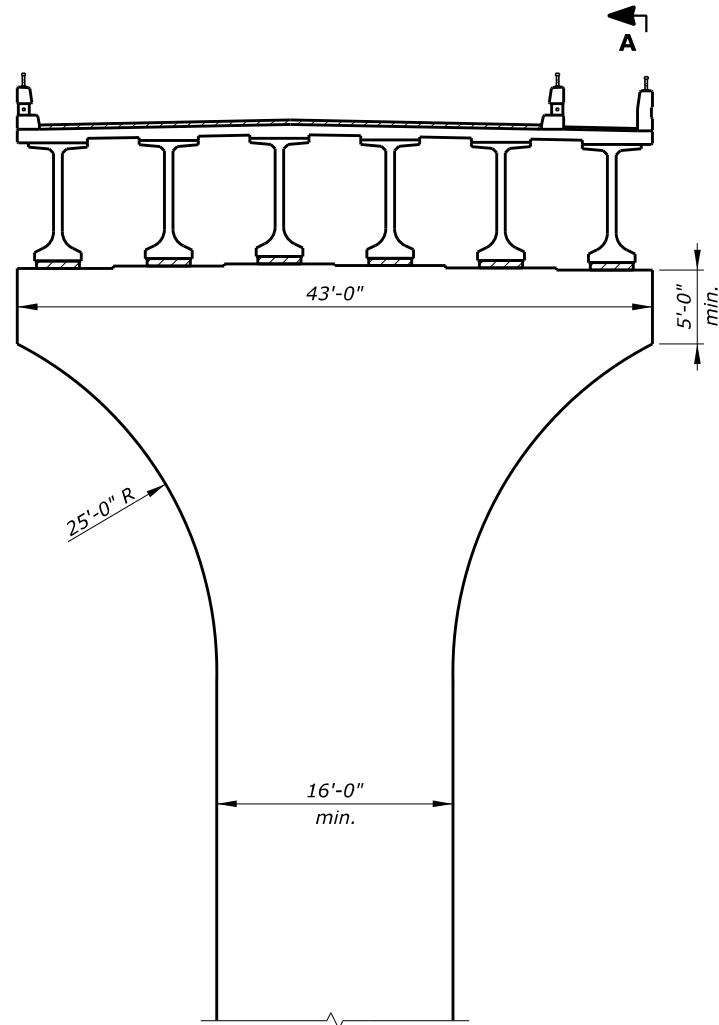
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$437,088,000
SCALE: No Scale DATE: March 2023

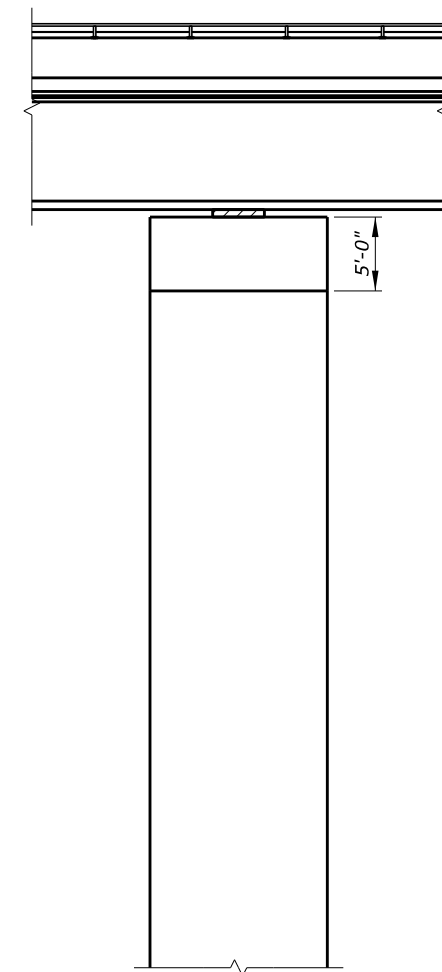
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)** PAGE 7 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

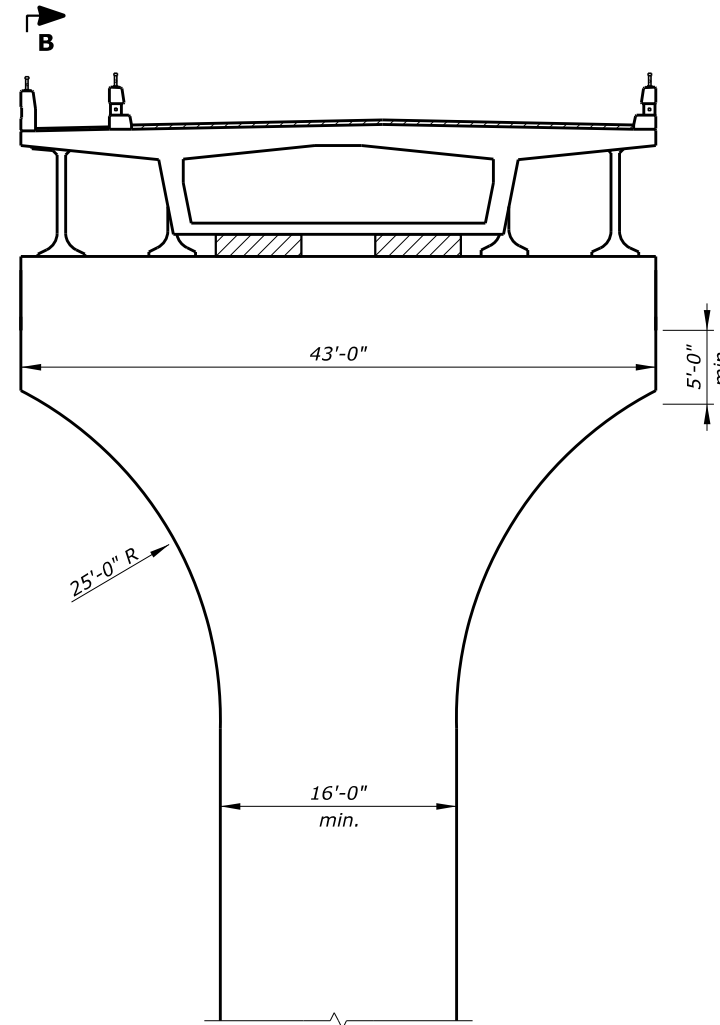
3/28/2023



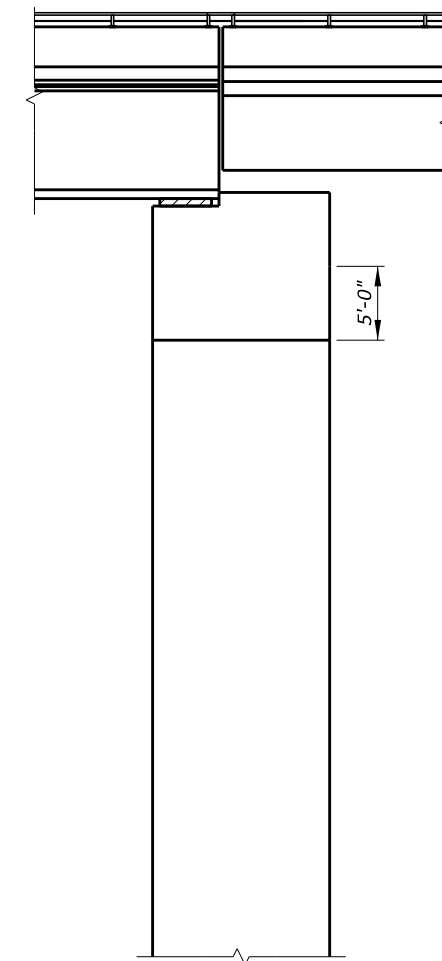
TYPICAL PIER SECTION
(Pier 1 - 10 and 15 - 27)
(Looking up station)



SECTION A-A



TYPICAL PIER SECTION
(Pier 11)
(Looking down station)
(Pier 14 Similar)



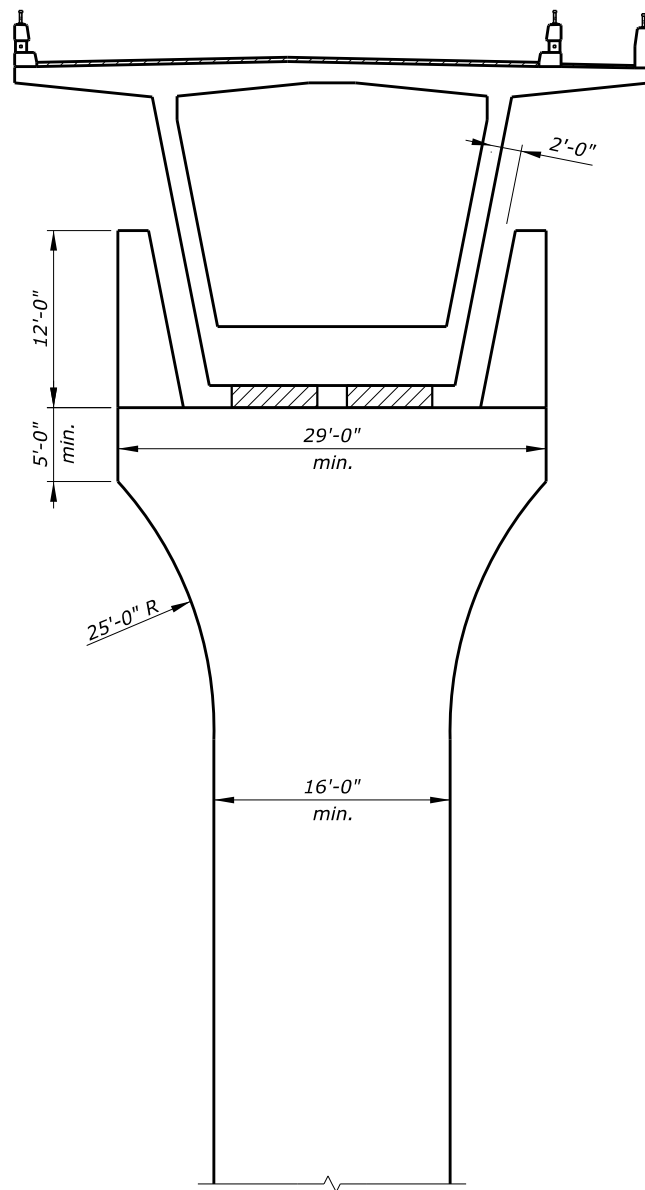
SECTION B-B

ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

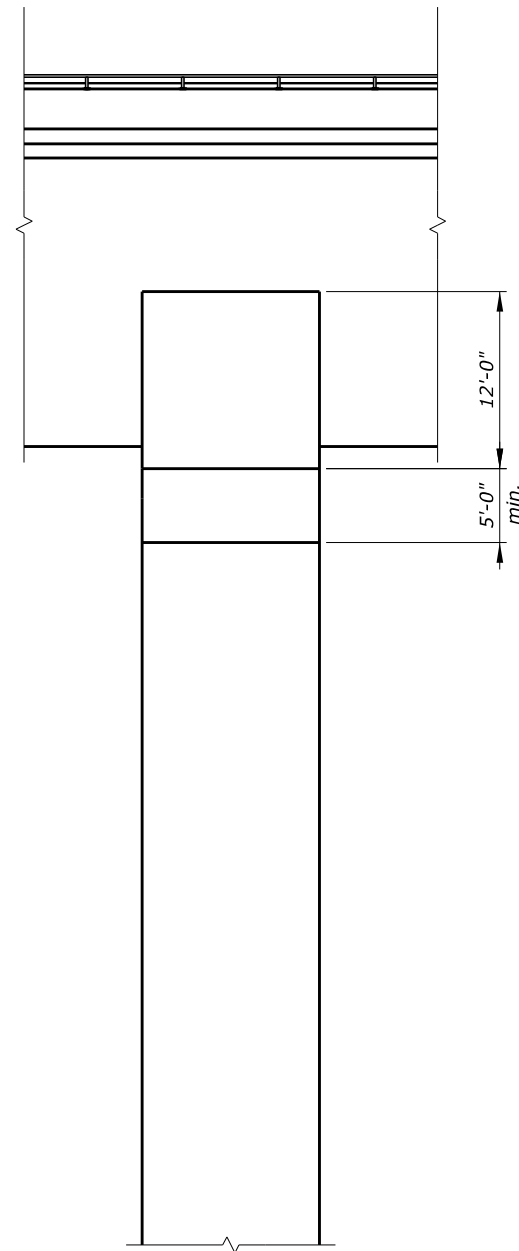
FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$437,088,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)** PAGE 8 OF 9

M:\PROJECTS\ndr\5570-028P\Bridge\Microstation\T S&L\T S&L - Fourth Alignment.dgn



TYPICAL PIER SECTION
(Pier 12 and 13)
(Looking up station)



SECTION A-A

3/28/2023

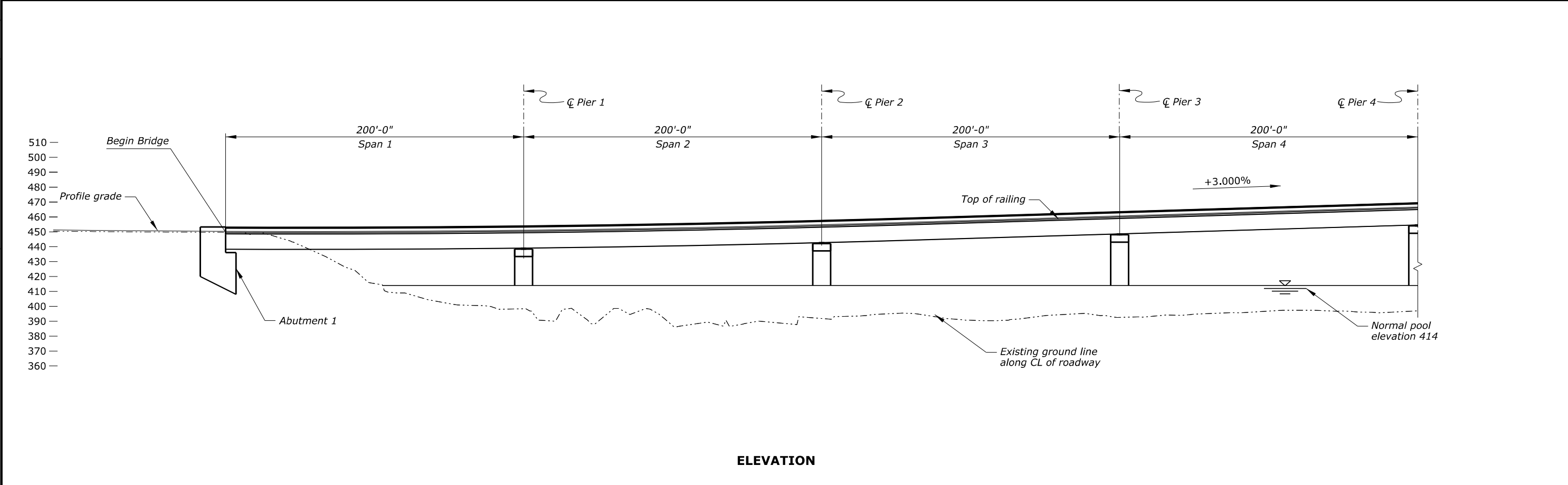
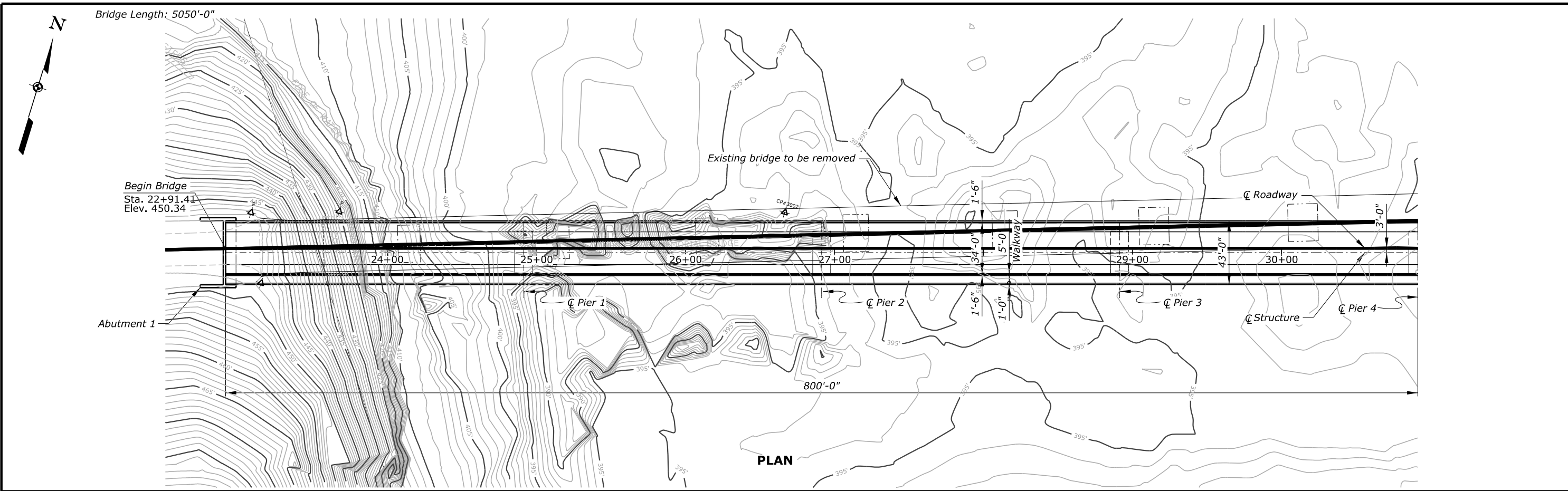
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$437,088,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 2 SEGMENTAL CONCRETE BOX
GIRDER/BULB TEE GIRDER (SKEWED ALIGNMENT)** PAGE 9 OF 9

M:\PROJECTS\0289\0289\Bridges\Microstation\TSS<\SL&L - Fourth Alignment.dgn

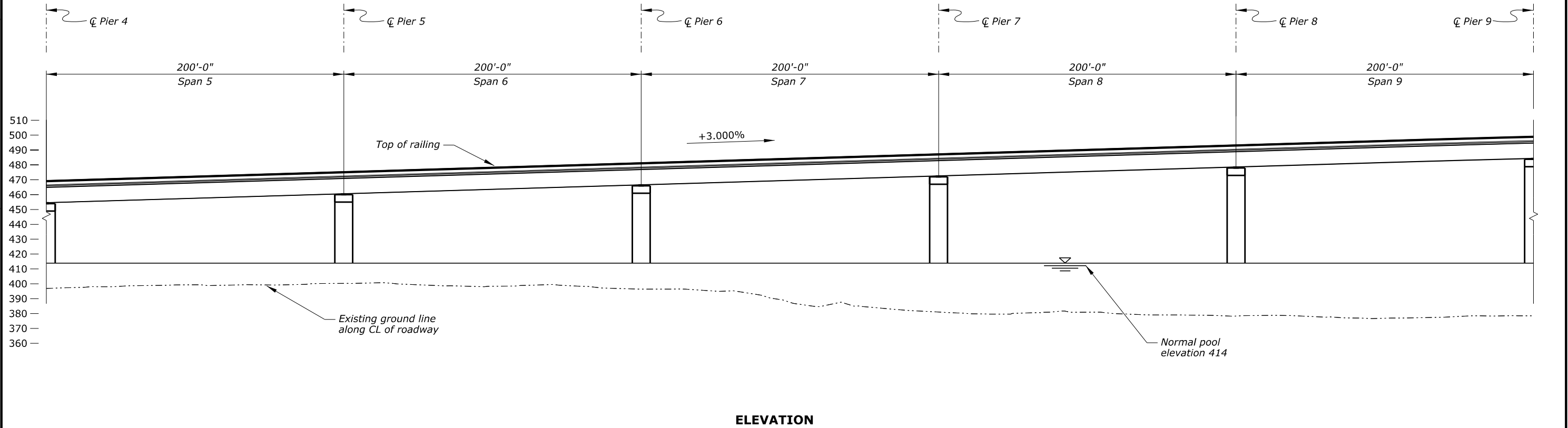
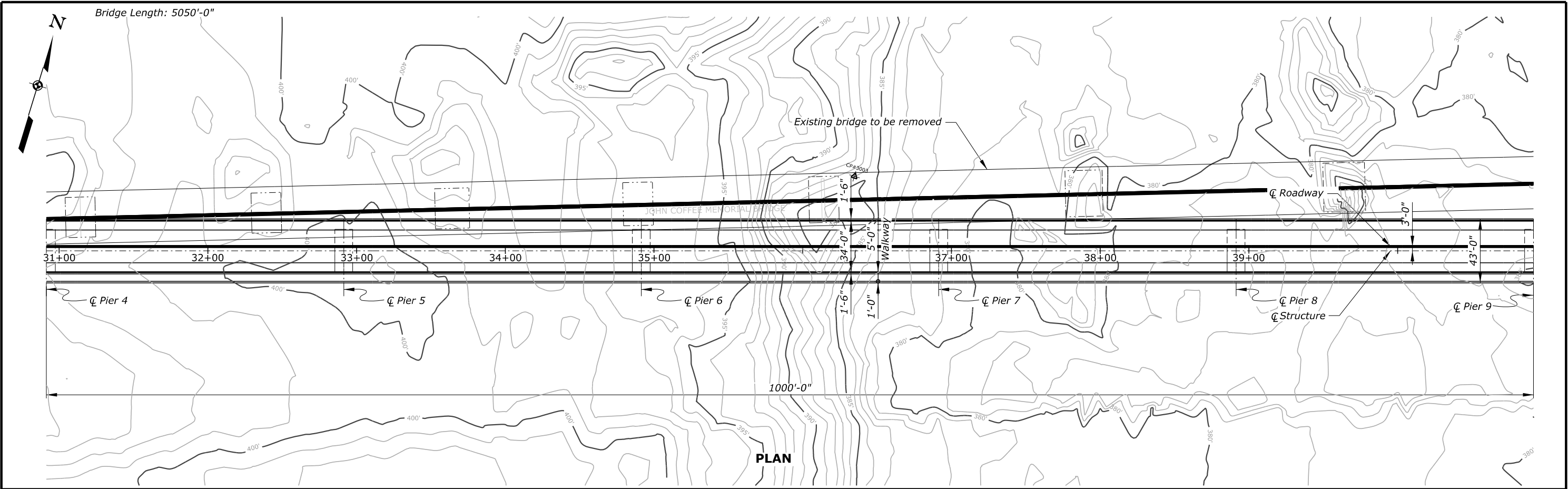
3/28/2023



ROUTE NAME AND NUMBER _____ Natchez Trace Parkway		FOREST/PARK/OTHER _____ Natchez Trace Parkway		U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME _____ JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 3 STEEL PLATE GIRDER (SKEWED ALIGNMENT)	
STATE _____ Alabama	COUNTY _____ Lauderdale	STREAM NAME _____	Tennessee River		
ACCOUNT NO. _____		ESTIMATED PROJECT COST _____ \$557,712,000			
PREPARED BY _____ Nicole Rojas		SCALE: _____ No Scale			
		DATE: _____ March 2023		PAGE 1 OF 9	

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

3/28/2023



ROUTE NAME AND NUMBER Natchez Trace Parkway

STATE Alabama COUNTY Lauderdale

ACCOUNT NO. _____

PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway

STREAM NAME Tennessee River

ESTIMATED PROJECT COST \$557,712,000

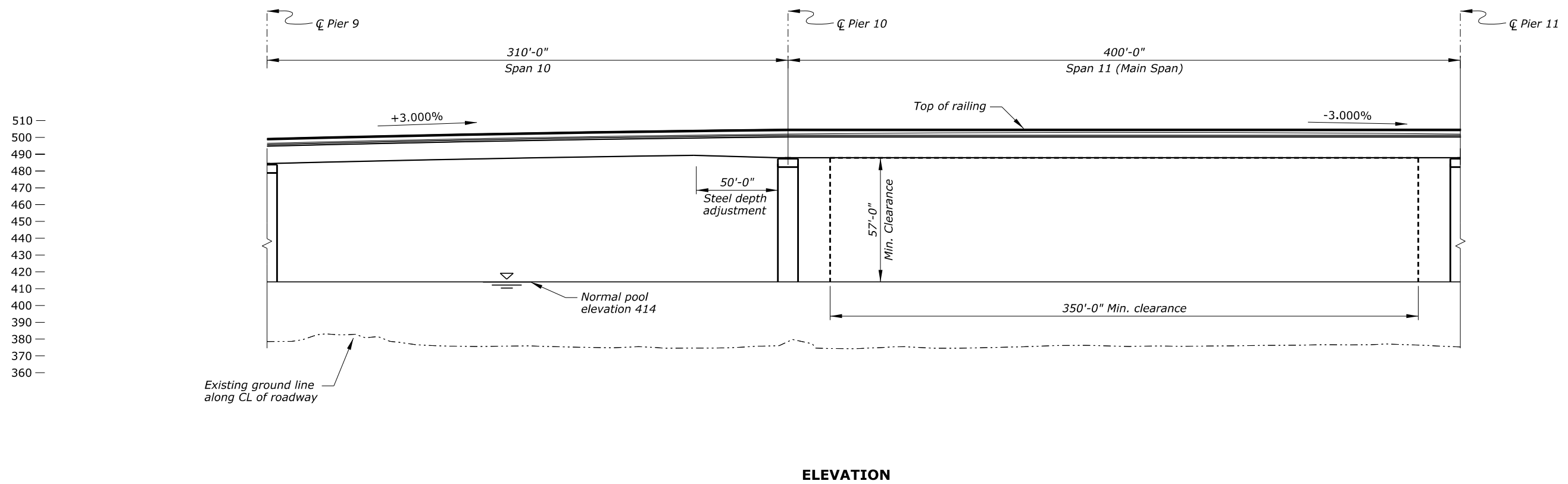
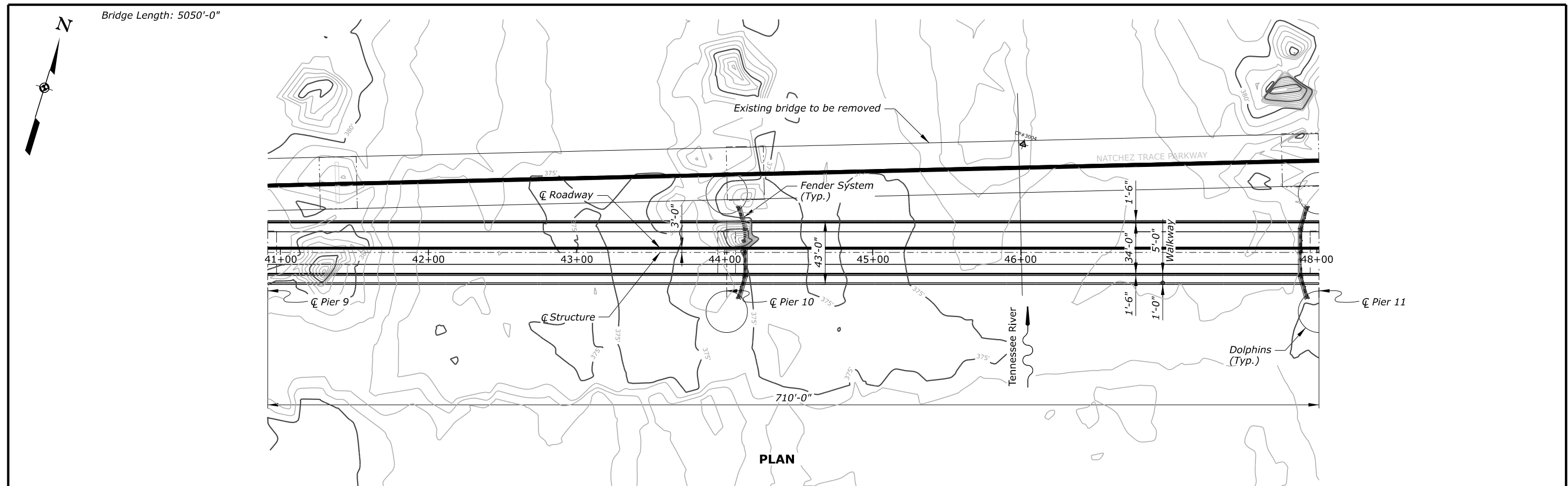
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION

BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE

**ALTERNATIVE 3 STEEL PLATE GIRDER
(SKEWED ALIGNMENT)**

PAGE 2 OF 9



ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
 STREAM NAME Tennessee River
 ESTIMATED PROJECT COST \$557,712,000

SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL HIGHWAY DIVISION

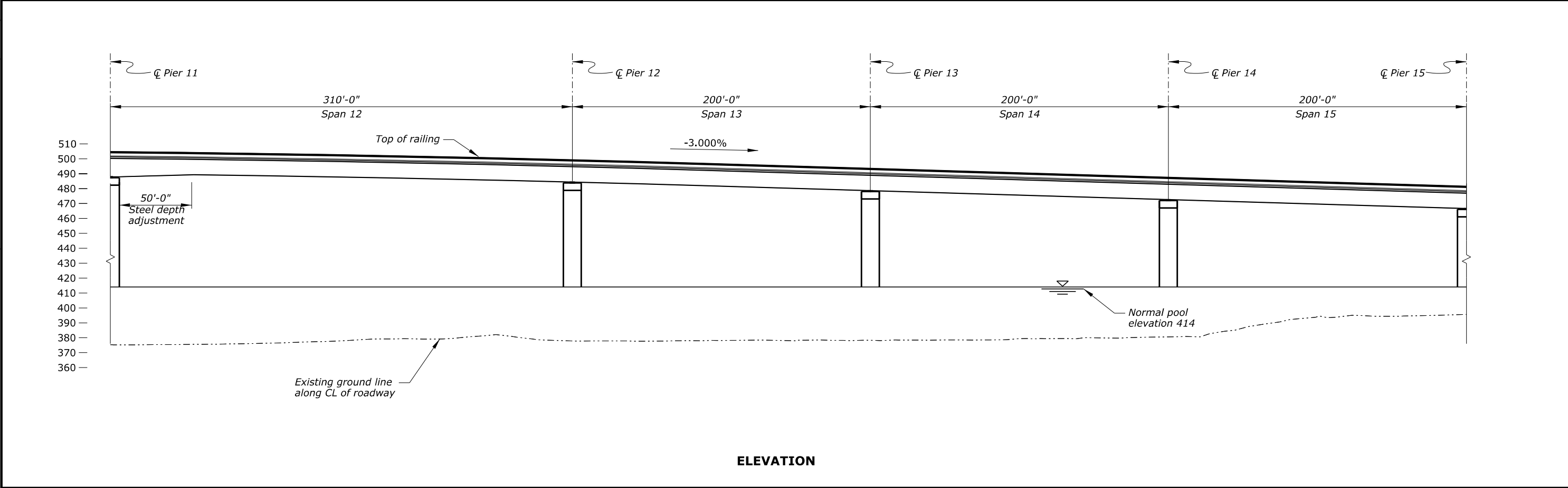
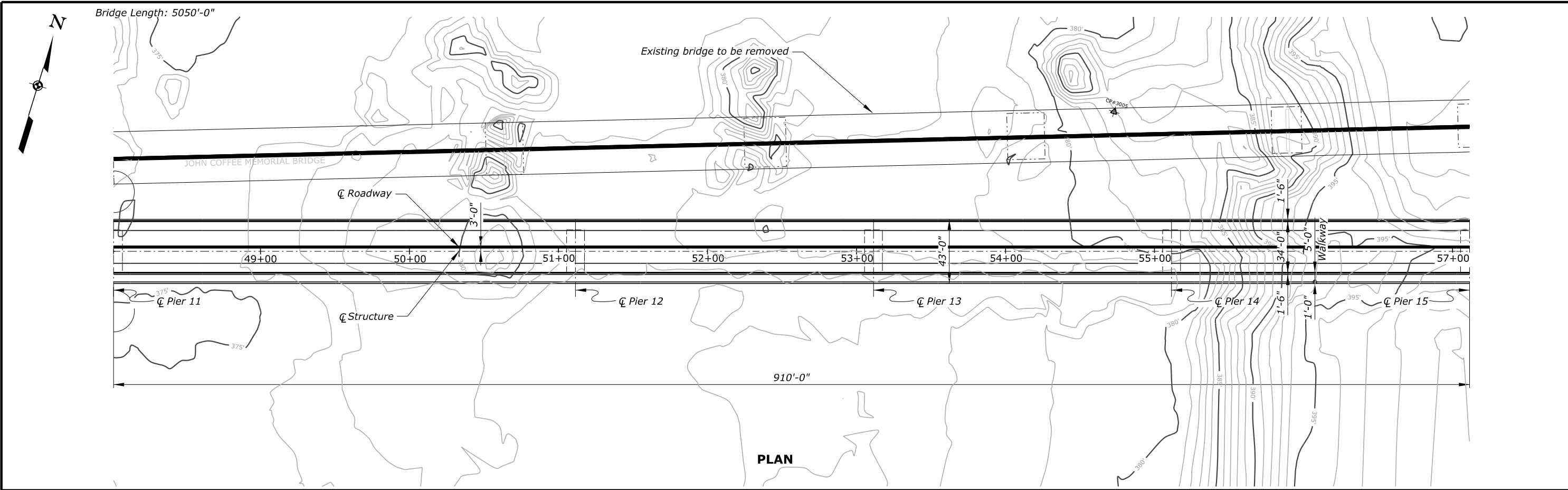
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE

**ALTERNATIVE 3 STEEL PLATE GIRDER
(SKEWED ALIGNMENT)**

PAGE 3 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\TSS<\SL&L - Fourth Alignment.dgn

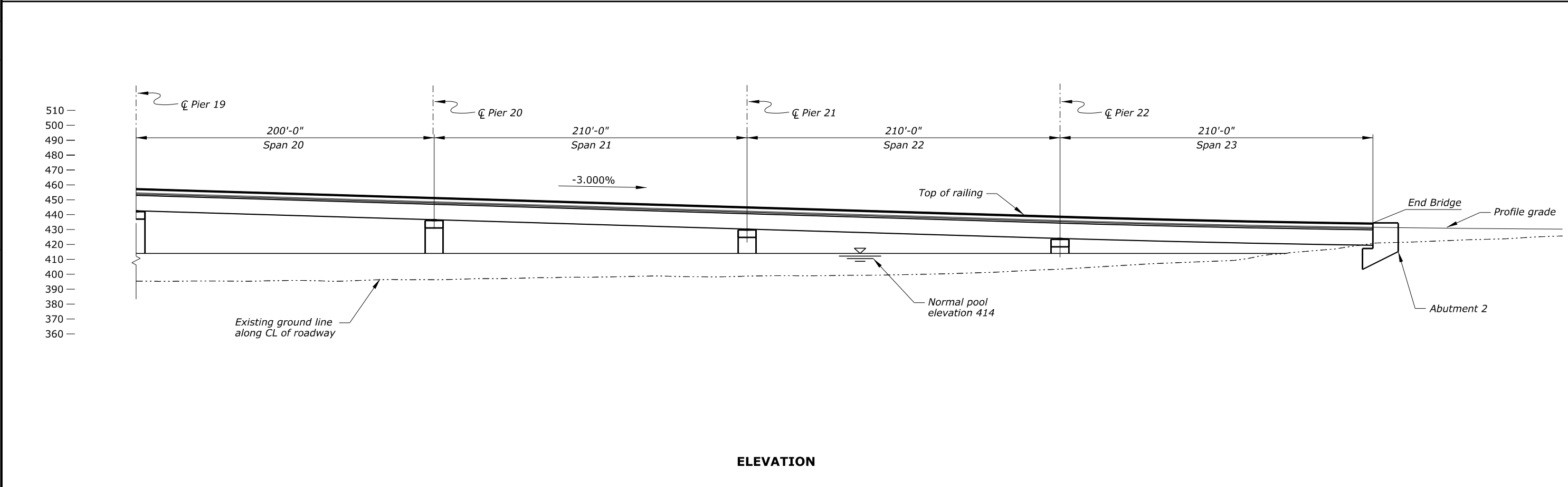
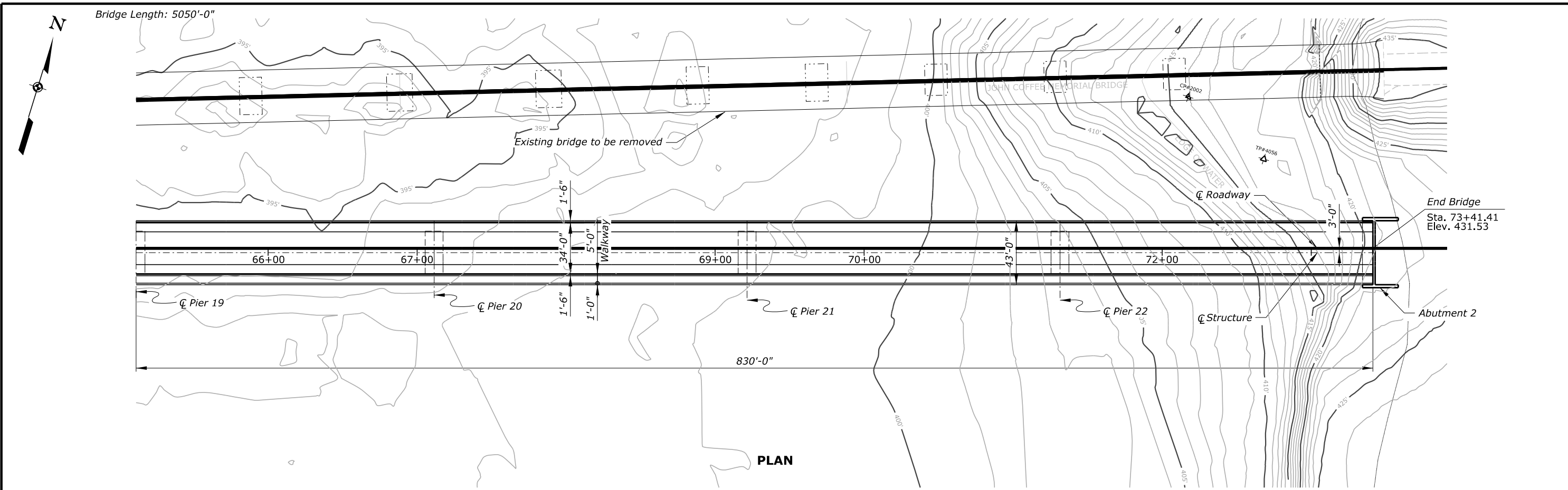
3/28/2023



ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 3 STEEL PLATE GIRDER (SKEWED ALIGNMENT)
STATE Alabama	COUNTY Lauderdale	STREAM NAME	Tennessee River	
ACCOUNT NO.		ESTIMATED PROJECT COST	\$557,712,000	
PREPARED BY	Nicole Rojas	SCALE:	No Scale	
		DATE:	March 2023	PAGE 4 OF 9

M:\PROJECTS\05570-028P\Bridges\Microstation\TSS<\SL&L - Fourth Alignment.dgn

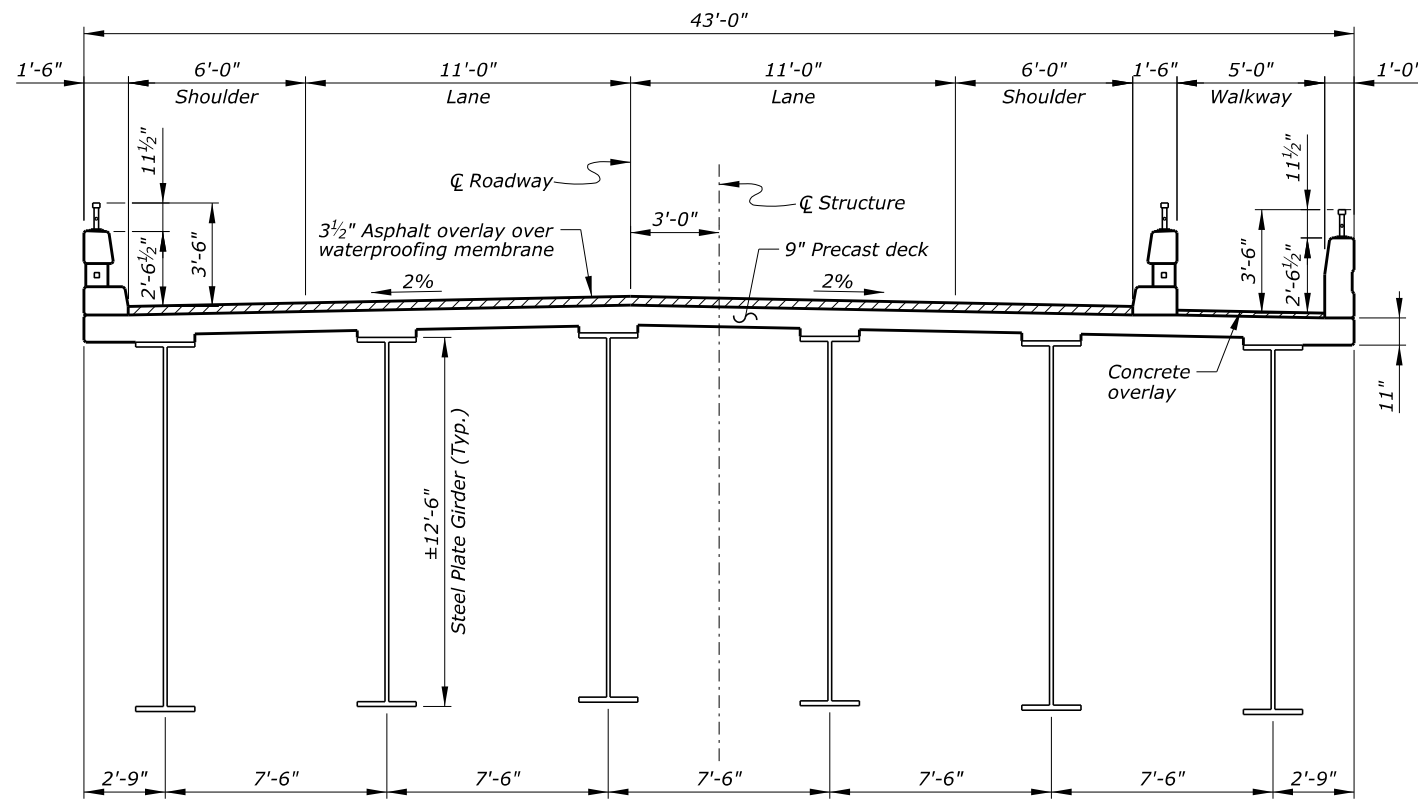
3/28/2023



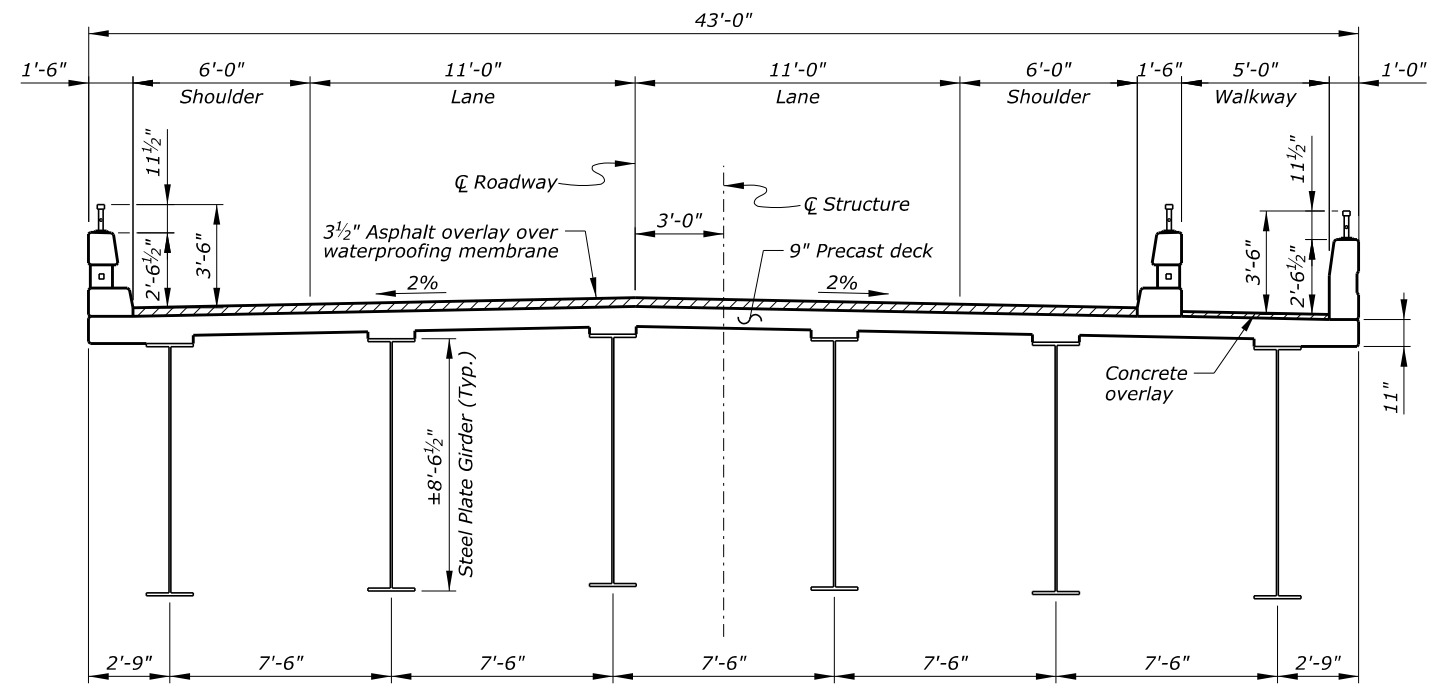
ROUTE NAME AND NUMBER	Natchez Trace Parkway	FOREST/PARK/OTHER	Natchez Trace Parkway	U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION EASTERN FEDERAL LANDS HIGHWAY DIVISION BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE ALTERNATIVE 3 STEEL PLATE GIRDER (SKEWED ALIGNMENT) PAGE 6 OF 9
STATE	Alabama	COUNTY	Lauderdale	
ACCOUNT NO.		ESTIMATED PROJECT COST	\$557,712,000	
PREPARED BY	Nicole Rojas	SCALE:	No Scale	
		DATE:	March 2023	

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S< SL&L - Fourth Alignment.dgn

3/28/2023



TYPICAL SECTION
(Span 11)



TYPICAL SECTION
(Spans 1 - 10 and Spans 12 - 23)

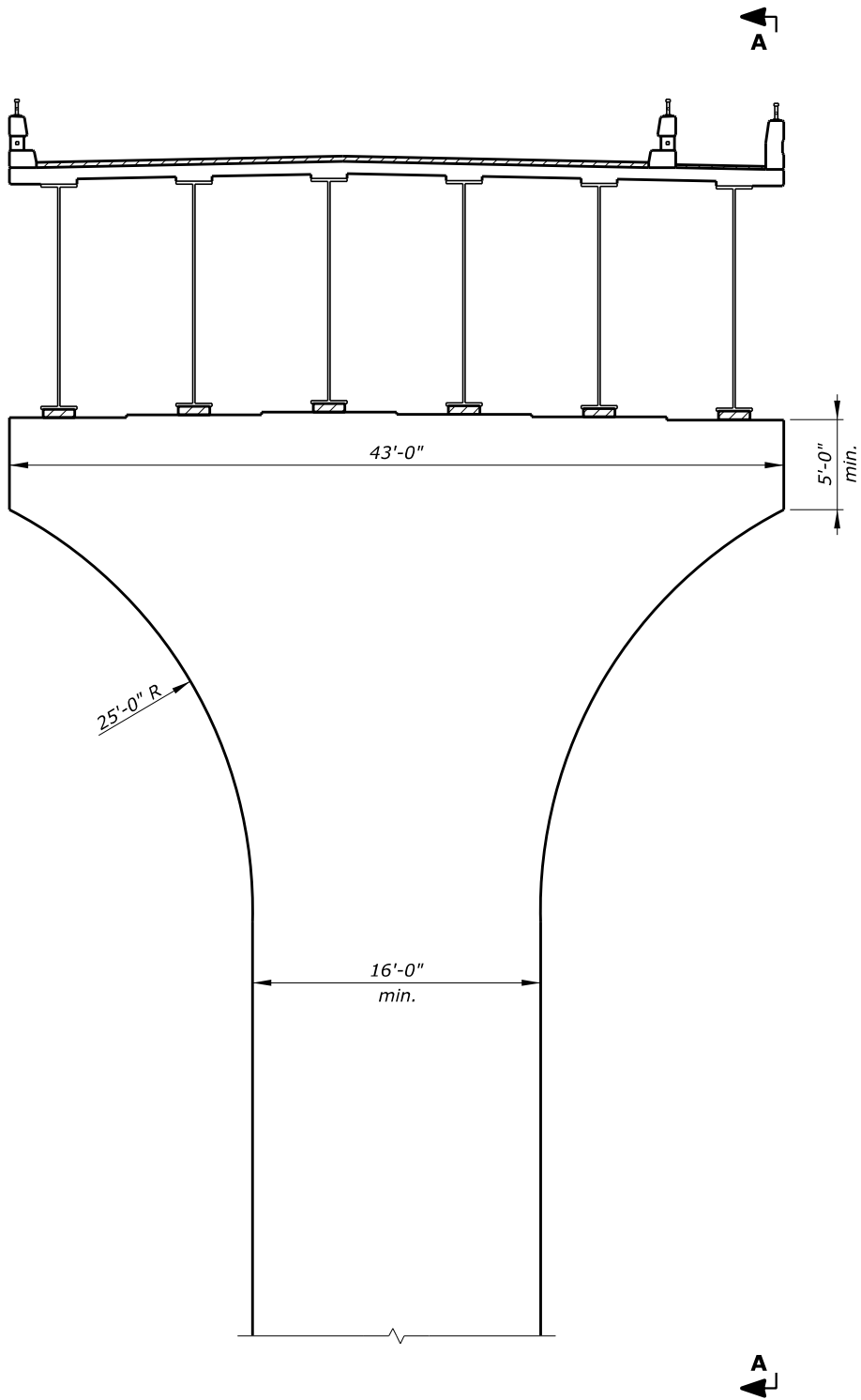
ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$557,712,000
SCALE: No Scale DATE: March 2023

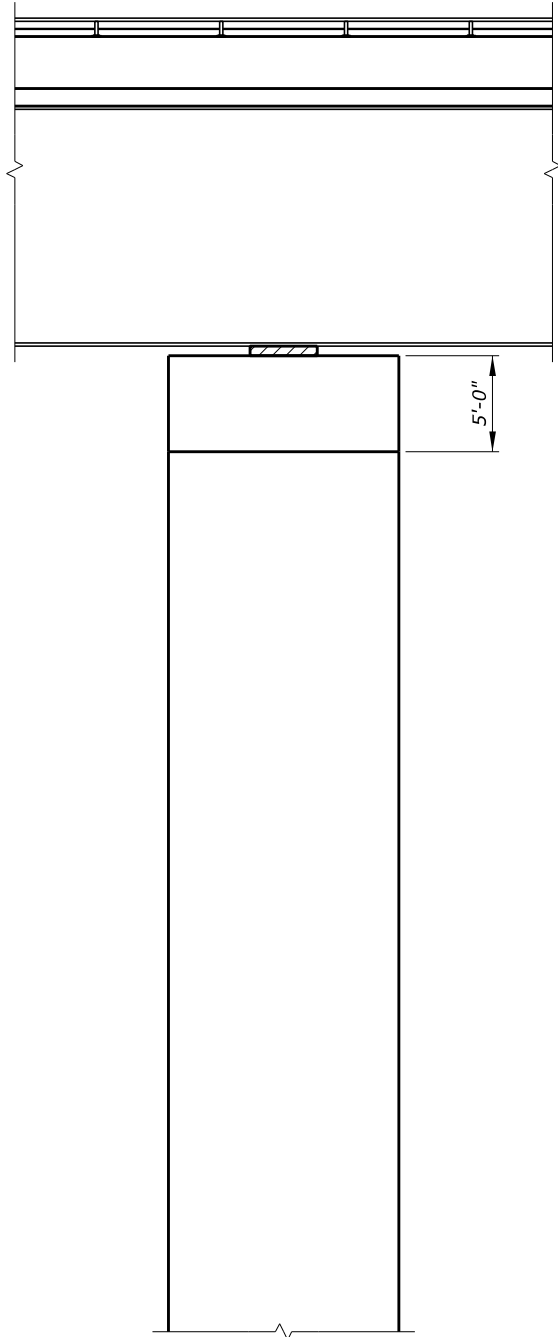
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
**ALTERNATIVE 3 STEEL PLATE GIRDER
(SKEWED ALIGNMENT)** PAGE 7 OF 9

M:\PROJECTS\ndr\5570-028P\Bridges\Microstation\T S&L\T S&L - Fourth Alignment.dgn

3/28/2023



TYPICAL PIER SECTION
(Looking up station)



SECTION A-A

ROUTE NAME AND NUMBER Natchez Trace Parkway
STATE Alabama COUNTY Lauderdale
ACCOUNT NO. _____
PREPARED BY Nicole Rojas

FOREST/PARK/OTHER Natchez Trace Parkway
STREAM NAME Tennessee River
ESTIMATED PROJECT COST \$557,712,000
SCALE: No Scale DATE: March 2023

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
EASTERN FEDERAL LANDS HIGHWAY DIVISION
BRIDGE NAME JOHN COFFEE MEMORIAL BRIDGE
ALTERNATIVE 3 STEEL PLATE GIRDER
(SKEWED ALIGNMENT) PAGE 8 OF 8

Appendix B:

Traffic Counts

Tube Reports

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
01:00 PM	0	1	0	3	3	0	0	0	0	0	0	0	0	0	7	26-35	6
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
03:00 PM	0	0	0	4	1	0	0	0	0	0	0	0	0	0	5	26-35	5
04:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
06:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	5	3	10	8	1	0	0	0	0	0	0	0	0	27	26-35	18
Percent	0%	18.5%	11.1%	37%	29.6%	3.7%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	6:00 AM	6:00 AM	7:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM		
	0	1	1	1	1	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	1:00 PM	5:00 PM	3:00 PM	1:00 PM	6:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM		
	0	1	1	4	3	1	0	0	0	0	0	0	0	0	7		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
12:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
02:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
05:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
06:00 PM	0	2	0	0	1	1	0	0	0	0	0	0	0	0	4	11-20	2
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	4	3	3	2	1	0	0	0	0	0	0	0	0	13	16-25	7
Percent	0%	30.8%	23.1%	23.1%	15.4%	7.7%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	0	2	1	0	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	6:00 PM	12:00 PM	12:00 PM	2:00 PM	6:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM		
	0	2	0	1	1	1	0	0	0	0	0	0	0	0	4		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	21-30	3
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
04:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
05:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	2	2	3	2	0	0	0	0	0	0	0	0	0	10	23-32	5
Percent	10%	20%	20%	30%	20%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM		
	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1		
PM Peak Volume	12:00 PM	3:00 PM	2:00 PM	2:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	1	1	2	1	1	0	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
06:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
11:00 AM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3	16-25	2
12:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
05:00 PM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	16-25	1
06:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
07:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2	11-20	1
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	2	4	1	8	2	0	0	0	0	0	0	0	0	18	31-40	10
Percent	5.6%	11.1%	22.2%	5.6%	44.4%	11.1%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	11:00 AM 1	11:00 AM 1	8:00 AM 1	10:00 AM 2	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3		
PM Peak Volume	6:00 PM 1	7:00 PM 1	4:00 PM 1	12:00 PM 0	12:00 PM 2	5:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 2		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	1	3	0	0	1	0	0	0	0	0	0	0	0	5	16-25	4
12:00 PM	0	1	0	1	0	1	0	0	0	0	0	0	0	0	3	11-20	1
01:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
03:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
04:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
06:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	3	5	4	5	2	0	0	0	0	0	0	0	0	19	21-30	9
Percent	0%	15.8%	26.3%	21.1%	26.3%	10.5%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	11:00 AM	7:00 AM	9:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	3	1	1	1	0	0	0	0	0	0	0	0	5		
PM Peak Volume	12:00 PM	12:00 PM	5:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	0	1	1	1	1	1	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
11:00 AM	0	0	0	3	1	0	0	0	0	0	0	0	0	0	4	26-35	4
12:00 PM	0	0	1	3	0	0	0	0	0	0	0	0	0	0	4	21-30	4
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	1
03:00 PM	0	0	2	0	1	0	0	0	0	0	0	0	0	0	3	16-25	2
04:00 PM	0	1	0	1	0	1	0	0	0	0	0	0	0	0	3	11-20	1
05:00 PM	1	0	1	0	0	2	0	0	0	0	0	0	0	0	4	31-40	2
06:00 PM	1	0	0	1	1	0	0	0	0	0	0	0	0	0	3	26-35	2
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	3	5	10	4	3	0	0	0	0	0	0	0	0	29	21-30	15
Percent	13.8%	10.3%	17.2%	34.5%	13.8%	10.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	3:00 AM 1	7:00 AM 1	12:00 AM 0	11:00 AM 3	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 4		
PM Peak Volume	2:00 PM 1	4:00 PM 1	3:00 PM 2	12:00 PM 3	3:00 PM 1	5:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 4		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
09:00 AM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	11-20	1
10:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	21-30	3
11:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
12:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	1
01:00 PM	0	0	1	2	1	0	0	0	0	0	0	0	0	0	4	26-35	3
02:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
03:00 PM	0	0	2	2	1	0	0	0	0	0	0	0	0	0	5	21-30	4
04:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
05:00 PM	0	0	0	1	2	0	0	0	0	0	0	0	0	0	3	26-35	3
06:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
07:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	3	10	9	6	1	0	0	0	0	0	0	0	0	30	21-30	19
Percent	3.3%	10%	33.3%	30%	20%	3.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	9:00 AM	10:00 AM	10:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	1	2	1	0	1	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	2:00 PM	3:00 PM	1:00 PM	5:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	1	1	2	2	2	0	0	0	0	0	0	0	0	0	5		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Overlook Rd Parking Area															QC JOB #: 15889230		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	7	22	32	40	35	10	0	0	0	0	0	0	0	0	146	26-35	75
Percent	4.8%	15.1%	21.9%	27.4%	24%	6.8%	0%	0%	0%	0%	0%	0%	0%	0%			
Cumulative Percent	4.8%	19.9%	41.8%	69.2%	93.2%	100%	100%	100%	100%	100%	100%	100%	100%	100%			
ADT 20															85th Percentile: 33 MPH Mean Speed(Average): 26 MPH Median: 26 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	1	4	0	0	2	0	0	0	0	0	0	0	0	0	7
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	16	6	0	4	0	0	0	0	0	0	0	0	0	27
Percent	3.7%	59.3%	22.2%	0%	14.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 27															
AM Peak Volume	12:00 AM	6:00 AM	4:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM
	0	1	1	0	1	0	0	0	0	0	0	0	0	0	2
PM Peak Volume	1:00 PM	1:00 PM	3:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM
	1	4	2	0	2	0	0	0	0	0	0	0	0	0	7

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	1	3	0	0	0	0	0	0	0	0	0	0	0	0	4
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	5	3	0	4	0	0	0	0	0	0	0	0	0	13
Percent	7.7%	38.5%	23.1%	0%	30.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 13															
AM Peak Volume	12:00 AM	11:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	2	0	0	1	0	0	0	0	0	0	0	0	0	2
PM Peak Volume	6:00 PM	6:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM
	1	3	1	0	1	0	0	0	0	0	0	0	0	0	4

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area


QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	7	2	0	1	0	0	0	0	0	0	0	0	0	10
Percent	0%	70%	20%	0%	10%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 10															
AM Peak Volume	12:00 AM 0	12:00 AM 0	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1
PM Peak Volume	12:00 PM 0	2:00 PM 2	4:00 PM 1	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 3

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area**QC JOB #:** 15889230**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	9	6	0	1	0	0	0	0	0	0	0	0	0	18
Percent	11.1%	50%	33.3%	0%	5.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 18															
AM Peak Volume	12:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	3	2	0	0	0	0	0	0	0	0	0	0	0	3
PM Peak Volume	12:00 PM	3:00 PM	12:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	1	1	1	0	1	0	0	0	0	0	0	0	0	0	2

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	3	0	0	2	0	0	0	0	0	0	0	0	0	5
12:00 PM	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	9	5	0	3	0	0	0	0	0	0	0	0	0	19
Percent	10.5%	47.4%	26.3%	0%	15.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 19															
AM Peak Volume	9:00 AM 1	11:00 AM 3	9:00 AM 1	12:00 AM 0	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 5
PM Peak Volume	12:00 PM 1	12:00 PM 1	12:00 PM 1	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 3

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	1	2
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	2	0	1	0	1	0	0	0	0	0	0	0	0	0	4
12:00 PM	0	1	1	0	1	0	0	1	0	0	0	0	0	0	4
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	2
03:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	14	6	0	3	0	0	1	0	0	0	0	0	2	29
Percent	10.3%	48.3%	20.7%	0%	10.3%	0%	0%	3.4%	0%	0%	0%	0%	0%	6.9%	
ADT 29															
AM Peak Volume	11:00 AM 2	2:00 AM 1	11:00 AM 1	12:00 AM 0	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	11:00 AM 4
PM Peak Volume	12:00 PM 0	4:00 PM 3	6:00 PM 2	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	12:00 PM 4

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
10:00 AM	2	0	1	0	0	0	0	0	0	0	0	0	0	0	3
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
02:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
04:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3
06:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	19	8	0	0	0	0	1	0	0	0	0	0	0	30
Percent	6.7%	63.3%	26.7%	0%	0%	0%	0%	3.3%	0%	0%	0%	0%	0%	0%	
ADT 30															
AM Peak Volume	10:00 AM 2	11:00 AM 2	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 3
PM Peak Volume	12:00 PM 0	1:00 PM 4	5:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 5

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	11	79	36	0	16	0	0	2	0	0	0	0	0	2	146
Percent	7.5%	54.1%	24.7%	0%	11%	0%	0%	1.4%	0%	0%	0%	0%	0%	1.4%	
ADT 20															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Overlook Rd Parking Area										QC JOB #: 15889230
SPECIFIC LOCATION:										DIRECTION: EB
CITY/STATE: Colbert, AL										DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0	0	0	0	0	0	0	0	
01:00 AM	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	1	0	0	
03:00 AM	0	0	0	0	0	0	1	0	0	
04:00 AM	1	0	0	0	0	0	0	0	0	
05:00 AM	0	0	0	1	0	0	0	0	0	
06:00 AM	2	0	1	1	0	1	0	1	1	<div></div>
07:00 AM	1	0	0	0	1	0	1	0	0	
08:00 AM	0	1	0	1	0	0	0	1	0	
09:00 AM	1	0	0	0	3	1	2	2	1	<div></div>
10:00 AM	0	1	0	2	0	1	1	3	1	<div></div>
11:00 AM	0	2	0	3	5	2	4	2	2	<div></div>
12:00 PM	2	1	1	2	3	2	4	2	2	<div></div>
01:00 PM	7	1	0	0	1	2	0	4	2	<div></div>
02:00 PM	1	1	3	0	1	1	2	2	1	<div></div>
03:00 PM	5	0	1	1	1	2	3	5	2	<div></div>
04:00 PM	1	1	1	1	1	1	3	1	1	<div></div>
05:00 PM	2	1	2	2	1	2	4	3	2	<div></div>
06:00 PM	2	4	0	1	1	2	3	2	2	<div></div>
07:00 PM	1	0	1	2	1	1	0	1	1	<div></div>
08:00 PM	1	0	0	1	0	0	0	0	0	
09:00 PM	0	0	0	0	0	0	0	0	0	
10:00 PM	0	0	0	0	0	0	0	1	0	
11:00 PM	0	0	0	0	0	0	0	0	0	
Day Total	27	13	10	18	19	18	29	30	18	
% Weekday Average	150%	72.2%	55.6%	100%	105.6%					
% Week Average	150%	72.2%	55.6%	100%	105.6%	100%	161.1%	166.7%		
AM Peak Volume	6:00 AM 2	11:00 AM 2	6:00 AM 1	11:00 AM 3	11:00 AM 5	11:00 AM 2	11:00 AM 4	10:00 AM 3	11:00 AM 2	
PM Peak Volume	1:00 PM 7	6:00 PM 4	2:00 PM 3	12:00 PM 2	12:00 PM 3	12:00 PM 2	12:00 PM 4	3:00 PM 5	12:00 PM 2	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	16-25	3
07:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	21-30	3
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	0	1	2	1	0	0	0	0	0	0	0	0	0	4	26-35	3
01:00 PM	2	1	1	3	4	2	1	0	0	0	0	0	0	0	14	26-35	7
02:00 PM	0	2	0	1	0	0	0	0	0	0	0	0	0	0	3	11-20	2
03:00 PM	0	0	0	7	2	0	0	0	0	0	0	0	0	0	9	26-35	9
04:00 PM	0	0	0	0	3	0	0	0	0	0	0	0	0	0	3	26-35	3
05:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4	16-25	4
06:00 PM	0	0	0	2	0	1	0	0	0	0	0	0	0	0	3	21-30	2
07:00 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3	11-20	2
08:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	11	7	16	13	5	1	0	0	0	0	0	0	0	55	26-35	29
Percent	3.6%	20%	12.7%	29.1%	23.6%	9.1%	1.8%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	6:00 AM	7:00 AM	7:00 AM	4:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM		
	0	2	2	1	1	1	0	0	0	0	0	0	0	0	3		
PM Peak Volume	1:00 PM	5:00 PM	12:00 PM	3:00 PM	1:00 PM	1:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM		
	2	3	1	7	4	2	1	0	0	0	0	0	0	0	14		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 AM	2	0	3	2	0	0	0	0	0	0	0	0	0	0	7	21-30	5
12:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
01:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
02:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
06:00 PM	0	3	1	1	1	2	0	0	0	0	0	0	0	0	8	16-25	4
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	5	8	9	2	2	0	0	0	0	0	0	0	0	28	21-30	17
Percent	7.1%	17.9%	28.6%	32.1%	7.1%	7.1%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	12:00 AM 0	11:00 AM 3	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 7		
PM Peak Volume	12:00 PM 0	6:00 PM 3	12:00 PM 1	4:00 PM 2	2:00 PM 1	6:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 8		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	11-20	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	4	1	0	0	0	0	0	0	0	0	0	0	5	21-30	5
03:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
04:00 PM	0	1	0	2	0	0	0	0	0	0	0	0	0	0	3	21-30	2
05:00 PM	0	2	1	1	0	0	0	0	0	0	0	0	0	0	4	16-25	3
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	5	5	5	4	0	0	0	0	0	0	0	0	0	20	16-25	10
Percent	5%	25%	25%	25%	20%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM		
	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	5:00 PM	2:00 PM	4:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	1	2	4	2	2	0	0	0	0	0	0	0	0	0	5		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
06:00 AM	0	0	0	0	2	1	0	0	0	0	0	0	0	0	3	31-40	3
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 AM	0	0	0	0	3	1	0	0	0	0	0	0	0	0	4	31-40	4
11:00 AM	0	2	1	1	2	0	0	0	0	0	0	0	0	0	6	16-25	3
12:00 PM	0	0	0	0	3	1	0	0	0	0	0	0	0	0	4	31-40	4
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
04:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
05:00 PM	0	1	1	0	0	2	0	0	0	0	0	0	0	0	4	31-40	2
06:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1-10	1
07:00 PM	0	2	0	1	1	0	0	0	0	0	0	0	0	0	4	11-20	2
08:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	5	4	5	14	6	0	0	0	0	0	0	0	0	36	31-40	20
Percent	5.6%	13.9%	11.1%	13.9%	38.9%	16.7%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	11:00 AM	8:00 AM	10:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	2	1	1	3	1	0	0	0	0	0	0	0	0	6		
PM Peak Volume	6:00 PM	7:00 PM	4:00 PM	4:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	2	2	1	1	3	2	0	0	0	0	0	0	0	0	4		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	2	2	1	0	0	0	0	0	0	0	0	0	5	21-30	4
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	3	3	2	0	2	0	0	0	0	0	0	0	0	10	16-25	6
12:00 PM	0	1	1	2	1	1	0	0	0	0	0	0	0	0	6	26-35	3
01:00 PM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	3	26-35	2
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	11-20	2
03:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
04:00 PM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	26-35	3
05:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
06:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
08:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	6	8	13	7	4	0	0	0	0	0	0	0	0	38	21-30	21
Percent	0%	15.8%	21.1%	34.2%	18.4%	10.5%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	11:00 AM	7:00 AM	9:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	3	3	2	1	2	0	0	0	0	0	0	0	0	10		
PM Peak Volume	12:00 PM	2:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	0	2	1	2	2	1	0	0	0	0	0	0	0	0	6		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 AM	1	0	0	2	0	0	0	0	0	0	0	0	0	0	3	21-30	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
09:00 AM	1	0	0	2	0	0	0	0	0	0	0	0	0	0	3	21-30	2
10:00 AM	1	1	0	2	1	0	0	0	0	0	0	0	0	0	5	26-35	3
11:00 AM	0	0	1	4	3	0	0	0	0	0	0	0	0	0	8	26-35	7
12:00 PM	0	0	1	5	0	0	0	0	0	0	0	0	0	0	6	21-30	6
01:00 PM	0	0	0	1	0	0	1	0	0	0	0	0	0	0	2	21-30	1
02:00 PM	1	0	1	0	1	0	0	0	0	0	0	0	0	0	3	16-25	1
03:00 PM	0	2	3	0	1	0	0	0	0	0	0	0	0	0	6	16-25	5
04:00 PM	0	1	0	1	3	2	0	0	0	0	0	0	0	0	7	31-40	5
05:00 PM	1	0	2	1	0	2	2	0	0	0	0	0	0	0	8	36-45	4
06:00 PM	1	0	1	1	1	0	0	0	0	0	0	0	0	0	4	21-30	2
07:00 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2	21-30	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	7	6	9	20	11	4	3	0	0	0	0	0	0	0	60	26-35	31
Percent	11.7%	10%	15%	33.3%	18.3%	6.7%	5%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	3:00 AM 1	7:00 AM 1	11:00 AM 1	11:00 AM 4	11:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 8		
PM Peak Volume	2:00 PM 1	3:00 PM 2	3:00 PM 3	12:00 PM 5	4:00 PM 3	4:00 PM 2	5:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	5:00 PM 8		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
09:00 AM	0	1	1	0	0	1	0	0	0	0	0	0	0	0	3	16-25	2
10:00 AM	0	1	2	2	0	0	0	0	0	0	0	0	0	0	5	21-30	4
11:00 AM	0	1	2	2	1	0	0	0	0	0	0	0	0	0	6	21-30	4
12:00 PM	1	1	1	1	0	0	0	0	0	0	0	0	0	0	4	16-25	2
01:00 PM	0	0	2	3	2	1	0	0	0	0	0	0	0	0	8	23-32	5
02:00 PM	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	16-25	2
03:00 PM	0	0	3	6	1	0	0	0	0	0	0	0	0	0	10	21-30	9
04:00 PM	0	0	0	3	0	0	0	0	0	0	0	0	0	0	3	21-30	3
05:00 PM	0	0	0	1	4	0	0	0	0	0	0	0	0	0	5	26-35	5
06:00 PM	0	0	0	2	3	0	0	0	0	0	0	0	0	0	5	26-35	5
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
Day Total	1	6	16	23	12	2	0	0	0	0	0	0	0	0	60	21-30	39
Percent	1.7%	10%	26.7%	38.3%	20%	3.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	9:00 AM	6:00 AM	10:00 AM	11:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	2	2	1	1	0	0	0	0	0	0	0	0	6		
PM Peak Volume	12:00 PM	12:00 PM	3:00 PM	3:00 PM	5:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	1	1	3	6	4	1	0	0	0	0	0	0	0	0	10		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Overlook Rd Parking Area															QC JOB #: 15889230		
SPECIFIC LOCATION:															DIRECTION: EB, WB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	15	44	57	91	63	23	4	0	0	0	0	0	0	0	297	26-35	154
Percent	5.1%	14.8%	19.2%	30.6%	21.2%	7.7%	1.3%	0%	0%	0%	0%	0%	0%	0%			
Cumulative Percent	5.1%	19.9%	39.1%	69.7%	90.9%	98.7%	100%	100%	100%	100%	100%	100%	100%	100%			
ADT 42															85th Percentile: 33 MPH Mean Speed(Average): 26 MPH Median: 26 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Off-Peak) Overlook Rd Parking Area**QC JOB #:** 15889230**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
07:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
01:00 PM	1	8	0	0	4	0	0	0	0	0	0	0	0	1	14
02:00 PM	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	6	3	0	0	0	0	0	0	0	0	0	0	0	9
04:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
07:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
08:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	32	12	0	8	0	0	0	0	0	0	0	0	1	55
Percent	3.6%	58.2%	21.8%	0%	14.5%	0%	0%	0%	0%	0%	0%	0%	0%	1.8%	
ADT 55															
AM Peak Volume	12:00 AM	6:00 AM	4:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM
	0	2	2	0	2	0	0	0	0	0	0	0	0	0	3
PM Peak Volume	1:00 PM	1:00 PM	3:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	1:00 PM
	1	8	3	0	4	0	0	0	0	0	0	0	0	1	14

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889230

DIRECTION: EB, WB

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	4	0	0	1	0	0	0	0	0	0	0	0	2	7
12:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	2	6	0	0	0	0	0	0	0	0	0	0	0	0	8
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	10	6	0	8	0	0	0	0	0	0	0	0	2	28
Percent	7.1%	35.7%	21.4%	0%	28.6%	0%	0%	0%	0%	0%	0%	0%	0%	7.1%	
ADT 28															
AM Peak Volume	12:00 AM	11:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM
	0	4	0	0	1	0	0	0	0	0	0	0	0	2	7
PM Peak Volume	6:00 PM	6:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM
	2	6	2	0	2	0	0	0	0	0	0	0	0	0	8

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area**QC JOB #:** 15889230**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	4	0	0	1	0	0	0	0	0	0	0	0	0	5
03:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	14	4	0	2	0	0	0	0	0	0	0	0	0	20
Percent	0%	70%	20%	0%	10%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 20															
AM Peak Volume	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM
	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
PM Peak Volume	12:00 PM	2:00 PM	4:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM
	0	4	2	0	1	0	0	0	0	0	0	0	0	0	5

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889230

DIRECTION: EB, WB

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
06:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4
11:00 AM	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
12:00 PM	2	0	2	0	0	0	0	0	0	0	0	0	0	0	4
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
07:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
08:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	4	18	12	0	2	0	0	0	0	0	0	0	0	0	36
Percent	11.1%	50%	33.3%	0%	5.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 36															
AM Peak Volume	12:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	6	4	0	0	0	0	0	0	0	0	0	0	0	6
PM Peak Volume	12:00 PM	3:00 PM	12:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	2	2	2	0	2	0	0	0	0	0	0	0	0	0	4

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area**QC JOB #:** 15889230**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	2	1	2	0	0	0	0	0	0	0	0	0	0	0	5
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	7	0	0	3	0	0	0	0	0	0	0	0	0	10
12:00 PM	2	1	2	0	1	0	0	0	0	0	0	0	0	0	6
01:00 PM	0	1	0	0	2	0	0	0	0	0	0	0	0	0	3
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	4	18	10	0	6	0	0	0	0	0	0	0	0	0	38
Percent	10.5%	47.4%	26.3%	0%	15.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 38															
AM Peak Volume	9:00 AM 2	11:00 AM 7	9:00 AM 2	12:00 AM 0	11:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 10
PM Peak Volume	12:00 PM 2	2:00 PM 2	12:00 PM 2	12:00 PM 0	1:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 6
Comments:															

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	2	3
10:00 AM	2	2	0	0	0	0	0	0	0	0	0	0	0	1	5
11:00 AM	4	0	2	0	2	0	0	0	0	0	0	0	0	0	8
12:00 PM	0	2	1	0	2	0	0	1	0	0	0	0	0	0	6
01:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
02:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	1	3
03:00 PM	0	3	1	0	2	0	0	0	0	0	0	0	0	0	6
04:00 PM	0	7	0	0	0	0	0	0	0	0	0	0	0	0	7
05:00 PM	0	6	2	0	0	0	0	0	0	0	0	0	0	0	8
06:00 PM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	8	28	12	0	6	0	0	2	0	0	0	0	0	4	60
Percent	13.3%	46.7%	20%	0%	10%	0%	0%	3.3%	0%	0%	0%	0%	0%	6.7%	
ADT 60															
AM Peak Volume	11:00 AM 4	3:00 AM 3	11:00 AM 2	12:00 AM 0	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 2	11:00 AM 8
PM Peak Volume	2:00 PM 1	4:00 PM 7	6:00 PM 3	12:00 PM 0	12:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	5:00 PM 8

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area**QC JOB #:** 15889230**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
10:00 AM	2	0	2	0	0	0	0	1	0	0	0	0	0	0	5
11:00 AM	2	4	0	0	0	0	0	0	0	0	0	0	0	0	6
12:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
01:00 PM	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8
02:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	9	1	0	0	0	0	0	0	0	0	0	0	0	10
04:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	0	5	0	0	0	0	0	0	0	0	0	0	0	5
06:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
07:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	4	39	15	0	0	0	0	2	0	0	0	0	0	0	60
Percent	6.7%	65%	25%	0%	0%	0%	0%	3.3%	0%	0%	0%	0%	0%	0%	
ADT 60															
AM Peak Volume	10:00 AM 2	11:00 AM 4	6:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 6
PM Peak Volume	12:00 PM 0	3:00 PM 9	5:00 PM 5	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 10

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	24	159	71	0	32	0	0	4	0	0	0	0	0	7	297
Percent	8.1%	53.5%	23.9%	0%	10.8%	0%	0%	1.3%	0%	0%	0%	0%	0%	2.4%	
ADT 42															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL										QC JOB #: 15889230 DIRECTION: EB, WB DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0	0	0	0	0	0	0	0	
01:00 AM	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	1	0	0	
03:00 AM	0	0	0	0	0	0	3	0	0	
04:00 AM	2	0	0	0	0	0	0	0	0	
05:00 AM	0	0	0	1	0	0	0	0	0	
06:00 AM	3	0	2	3	0	2	0	2	1	<div></div>
07:00 AM	3	0	0	0	2	1	1	0	1	<div></div>
08:00 AM	0	1	0	1	0	0	1	2	1	<div></div>
09:00 AM	2	1	0	1	5	2	3	3	2	<div></div>
10:00 AM	0	1	0	4	0	1	5	5	2	<div></div>
11:00 AM	0	7	0	6	10	5	8	6	5	<div></div>
12:00 PM	4	2	2	4	6	4	6	4	4	<div></div>
01:00 PM	14	2	0	0	3	4	2	8	4	<div></div>
02:00 PM	3	2	5	0	2	2	3	3	3	<div></div>
03:00 PM	9	0	2	2	1	3	6	10	4	<div></div>
04:00 PM	3	2	3	2	3	3	7	3	3	<div></div>
05:00 PM	4	2	4	4	2	3	8	5	4	<div></div>
06:00 PM	3	8	0	2	1	3	4	5	3	<div></div>
07:00 PM	3	0	2	4	2	2	2	2	2	<div></div>
08:00 PM	2	0	0	2	1	1	0	0	1	<div></div>
09:00 PM	0	0	0	0	0	0	0	0	0	
10:00 PM	0	0	0	0	0	0	0	1	0	
11:00 PM	0	0	0	0	0	0	0	1	0	
Day Total	55	28	20	36	38	36	60	60	40	
% Weekday Average	152.8%	77.8%	55.6%	100%	105.6%					
% Week Average	137.5%	70%	50%	90%	95%	90%	150%	150%		
AM Peak Volume	6:00 AM 3	11:00 AM 7	6:00 AM 2	11:00 AM 6	11:00 AM 10	11:00 AM 5	11:00 AM 8	11:00 AM 6	11:00 AM 5	
PM Peak Volume	1:00 PM 14	6:00 PM 8	2:00 PM 5	12:00 PM 4	12:00 PM 6	12:00 PM 4	5:00 PM 8	3:00 PM 10	12:00 PM 4	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
07:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
01:00 PM	2	0	1	0	1	2	1	0	0	0	0	0	0	0	7	36-45	3
02:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
03:00 PM	0	0	0	3	1	0	0	0	0	0	0	0	0	0	4	26-35	4
04:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
05:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	11-20	2
06:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	11-20	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	6	4	6	5	4	1	0	0	0	0	0	0	0	28	26-35	11
Percent	7.1%	21.4%	14.3%	21.4%	17.9%	14.3%	3.6%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	6:00 AM 1	7:00 AM 2	12:00 AM 0	12:00 AM 0	4:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 2		
PM Peak Volume	1:00 PM 2	5:00 PM 2	12:00 PM 1	3:00 PM 3	4:00 PM 2	1:00 PM 2	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 7		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	2	0	1	2	0	0	0	0	0	0	0	0	0	0	5	21-30	3
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
01:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
06:00 PM	0	1	1	1	0	1	0	0	0	0	0	0	0	0	4	16-25	2
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	1	5	6	0	1	0	0	0	0	0	0	0	0	15	21-30	11
Percent	13.3%	6.7%	33.3%	40%	0%	6.7%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	12:00 AM 0	1:00 AM 1	2:00 AM 2	3:00 AM 0	4:00 AM 0	5:00 AM 0	6:00 AM 0	7:00 AM 0	8:00 AM 0	9:00 AM 0	10:00 AM 0	11:00 AM 0	12:00 AM 0	5		
PM Peak Volume	12:00 PM 0	6:00 PM 1	12:00 PM 1	1:00 PM 1	12:00 PM 0	6:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
03:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	3	3	2	2	0	0	0	0	0	0	0	0	0	10	16-25	6
Percent	0%	30%	30%	20%	20%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM		
	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1		
PM Peak Volume	12:00 PM	12:00 PM	2:00 PM	3:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	0	1	2	1	1	0	0	0	0	0	0	0	0	0	2		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
11:00 AM	0	1	0	1	1	0	0	0	0	0	0	0	0	0	3	26-35	2
12:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
05:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	11-20	1
06:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
07:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	3	0	4	6	4	0	0	0	0	0	0	0	0	18	28-37	10
Percent	5.6%	16.7%	0%	22.2%	33.3%	22.2%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	12:00 AM	9:00 AM	6:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	0	1	2	1	0	0	0	0	0	0	0	0	3		
PM Peak Volume	6:00 PM	5:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	1	1	0	1	1	1	0	0	0	0	0	0	0	0	2		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	2	0	2	0	1	0	0	0	0	0	0	0	0	5	11-20	2
12:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
01:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
05:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
08:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	3	3	9	2	2	0	0	0	0	0	0	0	0	19	21-30	12
Percent	0%	15.8%	15.8%	47.4%	10.5%	10.5%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	9:00 AM	11:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	2	1	2	0	1	0	0	0	0	0	0	0	0	5		
PM Peak Volume	12:00 PM	2:00 PM	12:00 PM	4:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	0	1	1	2	1	1	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
09:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
10:00 AM	1	0	0	2	1	0	0	0	0	0	0	0	0	0	4	26-35	3
11:00 AM	0	0	1	1	2	0	0	0	0	0	0	0	0	0	4	26-35	3
12:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
01:00 PM	0	0	0	1	0	0	1	0	0	0	0	0	0	0	2	21-30	1
02:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	16-25	3
04:00 PM	0	0	0	0	3	1	0	0	0	0	0	0	0	0	4	31-40	4
05:00 PM	0	0	1	1	0	0	2	0	0	0	0	0	0	0	4	36-45	2
06:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
07:00 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2	21-30	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	3	4	10	7	1	3	0	0	0	0	0	0	0	31	26-35	17
Percent	9.7%	9.7%	12.9%	32.3%	22.6%	3.2%	9.7%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	9:00 AM 1	8:00 AM 1	11:00 AM 1	3:00 AM 2	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 4		
PM Peak Volume	7:00 PM 1	3:00 PM 2	3:00 PM 1	12:00 PM 2	4:00 PM 3	4:00 PM 1	5:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 4		
Comments:																	

Report generated on 9/2/2022 10:20 AM


SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889230 DIRECTION: WB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
10:00 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
11:00 AM	0	0	1	2	1	0	0	0	0	0	0	0	0	0	4	26-35	3
12:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
01:00 PM	0	0	1	1	1	1	0	0	0	0	0	0	0	0	4	21-30	2
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
03:00 PM	0	0	1	4	0	0	0	0	0	0	0	0	0	0	5	21-30	5
04:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
05:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
06:00 PM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	26-35	3
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
Day Total	0	3	6	14	6	1	0	0	0	0	0	0	0	0	30	21-30	20
Percent	0%	10%	20%	46.7%	20%	3.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	10:00 AM	6:00 AM	11:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	1	2	1	0	0	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	12:00 PM	1:00 PM	3:00 PM	5:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	0	1	1	4	2	1	0	0	0	0	0	0	0	0	5		
Comments:																	

Report generated on 9/2/2022 10:20 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Overlook Rd Parking Area															QC JOB #: 15889230		
SPECIFIC LOCATION:															DIRECTION: WB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	8	22	25	51	28	13	4	0	0	0	0	0	0	0	151	26-35	79
Percent	5.3%	14.6%	16.6%	33.8%	18.5%	8.6%	2.6%	0%	0%	0%	0%	0%	0%	0%			
Cumulative Percent	5.3%	19.9%	36.4%	70.2%	88.7%	97.4%	100%	100%	100%	100%	100%	100%	100%	100%			
ADT 21															85th Percentile: 34 MPH Mean Speed(Average): 27 MPH Median: 27 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	4	0	0	2	0	0	0	0	0	0	0	0	1	7
02:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
04:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	16	6	0	4	0	0	0	0	0	0	0	0	1	28
Percent	3.6%	57.1%	21.4%	0%	14.3%	0%	0%	0%	0%	0%	0%	0%	0%	3.6%	
ADT 28															
AM Peak Volume	12:00 AM	6:00 AM	4:00 AM	12:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM
	0	1	1	0	1	0	0	0	0	0	0	0	0	0	2
PM Peak Volume	2:00 PM	1:00 PM	4:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	1:00 PM
	1	4	2	0	2	0	0	0	0	0	0	0	0	1	7

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	2	5
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	1	3	0	0	0	0	0	0	0	0	0	0	0	0	4
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	5	3	0	4	0	0	0	0	0	0	0	0	2	15
Percent	6.7%	33.3%	20%	0%	26.7%	0%	0%	0%	0%	0%	0%	0%	0%	13.3%	
ADT 15															
AM Peak Volume	12:00 AM	11:00 AM	12:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM
	0	2	0	0	1	0	0	0	0	0	0	0	0	2	5
PM Peak Volume	6:00 PM	6:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM
	1	3	1	0	1	0	0	0	0	0	0	0	0	0	4

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	7	2	0	1	0	0	0	0	0	0	0	0	0	10
Percent	0%	70%	20%	0%	10%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 10															
AM Peak Volume	12:00 AM 0	12:00 AM 0	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1
PM Peak Volume	12:00 PM 0	2:00 PM 2	4:00 PM 1	12:00 PM 0	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 2

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	9	6	0	1	0	0	0	0	0	0	0	0	0	18
Percent	11.1%	50%	33.3%	0%	5.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 18															
AM Peak Volume	12:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	3	2	0	0	0	0	0	0	0	0	0	0	0	3
PM Peak Volume	12:00 PM	3:00 PM	12:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	1	1	1	0	1	0	0	0	0	0	0	0	0	0	2

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	4	0	0	1	0	0	0	0	0	0	0	0	0	5
12:00 PM	1	0	1	0	1	0	0	0	0	0	0	0	0	0	3
01:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	9	5	0	3	0	0	0	0	0	0	0	0	0	19
Percent	10.5%	47.4%	26.3%	0%	15.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 19															
AM Peak Volume	9:00 AM 1	11:00 AM 4	9:00 AM 1	12:00 AM 0	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 5
PM Peak Volume	12:00 PM 1	1:00 PM 1	12:00 PM 1	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 3

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
10:00 AM	2	1	0	0	0	0	0	0	0	0	0	0	0	1	4
11:00 AM	2	0	1	0	1	0	0	0	0	0	0	0	0	0	4
12:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
02:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
05:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	5	14	6	0	3	0	0	1	0	0	0	0	0	2	31
Percent	16.1%	45.2%	19.4%	0%	9.7%	0%	0%	3.2%	0%	0%	0%	0%	0%	6.5%	
ADT 31															
AM Peak Volume	10:00 AM 2	3:00 AM 2	11:00 AM 1	12:00 AM 0	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	10:00 AM 4
PM Peak Volume	2:00 PM 1	4:00 PM 4	1:00 PM 1	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 4

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
11:00 AM	2	2	0	0	0	0	0	0	0	0	0	0	0	0	4
12:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
04:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	2	20	7	0	0	0	0	1	0	0	0	0	0	0	30
Percent	6.7%	66.7%	23.3%	0%	0%	0%	0%	3.3%	0%	0%	0%	0%	0%	0%	
ADT 30															
AM Peak Volume	11:00 AM 2	11:00 AM 2	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 4
PM Peak Volume	12:00 PM 0	3:00 PM 5	5:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 5

Comments:

LOCATION: (Off-Peak) Overlook Rd Parking Area

QC JOB #: 15889230

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	13	80	35	0	16	0	0	2	0	0	0	0	0	5	151
Percent	8.6%	53%	23.2%	0%	10.6%	0%	0%	1.3%	0%	0%	0%	0%	0%	3.3%	
ADT 21															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Overlook Rd Parking Area										QC JOB #: 15889230
SPECIFIC LOCATION:										DIRECTION: WB
CITY/STATE: Colbert, AL										DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0	0	0	0	0	0	0	0	
01:00 AM	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	0	0	0	
03:00 AM	0	0	0	0	0	0	2	0	0	
04:00 AM	1	0	0	0	0	0	0	0	0	
05:00 AM	0	0	0	0	0	0	0	0	0	
06:00 AM	1	0	1	2	0	1	0	1	1	<div></div>
07:00 AM	2	0	0	0	1	1	0	0	0	
08:00 AM	0	0	0	0	0	0	1	1	0	
09:00 AM	1	1	0	1	2	1	1	1	1	<div></div>
10:00 AM	0	0	0	2	0	0	4	2	1	<div></div>
11:00 AM	0	5	0	3	5	3	4	4	3	<div></div>
12:00 PM	2	1	1	2	3	2	2	2	2	<div></div>
01:00 PM	7	1	0	0	2	2	2	4	2	<div></div>
02:00 PM	2	1	2	0	1	1	1	1	1	<div></div>
03:00 PM	4	0	1	1	0	1	3	5	2	<div></div>
04:00 PM	2	1	2	1	2	2	4	2	2	<div></div>
05:00 PM	2	1	2	2	1	2	4	2	2	<div></div>
06:00 PM	1	4	0	1	0	1	1	3	1	<div></div>
07:00 PM	2	0	1	2	1	1	2	1	1	<div></div>
08:00 PM	1	0	0	1	1	1	0	0	0	
09:00 PM	0	0	0	0	0	0	0	0	0	
10:00 PM	0	0	0	0	0	0	0	0	0	
11:00 PM	0	0	0	0	0	0	0	1	0	
Day Total	28	15	10	18	19	19	31	30	19	
% Weekday Average	147.4%	78.9%	52.6%	94.7%	100%					
% Week Average	147.4%	78.9%	52.6%	94.7%	100%	100%	163.2%	157.9%		
AM Peak Volume	7:00 AM 2	11:00 AM 5	6:00 AM 1	11:00 AM 3	11:00 AM 5	11:00 AM 3	10:00 AM 4	11:00 AM 4	11:00 AM 3	
PM Peak Volume	1:00 PM 7	6:00 PM 4	2:00 PM 2	12:00 PM 2	12:00 PM 3	12:00 PM 2	4:00 PM 4	3:00 PM 5	12:00 PM 2	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Colbert Ferry boat ramp**QC JOB #:** 15889232**SPECIFIC LOCATION:****DIRECTION:** NB**CITY/STATE:** Colbert, AL**DATE:** Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0		1
06:00 AM	0	0	3	0	1	0	0	0	0	0	0	0	0		4
07:00 AM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0		2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
10:00 AM	0	0	4	0	1	0	0	0	0	0	0	0	0		5
11:00 AM	0	0	3	0	1	0	0	0	0	0	0	0	0		4
12:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
01:00 PM	0	0	4	0	0	0	0	0	0	0	0	0	0		4
02:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0		1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
05:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
06:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0		1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
Day Total	0	0	33	0	4	0	0	0	0	0	0	0	0		37
Percent	0%	0%	89.2%	0%	10.8%	0%	0%	0%	0%	0%	0%	0%	0%		
ADT 37															
AM Peak Volume	12:00 AM 0	12:00 AM 0	10:00 AM 4	12:00 AM 0	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0		10:00 AM 5
PM Peak Volume	12:00 PM 0	12:00 PM 0	1:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0		1:00 PM 4

Comments:

LOCATION: (Off-Peak) Colbert Ferry boat ramp


QC JOB #: 15889232

SPECIFIC LOCATION:

DIRECTION: NB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	0	0	33	0	4	0	0	0	0	0	0	0	0		37
Percent	0%	0%	89.2%	0%	10.8%	0%	0%	0%	0%	0%	0%	0%	0%		
ADT 37															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Colbert Ferry boat ramp						QC JOB #: 15889232				
SPECIFIC LOCATION:						DIRECTION: NB				
CITY/STATE: Colbert, AL						DATE: Aug 27 2022 - Aug 27 2022				
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM							0		0	
01:00 AM							0		0	
02:00 AM							0		0	
03:00 AM							0		0	
04:00 AM							0		0	
05:00 AM							1		1	<div></div>
06:00 AM							4		4	<div></div>
07:00 AM							3		3	<div></div>
08:00 AM							2		2	<div></div>
09:00 AM							0		0	
10:00 AM							5		5	<div></div>
11:00 AM							4		4	<div></div>
12:00 PM							3		3	<div></div>
01:00 PM							4		4	<div></div>
02:00 PM							3		3	<div></div>
03:00 PM							1		1	<div></div>
04:00 PM							0		0	
05:00 PM		3		3	<div></div>					
06:00 PM		3		3	<div></div>					
07:00 PM		0		0						
08:00 PM		1		1	<div></div>					
09:00 PM		0		0						
10:00 PM		0		0						
11:00 PM		0		0						
Day Total							37		37	
% Weekday Average										
% Week Average						0%	100%			
AM Peak Volume							10:00 AM 5		10:00 AM 5	
PM Peak Volume							1:00 PM 4		1:00 PM 4	
Comments:										

LOCATION: (Off-Peak) Colbert Ferry boat ramp

QC JOB #: 15889232

SPECIFIC LOCATION:

DIRECTION: NB, SB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
06:00 AM	0	0	7	0	2	0	0	0	0	0	0	0	0	0	9
07:00 AM	0	0	5	0	0	0	0	0	0	0	0	0	0	0	5
08:00 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	4
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	8	0	2	0	0	0	0	0	0	0	0	0	10
11:00 AM	0	0	6	0	2	0	0	0	0	0	0	0	0	0	8
12:00 PM	0	0	6	0	0	0	0	0	0	0	0	0	0	0	6
01:00 PM	0	0	9	0	0	0	0	0	0	0	0	0	0	0	9
02:00 PM	0	0	5	0	0	0	0	0	0	0	0	0	0	0	5
03:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	6	0	0	0	0	0	0	0	0	0	0	0	6
06:00 PM	0	0	6	0	0	0	0	0	0	0	0	0	0	0	6
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	0	66	0	8	0	0	0	0	0	0	0	0	0	74
Percent	0%	0%	89.2%	0%	10.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 74															
AM Peak Volume	12:00 AM	12:00 AM	10:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM
	0	0	8	0	2	0	0	0	0	0	0	0	0	0	10
PM Peak Volume	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM
	0	0	9	0	0	0	0	0	0	0	0	0	0	0	9

Comments:

LOCATION: (Off-Peak) Colbert Ferry boat ramp


QC JOB #: 15889232

SPECIFIC LOCATION:

DIRECTION: NB, SB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	0	0	66	0	8	0	0	0	0	0	0	0	0	0	74
Percent	0%	0%	89.2%	0%	10.8%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 74															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Colbert Ferry boat ramp						QC JOB #: 15889232				
SPECIFIC LOCATION:						DIRECTION: NB, SB				
CITY/STATE: Colbert, AL						DATE: Aug 27 2022 - Aug 27 2022				
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM							0		0	
01:00 AM							0		0	
02:00 AM							0		0	
03:00 AM							0		0	
04:00 AM							0		0	
05:00 AM							2		2	
06:00 AM							9		9	
07:00 AM							5		5	
08:00 AM							4		4	
09:00 AM							0		0	
10:00 AM							10		10	
11:00 AM							8		8	
12:00 PM							6		6	
01:00 PM							9		9	
02:00 PM							5		5	
03:00 PM							2		2	
04:00 PM		0		0						
05:00 PM		6		6						
06:00 PM		6		6						
07:00 PM		0		0						
08:00 PM		2		2						
09:00 PM		0		0						
10:00 PM		0		0						
11:00 PM		0		0						
Day Total							74		74	
% Weekday Average										
% Week Average						0%	100%			
AM Peak Volume							10:00 AM 10		10:00 AM 10	
PM Peak Volume							1:00 PM 9		1:00 PM 9	
Comments:										

Report generated on 9/12/2022 2:54 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Colbert Ferry boat ramp**QC JOB #:** 15889232**SPECIFIC LOCATION:****DIRECTION:** SB**CITY/STATE:** Colbert, AL**DATE:** Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0		1
06:00 AM	0	0	4	0	1	0	0	0	0	0	0	0	0		5
07:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0		2
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0		2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
10:00 AM	0	0	4	0	1	0	0	0	0	0	0	0	0		5
11:00 AM	0	0	3	0	1	0	0	0	0	0	0	0	0		4
12:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
01:00 PM	0	0	5	0	0	0	0	0	0	0	0	0	0		5
02:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0		2
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0		1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
05:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
06:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0		3
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0		1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0		0
Day Total	0	0	33	0	4	0	0	0	0	0	0	0	0		37
Percent	0%	0%	89.2%	0%	10.8%	0%	0%	0%	0%	0%	0%	0%	0%		
ADT 37															
AM Peak Volume	12:00 AM	12:00 AM	6:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM
	0	0	4	0	1	0	0	0	0	0	0	0	0		5
PM Peak Volume	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM
	0	0	5	0	0	0	0	0	0	0	0	0	0		5

Comments:

LOCATION: (Off-Peak) Colbert Ferry boat ramp


QC JOB #: 15889232

SPECIFIC LOCATION:

DIRECTION: SB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	0	0	33	0	4	0	0	0	0	0	0	0	0		37
Percent	0%	0%	89.2%	0%	10.8%	0%	0%	0%	0%	0%	0%	0%	0%		
ADT 37															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Colbert Ferry boat ramp						QC JOB #: 15889232				
SPECIFIC LOCATION:						DIRECTION: SB				
CITY/STATE: Colbert, AL						DATE: Aug 27 2022 - Aug 27 2022				
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM							0		0	
01:00 AM							0		0	
02:00 AM							0		0	
03:00 AM							0		0	
04:00 AM							0		0	
05:00 AM							1		1	<div></div>
06:00 AM							5		5	<div></div>
07:00 AM							2		2	<div></div>
08:00 AM							2		2	<div></div>
09:00 AM							0		0	
10:00 AM							5		5	<div></div>
11:00 AM							4		4	<div></div>
12:00 PM							3		3	<div></div>
01:00 PM							5		5	<div></div>
02:00 PM							2		2	<div></div>
03:00 PM							1		1	<div></div>
04:00 PM							0		0	
05:00 PM		3		3	<div></div>					
06:00 PM		3		3	<div></div>					
07:00 PM		0		0						
08:00 PM		1		1	<div></div>					
09:00 PM		0		0						
10:00 PM		0		0						
11:00 PM		0		0						
Day Total							37		37	
% Weekday Average										
% Week Average						0%	100%			
AM Peak Volume							6:00 AM 5		6:00 AM 5	
PM Peak Volume							1:00 PM 5		1:00 PM 5	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889228 DIRECTION: EB DATE: Aug 22 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	1	1	0	0	0	2	56-65	2
01:00 AM	0	0	0	0	0	0	0	0	0	3	0	2	0	0	5	51-60	3
02:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
03:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	2	51-60	1
04:00 AM	0	0	0	0	0	0	0	0	0	3	2	2	0	1	8	56-65	5
05:00 AM	0	0	0	0	0	0	0	3	4	3	4	0	0	0	14	55-64	7
06:00 AM	1	0	0	0	0	0	0	7	6	7	3	2	1	0	27	49-58	13
07:00 AM	0	0	0	0	0	0	1	6	23	22	10	1	1	0	64	51-60	45
08:00 AM	0	0	0	0	0	0	0	4	3	7	0	2	0	0	16	51-60	10
09:00 AM	0	0	0	1	0	0	1	4	7	4	1	1	0	0	19	49-58	11
10:00 AM	0	0	0	0	0	0	0	1	3	5	4	2	0	0	15	56-65	9
11:00 AM	0	0	0	0	0	0	2	5	5	6	2	1	0	0	21	51-60	11
12:00 PM	1	0	0	0	0	2	3	5	8	3	1	0	0	0	23	46-55	13
01:00 PM	1	0	0	0	2	0	3	1	9	3	3	0	1	0	23	51-60	12
02:00 PM	0	0	0	0	1	2	0	4	11	8	5	3	1	0	35	51-60	19
03:00 PM	0	0	0	0	0	1	0	5	11	13	4	0	0	0	34	51-60	24
04:00 PM	0	0	0	0	0	0	0	8	16	16	5	2	1	0	48	51-60	32
05:00 PM	0	0	0	0	0	0	3	2	23	21	5	1	1	0	56	51-60	44
06:00 PM	1	0	0	0	0	0	1	0	6	10	4	2	1	0	25	51-60	16
07:00 PM	1	0	0	0	0	0	0	5	10	8	3	1	1	0	29	51-60	18
08:00 PM	0	0	0	0	0	0	1	1	5	2	3	0	0	0	12	51-60	7
09:00 PM	0	0	0	0	0	0	0	1	1	2	0	0	1	0	5	51-60	3
10:00 PM	0	0	0	0	0	0	0	0	0	2	2	0	1	0	5	56-65	4
11:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
Day Total	5	0	0	1	3	5	15	62	152	151	62	23	10	1	490	51-60	303
Percent	1%	0%	0%	0.2%	0.6%	1%	3.1%	12.7%	31%	30.8%	12.7%	4.7%	2%	0.2%			
AM Peak Volume	6:00 AM	12:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	11:00 AM	6:00 AM	7:00 AM	7:00 AM	7:00 AM	1:00 AM	6:00 AM	4:00 AM	7:00 AM		
	1	0	0	1	0	0	2	7	23	22	10	2	1	1	64		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	4:00 PM	5:00 PM	5:00 PM	2:00 PM	2:00 PM	1:00 PM	12:00 PM	5:00 PM		
	1	0	0	0	2	2	3	8	23	21	5	3	1	0	56		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	2	1	0	0	0	0	3	51-60	3
01:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
02:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
03:00 AM	0	0	0	0	0	0	0	0	0	1	1	0	0	0	2	56-65	2
04:00 AM	0	0	0	0	0	0	0	0	1	1	1	2	0	0	5	61-70	3
05:00 AM	0	0	0	0	0	0	0	1	4	4	6	2	0	0	17	56-65	10
06:00 AM	0	0	0	0	0	0	1	2	7	6	2	2	0	0	20	51-60	13
07:00 AM	0	0	0	0	0	0	1	4	15	19	6	5	0	0	50	51-60	34
08:00 AM	1	0	0	0	0	0	0	3	9	6	3	0	0	0	22	51-60	15
09:00 AM	0	0	0	0	1	1	0	6	10	8	2	1	0	0	29	51-60	18
10:00 AM	0	0	1	0	0	0	0	3	8	7	2	0	0	0	21	51-60	15
11:00 AM	0	0	0	0	0	0	3	7	9	3	1	0	0	0	23	46-55	16
12:00 PM	0	0	0	0	1	0	2	12	3	2	3	0	0	0	23	46-55	15
01:00 PM	0	0	0	1	0	0	1	5	11	7	3	1	0	0	29	51-60	18
02:00 PM	0	0	0	0	0	1	1	6	10	10	4	2	0	0	34	51-60	20
03:00 PM	1	0	0	0	0	1	3	11	16	10	1	0	0	0	43	46-55	27
04:00 PM	0	0	0	0	0	1	0	9	7	24	6	5	1	0	53	51-60	31
05:00 PM	0	0	0	0	0	0	1	3	15	23	5	1	1	1	50	51-60	38
06:00 PM	0	0	0	0	0	0	0	2	12	9	2	1	0	0	26	51-60	21
07:00 PM	0	0	0	0	0	1	1	7	9	9	7	3	1	0	38	51-60	18
08:00 PM	0	0	0	0	0	0	0	1	2	2	1	0	0	0	6	51-60	4
09:00 PM	0	0	0	0	0	0	1	0	0	2	0	1	0	0	4	51-60	2
10:00 PM	0	0	0	0	0	0	0	0	1	2	1	0	0	0	4	56-65	3
11:00 PM	0	0	0	0	0	0	0	1	0	2	0	0	0	0	3	51-60	2
Day Total	2	0	1	1	2	5	15	83	152	159	57	26	3	1	507	51-60	311
Percent	0.4%	0%	0.2%	0.2%	0.4%	1%	3%	16.4%	30%	31.4%	11.2%	5.1%	0.6%	0.2%			
AM Peak Volume	8:00 AM	12:00 AM	10:00 AM	12:00 AM	9:00 AM	9:00 AM	11:00 AM	11:00 AM	7:00 AM	7:00 AM	5:00 AM	7:00 AM	12:00 AM	12:00 AM	7:00 AM		
	1	0	1	0	1	1	3	7	15	19	6	5	0	0	50		
PM Peak Volume	3:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	2:00 PM	3:00 PM	12:00 PM	3:00 PM	4:00 PM	7:00 PM	4:00 PM	4:00 PM	5:00 PM	4:00 PM		
	1	0	0	1	1	1	3	12	16	24	7	5	1	1	53		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889228 DIRECTION: EB DATE: Aug 24 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
02:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
03:00 AM	0	0	0	0	1	0	0	1	1	0	0	0	0	0	3	46-55	2
04:00 AM	0	0	0	0	0	0	0	1	0	1	3	1	0	0	6	58-67	4
05:00 AM	0	0	0	0	0	0	1	1	6	3	5	3	0	0	19	51-60	9
06:00 AM	0	0	0	0	0	0	0	2	9	9	5	0	1	0	26	51-60	18
07:00 AM	0	0	0	0	0	1	1	13	21	12	2	1	0	1	52	46-55	34
08:00 AM	0	0	0	0	0	1	3	7	9	6	0	0	0	0	26	46-55	16
09:00 AM	0	0	0	0	0	0	0	1	4	2	2	1	0	0	10	51-60	6
10:00 AM	0	0	0	0	0	2	1	6	6	6	1	1	0	0	23	46-55	12
11:00 AM	2	0	0	0	0	0	2	2	7	4	1	1	0	1	20	51-60	11
12:00 PM	0	0	0	0	0	1	3	2	12	6	3	0	0	0	27	51-60	18
01:00 PM	0	0	0	0	0	0	1	4	6	6	0	0	0	0	17	51-60	12
02:00 PM	0	0	0	0	0	2	1	8	15	9	4	3	1	0	43	51-60	24
03:00 PM	0	0	0	0	0	0	2	18	11	9	5	0	1	0	46	46-55	29
04:00 PM	0	0	0	0	0	3	3	6	18	10	8	3	0	0	51	51-60	28
05:00 PM	1	0	0	0	0	0	4	6	25	20	7	1	0	1	65	51-60	45
06:00 PM	1	0	0	0	0	0	0	1	12	8	4	0	1	0	27	51-60	20
07:00 PM	0	0	0	0	0	0	2	2	9	7	7	1	0	0	28	51-60	16
08:00 PM	0	0	0	0	0	0	1	1	7	2	3	1	1	0	16	51-60	9
09:00 PM	0	0	0	0	0	0	1	0	1	5	0	1	0	0	8	51-60	6
10:00 PM	0	0	0	0	0	0	0	2	1	2	2	0	0	0	7	56-65	4
11:00 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1
Day Total	4	0	0	0	1	10	26	85	180	128	63	18	5	3	523	51-60	308
Percent	0.8%	0%	0%	0%	0.2%	1.9%	5%	16.3%	34.4%	24.5%	12%	3.4%	1%	0.6%			
AM Peak Volume	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	3:00 AM 1	10:00 AM 2	8:00 AM 3	7:00 AM 13	7:00 AM 21	7:00 AM 12	5:00 AM 5	5:00 AM 3	6:00 AM 1	7:00 AM 1	7:00 AM 52		
PM Peak Volume	5:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 3	5:00 PM 4	3:00 PM 18	5:00 PM 25	5:00 PM 20	4:00 PM 8	2:00 PM 3	2:00 PM 1	5:00 PM 1	5:00 PM 65		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge																QC JOB #: 15889228		
SPECIFIC LOCATION:																DIRECTION: EB		
CITY/STATE: Colbert, AL																DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace	
12:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	1	3	41-50	1	
01:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1	
02:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	46-55	2	
03:00 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	51-60	2	
04:00 AM	0	0	0	1	0	0	0	0	1	1	0	0	1	0	4	51-60	2	
05:00 AM	0	0	0	0	0	0	0	1	3	4	5	1	0	0	14	56-65	9	
06:00 AM	1	0	0	0	0	1	2	5	10	8	7	1	0	0	35	51-60	18	
07:00 AM	0	0	0	0	1	0	0	12	23	17	2	2	0	0	57	51-60	40	
08:00 AM	0	0	0	0	0	0	1	4	7	5	2	2	0	0	21	51-60	12	
09:00 AM	0	1	0	0	0	1	2	4	8	6	2	0	0	0	24	51-60	14	
10:00 AM	0	0	0	0	1	0	2	5	9	3	1	1	0	0	22	46-55	14	
11:00 AM	0	0	0	0	0	1	4	3	9	4	0	0	0	0	21	51-60	13	
12:00 PM	0	0	0	1	0	0	1	6	5	2	3	0	0	0	18	46-55	11	
01:00 PM	1	0	0	0	0	0	2	3	9	5	4	0	1	0	25	51-60	14	
02:00 PM	0	0	0	0	1	0	3	5	12	10	1	0	0	1	33	51-60	22	
03:00 PM	1	0	0	0	0	0	1	13	21	14	8	0	0	0	58	51-60	35	
04:00 PM	2	0	0	0	0	0	1	13	21	15	8	1	0	0	61	51-60	36	
05:00 PM	2	0	0	0	0	0	0	3	23	19	9	1	2	0	59	51-60	42	
06:00 PM	0	0	0	0	0	2	2	8	7	8	3	2	0	0	32	46-55	15	
07:00 PM	0	0	0	0	0	0	1	1	8	4	10	1	0	1	26	56-65	14	
08:00 PM	0	0	0	0	0	0	1	3	4	4	4	0	0	0	16	51-60	8	
09:00 PM	0	0	0	0	0	0	0	0	0	1	2	0	1	0	4	56-65	3	
10:00 PM	0	0	0	0	0	0	0	0	0	2	2	0	0	0	4	56-65	4	
11:00 PM	0	0	0	0	0	0	1	1	2	0	0	1	0	0	5	46-55	3	
Day Total	7	1	0	2	3	5	24	91	185	134	74	13	5	3				
Percent	1.3%	0.2%	0%	0.4%	0.5%	0.9%	4.4%	16.6%	33.8%	24.5%	13.5%	2.4%	0.9%	0.5%	547	51-60	319	
AM Peak Volume	6:00 AM 1	9:00 AM 1	12:00 AM 0	4:00 AM 1	7:00 AM 1	6:00 AM 1	11:00 AM 4	7:00 AM 12	7:00 AM 23	7:00 AM 17	6:00 AM 7	7:00 AM 2	4:00 AM 1	12:00 AM 1	7:00 AM 57			
PM Peak Volume	4:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 1	2:00 PM 1	6:00 PM 2	2:00 PM 3	3:00 PM 13	5:00 PM 23	5:00 PM 19	7:00 PM 10	6:00 PM 2	5:00 PM 2	2:00 PM 1	4:00 PM 61			
Comments:																		

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889228 DIRECTION: EB DATE: Aug 26 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	0	0	0	1	1	0	0	3	61-70	2
01:00 AM	0	0	0	0	0	0	0	0	2	1	0	0	0	0	3	51-60	3
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	1	0	0	0	1	0	0	0	2	36-45	1
04:00 AM	0	0	0	0	0	0	0	0	0	1	1	2	0	1	5	61-70	3
05:00 AM	0	0	0	0	0	0	0	1	0	4	7	0	0	1	13	56-65	11
06:00 AM	0	0	0	0	0	1	0	6	5	6	7	1	0	0	26	56-65	13
07:00 AM	0	0	0	0	0	0	7	9	13	19	5	0	0	0	53	51-60	32
08:00 AM	0	1	0	0	0	0	3	6	3	4	1	0	0	0	18	41-50	9
09:00 AM	0	0	0	0	0	1	2	0	12	7	2	0	0	1	25	51-60	19
10:00 AM	1	0	0	0	0	0	4	5	9	6	1	0	0	1	27	51-60	15
11:00 AM	0	0	0	0	0	0	3	11	9	8	1	0	0	0	32	46-55	20
12:00 PM	1	0	0	0	1	2	1	4	6	9	1	2	0	0	27	51-60	15
01:00 PM	0	0	0	0	0	0	3	8	9	10	1	0	2	0	33	51-60	19
02:00 PM	2	0	0	0	1	0	1	7	10	16	7	3	0	1	48	51-60	26
03:00 PM	0	0	0	0	0	0	4	18	14	19	8	3	1	1	68	51-60	33
04:00 PM	1	0	0	0	0	0	1	7	25	20	7	3	1	1	66	51-60	45
05:00 PM	0	0	0	0	1	0	1	12	36	30	10	2	1	0	93	51-60	66
06:00 PM	0	0	0	0	1	1	1	5	16	16	11	2	1	0	54	51-60	32
07:00 PM	0	0	0	0	0	0	1	2	14	20	8	2	0	1	48	51-60	34
08:00 PM	0	0	0	0	0	0	1	8	4	3	3	0	0	0	19	46-55	12
09:00 PM	0	0	0	0	0	0	0	1	3	2	0	0	0	0	6	51-60	5
10:00 PM	0	0	0	0	0	0	0	1	5	5	1	1	1	1	15	51-60	10
11:00 PM	0	0	0	0	0	0	0	2	0	3	1	0	1	0	7	56-65	4
Day Total	5	1	0	0	4	5	35	113	195	209	85	22	8	9	691	51-60	404
Percent	0.7%	0.1%	0%	0%	0.6%	0.7%	5.1%	16.4%	28.2%	30.2%	12.3%	3.2%	1.2%	1.3%			
AM Peak Volume	10:00 AM 1	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1	7:00 AM 7	11:00 AM 11	7:00 AM 13	7:00 AM 19	5:00 AM 7	4:00 AM 2	12:00 AM 0	4:00 AM 1	7:00 AM 53		
PM Peak Volume	2:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 1	12:00 PM 2	3:00 PM 4	3:00 PM 18	5:00 PM 36	5:00 PM 30	6:00 PM 11	2:00 PM 3	1:00 PM 2	2:00 PM 1	5:00 PM 93		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	3	0	4	2	0	2	12	61-70	6
01:00 AM	0	0	0	0	0	0	1	0	1	1	0	1	0	0	4	51-60	2
02:00 AM	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2	41-50	2
03:00 AM	0	0	0	0	0	0	1	0	2	1	1	0	0	0	5	51-60	3
04:00 AM	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2	46-55	1
05:00 AM	0	0	0	0	0	0	0	0	0	2	2	0	0	0	4	56-65	4
06:00 AM	1	0	0	0	0	0	0	1	3	2	0	0	0	0	7	51-60	5
07:00 AM	0	0	0	0	0	0	3	4	5	5	7	1	0	0	25	56-65	12
08:00 AM	0	0	0	0	0	0	0	1	7	6	4	1	0	2	21	51-60	13
09:00 AM	0	0	0	0	0	1	5	3	10	6	5	3	0	0	33	51-60	16
10:00 AM	0	0	0	0	0	0	3	4	23	13	3	2	0	0	48	51-60	36
11:00 AM	1	0	0	0	0	0	2	6	12	11	5	2	0	0	39	51-60	23
12:00 PM	0	0	0	0	0	0	6	9	12	6	2	2	0	0	37	46-55	21
01:00 PM	1	0	1	0	0	3	2	5	12	8	7	1	0	1	41	51-60	20
02:00 PM	2	0	0	0	0	0	5	4	16	21	9	2	0	0	59	51-60	37
03:00 PM	1	0	0	0	0	1	1	6	18	13	8	0	0	0	48	51-60	31
04:00 PM	1	0	0	1	0	2	3	7	13	10	6	1	0	1	45	51-60	23
05:00 PM	0	0	0	0	0	1	2	3	18	13	2	2	1	0	42	51-60	31
06:00 PM	0	0	0	0	0	0	0	5	7	10	3	1	1	0	27	51-60	17
07:00 PM	0	0	0	0	0	0	0	5	16	14	9	2	0	0	46	51-60	30
08:00 PM	0	0	0	0	0	0	2	5	5	7	1	2	0	0	22	51-60	12
09:00 PM	0	0	0	0	0	0	0	3	6	0	1	0	0	0	10	46-55	9
10:00 PM	0	0	0	0	0	0	0	3	6	2	2	1	0	0	14	46-55	9
11:00 PM	0	0	0	0	0	0	0	0	2	3	1	0	0	0	6	51-60	5
Day Total	7	0	1	1	0	8	36	77	198	154	83	26	2	6	599	51-60	352
Percent	1.2%	0%	0.2%	0.2%	0%	1.3%	6%	12.9%	33.1%	25.7%	13.9%	4.3%	0.3%	1%			
AM Peak Volume	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	9:00 AM 5	11:00 AM 6	10:00 AM 23	10:00 AM 13	7:00 AM 7	9:00 AM 3	12:00 AM 0	12:00 AM 2	10:00 AM 48		
PM Peak Volume	2:00 PM 2	12:00 PM 0	1:00 PM 1	4:00 PM 1	12:00 PM 0	1:00 PM 3	12:00 PM 6	12:00 PM 9	3:00 PM 18	2:00 PM 21	2:00 PM 9	12:00 PM 2	5:00 PM 1	1:00 PM 1	2:00 PM 59		
Comments:																	

Report generated on 9/2/2022 10:19 AM

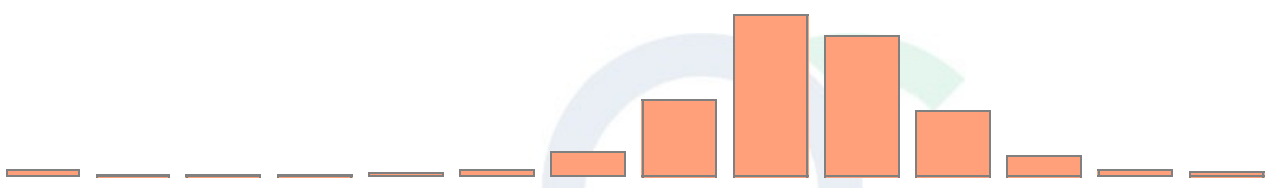
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge															QC JOB #: 15889228		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	1	0	3	41-50	1
01:00 AM	0	0	0	0	0	0	0	0	1	2	0	3	0	0	6	61-70	3
02:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	1	0	3	46-55	2
03:00 AM	0	0	0	0	0	0	0	1	3	1	1	0	0	0	6	48-57	4
04:00 AM	0	0	0	0	0	0	0	2	0	1	1	1	0	0	5	41-50	2
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	1	3	0	1	0	0	0	5	46-55	4
07:00 AM	0	0	0	0	0	1	0	1	5	5	2	0	1	0	15	51-60	10
08:00 AM	0	0	0	0	0	0	0	5	5	6	6	0	0	1	23	56-65	12
09:00 AM	1	0	1	0	0	0	2	7	3	3	1	1	0	0	19	46-55	10
10:00 AM	1	0	0	0	0	0	2	1	5	9	0	3	1	0	22	51-60	14
11:00 AM	2	0	0	0	0	0	1	3	11	9	6	2	0	0	34	51-60	20
12:00 PM	2	0	0	0	0	1	1	3	8	10	4	2	0	0	31	51-60	18
01:00 PM	0	0	0	0	0	0	4	7	9	6	11	2	0	0	39	56-65	17
02:00 PM	0	0	0	0	1	0	3	4	17	13	3	0	1	0	42	51-60	30
03:00 PM	0	0	0	0	0	0	0	2	14	9	3	2	1	0	31	51-60	23
04:00 PM	0	0	0	0	2	1	0	8	20	9	3	1	1	1	46	51-60	29
05:00 PM	0	0	0	0	0	0	3	5	8	13	4	1	0	0	34	51-60	21
06:00 PM	1	0	0	0	0	0	3	6	10	4	5	0	2	0	31	46-55	16
07:00 PM	1	0	0	0	0	1	1	4	10	10	7	0	0	0	34	51-60	20
08:00 PM	0	0	0	0	0	0	1	1	0	3	2	1	0	0	8	56-65	5
09:00 PM	0	0	0	1	0	0	0	2	3	5	3	0	0	0	14	51-60	8
10:00 PM	0	0	0	0	0	0	0	0	2	0	0	1	0	0	3	46-55	2
11:00 PM	0	0	0	0	0	0	0	1	2	2	0	0	0	0	5	51-60	4
Day Total	8	0	1	1	3	4	21	65	141	121	63	20	9	2	459	51-60	262
Percent	1.7%	0%	0.2%	0.2%	0.7%	0.9%	4.6%	14.2%	30.7%	26.4%	13.7%	4.4%	2%	0.4%			
AM Peak Volume	11:00 AM 2	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	7:00 AM 1	9:00 AM 2	9:00 AM 7	11:00 AM 11	10:00 AM 9	8:00 AM 6	1:00 AM 3	12:00 AM 1	8:00 AM 1	11:00 AM 34		
PM Peak Volume	12:00 PM 2	12:00 PM 0	12:00 PM 0	9:00 PM 1	4:00 PM 2	12:00 PM 1	1:00 PM 4	4:00 PM 8	4:00 PM 20	2:00 PM 13	1:00 PM 11	12:00 PM 2	6:00 PM 2	4:00 PM 1	4:00 PM 46		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) John Coffee Memorial Bridge															QC JOB #: 15889228		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	38	2	3	6	16	42	172	576	1203	1056	487	148	42	25	3816	51-60	2259
Percent	1%	0.1%	0.1%	0.2%	0.4%	1.1%	4.5%	15.1%	31.5%	27.7%	12.8%	3.9%	1.1%	0.7%			
Cumulative Percent	1%	1%	1.1%	1.3%	1.7%	2.8%	7.3%	22.4%	53.9%	81.6%	94.4%	98.2%	99.3%	100%			
ADT 545															85th Percentile: 61 MPH Mean Speed(Average): 54 MPH Median: 54 MPH Mode: 53 MPH		
Comments:																	

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5
02:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	6	1	1	0	0	0	0	0	0	0	0	0	0	8
05:00 AM	0	8	3	0	2	0	0	1	0	0	0	0	0	0	14
06:00 AM	0	14	5	0	6	0	0	1	0	0	0	0	0	1	27
07:00 AM	0	43	11	0	9	0	0	1	0	0	0	0	0	0	64
08:00 AM	0	7	6	0	2	0	0	1	0	0	0	0	0	0	16
09:00 AM	0	7	7	0	4	0	0	1	0	0	0	0	0	0	19
10:00 AM	0	7	5	0	3	0	0	0	0	0	0	0	0	0	15
11:00 AM	0	12	2	0	5	1	0	1	0	0	0	0	0	0	21
12:00 PM	0	12	8	0	2	0	0	0	0	0	0	0	0	1	23
01:00 PM	1	14	5	0	2	0	0	0	0	0	0	0	0	1	23
02:00 PM	1	21	10	0	1	0	0	1	0	0	0	0	0	1	35
03:00 PM	0	24	9	0	1	0	0	0	0	0	0	0	0	0	34
04:00 PM	0	28	15	0	4	0	0	1	0	0	0	0	0	0	48
05:00 PM	0	32	16	0	6	0	0	2	0	0	0	0	0	0	56
06:00 PM	0	15	5	0	4	0	0	0	0	0	0	0	0	1	25
07:00 PM	1	13	6	1	4	0	0	3	0	0	0	0	0	1	29
08:00 PM	0	7	2	0	2	0	0	1	0	0	0	0	0	0	12
09:00 PM	0	3	1	0	1	0	0	0	0	0	0	0	0	0	5
10:00 PM	0	3	1	0	1	0	0	0	0	0	0	0	0	0	5
11:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	3	280	124	2	60	1	0	14	0	0	0	0	0	6	490
Percent	0.6%	57.1%	25.3%	0.4%	12.2%	0.2%	0%	2.9%	0%	0%	0%	0%	0%	1.2%	
ADT 490															
AM Peak Volume	12:00 AM	7:00 AM	7:00 AM	4:00 AM	7:00 AM	11:00 AM	12:00 AM	5:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	7:00 AM
	0	43	11	1	9	1	0	1	0	0	0	0	0	1	64
PM Peak Volume	1:00 PM	5:00 PM	5:00 PM	7:00 PM	5:00 PM	12:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM
	1	32	16	1	6	0	0	3	0	0	0	0	0	1	56

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
05:00 AM	0	8	7	0	1	0	0	1	0	0	0	0	0	0	17
06:00 AM	0	12	4	0	4	0	0	0	0	0	0	0	0	0	20
07:00 AM	0	36	7	0	7	0	0	0	0	0	0	0	0	0	50
08:00 AM	1	13	6	0	1	0	0	0	0	0	0	0	0	1	22
09:00 AM	0	11	11	0	3	1	0	3	0	0	0	0	0	0	29
10:00 AM	2	13	5	0	1	0	0	0	0	0	0	0	0	0	21
11:00 AM	1	12	8	0	2	0	0	0	0	0	0	0	0	0	23
12:00 PM	1	10	8	0	3	0	0	1	0	0	0	0	0	0	23
01:00 PM	0	16	8	0	4	0	0	1	0	0	0	0	0	0	29
02:00 PM	0	25	7	0	0	0	0	2	0	0	0	0	0	0	34
03:00 PM	0	27	10	0	4	0	0	1	0	0	0	0	0	1	43
04:00 PM	0	30	18	0	5	0	0	0	0	0	0	0	0	0	53
05:00 PM	1	33	10	0	6	0	0	0	0	0	0	0	0	0	50
06:00 PM	1	14	5	0	5	0	0	1	0	0	0	0	0	0	26
07:00 PM	0	22	9	0	7	0	0	0	0	0	0	0	0	0	38
08:00 PM	0	3	3	0	0	0	0	0	0	0	0	0	0	0	6
09:00 PM	0	2	0	0	1	0	0	1	0	0	0	0	0	0	4
10:00 PM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4
11:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
Day Total	7	301	131	0	54	1	0	11	0	0	0	0	0	2	507
Percent	1.4%	59.4%	25.8%	0%	10.7%	0.2%	0%	2.2%	0%	0%	0%	0%	0%	0.4%	
ADT 507															
AM Peak Volume	10:00 AM	7:00 AM	9:00 AM	12:00 AM	7:00 AM	9:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	7:00 AM
	2	36	11	0	7	1	0	3	0	0	0	0	0	1	50
PM Peak Volume	12:00 PM	5:00 PM	4:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM	4:00 PM
	1	33	18	0	7	0	0	2	0	0	0	0	0	1	53

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
04:00 AM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
05:00 AM	0	12	6	0	1	0	0	0	0	0	0	0	0	0	19
06:00 AM	0	16	6	0	4	0	0	0	0	0	0	0	0	0	26
07:00 AM	0	31	16	0	5	0	0	0	0	0	0	0	0	0	52
08:00 AM	0	13	6	0	5	1	0	1	0	0	0	0	0	0	26
09:00 AM	0	6	1	0	3	0	0	0	0	0	0	0	0	0	10
10:00 AM	0	13	4	0	4	0	0	2	0	0	0	0	0	0	23
11:00 AM	0	11	4	0	1	0	0	2	0	0	0	0	0	2	20
12:00 PM	1	10	11	0	5	0	0	0	0	0	0	0	0	0	27
01:00 PM	0	7	4	0	5	0	0	1	0	0	0	0	0	0	17
02:00 PM	0	26	11	0	5	0	0	1	0	0	0	0	0	0	43
03:00 PM	0	30	10	0	5	0	0	1	0	0	0	0	0	0	46
04:00 PM	2	31	13	0	5	0	0	0	0	0	0	0	0	0	51
05:00 PM	0	39	18	0	6	0	0	1	0	0	0	0	0	1	65
06:00 PM	0	14	7	0	4	0	0	1	0	0	0	0	0	1	27
07:00 PM	0	14	7	0	6	0	0	1	0	0	0	0	0	0	28
08:00 PM	0	8	8	0	0	0	0	0	0	0	0	0	0	0	16
09:00 PM	0	1	4	0	2	0	0	1	0	0	0	0	0	0	8
10:00 PM	0	4	3	0	0	0	0	0	0	0	0	0	0	0	7
11:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	3	295	141	0	66	1	0	13	0	0	0	0	0	4	523
Percent	0.6%	56.4%	27%	0%	12.6%	0.2%	0%	2.5%	0%	0%	0%	0%	0%	0.8%	
ADT 523															
AM Peak Volume	12:00 AM	7:00 AM	7:00 AM	12:00 AM	7:00 AM	8:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	7:00 AM
	0	31	16	0	5	1	0	2	0	0	0	0	0	2	52
PM Peak Volume	4:00 PM	5:00 PM	5:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM	5:00 PM
	2	39	18	0	6	0	0	1	0	0	0	0	0	1	65

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
05:00 AM	0	9	2	1	2	0	0	0	0	0	0	0	0	0	14
06:00 AM	0	19	9	0	5	0	0	1	0	0	0	0	0	1	35
07:00 AM	0	42	10	1	2	0	0	2	0	0	0	0	0	0	57
08:00 AM	0	10	7	0	2	0	0	2	0	0	0	0	0	0	21
09:00 AM	1	14	6	0	1	0	0	2	0	0	0	0	0	0	24
10:00 AM	0	12	5	0	4	0	1	0	0	0	0	0	0	0	22
11:00 AM	1	12	5	0	1	0	0	2	0	0	0	0	0	0	21
12:00 PM	5	10	3	0	0	0	0	0	0	0	0	0	0	0	18
01:00 PM	0	11	8	1	4	0	0	0	0	0	0	0	0	1	25
02:00 PM	1	19	7	0	3	0	0	3	0	0	0	0	0	0	33
03:00 PM	0	31	16	0	8	0	0	2	0	0	0	0	0	1	58
04:00 PM	0	30	17	0	9	0	0	3	0	0	0	0	0	2	61
05:00 PM	0	35	14	0	5	0	0	3	0	0	0	0	0	2	59
06:00 PM	0	19	6	0	4	0	0	3	0	0	0	0	0	0	32
07:00 PM	1	10	7	0	7	0	0	1	0	0	0	0	0	0	26
08:00 PM	0	9	5	0	2	0	0	0	0	0	0	0	0	0	16
09:00 PM	0	2	1	0	1	0	0	0	0	0	0	0	0	0	4
10:00 PM	0	1	1	0	2	0	0	0	0	0	0	0	0	0	4
11:00 PM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
Day Total	9	309	131	3	63	0	1	24	0	0	0	0	0	7	547
Percent	1.6%	56.5%	23.9%	0.5%	11.5%	0%	0.2%	4.4%	0%	0%	0%	0%	0%	1.3%	
ADT 547															
AM Peak Volume	9:00 AM	7:00 AM	7:00 AM	5:00 AM	6:00 AM	12:00 AM	10:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	7:00 AM
	1	42	10	1	5	0	1	2	0	0	0	0	0	1	57
PM Peak Volume	12:00 PM	5:00 PM	4:00 PM	1:00 PM	4:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	4:00 PM
	5	35	17	1	9	0	0	3	0	0	0	0	0	2	61

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	2	1	0	1	0	0	1	0	0	0	0	0	0	5
05:00 AM	0	7	4	1	1	0	0	0	0	0	0	0	0	0	13
06:00 AM	0	18	4	0	4	0	0	0	0	0	0	0	0	0	26
07:00 AM	0	35	12	0	3	0	0	3	0	0	0	0	0	0	53
08:00 AM	0	10	3	0	2	0	0	2	1	0	0	0	0	0	18
09:00 AM	2	13	3	0	5	0	0	2	0	0	0	0	0	0	25
10:00 AM	1	14	7	0	3	0	0	1	0	0	0	0	0	1	27
11:00 AM	2	15	10	0	3	0	0	2	0	0	0	0	0	0	32
12:00 PM	2	13	3	0	6	0	0	2	0	0	0	0	0	1	27
01:00 PM	0	16	10	0	5	0	0	2	0	0	0	0	0	0	33
02:00 PM	1	29	8	0	8	0	0	0	0	0	0	0	0	2	48
03:00 PM	1	39	21	0	7	0	0	0	0	0	0	0	0	0	68
04:00 PM	0	36	16	1	10	0	0	2	0	0	0	0	0	1	66
05:00 PM	2	43	33	1	12	0	0	2	0	0	0	0	0	0	93
06:00 PM	0	30	14	0	10	0	0	0	0	0	0	0	0	0	54
07:00 PM	0	25	12	0	10	0	0	1	0	0	0	0	0	0	48
08:00 PM	0	12	4	0	1	0	0	2	0	0	0	0	0	0	19
09:00 PM	0	4	1	0	0	0	0	1	0	0	0	0	0	0	6
10:00 PM	1	7	4	0	3	0	0	0	0	0	0	0	0	0	15
11:00 PM	0	4	2	0	0	0	0	1	0	0	0	0	0	0	7
Day Total	12	375	177	3	94	0	0	24	1	0	0	0	0	5	691
Percent	1.7%	54.3%	25.6%	0.4%	13.6%	0%	0%	3.5%	0.1%	0%	0%	0%	0%	0.7%	
ADT 691															
AM Peak Volume	9:00 AM	7:00 AM	7:00 AM	5:00 AM	9:00 AM	12:00 AM	12:00 AM	7:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	7:00 AM
	2	35	12	1	5	0	0	3	1	0	0	0	0	1	53
PM Peak Volume	12:00 PM	5:00 PM	5:00 PM	4:00 PM	5:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	5:00 PM
	2	43	33	1	12	0	0	2	0	0	0	0	0	2	93

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	7	3	0	2	0	0	0	0	0	0	0	0	0	12
01:00 AM	0	1	1	0	1	0	0	1	0	0	0	0	0	0	4
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
04:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	2	1	1	0	0	0	0	0	0	0	0	0	0	4
06:00 AM	0	4	1	0	1	0	0	0	0	0	0	0	0	1	7
07:00 AM	0	13	5	0	6	0	0	1	0	0	0	0	0	0	25
08:00 AM	0	10	3	0	8	0	0	0	0	0	0	0	0	0	21
09:00 AM	5	15	8	0	4	0	0	1	0	0	0	0	0	0	33
10:00 AM	1	25	12	0	6	0	0	4	0	0	0	0	0	0	48
11:00 AM	3	18	9	0	4	0	0	4	0	0	0	0	0	1	39
12:00 PM	2	21	10	0	2	0	0	2	0	0	0	0	0	0	37
01:00 PM	5	15	13	0	4	0	0	3	0	0	0	0	0	1	41
02:00 PM	7	37	7	0	5	0	0	1	0	0	0	0	0	2	59
03:00 PM	6	22	7	0	10	0	0	2	0	0	0	0	0	1	48
04:00 PM	2	27	9	0	5	0	0	1	0	0	0	0	0	1	45
05:00 PM	0	21	8	0	9	0	0	4	0	0	0	0	0	0	42
06:00 PM	0	7	12	0	7	0	0	1	0	0	0	0	0	0	27
07:00 PM	3	13	19	0	10	0	0	1	0	0	0	0	0	0	46
08:00 PM	0	13	6	0	2	0	0	1	0	0	0	0	0	0	22
09:00 PM	0	4	4	0	2	0	0	0	0	0	0	0	0	0	10
10:00 PM	0	9	2	0	2	0	0	1	0	0	0	0	0	0	14
11:00 PM	1	1	1	0	1	0	0	2	0	0	0	0	0	0	6
Day Total	35	293	142	1	91	0	0	30	0	0	0	0	0	7	599
Percent	5.8%	48.9%	23.7%	0.2%	15.2%	0%	0%	5%	0%	0%	0%	0%	0%	1.2%	
ADT 599															
AM Peak Volume	9:00 AM	10:00 AM	10:00 AM	5:00 AM	8:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	10:00 AM
	5	25	12	1	8	0	0	4	0	0	0	0	0	1	48
PM Peak Volume	2:00 PM	2:00 PM	7:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	2:00 PM
	7	37	19	0	10	0	0	4	0	0	0	0	0	2	59

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	1	3	0	1	0	0	1	0	0	0	0	0	0	6
02:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
03:00 AM	0	2	3	0	1	0	0	0	0	0	0	0	0	0	6
04:00 AM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	2	0	0	0	0	2	0	0	0	0	0	0	5
07:00 AM	0	8	3	0	4	0	0	0	0	0	0	0	0	0	15
08:00 AM	0	14	3	0	6	0	0	0	0	0	0	0	0	0	23
09:00 AM	2	14	1	0	1	0	0	0	0	0	0	0	0	1	19
10:00 AM	3	10	5	0	2	0	0	1	0	0	0	0	0	1	22
11:00 AM	1	18	11	0	2	0	0	0	0	0	0	0	0	2	34
12:00 PM	0	16	8	0	5	0	0	0	0	0	0	0	0	2	31
01:00 PM	1	24	7	0	3	0	0	4	0	0	0	0	0	0	39
02:00 PM	5	19	13	0	3	0	0	2	0	0	0	0	0	0	42
03:00 PM	1	21	3	0	3	0	0	3	0	0	0	0	0	0	31
04:00 PM	0	24	13	0	6	0	0	3	0	0	0	0	0	0	46
05:00 PM	0	22	7	0	5	0	0	0	0	0	0	0	0	0	34
06:00 PM	1	20	8	0	1	0	0	0	0	0	0	0	0	1	31
07:00 PM	1	11	14	0	5	0	0	2	0	0	0	0	0	1	34
08:00 PM	0	6	1	0	1	0	0	0	0	0	0	0	0	0	8
09:00 PM	0	12	2	0	0	0	0	0	0	0	0	0	0	0	14
10:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
11:00 PM	0	2	2	0	0	0	0	1	0	0	0	0	0	0	5
Day Total	15	254	114	0	49	0	0	19	0	0	0	0	0	8	459
Percent	3.3%	55.3%	24.8%	0%	10.7%	0%	0%	4.1%	0%	0%	0%	0%	0%	1.7%	
ADT 459															
AM Peak Volume	10:00 AM 3	11:00 AM 18	11:00 AM 11	12:00 AM 0	8:00 AM 6	12:00 AM 0	12:00 AM 0	6:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 2	11:00 AM 34
PM Peak Volume	2:00 PM 5	1:00 PM 24	7:00 PM 14	12:00 PM 0	4:00 PM 6	12:00 PM 0	12:00 PM 0	1:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 2	4:00 PM 46

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge


QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	84	2107	960	9	477	3	1	135	1	0	0	0	0	39	3816
Percent	2.2%	55.2%	25.2%	0.2%	12.5%	0.1%	0%	3.5%	0%	0%	0%	0%	0%	1%	
ADT 545															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge										QC JOB #: 15889228
SPECIFIC LOCATION:										DIRECTION: EB
CITY/STATE: Colbert, AL										DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	2	3	0	3	3	2	12	3	4	
01:00 AM	5	1	1	1	3	2	4	6	3	
02:00 AM	1	1	1	2	0	1	2	3	1	
03:00 AM	2	2	3	2	2	2	5	6	3	
04:00 AM	8	5	6	4	5	6	2	5	5	
05:00 AM	14	17	19	14	13	15	4	0	12	
06:00 AM	27	20	26	35	26	27	7	5	21	
07:00 AM	64	50	52	57	53	55	25	15	45	
08:00 AM	16	22	26	21	18	21	21	23	21	
09:00 AM	19	29	10	24	25	21	33	19	23	
10:00 AM	15	21	23	22	27	22	48	22	25	
11:00 AM	21	23	20	21	32	23	39	34	27	
12:00 PM	23	23	27	18	27	24	37	31	27	
01:00 PM	23	29	17	25	33	25	41	39	30	
02:00 PM	35	34	43	33	48	39	59	42	42	
03:00 PM	34	43	46	58	68	50	48	31	47	
04:00 PM	48	53	51	61	66	56	45	46	53	
05:00 PM	56	50	65	59	93	65	42	34	57	
06:00 PM	25	26	27	32	54	33	27	31	32	
07:00 PM	29	38	28	26	48	34	46	34	36	
08:00 PM	12	6	16	16	19	14	22	8	14	
09:00 PM	5	4	8	4	6	5	10	14	7	
10:00 PM	5	4	7	4	15	7	14	3	7	
11:00 PM	1	3	1	5	7	3	6	5	4	
Day Total	490	507	523	547	691	552	599	459	546	
% Weekday Average	88.8%	91.8%	94.7%	99.1%	125.2%					
% Week Average	89.7%	92.9%	95.8%	100.2%	126.6%	101.1%	109.7%	84.1%		
AM Peak Volume	7:00 AM 64	7:00 AM 50	7:00 AM 52	7:00 AM 57	7:00 AM 53	7:00 AM 55	10:00 AM 48	11:00 AM 34	7:00 AM 45	
PM Peak Volume	5:00 PM 56	4:00 PM 53	5:00 PM 65	4:00 PM 61	5:00 PM 93	5:00 PM 65	2:00 PM 59	4:00 PM 46	5:00 PM 57	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB, WB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	0	1	2	1	0	0	0	5	56-65	3
01:00 AM	0	0	0	0	0	0	0	0	1	3	1	3	0	0	8	53-62	4
02:00 AM	0	0	0	0	0	0	0	1	1	1	0	1	0	0	4	46-55	2
03:00 AM	0	0	0	0	0	0	0	0	1	2	0	1	0	0	4	51-60	3
04:00 AM	0	0	0	0	0	0	1	0	1	4	4	5	0	2	17	61-70	9
05:00 AM	0	0	0	0	0	0	1	12	19	21	18	6	0	0	77	51-60	40
06:00 AM	1	0	0	0	0	0	0	12	22	22	10	5	2	1	75	51-60	44
07:00 AM	0	0	0	0	0	0	2	11	31	35	18	3	2	1	103	51-60	66
08:00 AM	0	0	0	0	0	0	2	9	10	12	2	2	0	1	38	51-60	22
09:00 AM	0	0	0	1	0	1	2	9	19	9	5	2	0	0	48	46-55	28
10:00 AM	0	0	0	0	0	0	2	3	9	9	4	2	2	0	31	51-60	18
11:00 AM	1	0	0	0	0	0	3	5	10	9	6	1	2	0	37	51-60	19
12:00 PM	1	0	0	1	0	3	3	8	15	8	4	0	1	0	44	50-59	23
01:00 PM	1	0	0	0	3	1	6	5	24	9	7	1	1	0	58	51-60	33
02:00 PM	0	0	0	0	2	3	2	7	20	11	6	4	1	1	57	51-60	31
03:00 PM	0	0	0	0	0	1	0	13	29	24	7	2	0	0	76	51-60	53
04:00 PM	1	0	0	0	0	2	3	20	36	25	6	3	1	0	97	51-60	61
05:00 PM	0	0	0	0	0	1	4	5	39	36	10	2	1	0	98	51-60	75
06:00 PM	2	0	0	0	0	0	1	4	11	16	10	2	1	0	47	51-60	27
07:00 PM	1	0	0	0	0	2	1	8	14	10	4	2	1	0	43	51-60	24
08:00 PM	0	0	0	0	0	0	2	2	9	8	3	0	0	0	24	51-60	17
09:00 PM	0	0	0	0	0	0	1	1	4	3	0	0	2	0	11	51-60	7
10:00 PM	0	0	0	0	0	0	0	1	0	4	2	0	1	0	8	56-65	6
11:00 PM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	51-60	2
Day Total	8	0	0	2	5	14	37	136	326	285	128	47	18	6	1012	51-60	611
Percent	0.8%	0%	0%	0.2%	0.5%	1.4%	3.7%	13.4%	32.2%	28.2%	12.6%	4.6%	1.8%	0.6%			
AM Peak Volume	6:00 AM 1	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	9:00 AM 1	11:00 AM 3	5:00 AM 12	7:00 AM 31	7:00 AM 35	5:00 AM 18	5:00 AM 6	6:00 AM 2	4:00 AM 2	7:00 AM 103		
PM Peak Volume	6:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 1	1:00 PM 3	12:00 PM 3	1:00 PM 6	4:00 PM 20	5:00 PM 39	5:00 PM 36	5:00 PM 10	2:00 PM 4	9:00 PM 2	2:00 PM 1	5:00 PM 98		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB, WB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	2	3	1	0	0	0	6	51-60	5
01:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
02:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	1	0	2	51-60	1
03:00 AM	0	0	0	0	0	0	0	0	0	2	1	0	0	0	3	56-65	3
04:00 AM	0	0	0	0	0	0	1	0	3	3	1	3	1	1	13	51-60	6
05:00 AM	0	0	0	0	0	0	0	8	24	18	13	9	0	1	73	51-60	42
06:00 AM	0	0	0	0	0	0	1	4	15	33	9	2	3	1	68	51-60	48
07:00 AM	0	0	0	0	0	0	3	7	26	30	12	7	0	0	85	51-60	56
08:00 AM	2	0	0	0	0	1	0	4	14	10	4	0	1	0	36	51-60	24
09:00 AM	1	0	0	0	1	1	0	13	21	15	6	2	0	0	60	51-60	36
10:00 AM	0	0	1	0	0	0	1	7	17	11	3	0	0	0	40	51-60	28
11:00 AM	1	0	0	0	0	0	3	11	19	7	4	1	0	0	46	46-55	30
12:00 PM	0	0	0	0	1	2	5	20	15	8	4	1	0	0	56	46-55	35
01:00 PM	1	0	0	1	0	1	3	10	17	13	3	2	0	0	51	51-60	30
02:00 PM	1	0	0	1	0	2	3	12	19	12	5	2	0	0	57	46-55	31
03:00 PM	2	0	0	0	0	2	6	19	28	18	3	1	1	0	80	46-55	47
04:00 PM	0	0	0	0	0	2	0	17	23	38	7	6	1	0	94	51-60	61
05:00 PM	0	0	0	0	1	3	3	11	33	35	8	4	1	1	100	51-60	68
06:00 PM	1	0	0	0	0	0	0	5	28	23	3	4	1	0	65	51-60	51
07:00 PM	1	0	0	0	0	1	3	10	21	9	8	3	1	0	57	46-55	31
08:00 PM	0	0	0	0	0	1	0	6	3	3	4	0	0	0	17	46-55	9
09:00 PM	0	0	0	0	0	0	1	1	2	3	0	2	0	0	9	51-60	5
10:00 PM	0	0	0	0	0	0	1	0	2	3	1	0	0	0	7	51-60	5
11:00 PM	0	0	0	0	0	0	0	1	0	3	0	0	0	0	4	51-60	3
Day Total	10	0	1	2	3	16	34	166	333	301	100	49	11	4	1030	51-60	634
Percent	1%	0%	0.1%	0.2%	0.3%	1.6%	3.3%	16.1%	32.3%	29.2%	9.7%	4.8%	1.1%	0.4%			
AM Peak Volume	8:00 AM 2	12:00 AM 0	10:00 AM 1	12:00 AM 0	9:00 AM 1	8:00 AM 1	7:00 AM 3	9:00 AM 13	7:00 AM 26	6:00 AM 33	5:00 AM 13	5:00 AM 9	6:00 AM 3	4:00 AM 1	7:00 AM 85		
PM Peak Volume	3:00 PM 2	12:00 PM 0	12:00 PM 0	1:00 PM 1	12:00 PM 1	5:00 PM 3	3:00 PM 6	12:00 PM 20	5:00 PM 33	4:00 PM 38	5:00 PM 8	4:00 PM 6	3:00 PM 1	5:00 PM 1	5:00 PM 100		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge																QC JOB #: 15889228		
SPECIFIC LOCATION:																DIRECTION: EB, WB		
CITY/STATE: Colbert, AL																DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace	
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0	
01:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1	
02:00 AM	0	0	0	0	0	1	0	0	0	1	0	0	0	0	2	31-40	1	
03:00 AM	0	0	0	0	1	0	0	3	2	1	0	0	0	0	7	46-55	5	
04:00 AM	0	0	0	0	0	0	0	1	4	4	5	2	3	0	19	56-65	9	
05:00 AM	0	0	0	0	0	0	1	3	20	20	11	13	1	1	70	51-60	40	
06:00 AM	0	0	0	0	0	0	0	6	20	24	16	3	2	2	73	51-60	44	
07:00 AM	0	0	0	0	0	1	3	14	34	26	11	2	1	1	93	51-60	60	
08:00 AM	0	0	0	0	0	2	3	9	16	13	2	2	0	0	47	51-60	29	
09:00 AM	0	0	0	0	0	0	0	3	15	8	5	2	0	0	33	51-60	23	
10:00 AM	0	0	0	0	0	2	2	15	13	7	4	3	0	0	46	46-55	28	
11:00 AM	2	0	0	0	0	1	2	7	18	11	2	3	0	1	47	51-60	29	
12:00 PM	0	0	0	0	0	1	5	4	20	8	3	0	0	0	41	51-60	28	
01:00 PM	0	0	0	0	0	1	3	7	9	14	1	2	0	0	37	51-60	23	
02:00 PM	0	0	0	0	1	2	2	11	23	16	5	3	2	0	65	51-60	39	
03:00 PM	0	0	0	0	0	0	2	27	31	13	6	0	2	0	81	46-55	58	
04:00 PM	1	0	0	0	0	3	6	13	34	18	13	6	1	0	95	51-60	52	
05:00 PM	2	0	0	0	0	0	9	13	41	39	12	2	0	1	119	51-60	80	
06:00 PM	3	0	0	0	0	1	0	8	29	19	6	4	1	0	71	51-60	48	
07:00 PM	1	0	0	0	0	0	2	5	13	13	7	1	1	0	43	51-60	26	
08:00 PM	0	0	0	0	0	0	2	5	10	6	6	2	1	0	32	51-60	16	
09:00 PM	0	0	0	0	0	0	1	0	1	8	0	2	0	0	12	51-60	9	
10:00 PM	0	0	0	0	0	0	1	2	2	2	3	0	0	1	11	56-65	5	
11:00 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1	
Day Total	9	0	0	0	2	15	44	157	355	271	119	52	15	7	1046	51-60	626	
Percent	0.9%	0%	0%	0%	0.2%	1.4%	4.2%	15%	33.9%	25.9%	11.4%	5%	1.4%	0.7%				
AM Peak Volume	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	3:00 AM 1	8:00 AM 2	7:00 AM 3	10:00 AM 15	7:00 AM 34	7:00 AM 26	6:00 AM 16	5:00 AM 13	4:00 AM 3	6:00 AM 2	7:00 AM 93			
PM Peak Volume	6:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	4:00 PM 3	5:00 PM 9	3:00 PM 27	5:00 PM 41	5:00 PM 39	4:00 PM 13	4:00 PM 6	2:00 PM 2	5:00 PM 1	5:00 PM 119			
Comments:																		

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB, WB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	1	3	41-50	1
01:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	46-55	2
02:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	46-55	2
03:00 AM	0	0	0	0	0	1	1	0	0	2	0	0	0	0	4	51-60	2
04:00 AM	0	0	0	1	0	2	1	0	4	6	1	2	1	0	18	51-60	10
05:00 AM	0	0	0	0	0	0	0	5	21	19	18	9	0	1	73	51-60	40
06:00 AM	2	0	0	0	0	1	2	8	29	21	15	3	0	0	81	51-60	50
07:00 AM	0	0	0	0	1	1	0	17	36	31	9	4	2	0	101	51-60	67
08:00 AM	1	0	0	0	0	0	1	7	19	14	3	3	0	0	48	51-60	33
09:00 AM	0	1	0	0	0	1	4	9	19	10	2	0	1	0	47	51-60	29
10:00 AM	0	0	0	0	1	0	3	16	15	6	1	2	0	0	44	46-55	31
11:00 AM	0	0	0	0	0	2	8	10	20	6	1	0	0	0	47	46-55	30
12:00 PM	0	0	0	1	0	0	1	11	15	9	5	1	1	0	44	46-55	26
01:00 PM	1	0	0	0	0	2	2	11	20	13	5	1	1	0	56	51-60	33
02:00 PM	1	0	0	0	1	0	4	11	23	16	2	0	0	1	59	51-60	39
03:00 PM	3	0	0	0	0	0	4	17	38	24	12	2	1	1	102	51-60	62
04:00 PM	2	0	0	0	0	1	8	25	37	22	11	2	0	0	108	46-55	62
05:00 PM	3	0	0	0	0	0	3	11	42	31	15	5	2	0	112	51-60	73
06:00 PM	4	0	0	0	1	3	6	16	21	12	8	3	0	0	74	46-55	37
07:00 PM	0	0	0	0	0	0	2	5	17	9	11	2	0	1	47	51-60	26
08:00 PM	0	0	0	0	0	0	1	6	8	8	4	0	0	0	27	51-60	16
09:00 PM	0	0	0	0	0	0	1	2	3	5	3	0	1	1	16	51-60	8
10:00 PM	0	0	0	0	0	0	0	0	2	3	2	1	0	1	9	53-62	5
11:00 PM	0	0	0	0	1	0	1	1	4	0	1	1	0	0	9	46-55	5
Day Total	17	1	0	2	5	14	53	189	397	267	130	41	10	7	1133	51-60	664
Percent	1.5%	0.1%	0%	0.2%	0.4%	1.2%	4.7%	16.7%	35%	23.6%	11.5%	3.6%	0.9%	0.6%			
AM Peak Volume	6:00 AM 2	9:00 AM 1	12:00 AM 0	4:00 AM 1	7:00 AM 1	4:00 AM 2	11:00 AM 8	7:00 AM 17	7:00 AM 36	7:00 AM 31	5:00 AM 18	5:00 AM 9	7:00 AM 2	12:00 AM 1	7:00 AM 101		
PM Peak Volume	6:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 1	2:00 PM 1	6:00 PM 3	4:00 PM 8	4:00 PM 25	5:00 PM 42	5:00 PM 31	5:00 PM 15	5:00 PM 5	5:00 PM 2	2:00 PM 1	5:00 PM 112		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB, WB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	1	1	0	2	1	0	0	6	61-70	3
01:00 AM	0	0	0	0	0	0	0	0	2	1	0	0	1	0	4	51-60	3
02:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	2	41-50	1
03:00 AM	0	0	0	0	0	0	1	1	1	1	2	0	0	0	6	56-65	3
04:00 AM	0	0	0	0	0	1	0	1	2	2	5	2	1	2	16	60-69	7
05:00 AM	0	0	0	0	0	0	0	2	13	17	15	6	2	4	59	56-65	32
06:00 AM	0	0	0	0	0	1	1	8	25	24	14	2	0	2	77	51-60	49
07:00 AM	1	0	0	0	0	1	10	13	23	34	8	7	0	0	97	51-60	57
08:00 AM	0	1	0	0	0	0	3	10	7	12	3	1	1	0	38	51-60	19
09:00 AM	1	0	0	0	0	2	5	4	19	11	5	1	0	2	50	51-60	30
10:00 AM	1	0	0	0	0	0	5	10	21	12	2	0	0	1	52	51-60	33
11:00 AM	0	0	0	0	0	1	3	13	17	19	2	1	0	0	56	51-60	36
12:00 PM	2	0	0	0	1	2	3	11	18	18	5	5	0	0	65	51-60	36
01:00 PM	1	0	0	0	1	1	5	17	27	19	1	2	2	1	77	51-60	46
02:00 PM	3	0	0	0	1	1	3	12	25	22	8	3	0	1	79	51-60	47
03:00 PM	0	0	0	0	0	0	6	31	31	32	16	4	1	1	122	51-60	63
04:00 PM	1	0	0	0	0	0	3	19	49	35	14	5	1	1	128	51-60	84
05:00 PM	0	0	0	1	1	0	3	16	53	45	14	4	1	0	138	51-60	98
06:00 PM	0	0	0	0	1	2	1	12	34	28	16	3	2	0	99	51-60	62
07:00 PM	0	0	0	0	0	0	1	5	27	28	8	2	0	1	72	51-60	55
08:00 PM	0	0	0	0	0	1	1	14	12	11	6	0	0	0	45	46-55	26
09:00 PM	0	0	0	0	0	0	3	17	20	16	5	0	0	0	61	46-55	37
10:00 PM	0	0	0	0	0	0	0	4	10	9	3	2	1	1	30	51-60	19
11:00 PM	0	0	0	0	0	0	1	2	1	4	1	0	1	0	10	55-64	5
Day Total	10	1	0	1	5	13	59	224	438	401	155	51	14	17	1389	51-60	839
Percent	0.7%	0.1%	0%	0.1%	0.4%	0.9%	4.2%	16.1%	31.5%	28.9%	11.2%	3.7%	1%	1.2%			
AM Peak Volume	7:00 AM 1	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 2	7:00 AM 10	7:00 AM 13	6:00 AM 25	7:00 AM 34	5:00 AM 15	7:00 AM 7	5:00 AM 2	5:00 AM 4	7:00 AM 97		
PM Peak Volume	2:00 PM 3	12:00 PM 0	12:00 PM 0	5:00 PM 1	12:00 PM 1	12:00 PM 2	3:00 PM 6	3:00 PM 31	5:00 PM 53	5:00 PM 45	3:00 PM 16	12:00 PM 5	1:00 PM 2	1:00 PM 1	5:00 PM 138		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB, WB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	1	3	0	5	3	0	2	15	61-70	8
01:00 AM	0	0	0	0	0	0	1	0	1	1	0	2	0	0	5	61-70	2
02:00 AM	0	0	0	0	0	0	0	3	1	0	1	0	0	0	5	46-55	4
03:00 AM	0	0	0	0	0	0	1	1	2	1	1	0	0	0	6	51-60	3
04:00 AM	0	0	0	0	0	1	2	0	4	2	2	0	0	1	12	51-60	6
05:00 AM	0	0	0	0	0	1	0	3	3	7	5	3	2	1	25	56-65	12
06:00 AM	1	0	0	0	0	0	0	6	12	7	1	0	0	1	28	51-60	19
07:00 AM	1	0	0	0	0	0	4	6	10	8	9	3	0	0	41	51-60	18
08:00 AM	1	0	0	0	0	0	1	5	14	10	8	2	1	2	44	51-60	24
09:00 AM	0	0	0	0	0	1	8	7	21	15	5	3	0	0	60	51-60	36
10:00 AM	0	0	0	0	0	0	4	10	32	26	7	4	1	0	84	51-60	58
11:00 AM	3	0	0	1	0	0	5	17	25	26	9	3	0	0	89	51-60	51
12:00 PM	0	0	0	0	0	0	8	23	29	20	8	2	0	0	90	46-55	52
01:00 PM	3	0	1	0	0	3	7	14	28	15	12	3	0	1	87	51-60	43
02:00 PM	2	0	1	0	0	1	5	9	30	31	15	2	0	0	96	51-60	61
03:00 PM	1	0	0	0	0	1	4	18	37	25	11	0	0	0	97	51-60	62
04:00 PM	2	0	0	1	0	4	5	21	26	23	11	2	1	1	97	51-60	49
05:00 PM	1	0	0	0	0	1	5	6	40	31	4	2	1	0	91	51-60	71
06:00 PM	2	0	0	0	0	0	5	11	18	18	8	4	2	0	68	51-60	36
07:00 PM	0	0	0	0	0	0	0	14	29	25	11	2	0	0	81	51-60	54
08:00 PM	0	0	0	0	0	0	3	8	12	10	4	2	0	0	39	51-60	22
09:00 PM	0	0	0	0	0	0	0	6	14	4	3	0	1	0	28	46-55	20
10:00 PM	0	0	0	0	0	0	0	5	10	7	4	3	0	0	29	51-60	17
11:00 PM	0	0	0	0	0	0	0	0	8	4	5	1	0	0	18	51-60	12
Day Total	17	0	2	2	0	13	69	194	409	316	149	46	9	9	1235	51-60	725
Percent	1.4%	0%	0.2%	0.2%	0%	1.1%	5.6%	15.7%	33.1%	25.6%	12.1%	3.7%	0.7%	0.7%			
AM Peak Volume	11:00 AM 3	12:00 AM 0	12:00 AM 0	11:00 AM 1	12:00 AM 0	4:00 AM 1	9:00 AM 8	11:00 AM 17	10:00 AM 32	10:00 AM 26	7:00 AM 9	10:00 AM 4	5:00 AM 2	12:00 AM 2	11:00 AM 89		
PM Peak Volume	1:00 PM 3	12:00 PM 0	1:00 PM 1	4:00 PM 1	12:00 PM 0	4:00 PM 4	12:00 PM 8	12:00 PM 23	5:00 PM 40	2:00 PM 31	2:00 PM 15	6:00 PM 4	6:00 PM 2	1:00 PM 1	3:00 PM 97		
Comments:																	

Report generated on 9/2/2022 10:19 AM


SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: EB, WB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	3	0	0	1	0	5	51-60	3
01:00 AM	0	0	0	0	0	0	0	1	1	3	0	3	0	0	8	51-60	4
02:00 AM	0	0	0	0	0	0	0	0	2	1	1	0	1	0	5	51-60	3
03:00 AM	0	0	0	0	0	0	0	1	3	1	1	0	0	0	6	48-57	4
04:00 AM	0	0	0	0	0	0	0	2	1	4	3	1	0	0	11	56-65	7
05:00 AM	0	0	0	0	0	0	0	0	0	1	2	0	1	0	4	56-65	3
06:00 AM	0	0	0	0	0	0	0	2	8	7	3	0	0	0	20	51-60	15
07:00 AM	0	0	0	0	0	1	0	4	10	7	3	0	1	0	26	51-60	17
08:00 AM	1	0	0	0	0	0	1	8	9	9	9	0	0	2	39	51-60	18
09:00 AM	2	0	1	0	0	0	5	11	9	10	3	2	0	0	43	46-55	20
10:00 AM	2	0	0	0	0	0	7	7	14	15	3	3	2	0	53	51-60	29
11:00 AM	2	0	0	0	0	0	1	6	25	11	8	3	0	0	56	51-60	36
12:00 PM	3	0	0	0	0	1	3	17	29	25	11	2	1	0	92	51-60	54
01:00 PM	2	0	0	0	0	0	7	10	29	14	16	2	1	0	81	51-60	43
02:00 PM	0	0	0	0	1	0	4	7	38	27	12	1	1	1	92	51-60	65
03:00 PM	1	0	0	0	0	1	2	10	40	23	6	4	1	0	88	51-60	63
04:00 PM	1	0	0	0	2	1	2	12	31	29	9	2	2	1	92	51-60	60
05:00 PM	0	0	0	0	0	0	4	15	29	24	7	2	0	0	81	51-60	53
06:00 PM	1	0	0	0	0	0	3	10	24	17	11	3	3	0	72	51-60	41
07:00 PM	2	0	0	0	1	1	1	10	15	17	8	0	0	0	55	51-60	32
08:00 PM	0	0	0	1	1	0	3	4	3	4	5	1	0	0	22	56-65	9
09:00 PM	0	0	0	2	1	0	1	2	4	9	3	0	1	0	23	51-60	13
10:00 PM	0	0	0	0	0	0	0	1	3	1	2	1	0	0	8	48-57	4
11:00 PM	0	0	0	0	0	0	0	1	2	3	0	0	0	0	6	51-60	5
Day Total	17	0	1	3	6	5	44	142	329	265	126	30	16	4	988	51-60	594
Percent	1.7%	0%	0.1%	0.3%	0.6%	0.5%	4.5%	14.4%	33.3%	26.8%	12.8%	3%	1.6%	0.4%			
AM Peak Volume	9:00 AM 2	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	7:00 AM 1	10:00 AM 7	9:00 AM 11	11:00 AM 25	10:00 AM 15	8:00 AM 9	1:00 AM 3	10:00 AM 2	8:00 AM 2	11:00 AM 56		
PM Peak Volume	12:00 PM 3	12:00 PM 0	12:00 PM 0	9:00 PM 2	4:00 PM 2	12:00 PM 1	1:00 PM 7	12:00 PM 17	3:00 PM 40	4:00 PM 29	1:00 PM 16	3:00 PM 4	6:00 PM 3	2:00 PM 1	12:00 PM 92		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) John Coffee Memorial Bridge														QC JOB #: 15889228			
SPECIFIC LOCATION:														DIRECTION: EB, WB			
CITY/STATE: Colbert, AL														DATE: Aug 22 2022 - Aug 28 2022			
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	88	2	4	12	26	90	340	1208	2587	2106	907	316	93	54	7833	51-60	4693
Percent	1.1%	0%	0.1%	0.2%	0.3%	1.1%	4.3%	15.4%	33%	26.9%	11.6%	4%	1.2%	0.7%			
Cumulative Percent	1.1%	1.1%	1.2%	1.4%	1.7%	2.8%	7.2%	22.6%	55.6%	82.5%	94.1%	98.1%	99.3%	100%			
ADT 1119															85th Percentile: 61 MPH Mean Speed(Average): 54 MPH Median: 54 MPH Mode: 53 MPH		
Comments:																	

LOCATION: (Off-Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889228

DIRECTION: EB, WB

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	2	1	0	1	0	0	1	0	0	0	0	0	0	5
01:00 AM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
02:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
03:00 AM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4
04:00 AM	0	12	2	1	2	0	0	0	0	0	0	0	0	0	17
05:00 AM	0	50	23	0	3	0	0	1	0	0	0	0	0	0	77
06:00 AM	0	40	21	0	12	0	0	1	0	0	0	0	0	1	75
07:00 AM	0	71	17	0	14	0	0	1	0	0	0	0	0	0	103
08:00 AM	0	19	13	0	5	0	0	1	0	0	0	0	0	0	38
09:00 AM	0	18	21	0	6	0	0	3	0	0	0	0	0	0	48
10:00 AM	2	16	8	0	4	0	0	1	0	0	0	0	0	0	31
11:00 AM	1	19	5	0	9	1	0	1	0	0	0	0	0	1	37
12:00 PM	0	25	15	0	3	0	0	0	0	0	0	0	0	1	44
01:00 PM	1	37	15	0	3	1	0	0	0	0	0	0	0	1	58
02:00 PM	1	36	17	0	1	0	0	1	0	0	0	0	0	1	57
03:00 PM	0	46	27	1	2	0	0	0	0	0	0	0	0	0	76
04:00 PM	1	58	28	0	6	0	0	3	0	0	0	0	0	1	97
05:00 PM	0	63	21	0	9	0	0	5	0	0	0	0	0	0	98
06:00 PM	1	24	12	0	8	0	0	0	0	0	0	0	0	2	47
07:00 PM	1	22	11	1	4	0	0	3	0	0	0	0	0	1	43
08:00 PM	0	14	5	0	4	0	0	1	0	0	0	0	0	0	24
09:00 PM	0	8	1	0	2	0	0	0	0	0	0	0	0	0	11
10:00 PM	0	5	2	0	1	0	0	0	0	0	0	0	0	0	8
11:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
Day Total	8	593	275	3	99	2	0	23	0	0	0	0	0	9	1012
Percent	0.8%	58.6%	27.2%	0.3%	9.8%	0.2%	0%	2.3%	0%	0%	0%	0%	0%	0.9%	
ADT 1012															
AM Peak Volume	10:00 AM 2	7:00 AM 71	5:00 AM 23	4:00 AM 1	7:00 AM 14	11:00 AM 1	12:00 AM 0	9:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1	7:00 AM 103
PM Peak Volume	1:00 PM 1	5:00 PM 63	4:00 PM 28	3:00 PM 1	5:00 PM 9	1:00 PM 1	12:00 PM 0	5:00 PM 5	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 2	5:00 PM 98

Comments:

Type of report: Tube Count - Vehicle Classification Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889228

DIRECTION: EB, WB

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	2	2	0	1	0	0	1	0	0	0	0	0	0	6
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
04:00 AM	0	8	5	0	0	0	0	0	0	0	0	0	0	0	13
05:00 AM	0	45	22	0	5	0	0	1	0	0	0	0	0	0	73
06:00 AM	0	38	23	0	7	0	0	0	0	0	0	0	0	0	68
07:00 AM	0	61	16	0	8	0	0	0	0	0	0	0	0	0	85
08:00 AM	1	23	9	0	1	0	0	0	0	0	0	0	0	2	36
09:00 AM	0	23	27	0	3	1	0	5	0	0	0	0	0	1	60
10:00 AM	2	23	12	0	3	0	0	0	0	0	0	0	0	0	40
11:00 AM	1	23	16	0	5	0	0	0	0	0	0	0	0	1	46
12:00 PM	4	27	18	0	5	0	0	2	0	0	0	0	0	0	56
01:00 PM	0	29	15	0	4	0	0	2	0	0	0	0	0	1	51
02:00 PM	0	37	15	0	2	0	0	2	0	0	0	0	0	1	57
03:00 PM	0	51	21	0	5	0	0	1	0	0	0	0	0	2	80
04:00 PM	0	52	35	0	7	0	0	0	0	0	0	0	0	0	94
05:00 PM	2	72	15	0	9	1	0	1	0	0	0	0	0	0	100
06:00 PM	1	38	17	0	7	0	0	1	0	0	0	0	0	1	65
07:00 PM	0	32	15	0	8	0	0	1	0	0	0	0	0	1	57
08:00 PM	0	11	6	0	0	0	0	0	0	0	0	0	0	0	17
09:00 PM	0	3	3	0	2	0	0	1	0	0	0	0	0	0	9
10:00 PM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
11:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
Day Total	11	608	298	0	83	2	0	18	0	0	0	0	0	10	1030
Percent	1.1%	59%	28.9%	0%	8.1%	0.2%	0%	1.7%	0%	0%	0%	0%	0%	1%	
ADT 1030															
AM Peak Volume	10:00 AM 2	7:00 AM 61	9:00 AM 27	12:00 AM 0	7:00 AM 8	9:00 AM 1	12:00 AM 0	9:00 AM 5	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	8:00 AM 2	7:00 AM 85
PM Peak Volume	12:00 PM 4	5:00 PM 72	4:00 PM 35	12:00 PM 0	5:00 PM 9	5:00 PM 1	12:00 PM 0	12:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 2	5:00 PM 100

Comments:

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889228

DIRECTION: EB, WB

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	4	0	0	0	0	1	0	0	0	0	0	0	7
04:00 AM	0	12	6	0	1	0	0	0	0	0	0	0	0	0	19
05:00 AM	0	47	18	0	4	0	0	1	0	0	0	0	0	0	70
06:00 AM	0	43	24	0	5	0	0	1	0	0	0	0	0	0	73
07:00 AM	0	59	26	0	6	0	0	2	0	0	0	0	0	0	93
08:00 AM	0	27	11	0	7	1	0	1	0	0	0	0	0	0	47
09:00 AM	0	15	11	0	7	0	0	0	0	0	0	0	0	0	33
10:00 AM	1	23	9	0	11	0	0	2	0	0	0	0	0	0	46
11:00 AM	0	19	14	0	10	0	0	2	0	0	0	0	0	2	47
12:00 PM	1	16	14	1	8	0	0	1	0	0	0	0	0	0	41
01:00 PM	0	15	14	0	7	0	0	1	0	0	0	0	0	0	37
02:00 PM	0	36	20	0	7	0	0	2	0	0	0	0	0	0	65
03:00 PM	1	47	21	0	9	0	0	3	0	0	0	0	0	0	81
04:00 PM	2	56	26	0	9	0	0	1	0	0	0	0	0	1	95
05:00 PM	3	72	31	0	10	0	0	1	0	0	0	0	0	2	119
06:00 PM	0	35	23	0	9	0	0	1	0	0	0	0	0	3	71
07:00 PM	0	25	10	0	6	0	0	1	0	0	0	0	0	1	43
08:00 PM	0	18	14	0	0	0	0	0	0	0	0	0	0	0	32
09:00 PM	0	2	6	0	2	0	0	2	0	0	0	0	0	0	12
10:00 PM	0	6	4	0	1	0	0	0	0	0	0	0	0	0	11
11:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	8	578	307	1	119	1	0	23	0	0	0	0	0	9	1046
Percent	0.8%	55.3%	29.3%	0.1%	11.4%	0.1%	0%	2.2%	0%	0%	0%	0%	0%	0.9%	
ADT 1046															
AM Peak Volume	10:00 AM 1	7:00 AM 59	7:00 AM 26	12:00 AM 0	10:00 AM 11	8:00 AM 1	12:00 AM 0	7:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 2	7:00 AM 93
PM Peak Volume	5:00 PM 3	5:00 PM 72	5:00 PM 31	12:00 PM 1	5:00 PM 10	12:00 PM 0	12:00 PM 0	3:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 3	5:00 PM 119

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889228

DIRECTION: EB, WB

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
02:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	2	0	0	0	0	1	0	0	0	0	0	0	4
04:00 AM	0	10	8	0	0	0	0	0	0	0	0	0	0	0	18
05:00 AM	0	50	16	1	5	0	0	1	0	0	0	0	0	0	73
06:00 AM	0	42	26	0	8	0	0	3	0	0	0	0	0	2	81
07:00 AM	0	74	20	1	4	0	0	2	0	0	0	0	0	0	101
08:00 AM	0	24	15	0	5	0	0	3	0	0	0	0	0	1	48
09:00 AM	1	21	14	0	3	0	0	8	0	0	0	0	0	0	47
10:00 AM	2	22	15	0	4	0	1	0	0	0	0	0	0	0	44
11:00 AM	2	29	7	0	5	0	0	4	0	0	0	0	0	0	47
12:00 PM	5	19	16	0	3	1	0	0	0	0	0	0	0	0	44
01:00 PM	2	26	17	1	6	0	0	3	0	0	0	0	0	1	56
02:00 PM	2	37	11	1	3	0	0	4	0	0	0	0	0	1	59
03:00 PM	1	58	25	1	11	0	0	4	0	0	0	0	0	2	102
04:00 PM	2	57	30	0	13	0	0	4	0	0	0	0	0	2	108
05:00 PM	0	68	26	0	10	0	0	5	0	0	0	0	0	3	112
06:00 PM	0	41	17	0	7	0	0	4	0	1	0	0	0	4	74
07:00 PM	1	22	12	0	11	0	0	1	0	0	0	0	0	0	47
08:00 PM	1	14	9	0	2	0	0	1	0	0	0	0	0	0	27
09:00 PM	0	8	3	0	5	0	0	0	0	0	0	0	0	0	16
10:00 PM	0	5	2	0	2	0	0	0	0	0	0	0	0	0	9
11:00 PM	0	9	0	0	0	0	0	0	0	0	0	0	0	0	9
Day Total	19	643	291	5	108	1	1	48	0	1	0	0	0	16	1133
Percent	1.7%	56.8%	25.7%	0.4%	9.5%	0.1%	0.1%	4.2%	0%	0.1%	0%	0%	0%	1.4%	
ADT 1133															
AM Peak Volume	10:00 AM 2	7:00 AM 74	6:00 AM 26	5:00 AM 1	6:00 AM 8	12:00 AM 0	10:00 AM 1	9:00 AM 8	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 2	7:00 AM 101
PM Peak Volume	12:00 PM 5	5:00 PM 68	4:00 PM 30	1:00 PM 1	4:00 PM 13	12:00 PM 1	12:00 PM 0	5:00 PM 5	12:00 PM 0	6:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 4	5:00 PM 112

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	4	2	0	0	0	0	0	0	0	0	0	0	0	6
01:00 AM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4
02:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	4	0	0	0	0	1	0	0	0	0	0	0	6
04:00 AM	0	7	7	0	1	0	0	1	0	0	0	0	0	0	16
05:00 AM	0	36	18	1	3	0	0	1	0	0	0	0	0	0	59
06:00 AM	0	48	21	0	7	0	0	1	0	0	0	0	0	0	77
07:00 AM	0	65	21	0	7	0	0	3	0	0	0	0	0	1	97
08:00 AM	1	18	11	0	5	0	0	2	1	0	0	0	0	0	38
09:00 AM	3	22	14	0	7	0	0	3	0	0	0	0	0	1	50
10:00 AM	1	27	14	0	7	0	0	2	0	0	0	0	0	1	52
11:00 AM	4	31	15	0	4	0	0	2	0	0	0	0	0	0	56
12:00 PM	4	37	9	0	9	0	0	4	0	0	0	0	0	2	65
01:00 PM	2	35	24	0	9	0	0	6	0	0	0	0	0	1	77
02:00 PM	2	50	16	0	8	0	0	0	0	0	0	0	0	3	79
03:00 PM	1	73	37	0	9	0	0	2	0	0	0	0	0	0	122
04:00 PM	0	71	35	1	18	0	0	2	0	0	0	0	0	1	128
05:00 PM	2	73	39	1	18	0	0	5	0	0	0	0	0	0	138
06:00 PM	0	60	27	0	12	0	0	0	0	0	0	0	0	0	99
07:00 PM	0	47	13	0	11	0	0	1	0	0	0	0	0	0	72
08:00 PM	0	25	13	0	5	0	0	2	0	0	0	0	0	0	45
09:00 PM	0	41	17	2	0	0	0	1	0	0	0	0	0	0	61
10:00 PM	1	17	8	0	4	0	0	0	0	0	0	0	0	0	30
11:00 PM	0	7	2	0	0	0	0	1	0	0	0	0	0	0	10
Day Total	21	797	371	5	144	0	0	40	1	0	0	0	0	10	1389
Percent	1.5%	57.4%	26.7%	0.4%	10.4%	0%	0%	2.9%	0.1%	0%	0%	0%	0%	0.7%	
ADT 1389															
AM Peak Volume	11:00 AM 4	7:00 AM 65	6:00 AM 21	5:00 AM 1	6:00 AM 7	12:00 AM 0	12:00 AM 0	7:00 AM 3	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 1	7:00 AM 97
PM Peak Volume	12:00 PM 4	3:00 PM 73	5:00 PM 39	9:00 PM 2	4:00 PM 18	12:00 PM 0	12:00 PM 0	1:00 PM 6	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 3	5:00 PM 138

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	8	4	0	3	0	0	0	0	0	0	0	0	0	15
01:00 AM	0	1	1	0	1	0	0	2	0	0	0	0	0	0	5
02:00 AM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
03:00 AM	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
04:00 AM	0	5	7	0	0	0	0	0	0	0	0	0	0	0	12
05:00 AM	0	14	9	1	1	0	0	0	0	0	0	0	0	0	25
06:00 AM	0	12	9	0	2	0	0	3	1	0	0	0	0	1	28
07:00 AM	0	24	9	0	6	0	0	1	0	0	0	0	0	1	41
08:00 AM	0	18	13	0	11	0	0	0	0	0	0	0	0	2	44
09:00 AM	14	22	15	0	8	0	0	1	0	0	0	0	0	0	60
10:00 AM	1	43	24	0	12	0	0	4	0	0	0	0	0	0	84
11:00 AM	7	43	22	0	9	0	0	5	0	0	0	0	0	3	89
12:00 PM	13	44	25	0	4	0	0	4	0	0	0	0	0	0	90
01:00 PM	9	38	26	0	7	0	0	4	0	0	0	0	0	3	87
02:00 PM	10	51	20	0	7	0	0	6	0	0	0	0	0	2	96
03:00 PM	12	47	20	0	15	0	0	2	0	0	0	0	0	1	97
04:00 PM	9	48	26	0	7	0	0	5	0	0	0	0	0	2	97
05:00 PM	6	45	22	0	12	0	0	5	0	0	0	0	0	1	91
06:00 PM	0	22	27	0	12	0	0	5	0	0	0	0	0	2	68
07:00 PM	8	35	23	0	13	0	0	2	0	0	0	0	0	0	81
08:00 PM	0	21	11	0	5	0	0	2	0	0	0	0	0	0	39
09:00 PM	1	15	7	0	5	0	0	0	0	0	0	0	0	0	28
10:00 PM	0	20	3	0	4	0	0	2	0	0	0	0	0	0	29
11:00 PM	1	3	8	0	2	0	0	4	0	0	0	0	0	0	18
Day Total	91	589	332	1	146	0	0	57	1	0	0	0	0	18	1235
Percent	7.4%	47.7%	26.9%	0.1%	11.8%	0%	0%	4.6%	0.1%	0%	0%	0%	0%	1.5%	
ADT 1235															
AM Peak Volume	9:00 AM 14	10:00 AM 43	10:00 AM 24	5:00 AM 1	10:00 AM 12	12:00 AM 0	12:00 AM 0	11:00 AM 5	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3	11:00 AM 89
PM Peak Volume	12:00 PM 13	2:00 PM 51	6:00 PM 27	12:00 PM 0	3:00 PM 15	12:00 PM 0	12:00 PM 0	2:00 PM 6	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 3	3:00 PM 97

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889228

DIRECTION: EB, WB

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	3	1	0	1	0	0	0	0	0	0	0	0	0	5
01:00 AM	0	3	3	0	1	0	0	1	0	0	0	0	0	0	8
02:00 AM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
03:00 AM	0	2	3	0	1	0	0	0	0	0	0	0	0	0	6
04:00 AM	0	3	8	0	0	0	0	0	0	0	0	0	0	0	11
05:00 AM	0	2	1	0	0	0	0	1	0	0	0	0	0	0	4
06:00 AM	0	6	11	0	1	0	0	2	0	0	0	0	0	0	20
07:00 AM	0	14	5	0	5	0	0	2	0	0	0	0	0	0	26
08:00 AM	0	22	9	0	6	0	0	2	0	0	0	0	0	0	39
09:00 AM	3	28	7	0	2	0	0	1	0	0	0	0	0	2	43
10:00 AM	5	24	12	0	7	0	0	3	0	0	0	0	0	2	53
11:00 AM	2	34	16	0	2	0	0	0	0	0	0	0	0	2	56
12:00 PM	8	43	24	0	13	0	0	1	0	0	0	0	0	3	92
01:00 PM	2	49	19	0	5	0	0	4	0	0	0	0	0	2	81
02:00 PM	9	49	27	1	3	0	0	3	0	0	0	0	0	0	92
03:00 PM	5	56	16	0	7	0	0	3	0	0	0	0	0	1	88
04:00 PM	0	50	24	0	12	0	0	5	0	0	0	0	0	1	92
05:00 PM	4	43	22	0	8	0	0	4	0	0	0	0	0	0	81
06:00 PM	1	44	20	0	4	0	0	2	0	0	0	0	0	1	72
07:00 PM	2	26	15	0	7	0	0	3	0	0	0	0	0	2	55
08:00 PM	0	15	5	0	1	0	0	1	0	0	0	0	0	0	22
09:00 PM	0	18	4	0	1	0	0	0	0	0	0	0	0	0	23
10:00 PM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
11:00 PM	0	3	2	0	0	0	0	1	0	0	0	0	0	0	6
Day Total	41	545	259	1	87	0	0	39	0	0	0	0	0	16	988
Percent	4.1%	55.2%	26.2%	0.1%	8.8%	0%	0%	3.9%	0%	0%	0%	0%	0%	1.6%	
ADT 988															
AM Peak Volume	10:00 AM	11:00 AM	11:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM	11:00 AM
	5	34	16	0	7	0	0	3	0	0	0	0	0	2	56
PM Peak Volume	2:00 PM	3:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	9	56	27	1	13	0	0	5	0	0	0	0	0	3	92

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge


QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	199	4353	2133	16	786	6	1	248	2	1	0	0	0	88	7833
Percent	2.5%	55.6%	27.2%	0.2%	10%	0.1%	0%	3.2%	0%	0%	0%	0%	0%	1.1%	
ADT 1119															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge										QC JOB #: 15889228
SPECIFIC LOCATION:										DIRECTION: EB, WB
CITY/STATE: Colbert, AL										DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	5	6	0	3	6	4	15	5	6	
01:00 AM	8	1	1	2	4	3	5	8	4	
02:00 AM	4	2	2	2	2	2	5	5	3	
03:00 AM	4	3	7	4	6	5	6	6	5	
04:00 AM	17	13	19	18	16	17	12	11	15	
05:00 AM	77	73	70	73	59	70	25	4	54	
06:00 AM	75	68	73	81	77	75	28	20	60	
07:00 AM	103	85	93	101	97	96	41	26	78	
08:00 AM	38	36	47	48	38	41	44	39	41	
09:00 AM	48	60	33	47	50	48	60	43	49	
10:00 AM	31	40	46	44	52	43	84	53	50	
11:00 AM	37	46	47	47	56	47	89	56	54	
12:00 PM	44	56	41	44	65	50	90	92	62	
01:00 PM	58	51	37	56	77	56	87	81	64	
02:00 PM	57	57	65	59	79	63	96	92	72	
03:00 PM	76	80	81	102	122	92	97	88	92	
04:00 PM	97	94	95	108	128	104	97	92	102	
05:00 PM	98	100	119	112	138	113	91	81	106	
06:00 PM	47	65	71	74	99	71	68	72	71	
07:00 PM	43	57	43	47	72	52	81	55	57	
08:00 PM	24	17	32	27	45	29	39	22	29	
09:00 PM	11	9	12	16	61	22	28	23	23	
10:00 PM	8	7	11	9	30	13	29	8	15	
11:00 PM	2	4	1	9	10	5	18	6	7	
Day Total	1012	1030	1046	1133	1389	1121	1235	988	1119	
% Weekday Average	90.3%	91.9%	93.3%	101.1%	123.9%					
% Week Average	90.4%	92%	93.5%	101.3%	124.1%	100.2%	110.4%	88.3%		
AM Peak Volume	7:00 AM 103	7:00 AM 85	7:00 AM 93	7:00 AM 101	7:00 AM 97	7:00 AM 96	11:00 AM 89	11:00 AM 56	7:00 AM 78	
PM Peak Volume	5:00 PM 98	5:00 PM 100	5:00 PM 119	5:00 PM 112	5:00 PM 138	5:00 PM 113	3:00 PM 97	12:00 PM 92	5:00 PM 106	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: WB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	0	1	1	0	0	0	0	3	51-60	2
01:00 AM	0	0	0	0	0	0	0	0	1	0	1	1	0	0	3	61-70	2
02:00 AM	0	0	0	0	0	0	0	1	0	1	0	1	0	0	3	41-50	1
03:00 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	2	51-60	2
04:00 AM	0	0	0	0	0	0	1	0	1	1	2	3	0	1	9	61-70	5
05:00 AM	0	0	0	0	0	0	1	9	15	18	14	6	0	0	63	51-60	33
06:00 AM	0	0	0	0	0	0	0	5	16	15	7	3	1	1	48	51-60	31
07:00 AM	0	0	0	0	0	0	1	5	8	13	8	2	1	1	39	52-61	21
08:00 AM	0	0	0	0	0	0	2	5	7	5	2	0	0	1	22	46-55	12
09:00 AM	0	0	0	0	0	1	1	5	12	5	4	1	0	0	29	46-55	17
10:00 AM	0	0	0	0	0	0	2	2	6	4	0	0	2	0	16	51-60	10
11:00 AM	1	0	0	0	0	0	1	0	5	3	4	0	2	0	16	51-60	8
12:00 PM	0	0	0	1	0	1	0	3	7	5	3	0	1	0	21	51-60	12
01:00 PM	0	0	0	0	1	1	3	4	15	6	4	1	0	0	35	51-60	21
02:00 PM	0	0	0	0	1	1	2	3	9	3	1	1	0	1	22	46-55	12
03:00 PM	0	0	0	0	0	0	0	8	18	11	3	2	0	0	42	51-60	29
04:00 PM	1	0	0	0	0	2	3	12	20	9	1	1	0	0	49	46-55	32
05:00 PM	0	0	0	0	0	1	1	3	16	15	5	1	0	0	42	51-60	31
06:00 PM	1	0	0	0	0	0	0	4	5	6	6	0	0	0	22	56-65	12
07:00 PM	0	0	0	0	0	2	1	3	4	2	1	1	0	0	14	46-55	7
08:00 PM	0	0	0	0	0	0	1	1	4	6	0	0	0	0	12	51-60	10
09:00 PM	0	0	0	0	0	0	1	0	3	1	0	0	1	0	6	51-60	4
10:00 PM	0	0	0	0	0	0	0	1	0	2	0	0	0	0	3	51-60	2
11:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
Day Total	3	0	0	1	2	9	22	74	174	134	66	24	8	5	522	51-60	308
Percent	0.6%	0%	0%	0.2%	0.4%	1.7%	4.2%	14.2%	33.3%	25.7%	12.6%	4.6%	1.5%	1%			
AM Peak Volume	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM	8:00 AM	5:00 AM	6:00 AM	5:00 AM	5:00 AM	5:00 AM	10:00 AM	4:00 AM	5:00 AM		
	1	0	0	0	0	1	2	9	16	18	14	6	2	1	63		
PM Peak Volume	4:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	4:00 PM	1:00 PM	4:00 PM	4:00 PM	5:00 PM	6:00 PM	3:00 PM	12:00 PM	2:00 PM	4:00 PM		
	1	0	0	1	1	2	3	12	20	15	6	2	1	1	49		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: WB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	2	1	0	0	0	3	56-65	3
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	66-75	1
03:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
04:00 AM	0	0	0	0	0	0	1	0	2	2	0	1	1	1	8	51-60	4
05:00 AM	0	0	0	0	0	0	0	7	20	14	7	7	0	1	56	51-60	34
06:00 AM	0	0	0	0	0	0	0	2	8	27	7	0	3	1	48	51-60	35
07:00 AM	0	0	0	0	0	0	2	3	11	11	6	2	0	0	35	51-60	22
08:00 AM	1	0	0	0	0	1	0	1	5	4	1	0	1	0	14	51-60	9
09:00 AM	1	0	0	0	0	0	0	7	11	7	4	1	0	0	31	49-58	18
10:00 AM	0	0	0	0	0	0	1	4	9	4	1	0	0	0	19	49-58	13
11:00 AM	1	0	0	0	0	0	0	4	10	4	3	1	0	0	23	50-59	14
12:00 PM	0	0	0	0	0	2	3	8	12	6	1	1	0	0	33	46-55	20
01:00 PM	1	0	0	0	0	1	2	5	6	6	0	1	0	0	22	51-60	12
02:00 PM	1	0	0	1	0	1	2	6	9	2	1	0	0	0	23	46-55	15
03:00 PM	1	0	0	0	0	1	3	8	12	8	2	1	1	0	37	48-57	20
04:00 PM	0	0	0	0	0	1	0	8	16	14	1	1	0	0	41	51-60	30
05:00 PM	0	0	0	0	1	3	2	8	18	12	3	3	0	0	50	51-60	30
06:00 PM	1	0	0	0	0	0	0	3	16	14	1	3	1	0	39	51-60	30
07:00 PM	1	0	0	0	0	0	2	3	12	0	1	0	0	0	19	46-55	15
08:00 PM	0	0	0	0	0	1	0	5	1	1	3	0	0	0	11	46-55	6
09:00 PM	0	0	0	0	0	0	0	1	2	1	0	1	0	0	5	51-60	3
10:00 PM	0	0	0	0	0	0	1	0	1	1	0	0	0	0	3	51-60	2
11:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
Day Total	8	0	0	1	1	11	19	83	181	142	43	23	8	3	523	51-60	323
Percent	1.5%	0%	0%	0.2%	0.2%	2.1%	3.6%	15.9%	34.6%	27.2%	8.2%	4.4%	1.5%	0.6%			
AM Peak Volume	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	7:00 AM	5:00 AM	5:00 AM	6:00 AM	5:00 AM	5:00 AM	6:00 AM	4:00 AM	5:00 AM		
	1	0	0	0	0	1	2	7	20	27	7	7	3	1	56		
PM Peak Volume	1:00 PM	12:00 PM	12:00 PM	2:00 PM	5:00 PM	5:00 PM	12:00 PM	12:00 PM	5:00 PM	4:00 PM	5:00 PM	5:00 PM	3:00 PM	12:00 PM	5:00 PM		
	1	0	0	1	1	3	3	8	18	14	3	3	1	0	50		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889228 DIRECTION: WB DATE: Aug 24 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 AM	0	0	0	0	0	0	0	2	1	1	0	0	0	0	4	46-55	3
04:00 AM	0	0	0	0	0	0	0	0	4	3	2	1	3	0	13	51-60	7
05:00 AM	0	0	0	0	0	0	0	2	14	17	6	10	1	1	51	51-60	31
06:00 AM	0	0	0	0	0	0	0	4	11	15	11	3	1	2	47	51-60	26
07:00 AM	0	0	0	0	0	0	2	1	13	14	9	1	1	0	41	51-60	27
08:00 AM	0	0	0	0	0	1	0	2	7	7	2	2	0	0	21	51-60	14
09:00 AM	0	0	0	0	0	0	0	2	11	6	3	1	0	0	23	51-60	17
10:00 AM	0	0	0	0	0	0	1	9	7	1	3	2	0	0	23	46-55	16
11:00 AM	0	0	0	0	0	1	0	5	11	7	1	2	0	0	27	51-60	18
12:00 PM	0	0	0	0	0	0	2	2	8	2	0	0	0	0	14	50-59	10
01:00 PM	0	0	0	0	0	1	2	3	3	8	1	2	0	0	20	51-60	11
02:00 PM	0	0	0	0	1	0	1	3	8	7	1	0	1	0	22	51-60	15
03:00 PM	0	0	0	0	0	0	0	9	20	4	1	0	1	0	35	46-55	29
04:00 PM	1	0	0	0	0	0	3	7	16	8	5	3	1	0	44	51-60	24
05:00 PM	1	0	0	0	0	0	5	7	16	19	5	1	0	0	54	51-60	35
06:00 PM	2	0	0	0	0	1	0	7	17	11	2	4	0	0	44	51-60	28
07:00 PM	1	0	0	0	0	0	0	3	4	6	0	0	1	0	15	51-60	10
08:00 PM	0	0	0	0	0	0	1	4	3	4	3	1	0	0	16	50-59	7
09:00 PM	0	0	0	0	0	0	0	0	0	3	0	1	0	0	4	51-60	3
10:00 PM	0	0	0	0	0	0	1	0	1	0	1	0	0	1	4	36-45	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	5	0	0	0	1	5	18	72	175	143	56	34	10	4	523	51-60	318
Percent	1%	0%	0%	0%	0.2%	1%	3.4%	13.8%	33.5%	27.3%	10.7%	6.5%	1.9%	0.8%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	2:00 AM	7:00 AM	10:00 AM	5:00 AM	5:00 AM	6:00 AM	5:00 AM	4:00 AM	6:00 AM	5:00 AM		
	0	0	0	0	0	1	2	9	14	17	11	10	3	2	51		
PM Peak Volume	6:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	1:00 PM	5:00 PM	3:00 PM	3:00 PM	5:00 PM	4:00 PM	6:00 PM	2:00 PM	10:00 PM	5:00 PM		
	2	0	0	0	1	1	5	9	20	19	5	4	1	1	54		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: WB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	36-45	2
04:00 AM	0	0	0	0	0	2	1	0	3	5	1	2	0	0	14	51-60	8
05:00 AM	0	0	0	0	0	0	0	4	18	15	13	8	0	1	59	51-60	33
06:00 AM	1	0	0	0	0	0	0	3	19	13	8	2	0	0	46	51-60	32
07:00 AM	0	0	0	0	0	1	0	5	13	14	7	2	2	0	44	51-60	27
08:00 AM	1	0	0	0	0	0	0	3	12	9	1	1	0	0	27	51-60	21
09:00 AM	0	0	0	0	0	0	2	5	11	4	0	0	1	0	23	46-55	16
10:00 AM	0	0	0	0	0	0	1	11	6	3	0	1	0	0	22	46-55	17
11:00 AM	0	0	0	0	0	1	4	7	11	2	1	0	0	0	26	46-55	18
12:00 PM	0	0	0	0	0	0	0	5	10	7	2	1	1	0	26	51-60	17
01:00 PM	0	0	0	0	0	2	0	8	11	8	1	1	0	0	31	48-57	19
02:00 PM	1	0	0	0	0	0	1	6	11	6	1	0	0	0	26	46-55	17
03:00 PM	2	0	0	0	0	0	3	4	17	10	4	2	1	1	44	51-60	27
04:00 PM	0	0	0	0	0	1	7	12	16	7	3	1	0	0	47	46-55	28
05:00 PM	1	0	0	0	0	0	3	8	19	12	6	4	0	0	53	51-60	31
06:00 PM	4	0	0	0	1	1	4	8	14	4	5	1	0	0	42	46-55	22
07:00 PM	0	0	0	0	0	0	1	4	9	5	1	1	0	0	21	51-60	14
08:00 PM	0	0	0	0	0	0	0	3	4	4	0	0	0	0	11	51-60	8
09:00 PM	0	0	0	0	0	0	1	2	3	4	1	0	0	1	12	51-60	7
10:00 PM	0	0	0	0	0	0	0	0	2	1	0	1	0	1	5	51-60	3
11:00 PM	0	0	0	0	1	0	0	0	2	0	1	0	0	0	4	46-55	2
Day Total	10	0	0	0	2	9	29	98	212	133	56	28	5	4	586	51-60	345
Percent	1.7%	0%	0%	0%	0.3%	1.5%	4.9%	16.7%	36.2%	22.7%	9.6%	4.8%	0.9%	0.7%			
AM Peak Volume	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	4:00 AM	11:00 AM	10:00 AM	6:00 AM	5:00 AM	5:00 AM	5:00 AM	7:00 AM	5:00 AM	5:00 AM		
	1	0	0	0	0	2	4	11	19	15	13	8	2	1	59		
PM Peak Volume	6:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM	1:00 PM	4:00 PM	4:00 PM	5:00 PM	5:00 PM	5:00 PM	5:00 PM	12:00 PM	3:00 PM	5:00 PM		
	4	0	0	0	1	2	7	12	19	12	6	4	1	1	53		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: WB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	1	0	1	0	0	0	3	46-55	2
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	66-75	1
02:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	2	41-50	1
03:00 AM	0	0	0	0	0	0	0	1	1	1	1	0	0	0	4	46-55	2
04:00 AM	0	0	0	0	0	1	0	1	2	1	4	0	1	1	11	56-65	5
05:00 AM	0	0	0	0	0	0	0	1	13	13	8	6	2	3	46	51-60	26
06:00 AM	0	0	0	0	0	0	1	2	20	18	7	1	0	2	51	51-60	38
07:00 AM	1	0	0	0	0	1	3	4	10	15	3	7	0	0	44	51-60	25
08:00 AM	0	0	0	0	0	0	0	4	4	8	2	1	1	0	20	51-60	12
09:00 AM	1	0	0	0	0	1	3	4	7	4	3	1	0	1	25	49-58	11
10:00 AM	0	0	0	0	0	0	1	5	12	6	1	0	0	0	25	51-60	18
11:00 AM	0	0	0	0	0	1	0	2	8	11	1	1	0	0	24	51-60	19
12:00 PM	1	0	0	0	0	0	2	7	12	9	4	3	0	0	38	51-60	21
01:00 PM	1	0	0	0	1	1	2	9	18	9	0	2	0	1	44	46-55	27
02:00 PM	1	0	0	0	0	1	2	5	15	6	1	0	0	0	31	51-60	21
03:00 PM	0	0	0	0	0	0	2	13	17	13	8	1	0	0	54	51-60	30
04:00 PM	0	0	0	0	0	0	2	12	24	15	7	2	0	0	62	51-60	39
05:00 PM	0	0	0	1	0	0	2	4	17	15	4	2	0	0	45	51-60	32
06:00 PM	0	0	0	0	0	1	0	7	18	12	5	1	1	0	45	51-60	30
07:00 PM	0	0	0	0	0	0	0	3	13	8	0	0	0	0	24	51-60	21
08:00 PM	0	0	0	0	0	1	0	6	8	8	3	0	0	0	26	51-60	16
09:00 PM	0	0	0	0	0	0	3	16	17	14	5	0	0	0	55	46-55	33
10:00 PM	0	0	0	0	0	0	0	3	5	4	2	1	0	0	15	51-60	9
11:00 PM	0	0	0	0	0	0	1	0	1	1	0	0	0	0	3	51-60	2
Day Total	5	0	0	1	1	8	24	111	243	192	70	29	6	8	698	51-60	435
Percent	0.7%	0%	0%	0.1%	0.1%	1.1%	3.4%	15.9%	34.8%	27.5%	10%	4.2%	0.9%	1.1%			
AM Peak Volume	7:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	4:00 AM 1	7:00 AM 3	10:00 AM 5	6:00 AM 20	6:00 AM 18	5:00 AM 8	7:00 AM 7	5:00 AM 2	5:00 AM 3	6:00 AM 51		
PM Peak Volume	12:00 PM 1	12:00 PM 0	12:00 PM 0	5:00 PM 1	1:00 PM 1	1:00 PM 1	9:00 PM 3	9:00 PM 16	4:00 PM 24	4:00 PM 15	3:00 PM 8	12:00 PM 3	6:00 PM 1	1:00 PM 1	4:00 PM 62		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889228 DIRECTION: WB DATE: Aug 27 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	0	0	0	1	1	0	0	3	61-70	2
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	61-70	1
02:00 AM	0	0	0	0	0	0	0	1	1	0	1	0	0	0	3	46-55	2
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
04:00 AM	0	0	0	0	0	1	2	0	3	2	1	0	0	1	10	51-60	5
05:00 AM	0	0	0	0	0	1	0	3	3	5	3	3	2	1	21	51-60	8
06:00 AM	0	0	0	0	0	0	0	5	9	5	1	0	0	1	21	46-55	14
07:00 AM	1	0	0	0	0	0	1	2	5	3	2	2	0	0	16	51-60	8
08:00 AM	1	0	0	0	0	0	1	4	7	4	4	1	1	0	23	49-58	11
09:00 AM	0	0	0	0	0	0	3	4	11	9	0	0	0	0	27	51-60	20
10:00 AM	0	0	0	0	0	0	1	6	9	13	4	2	1	0	36	51-60	22
11:00 AM	2	0	0	1	0	0	3	11	13	15	4	1	0	0	50	51-60	28
12:00 PM	0	0	0	0	0	0	2	14	17	14	6	0	0	0	53	51-60	31
01:00 PM	2	0	0	0	0	0	5	9	16	7	5	2	0	0	46	46-55	25
02:00 PM	0	0	1	0	0	1	0	5	14	10	6	0	0	0	37	51-60	24
03:00 PM	0	0	0	0	0	0	3	12	19	12	3	0	0	0	49	46-55	31
04:00 PM	1	0	0	0	0	2	2	14	13	13	5	1	1	0	52	46-55	27
05:00 PM	1	0	0	0	0	0	3	3	22	18	2	0	0	0	49	51-60	40
06:00 PM	2	0	0	0	0	0	5	6	11	8	5	3	1	0	41	51-60	19
07:00 PM	0	0	0	0	0	0	0	9	13	11	2	0	0	0	35	51-60	24
08:00 PM	0	0	0	0	0	0	1	3	7	3	3	0	0	0	17	46-55	10
09:00 PM	0	0	0	0	0	0	0	3	8	4	2	0	1	0	18	51-60	12
10:00 PM	0	0	0	0	0	0	0	2	4	5	2	2	0	0	15	51-60	9
11:00 PM	0	0	0	0	0	0	0	0	6	1	4	1	0	0	12	51-60	7
Day Total	10	0	1	1	0	5	33	117	211	162	66	20	7	3	636	51-60	373
Percent	1.6%	0%	0.2%	0.2%	0%	0.8%	5.2%	18.4%	33.2%	25.5%	10.4%	3.1%	1.1%	0.5%			
AM Peak Volume	11:00 AM	12:00 AM	12:00 AM	11:00 AM	12:00 AM	4:00 AM	9:00 AM	11:00 AM	11:00 AM	11:00 AM	8:00 AM	5:00 AM	5:00 AM	4:00 AM	11:00 AM		
	2	0	0	1	0	1	3	11	13	15	4	3	2	1	50		
PM Peak Volume	1:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	4:00 PM	1:00 PM	12:00 PM	5:00 PM	5:00 PM	12:00 PM	6:00 PM	4:00 PM	12:00 PM	12:00 PM		
	2	0	1	0	0	2	5	14	22	18	6	3	1	0	53		
Comments:																	

Report generated on 9/2/2022 10:19 AM


SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889228 DIRECTION: WB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	51-60	2
01:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	2	41-50	1
02:00 AM	0	0	0	0	0	0	0	0	0	1	1	0	0	0	2	56-65	2
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	1	3	2	0	0	0	6	56-65	5
05:00 AM	0	0	0	0	0	0	0	0	0	1	2	0	1	0	4	56-65	3
06:00 AM	0	0	0	0	0	0	0	1	5	7	2	0	0	0	15	51-60	12
07:00 AM	0	0	0	0	0	0	0	3	5	2	1	0	0	0	11	46-55	8
08:00 AM	1	0	0	0	0	0	1	3	4	3	3	0	0	1	16	46-55	7
09:00 AM	1	0	0	0	0	0	3	4	6	7	2	1	0	0	24	51-60	13
10:00 AM	1	0	0	0	0	0	5	6	9	6	3	0	1	0	31	46-55	15
11:00 AM	0	0	0	0	0	0	0	3	14	2	2	1	0	0	22	46-55	17
12:00 PM	1	0	0	0	0	0	2	14	21	15	7	0	1	0	61	51-60	36
01:00 PM	2	0	0	0	0	0	3	3	20	8	5	0	1	0	42	51-60	28
02:00 PM	0	0	0	0	0	0	1	3	21	14	9	1	0	1	50	51-60	35
03:00 PM	1	0	0	0	0	1	2	8	26	14	3	2	0	0	57	51-60	40
04:00 PM	1	0	0	0	0	0	2	4	11	20	6	1	1	0	46	51-60	31
05:00 PM	0	0	0	0	0	0	1	10	21	11	3	1	0	0	47	51-60	32
06:00 PM	0	0	0	0	0	0	0	4	14	13	6	3	1	0	41	51-60	27
07:00 PM	1	0	0	0	1	0	0	6	5	7	1	0	0	0	21	51-60	12
08:00 PM	0	0	0	1	1	0	2	3	3	1	3	0	0	0	14	46-55	6
09:00 PM	0	0	0	1	1	0	1	0	1	4	0	0	1	0	9	51-60	5
10:00 PM	0	0	0	0	0	0	0	1	1	1	2	0	0	0	5	56-65	3
11:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
Day Total	9	0	0	2	3	1	23	77	188	144	63	10	7	2	529	51-60	332
Percent	1.7%	0%	0%	0.4%	0.6%	0.2%	4.3%	14.6%	35.5%	27.2%	11.9%	1.9%	1.3%	0.4%			
AM Peak Volume	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	10:00 AM	11:00 AM	6:00 AM	8:00 AM	9:00 AM	5:00 AM	8:00 AM	10:00 AM		
	1	0	0	0	0	0	5	6	14	7	3	1	1	1	31		
PM Peak Volume	1:00 PM	12:00 PM	12:00 PM	8:00 PM	7:00 PM	3:00 PM	1:00 PM	12:00 PM	3:00 PM	4:00 PM	2:00 PM	6:00 PM	12:00 PM	2:00 PM	12:00 PM		
	2	0	0	1	1	1	3	14	26	20	9	3	1	1	61		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) John Coffee Memorial Bridge														QC JOB #: 15889228			
SPECIFIC LOCATION:														DIRECTION: WB			
CITY/STATE: Colbert, AL														DATE: Aug 22 2022 - Aug 28 2022			
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	50	0	1	6	10	48	168	632	1384	1050	420	168	51	29	4017	51-60	2434
Percent	1.2%	0%	0%	0.1%	0.2%	1.2%	4.2%	15.7%	34.5%	26.1%	10.5%	4.2%	1.3%	0.7%			
Cumulative Percent	1.2%	1.2%	1.3%	1.4%	1.7%	2.9%	7%	22.8%	57.2%	83.4%	93.8%	98%	99.3%	100%			
ADT 573															85th Percentile: 60 MPH Mean Speed(Average): 53 MPH Median: 53 MPH Mode: 53 MPH		
Comments:																	

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
03:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	6	1	0	2	0	0	0	0	0	0	0	0	0	9
05:00 AM	0	42	20	0	1	0	0	0	0	0	0	0	0	0	63
06:00 AM	0	26	16	0	6	0	0	0	0	0	0	0	0	0	48
07:00 AM	0	28	6	0	5	0	0	0	0	0	0	0	0	0	39
08:00 AM	0	12	7	0	3	0	0	0	0	0	0	0	0	0	22
09:00 AM	0	11	14	0	2	0	0	2	0	0	0	0	0	0	29
10:00 AM	2	9	3	0	1	0	0	1	0	0	0	0	0	0	16
11:00 AM	1	7	3	0	4	0	0	0	0	0	0	0	0	1	16
12:00 PM	0	13	7	0	1	0	0	0	0	0	0	0	0	0	21
01:00 PM	0	23	10	0	1	1	0	0	0	0	0	0	0	0	35
02:00 PM	0	15	7	0	0	0	0	0	0	0	0	0	0	0	22
03:00 PM	0	22	18	1	1	0	0	0	0	0	0	0	0	0	42
04:00 PM	1	30	13	0	2	0	0	2	0	0	0	0	0	1	49
05:00 PM	0	31	5	0	3	0	0	3	0	0	0	0	0	0	42
06:00 PM	1	9	7	0	4	0	0	0	0	0	0	0	0	1	22
07:00 PM	0	9	5	0	0	0	0	0	0	0	0	0	0	0	14
08:00 PM	0	7	3	0	2	0	0	0	0	0	0	0	0	0	12
09:00 PM	0	5	0	0	1	0	0	0	0	0	0	0	0	0	6
10:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	5	313	151	1	39	1	0	9	0	0	0	0	0	3	522
Percent	1%	60%	28.9%	0.2%	7.5%	0.2%	0%	1.7%	0%	0%	0%	0%	0%	0.6%	
ADT 522															
AM Peak Volume	10:00 AM 2	5:00 AM 42	5:00 AM 20	12:00 AM 0	6:00 AM 6	12:00 AM 0	12:00 AM 0	9:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 1	5:00 AM 63
PM Peak Volume	4:00 PM 1	5:00 PM 31	3:00 PM 18	3:00 PM 1	6:00 PM 4	1:00 PM 1	12:00 PM 0	5:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 1	4:00 PM 49

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	1	0	1	0	0	1	0	0	0	0	0	0	3
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
05:00 AM	0	37	15	0	4	0	0	0	0	0	0	0	0	0	56
06:00 AM	0	26	19	0	3	0	0	0	0	0	0	0	0	0	48
07:00 AM	0	25	9	0	1	0	0	0	0	0	0	0	0	0	35
08:00 AM	0	10	3	0	0	0	0	0	0	0	0	0	0	1	14
09:00 AM	0	12	16	0	0	0	0	2	0	0	0	0	0	1	31
10:00 AM	0	10	7	0	2	0	0	0	0	0	0	0	0	0	19
11:00 AM	0	11	8	0	3	0	0	0	0	0	0	0	0	1	23
12:00 PM	3	17	10	0	2	0	0	1	0	0	0	0	0	0	33
01:00 PM	0	13	7	0	0	0	0	1	0	0	0	0	0	1	22
02:00 PM	0	12	8	0	2	0	0	0	0	0	0	0	0	1	23
03:00 PM	0	24	11	0	1	0	0	0	0	0	0	0	0	1	37
04:00 PM	0	22	17	0	2	0	0	0	0	0	0	0	0	0	41
05:00 PM	1	39	5	0	3	1	0	1	0	0	0	0	0	0	50
06:00 PM	0	24	12	0	2	0	0	0	0	0	0	0	0	1	39
07:00 PM	0	10	6	0	1	0	0	1	0	0	0	0	0	1	19
08:00 PM	0	8	3	0	0	0	0	0	0	0	0	0	0	0	11
09:00 PM	0	1	3	0	1	0	0	0	0	0	0	0	0	0	5
10:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
11:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	4	307	167	0	29	1	0	7	0	0	0	0	0	8	523
Percent	0.8%	58.7%	31.9%	0%	5.5%	0.2%	0%	1.3%	0%	0%	0%	0%	0%	1.5%	
ADT 523															
AM Peak Volume	12:00 AM	5:00 AM	6:00 AM	12:00 AM	5:00 AM	12:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	5:00 AM
	0	37	19	0	4	0	0	2	0	0	0	0	0	1	56
PM Peak Volume	12:00 PM	5:00 PM	4:00 PM	12:00 PM	5:00 PM	5:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	5:00 PM
	3	39	17	0	3	1	0	1	0	0	0	0	0	1	50

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4
04:00 AM	0	7	5	0	1	0	0	0	0	0	0	0	0	0	13
05:00 AM	0	35	12	0	3	0	0	1	0	0	0	0	0	0	51
06:00 AM	0	27	18	0	1	0	0	1	0	0	0	0	0	0	47
07:00 AM	0	28	10	0	1	0	0	2	0	0	0	0	0	0	41
08:00 AM	0	14	5	0	2	0	0	0	0	0	0	0	0	0	21
09:00 AM	0	9	10	0	4	0	0	0	0	0	0	0	0	0	23
10:00 AM	1	10	5	0	7	0	0	0	0	0	0	0	0	0	23
11:00 AM	0	8	10	0	9	0	0	0	0	0	0	0	0	0	27
12:00 PM	0	6	3	1	3	0	0	1	0	0	0	0	0	0	14
01:00 PM	0	8	10	0	2	0	0	0	0	0	0	0	0	0	20
02:00 PM	0	10	9	0	2	0	0	1	0	0	0	0	0	0	22
03:00 PM	1	17	11	0	4	0	0	2	0	0	0	0	0	0	35
04:00 PM	0	25	13	0	4	0	0	1	0	0	0	0	0	1	44
05:00 PM	3	33	13	0	4	0	0	0	0	0	0	0	0	1	54
06:00 PM	0	21	16	0	5	0	0	0	0	0	0	0	0	2	44
07:00 PM	0	11	3	0	0	0	0	0	0	0	0	0	0	1	15
08:00 PM	0	10	6	0	0	0	0	0	0	0	0	0	0	0	16
09:00 PM	0	1	2	0	0	0	0	1	0	0	0	0	0	0	4
10:00 PM	0	2	1	0	1	0	0	0	0	0	0	0	0	0	4
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	5	283	166	1	53	0	0	10	0	0	0	0	0	5	523
Percent	1%	54.1%	31.7%	0.2%	10.1%	0%	0%	1.9%	0%	0%	0%	0%	0%	1%	
ADT 523															
AM Peak Volume	10:00 AM	5:00 AM	6:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	5:00 AM
	1	35	18	0	9	0	0	2	0	0	0	0	0	0	51
PM Peak Volume	5:00 PM	5:00 PM	6:00 PM	12:00 PM	6:00 PM	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM	5:00 PM
	3	33	16	1	5	0	0	2	0	0	0	0	0	2	54

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
04:00 AM	0	7	7	0	0	0	0	0	0	0	0	0	0	0	14
05:00 AM	0	41	14	0	3	0	0	1	0	0	0	0	0	0	59
06:00 AM	0	23	17	0	3	0	0	2	0	0	0	0	0	1	46
07:00 AM	0	32	10	0	2	0	0	0	0	0	0	0	0	0	44
08:00 AM	0	14	8	0	3	0	0	1	0	0	0	0	0	1	27
09:00 AM	0	7	8	0	2	0	0	6	0	0	0	0	0	0	23
10:00 AM	2	10	10	0	0	0	0	0	0	0	0	0	0	0	22
11:00 AM	1	17	2	0	4	0	0	2	0	0	0	0	0	0	26
12:00 PM	0	9	13	0	3	1	0	0	0	0	0	0	0	0	26
01:00 PM	2	15	9	0	2	0	0	3	0	0	0	0	0	0	31
02:00 PM	1	18	4	1	0	0	0	1	0	0	0	0	0	1	26
03:00 PM	1	27	9	1	3	0	0	2	0	0	0	0	0	1	44
04:00 PM	2	27	13	0	4	0	0	1	0	0	0	0	0	0	47
05:00 PM	0	33	12	0	5	0	0	2	0	0	0	0	0	1	53
06:00 PM	0	22	11	0	3	0	0	1	0	1	0	0	0	4	42
07:00 PM	0	12	5	0	4	0	0	0	0	0	0	0	0	0	21
08:00 PM	1	5	4	0	0	0	0	1	0	0	0	0	0	0	11
09:00 PM	0	6	2	0	4	0	0	0	0	0	0	0	0	0	12
10:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
11:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
Day Total	10	334	160	2	45	1	0	24	0	1	0	0	0	9	586
Percent	1.7%	57%	27.3%	0.3%	7.7%	0.2%	0%	4.1%	0%	0.2%	0%	0%	0%	1.5%	
ADT 586															
AM Peak Volume	10:00 AM 2	5:00 AM 41	6:00 AM 17	12:00 AM 0	11:00 AM 4	12:00 AM 0	12:00 AM 0	9:00 AM 6	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1	5:00 AM 59
PM Peak Volume	1:00 PM 2	5:00 PM 33	12:00 PM 13	2:00 PM 1	5:00 PM 5	12:00 PM 1	12:00 PM 0	1:00 PM 3	12:00 PM 0	6:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 4	5:00 PM 53

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	2	0	0	0	0	1	0	0	0	0	0	0	4
04:00 AM	0	5	6	0	0	0	0	0	0	0	0	0	0	0	11
05:00 AM	0	29	14	0	2	0	0	1	0	0	0	0	0	0	46
06:00 AM	0	30	17	0	3	0	0	1	0	0	0	0	0	0	51
07:00 AM	0	30	9	0	4	0	0	0	0	0	0	0	0	1	44
08:00 AM	1	8	8	0	3	0	0	0	0	0	0	0	0	0	20
09:00 AM	1	9	11	0	2	0	0	1	0	0	0	0	0	1	25
10:00 AM	0	13	7	0	4	0	0	1	0	0	0	0	0	0	25
11:00 AM	2	16	5	0	1	0	0	0	0	0	0	0	0	0	24
12:00 PM	2	24	6	0	3	0	0	2	0	0	0	0	0	1	38
01:00 PM	2	19	14	0	4	0	0	4	0	0	0	0	0	1	44
02:00 PM	1	21	8	0	0	0	0	0	0	0	0	0	0	1	31
03:00 PM	0	34	16	0	2	0	0	2	0	0	0	0	0	0	54
04:00 PM	0	35	19	0	8	0	0	0	0	0	0	0	0	0	62
05:00 PM	0	30	6	0	6	0	0	3	0	0	0	0	0	0	45
06:00 PM	0	30	13	0	2	0	0	0	0	0	0	0	0	0	45
07:00 PM	0	22	1	0	1	0	0	0	0	0	0	0	0	0	24
08:00 PM	0	13	9	0	4	0	0	0	0	0	0	0	0	0	26
09:00 PM	0	37	16	2	0	0	0	0	0	0	0	0	0	0	55
10:00 PM	0	10	4	0	1	0	0	0	0	0	0	0	0	0	15
11:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
Day Total	9	422	194	2	50	0	0	16	0	0	0	0	0	5	698
Percent	1.3%	60.5%	27.8%	0.3%	7.2%	0%	0%	2.3%	0%	0%	0%	0%	0%	0.7%	
ADT 698															
AM Peak Volume	11:00 AM 2	6:00 AM 30	6:00 AM 17	12:00 AM 0	7:00 AM 4	12:00 AM 0	12:00 AM 0	3:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 1	6:00 AM 51
PM Peak Volume	12:00 PM 2	9:00 PM 37	4:00 PM 19	9:00 PM 2	4:00 PM 8	12:00 PM 0	12:00 PM 0	1:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 1	4:00 PM 62

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
02:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	4	6	0	0	0	0	0	0	0	0	0	0	0	10
05:00 AM	0	12	8	0	1	0	0	0	0	0	0	0	0	0	21
06:00 AM	0	8	8	0	1	0	0	3	1	0	0	0	0	0	21
07:00 AM	0	11	4	0	0	0	0	0	0	0	0	0	0	1	16
08:00 AM	0	8	10	0	3	0	0	0	0	0	0	0	0	2	23
09:00 AM	9	7	7	0	4	0	0	0	0	0	0	0	0	0	27
10:00 AM	0	18	12	0	6	0	0	0	0	0	0	0	0	0	36
11:00 AM	4	25	13	0	5	0	0	1	0	0	0	0	0	2	50
12:00 PM	11	23	15	0	2	0	0	2	0	0	0	0	0	0	53
01:00 PM	4	23	13	0	3	0	0	1	0	0	0	0	0	2	46
02:00 PM	3	14	13	0	2	0	0	5	0	0	0	0	0	0	37
03:00 PM	6	25	13	0	5	0	0	0	0	0	0	0	0	0	49
04:00 PM	7	21	17	0	2	0	0	4	0	0	0	0	0	1	52
05:00 PM	6	24	14	0	3	0	0	1	0	0	0	0	0	1	49
06:00 PM	0	15	15	0	5	0	0	4	0	0	0	0	0	2	41
07:00 PM	5	22	4	0	3	0	0	1	0	0	0	0	0	0	35
08:00 PM	0	8	5	0	3	0	0	1	0	0	0	0	0	0	17
09:00 PM	1	11	3	0	3	0	0	0	0	0	0	0	0	0	18
10:00 PM	0	11	1	0	2	0	0	1	0	0	0	0	0	0	15
11:00 PM	0	2	7	0	1	0	0	2	0	0	0	0	0	0	12
Day Total	56	296	190	0	55	0	0	27	1	0	0	0	0	11	636
Percent	8.8%	46.5%	29.9%	0%	8.6%	0%	0%	4.2%	0.2%	0%	0%	0%	0%	1.7%	
ADT 636															
AM Peak Volume	9:00 AM	11:00 AM	11:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	6:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	11:00 AM
	9	25	13	0	6	0	0	3	1	0	0	0	0	2	50
PM Peak Volume	12:00 PM	3:00 PM	4:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM
	11	25	17	0	5	0	0	5	0	0	0	0	0	2	53

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
02:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	1	5	0	0	0	0	0	0	0	0	0	0	0	6
05:00 AM	0	2	1	0	0	0	0	1	0	0	0	0	0	0	4
06:00 AM	0	5	9	0	1	0	0	0	0	0	0	0	0	0	15
07:00 AM	0	6	2	0	1	0	0	2	0	0	0	0	0	0	11
08:00 AM	0	8	6	0	0	0	0	2	0	0	0	0	0	0	16
09:00 AM	1	14	6	0	1	0	0	1	0	0	0	0	0	1	24
10:00 AM	2	14	7	0	5	0	0	2	0	0	0	0	0	1	31
11:00 AM	1	16	5	0	0	0	0	0	0	0	0	0	0	0	22
12:00 PM	8	27	16	0	8	0	0	1	0	0	0	0	0	1	61
01:00 PM	1	25	12	0	2	0	0	0	0	0	0	0	0	2	42
02:00 PM	4	30	14	1	0	0	0	1	0	0	0	0	0	0	50
03:00 PM	4	35	13	0	4	0	0	0	0	0	0	0	0	1	57
04:00 PM	0	26	11	0	6	0	0	2	0	0	0	0	0	1	46
05:00 PM	4	21	15	0	3	0	0	4	0	0	0	0	0	0	47
06:00 PM	0	24	12	0	3	0	0	2	0	0	0	0	0	0	41
07:00 PM	1	15	1	0	2	0	0	1	0	0	0	0	0	1	21
08:00 PM	0	9	4	0	0	0	0	1	0	0	0	0	0	0	14
09:00 PM	0	6	2	0	1	0	0	0	0	0	0	0	0	0	9
10:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	26	291	145	1	38	0	0	20	0	0	0	0	0	8	529
Percent	4.9%	55%	27.4%	0.2%	7.2%	0%	0%	3.8%	0%	0%	0%	0%	0%	1.5%	
ADT 529															
AM Peak Volume	10:00 AM 2	11:00 AM 16	6:00 AM 9	12:00 AM 0	10:00 AM 5	12:00 AM 0	12:00 AM 0	7:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	10:00 AM 31
PM Peak Volume	12:00 PM 8	3:00 PM 35	12:00 PM 16	2:00 PM 1	12:00 PM 8	12:00 PM 0	12:00 PM 0	5:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 2	12:00 PM 61

Comments:

LOCATION: (Off-Peak) John Coffee Memorial Bridge

QC JOB #: 15889228

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	115	2246	1173	7	309	3	0	113	1	1	0	0	0	49	4017
Percent	2.9%	55.9%	29.2%	0.2%	7.7%	0.1%	0%	2.8%	0%	0%	0%	0%	0%	1.2%	
ADT 573															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) John Coffee Memorial Bridge										QC JOB #: 15889228
SPECIFIC LOCATION:										DIRECTION: WB
CITY/STATE: Colbert, AL										DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	3	3	0	0	3	2	3	2	2	
01:00 AM	3	0	0	1	1	1	1	2	1	
02:00 AM	3	1	1	0	2	1	3	2	2	
03:00 AM	2	1	4	2	4	3	1	0	2	
04:00 AM	9	8	13	14	11	11	10	6	10	
05:00 AM	63	56	51	59	46	55	21	4	43	
06:00 AM	48	48	47	46	51	48	21	15	39	
07:00 AM	39	35	41	44	44	41	16	11	33	
08:00 AM	22	14	21	27	20	21	23	16	20	
09:00 AM	29	31	23	23	25	26	27	24	26	
10:00 AM	16	19	23	22	25	21	36	31	25	
11:00 AM	16	23	27	26	24	23	50	22	27	
12:00 PM	21	33	14	26	38	26	53	61	35	
01:00 PM	35	22	20	31	44	30	46	42	34	
02:00 PM	22	23	22	26	31	25	37	50	30	
03:00 PM	42	37	35	44	54	42	49	57	45	
04:00 PM	49	41	44	47	62	49	52	46	49	
05:00 PM	42	50	54	53	45	49	49	47	49	
06:00 PM	22	39	44	42	45	38	41	41	39	
07:00 PM	14	19	15	21	24	19	35	21	21	
08:00 PM	12	11	16	11	26	15	17	14	15	
09:00 PM	6	5	4	12	55	16	18	9	16	
10:00 PM	3	3	4	5	15	6	15	5	7	
11:00 PM	1	1	0	4	3	2	12	1	3	
Day Total	522	523	523	586	698	570	636	529	573	
% Weekday Average	91.6%	91.8%	91.8%	102.8%	122.5%					
% Week Average	91.1%	91.3%	91.3%	102.3%	121.8%	99.5%	111%	92.3%		
AM Peak Volume	5:00 AM 63	5:00 AM 56	5:00 AM 51	5:00 AM 59	6:00 AM 51	5:00 AM 55	11:00 AM 50	10:00 AM 31	5:00 AM 43	
PM Peak Volume	4:00 PM 49	5:00 PM 50	5:00 PM 54	5:00 PM 53	4:00 PM 62	4:00 PM 49	12:00 PM 53	12:00 PM 61	4:00 PM 49	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
05:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
10:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	2	0	0	0	0	1	0	0	0	0	0	0	0	0	3	1-10	1
01:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
02:00 PM	0	1	0	1	0	1	0	1	0	0	0	0	0	0	4	11-20	1
03:00 PM	0	0	1	0	2	1	0	0	0	0	0	0	0	0	4	31-40	3
04:00 PM	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	26-35	1
05:00 PM	0	1	1	1	0	0	1	0	0	0	0	0	0	0	4	16-25	2
06:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	3	4	8	5	4	1	2	0	0	0	0	0	0	29	26-35	13
Percent	6.9%	10.3%	13.8%	27.6%	17.2%	13.8%	3.4%	6.9%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	12:00 AM 0	7:00 AM 1	5:00 AM 1	9:00 AM 1	4:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 2		
PM Peak Volume	12:00 PM 2	2:00 PM 1	3:00 PM 1	1:00 PM 2	3:00 PM 2	12:00 PM 1	5:00 PM 1	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 4		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	3	0	0	0	0	0	0	0	0	3	31-40	3
05:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
06:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
09:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
10:00 AM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	26-35	1
11:00 AM	1	0	0	2	1	0	1	0	0	0	0	0	0	0	5	26-35	3
12:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
01:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
02:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
03:00 PM	0	0	0	0	2	1	1	0	0	0	0	0	0	0	4	31-40	3
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
05:00 PM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	16-25	1
06:00 PM	1	1	0	1	1	0	0	0	0	0	0	0	0	0	4	26-35	2
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	2	2	7	11	7	3	0	0	0	0	0	0	0	34	29-38	18
Percent	5.9%	5.9%	5.9%	20.6%	32.4%	20.6%	8.8%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 1	12:00 AM 0	12:00 AM 0	11:00 AM 2	5:00 AM 1	4:00 AM 3	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 5		
PM Peak Volume	6:00 PM 1	1:00 PM 1	4:00 PM 1	1:00 PM 1	3:00 PM 2	2:00 PM 1	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 4		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
07:00 AM	2	1	0	1	1	0	0	0	0	0	0	0	0	0	5	26-35	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
10:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
11:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
02:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
03:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
05:00 PM	0	1	0	3	0	0	0	0	0	0	0	0	0	0	4	21-30	3
06:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	2	3	7	3	3	0	0	0	0	0	0	0	0	21	21-30	10
Percent	14.3%	9.5%	14.3%	33.3%	14.3%	14.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	7:00 AM 2	7:00 AM 1	9:00 AM 1	7:00 AM 1	7:00 AM 1	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 5		
PM Peak Volume	12:00 PM 0	5:00 PM 1	1:00 PM 1	5:00 PM 3	2:00 PM 1	8:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	5:00 PM 4		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
07:00 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	1	0	2	1	0	0	0	0	0	0	0	4	36-45	3
10:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
11:00 AM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	26-35	1
12:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	1
01:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
02:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 PM	0	0	0	0	0	3	0	0	0	0	0	0	0	0	3	31-40	3
04:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
05:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
06:00 PM	1	0	0	2	0	0	0	0	0	0	0	0	0	0	3	21-30	2
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	2	1	8	3	9	1	0	0	0	0	0	0	0	27	31-40	12
Percent	11.1%	7.4%	3.7%	29.6%	11.1%	33.3%	3.7%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 1	7:00 AM 1	12:00 AM 0	3:00 AM 1	11:00 AM 1	9:00 AM 2	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 4		
PM Peak Volume	12:00 PM 1	7:00 PM 1	12:00 PM 1	5:00 PM 2	1:00 PM 1	3:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 3		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	36-45	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
09:00 AM	0	0	1	1	0	2	0	0	0	0	0	0	0	0	4	31-40	2
10:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
11:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
12:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
01:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
02:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
05:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	2	1	0	0	0	0	0	0	0	0	3	31-40	3
08:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
Day Total	0	0	3	8	7	8	1	0	0	0	0	0	0	0	27	26-35	15
Percent	0%	0%	11.1%	29.6%	25.9%	29.6%	3.7%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	9:00 AM	11:00 AM	6:00 AM	9:00 AM	3:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM		
	0	0	1	2	1	2	1	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	12:00 PM	1:00 PM	12:00 PM	7:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM		
	0	0	1	1	2	1	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
06:00 AM	0	1	0	1	2	1	0	0	0	0	0	0	0	0	5	31-40	3
07:00 AM	0	0	2	0	2	0	0	0	0	0	0	0	0	0	4	16-25	2
08:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
09:00 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	36-45	1
10:00 AM	0	0	0	1	2	0	1	0	0	0	0	0	0	0	4	26-35	3
11:00 AM	0	0	0	3	1	1	0	0	0	0	0	0	0	0	5	26-35	4
12:00 PM	0	0	0	2	5	1	0	0	0	0	0	0	0	0	8	26-35	7
01:00 PM	1	0	0	3	2	0	1	0	0	0	0	0	0	0	7	26-35	5
02:00 PM	0	1	1	1	2	0	0	0	0	0	0	0	0	0	5	26-35	3
03:00 PM	1	0	0	0	0	2	0	0	0	0	0	0	0	0	3	31-40	2
04:00 PM	1	0	0	1	0	0	0	1	0	0	0	0	0	0	3	21-30	1
05:00 PM	0	1	0	0	1	3	1	0	0	0	0	0	0	0	6	33-42	4
06:00 PM	1	0	1	2	1	2	1	0	0	0	0	0	0	0	8	26-35	3
07:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	3	4	16	21	12	5	1	0	0	0	0	0	0	66	26-35	37
Percent	6.1%	4.5%	6.1%	24.2%	31.8%	18.2%	7.6%	1.5%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	6:00 AM	7:00 AM	11:00 AM	6:00 AM	2:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM		
	0	1	2	3	2	1	1	0	0	0	0	0	0	0	5		
PM Peak Volume	1:00 PM	2:00 PM	2:00 PM	1:00 PM	12:00 PM	5:00 PM	1:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	1	1	1	3	5	3	1	1	0	0	0	0	0	0	8		
Comments:																	

Report generated on 9/2/2022 10:19 AM

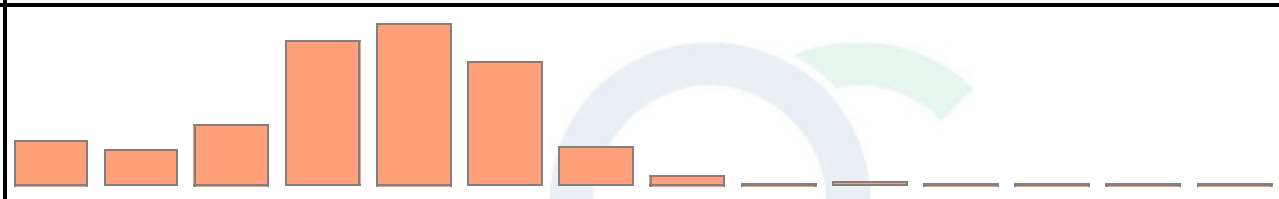
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area															QC JOB #: 15889229		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
06:00 AM	0	0	0	1	2	0	0	0	0	0	0	0	0	0	3	26-35	3
07:00 AM	0	0	0	0	2	1	2	0	0	0	0	0	0	0	5	31-40	3
08:00 AM	0	0	0	1	3	0	0	0	0	0	0	0	0	0	4	26-35	4
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 AM	0	1	0	1	0	1	0	0	0	0	0	0	0	0	3	11-20	1
11:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
12:00 PM	3	0	0	0	1	0	0	0	0	0	0	0	0	0	4	1-10	2
01:00 PM	0	0	1	1	1	3	1	0	0	0	0	0	0	0	7	33-42	4
02:00 PM	1	0	1	1	1	2	1	0	0	1	0	0	0	0	8	36-45	3
03:00 PM	0	1	0	0	2	2	0	0	0	0	0	0	0	0	5	31-40	4
04:00 PM	1	0	3	1	1	1	1	0	0	0	0	0	0	0	8	21-30	4
05:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
06:00 PM	0	1	1	0	3	0	0	0	0	0	0	0	0	0	5	26-35	3
07:00 PM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	3	26-35	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	5	3	9	8	19	10	5	1	0	1	0	0	0	0	61	31-40	29
Percent	8.2%	4.9%	14.8%	13.1%	31.1%	16.4%	8.2%	1.6%	0%	1.6%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	10:00 AM 1	11:00 AM 1	6:00 AM 1	8:00 AM 3	7:00 AM 1	7:00 AM 2	5:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 5		
PM Peak Volume	12:00 PM 3	3:00 PM 1	4:00 PM 3	1:00 PM 1	6:00 PM 3	1:00 PM 3	1:00 PM 1	12:00 PM 0	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 8		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Comfort Station Parking Area															QC JOB #: 15889229		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	19	15	26	62	69	53	16	4	0	1	0	0	0	0	265	26-35	131
Percent	7.2%	5.7%	9.8%	23.4%	26%	20%	6%	1.5%	0%	0.4%	0%	0%	0%	0%			
Cumulative Percent	7.2%	12.8%	22.6%	46%	72.1%	92.1%	98.1%	99.6%	99.6%	100%	100%	100%	100%	100%			
ADT 37															85th Percentile: 38 MPH Mean Speed(Average): 30 MPH Median: 30 MPH Mode: 33 MPH		
Comments:																	

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	2	3
01:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
03:00 PM	0	2	0	0	2	0	0	0	0	0	0	0	0	0	4
04:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
05:00 PM	0	3	0	0	1	0	0	0	0	0	0	0	0	0	4
06:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	14	7	0	3	0	0	2	0	0	0	0	0	2	29
Percent	3.4%	48.3%	24.1%	0%	10.3%	0%	0%	6.9%	0%	0%	0%	0%	0%	6.9%	
ADT 29															
AM Peak Volume	12:00 AM	10:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	5:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	1	2	0	0	0	0	1	0	0	0	0	0	0	2
PM Peak Volume	6:00 PM	2:00 PM	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM
	1	4	1	0	2	0	0	1	0	0	0	0	0	2	4

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	1	0	0	0	0	2	0	0	0	0	0	0	3
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
09:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
10:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
11:00 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	1	5
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	1	1	0	1	0	0	1	0	0	0	0	0	0	4
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
06:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	1	4
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	12	11	0	1	0	0	8	0	0	0	0	0	2	34
Percent	0%	35.3%	32.4%	0%	2.9%	0%	0%	23.5%	0%	0%	0%	0%	0%	5.9%	
ADT 34															
AM Peak Volume	12:00 AM	11:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM
	0	3	1	0	0	0	0	2	0	0	0	0	0	1	5
PM Peak Volume	12:00 PM	6:00 PM	2:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM	3:00 PM
	0	3	2	0	1	0	0	1	0	0	0	0	0	1	4

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
07:00 AM	0	1	2	0	0	0	0	1	0	0	0	0	0	1	5
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
02:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	10	5	0	1	0	0	4	0	0	0	0	0	1	21
Percent	0%	47.6%	23.8%	0%	4.8%	0%	0%	19%	0%	0%	0%	0%	0%	4.8%	
ADT 21															
AM Peak Volume	12:00 AM	7:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM	7:00 AM
	0	1	2	0	0	0	0	1	0	0	0	0	0	1	5
PM Peak Volume	12:00 PM	5:00 PM	2:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM
	0	4	1	0	1	0	0	1	0	0	0	0	0	0	4

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	2	0	0	0	0	0	2	0	0	0	0	0	0	4
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	1	2
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	13	5	0	3	0	0	4	0	0	0	0	0	1	27
Percent	3.7%	48.1%	18.5%	0%	11.1%	0%	0%	14.8%	0%	0%	0%	0%	0%	3.7%	
ADT 27															
AM Peak Volume	12:00 AM	9:00 AM	7:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	2	1	0	1	0	0	2	0	0	0	0	0	0	4
PM Peak Volume	12:00 PM	3:00 PM	4:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	6:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM
	1	2	1	0	1	0	0	1	0	0	0	0	0	1	3

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2
09:00 AM	0	1	2	0	1	0	0	0	0	0	0	0	0	0	4
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	0	10	9	0	1	0	0	7	0	0	0	0	0	0	27
Percent	0%	37%	33.3%	0%	3.7%	0%	0%	25.9%	0%	0%	0%	0%	0%	0%	
ADT 27															
AM Peak Volume	12:00 AM	11:00 AM	9:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	2	2	0	1	0	0	2	0	0	0	0	0	0	4
PM Peak Volume	12:00 PM	7:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM
	0	2	2	0	0	0	0	2	0	0	0	0	0	0	3

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	5	0	0	0	0	0	0	5
07:00 AM	0	1	2	0	0	0	0	1	0	0	0	0	0	0	4
08:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	1	2	0	0	0	0	1	0	0	0	0	0	0	4
11:00 AM	0	1	2	0	1	0	0	1	0	0	0	0	0	0	5
12:00 PM	4	3	1	0	0	0	0	0	0	0	0	0	0	0	8
01:00 PM	0	0	3	0	0	0	0	3	0	0	0	0	0	1	7
02:00 PM	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5
03:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	2	1	0	0	0	0	3	0	0	0	0	0	0	6
06:00 PM	0	3	2	0	0	0	0	3	0	0	0	0	0	0	8
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	7	21	16	0	2	0	0	19	0	0	0	0	0	1	66
Percent	10.6%	31.8%	24.2%	0%	3%	0%	0%	28.8%	0%	0%	0%	0%	0%	1.5%	
ADT 66															
AM Peak Volume	12:00 AM	2:00 AM	7:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM
	0	1	2	0	1	0	0	5	0	0	0	0	0	0	5
PM Peak Volume	12:00 PM	12:00 PM	1:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM
	4	3	3	0	1	0	0	3	0	0	0	0	0	1	8

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	3	0	0	0	0	0	0	3
07:00 AM	0	1	1	0	0	0	0	3	0	0	0	0	0	0	5
08:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	1	0	1	0	0	1	0	0	0	0	0	0	3
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:00 PM	0	2	0	0	0	0	0	1	0	0	0	0	0	1	4
01:00 PM	0	5	2	0	0	0	0	0	0	0	0	0	0	0	7
02:00 PM	0	6	0	0	2	0	0	0	0	0	0	0	0	0	8
03:00 PM	0	3	1	0	1	0	0	0	0	0	0	0	0	0	5
04:00 PM	0	3	1	0	2	0	0	2	0	0	0	0	0	0	8
05:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
06:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
07:00 PM	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	32	10	0	6	0	0	11	0	0	0	0	0	1	61
Percent	1.6%	52.5%	16.4%	0%	9.8%	0%	0%	18%	0%	0%	0%	0%	0%	1.6%	
ADT 61															
AM Peak Volume	12:00 AM	8:00 AM	7:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM
	0	4	1	0	1	0	0	3	0	0	0	0	0	0	5
PM Peak Volume	7:00 PM	2:00 PM	1:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM
	1	6	2	0	2	0	0	2	0	0	0	0	0	1	8

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area


QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	10	112	63	0	17	0	0	55	0	0	0	0	0	8	265
Percent	3.8%	42.3%	23.8%	0%	6.4%	0%	0%	20.8%	0%	0%	0%	0%	0%	3%	
ADT 37															
Comments:															

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Comfort Station Parking Area										QC JOB #: 15889229
SPECIFIC LOCATION:										DIRECTION: EB
CITY/STATE: Colbert, AL										DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0	0	0	0	0	0	0	0	
01:00 AM	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	1	0	0	
03:00 AM	0	0	0	1	2	1	1	0	1	<div></div>
04:00 AM	1	3	0	0	0	1	0	0	1	<div></div>
05:00 AM	1	1	0	0	0	0	1	1	1	<div></div>
06:00 AM	0	1	1	2	2	1	5	3	2	<div></div>
07:00 AM	1	1	5	2	0	2	4	5	3	<div></div>
08:00 AM	0	2	0	0	2	1	2	4	1	<div></div>
09:00 AM	2	2	1	4	4	3	1	1	2	<div></div>
10:00 AM	1	2	1	1	1	1	4	3	2	<div></div>
11:00 AM	0	5	1	2	2	2	5	1	2	<div></div>
12:00 PM	3	1	0	2	1	1	8	4	3	<div></div>
01:00 PM	2	2	1	1	2	2	7	7	3	<div></div>
02:00 PM	4	2	2	1	1	2	5	8	3	<div></div>
03:00 PM	4	4	1	3	1	3	3	5	3	<div></div>
04:00 PM	2	1	1	1	2	1	3	8	3	<div></div>
05:00 PM	4	2	4	2	2	3	6	3	3	<div></div>
06:00 PM	1	4	1	3	0	2	8	5	3	<div></div>
07:00 PM	2	0	0	1	3	1	1	3	1	<div></div>
08:00 PM	1	0	2	1	1	1	1	0	1	<div></div>
09:00 PM	0	1	0	0	0	0	0	0	0	
10:00 PM	0	0	0	0	0	0	0	0	0	
11:00 PM	0	0	0	0	1	0	0	0	0	
Day Total	29	34	21	27	27	28	66	61	38	
% Weekday Average	103.6%	121.4%	75%	96.4%	96.4%					
% Week Average	76.3%	89.5%	55.3%	71.1%	71.1%	73.7%	173.7%	160.5%		
AM Peak Volume	9:00 AM 2	11:00 AM 5	7:00 AM 5	9:00 AM 4	9:00 AM 4	9:00 AM 3	6:00 AM 5	7:00 AM 5	7:00 AM 3	
PM Peak Volume	2:00 PM 4	3:00 PM 4	5:00 PM 4	3:00 PM 3	7:00 PM 3	3:00 PM 3	12:00 PM 8	2:00 PM 8	12:00 PM 3	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
05:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	26-35	3
10:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
12:00 PM	4	0	1	0	0	2	0	0	0	0	0	0	0	0	7	1-10	3
01:00 PM	0	0	0	2	1	0	1	0	0	0	0	0	0	0	4	26-35	3
02:00 PM	0	1	0	3	0	1	0	1	0	0	0	0	0	0	6	21-30	3
03:00 PM	0	0	1	1	3	1	0	0	0	0	0	0	0	0	6	28-37	4
04:00 PM	0	0	0	0	1	1	0	1	0	0	0	0	0	0	3	31-40	2
05:00 PM	0	3	2	3	0	1	1	0	0	0	0	0	0	0	10	16-25	5
06:00 PM	0	0	0	2	0	0	0	1	0	0	0	0	0	0	3	21-30	2
07:00 PM	0	2	2	2	1	0	0	0	0	0	0	0	0	0	7	16-25	4
08:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	6	7	19	11	7	2	3	0	0	0	0	0	0	59	26-35	30
Percent	6.8%	10.2%	11.9%	32.2%	18.6%	11.9%	3.4%	5.1%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	7:00 AM	9:00 AM	4:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM		
	0	0	1	2	1	1	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	5:00 PM	5:00 PM	2:00 PM	3:00 PM	12:00 PM	1:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM		
	4	3	2	3	3	2	1	1	0	0	0	0	0	0	10		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	3	0	0	0	0	0	0	0	0	3	31-40	3
05:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
06:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	1	0	2	1	0	0	0	0	0	0	0	0	4	31-40	3
09:00 AM	0	0	0	1	2	0	0	0	0	0	0	0	0	0	3	26-35	3
10:00 AM	1	1	0	0	1	0	1	0	0	0	0	0	0	0	4	11-20	1
11:00 AM	1	0	1	3	2	0	1	0	0	0	0	0	0	0	8	26-35	5
12:00 PM	0	0	0	1	4	0	0	0	0	0	0	0	0	0	5	26-35	5
01:00 PM	1	1	0	4	2	0	0	0	1	0	0	0	0	0	9	26-35	6
02:00 PM	0	0	0	0	1	2	0	0	0	0	0	0	0	0	3	31-40	3
03:00 PM	0	0	0	0	2	2	1	0	0	0	0	0	0	0	5	31-40	4
04:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
05:00 PM	0	1	1	0	0	3	0	0	0	0	0	0	0	0	5	31-40	3
06:00 PM	3	2	0	1	1	2	0	0	0	0	0	0	0	0	9	11-20	3
07:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	6	5	4	15	20	14	3	0	1	0	0	0	0	0	68	26-35	35
Percent	8.8%	7.4%	5.9%	22.1%	29.4%	20.6%	4.4%	0%	1.5%	0%	0%	0%	0%	0%			
AM Peak Volume	10:00 AM 1	10:00 AM 1	8:00 AM 1	11:00 AM 3	8:00 AM 2	4:00 AM 3	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 8		
PM Peak Volume	6:00 PM 3	6:00 PM 2	4:00 PM 1	1:00 PM 4	12:00 PM 4	5:00 PM 3	3:00 PM 1	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 9		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
07:00 AM	4	2	0	1	1	0	0	0	0	0	0	0	0	0	8	11-20	3
08:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2	11-20	1
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
10:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
11:00 AM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1-10	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
02:00 PM	0	0	1	2	2	2	0	0	0	0	0	0	0	0	7	26-35	4
03:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 PM	1	0	1	1	1	0	0	0	0	0	0	0	0	0	4	21-30	2
05:00 PM	1	1	0	4	2	0	0	0	0	0	0	0	0	0	8	26-35	6
06:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	1	0	1	3	0	0	0	0	0	0	0	0	5	31-40	4
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	8	4	6	9	9	7	0	0	0	0	0	0	0	0	43	26-35	18
Percent	18.6%	9.3%	14%	20.9%	20.9%	16.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	7:00 AM 4	7:00 AM 2	9:00 AM 1	7:00 AM 1	7:00 AM 1	6:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 8		
PM Peak Volume	4:00 PM 1	5:00 PM 1	1:00 PM 1	5:00 PM 4	2:00 PM 2	8:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	5:00 PM 8		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	2	0	1	0	0	0	0	0	0	0	0	3	21-30	2
07:00 AM	0	2	0	1	0	0	0	0	0	0	0	0	0	0	3	11-20	2
08:00 AM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	3	26-35	2
09:00 AM	1	0	0	1	1	2	2	0	0	0	0	0	0	0	7	36-45	4
10:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
11:00 AM	1	1	0	0	1	0	0	0	0	0	0	0	0	0	3	11-20	1
12:00 PM	1	0	1	1	0	0	0	0	0	0	0	0	0	0	3	21-30	2
01:00 PM	2	0	0	0	2	0	0	0	0	0	0	0	0	0	4	26-35	2
02:00 PM	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2	31-40	2
03:00 PM	0	0	0	0	0	4	0	0	0	0	0	0	0	0	4	31-40	4
04:00 PM	0	0	0	0	1	2	0	0	0	0	0	0	0	0	3	31-40	3
05:00 PM	0	0	0	2	1	1	0	0	0	0	0	0	0	0	4	26-35	3
06:00 PM	1	0	0	4	1	0	0	0	0	0	0	0	0	0	6	26-35	5
07:00 PM	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	11-20	2
08:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
Day Total	8	4	2	13	13	13	2	0	0	0	0	0	0	0	55	26-35	26
Percent	14.5%	7.3%	3.6%	23.6%	23.6%	23.6%	3.6%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	9:00 AM 1	7:00 AM 2	8:00 AM 1	6:00 AM 2	8:00 AM 2	9:00 AM 2	9:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 7		
PM Peak Volume	1:00 PM 2	7:00 PM 1	12:00 PM 1	6:00 PM 4	1:00 PM 2	3:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 6		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	36-45	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
07:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 AM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
09:00 AM	0	0	1	1	0	2	1	0	0	0	0	0	0	0	5	36-45	3
10:00 AM	1	0	1	1	2	1	0	0	0	0	0	0	0	0	6	31-40	3
11:00 AM	0	0	0	2	2	1	0	0	0	0	0	0	0	0	5	26-35	4
12:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
01:00 PM	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	21-30	3
02:00 PM	0	0	1	0	3	1	0	0	0	0	0	0	0	0	5	31-40	4
03:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 PM	0	1	2	0	1	0	0	0	0	0	0	0	0	0	4	16-25	3
05:00 PM	0	0	0	2	1	1	0	0	0	0	0	0	0	0	4	26-35	3
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	1	0	4	1	0	0	0	0	0	0	0	0	6	31-40	5
08:00 PM	0	0	0	1	2	0	0	0	0	0	0	0	0	0	3	26-35	3
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
Day Total	1	1	8	13	17	11	2	0	0	0	0	0	0	0	53	26-35	30
Percent	1.9%	1.9%	15.1%	24.5%	32.1%	20.8%	3.8%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	10:00 AM 1	12:00 AM 0	9:00 AM 1	11:00 AM 2	10:00 AM 2	9:00 AM 2	3:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 6		
PM Peak Volume	12:00 PM 0	4:00 PM 1	4:00 PM 2	1:00 PM 2	7:00 PM 4	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	7:00 PM 6		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2	31-40	2
03:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
06:00 AM	0	1	0	1	2	1	0	0	0	0	0	0	0	0	5	31-40	3
07:00 AM	0	0	2	1	2	0	0	0	0	0	0	0	0	0	5	21-30	3
08:00 AM	0	0	0	0	2	1	1	0	0	0	0	0	0	0	4	31-40	3
09:00 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	36-45	1
10:00 AM	0	0	0	2	3	1	1	0	0	0	0	0	0	0	7	26-35	5
11:00 AM	0	0	0	4	3	3	0	0	0	0	0	0	0	0	10	26-35	7
12:00 PM	0	0	0	7	6	2	0	0	0	0	0	0	0	0	15	26-35	13
01:00 PM	1	0	1	4	4	1	1	1	0	0	0	0	0	0	13	26-35	8
02:00 PM	3	1	4	1	8	0	0	0	0	0	0	0	0	0	17	26-35	9
03:00 PM	1	0	1	0	1	3	0	0	0	0	0	0	0	0	6	31-40	4
04:00 PM	1	1	1	1	0	0	0	2	0	0	0	0	0	0	6	41-50	2
05:00 PM	0	1	0	2	1	3	2	0	0	0	0	0	0	0	9	36-45	5
06:00 PM	2	0	1	3	5	4	2	0	0	0	0	0	0	0	17	31-40	9
07:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
08:00 PM	0	0	0	3	1	1	0	0	0	0	0	0	0	0	5	26-35	4
09:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
Day Total	8	4	11	32	41	24	8	3	0	0	0	0	0	0	131	26-35	73
Percent	6.1%	3.1%	8.4%	24.4%	31.3%	18.3%	6.1%	2.3%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	6:00 AM	7:00 AM	11:00 AM	10:00 AM	11:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	2	4	3	3	1	0	0	0	0	0	0	0	10		
PM Peak Volume	2:00 PM	2:00 PM	2:00 PM	12:00 PM	2:00 PM	6:00 PM	5:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	3	1	4	7	8	4	2	2	0	0	0	0	0	0	17		
Comments:																	

Report generated on 9/2/2022 10:19 AM


SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
06:00 AM	0	0	0	1	3	0	0	0	0	0	0	0	0	0	4	26-35	4
07:00 AM	0	0	1	0	2	2	2	0	0	0	0	0	0	0	7	31-40	4
08:00 AM	0	0	0	1	3	1	0	0	0	0	0	0	0	0	5	28-37	4
09:00 AM	0	0	0	1	1	1	1	0	0	0	0	0	0	0	4	26-35	2
10:00 AM	0	1	0	3	1	1	0	0	0	0	0	0	0	0	6	26-35	4
11:00 AM	1	0	1	2	0	0	0	0	0	0	0	0	0	0	4	21-30	3
12:00 PM	3	0	1	0	2	0	0	0	0	0	0	0	0	0	6	26-35	2
01:00 PM	1	0	2	2	1	5	3	0	0	1	0	0	0	0	15	36-45	8
02:00 PM	2	0	1	2	3	3	2	0	0	1	0	0	0	0	14	31-40	6
03:00 PM	3	1	1	0	5	3	1	0	0	0	0	0	0	0	14	31-40	8
04:00 PM	2	0	5	3	3	1	1	0	0	0	0	0	0	0	15	21-30	8
05:00 PM	0	0	3	2	3	0	0	0	0	0	0	0	0	0	8	21-30	5
06:00 PM	0	2	3	2	5	0	0	0	0	0	0	0	0	0	12	26-35	7
07:00 PM	1	1	2	3	3	0	0	0	0	0	0	0	0	0	10	26-35	6
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	13	5	20	22	35	18	10	1	0	2	0	0	0	0	126	26-35	57
Percent	10.3%	4%	15.9%	17.5%	27.8%	14.3%	7.9%	0.8%	0%	1.6%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 1	10:00 AM 1	7:00 AM 1	10:00 AM 3	6:00 AM 3	7:00 AM 2	7:00 AM 2	5:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 7		
PM Peak Volume	12:00 PM 3	6:00 PM 2	4:00 PM 5	4:00 PM 3	3:00 PM 5	1:00 PM 5	1:00 PM 3	12:00 PM 0	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 15		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Comfort Station Parking Area															QC JOB #: 15889229		
SPECIFIC LOCATION:															DIRECTION: EB, WB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	48	29	58	123	146	94	27	7	1	2	0	0	0	0	535	26-35	269
Percent	9%	5.4%	10.8%	23%	27.3%	17.6%	5%	1.3%	0.2%	0.4%	0%	0%	0%	0%			
Cumulative Percent	9%	14.4%	25.2%	48.2%	75.5%	93.1%	98.1%	99.4%	99.6%	100%	100%	100%	100%	100%			
ADT 76															85th Percentile: 37 MPH Mean Speed(Average): 30 MPH Median: 30 MPH Mode: 33 MPH		
Comments:																	

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: EB, WB

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	2	0	1	0	0	0	0	0	0	0	0	0	3
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
12:00 PM	0	0	2	0	0	0	0	1	0	0	0	0	0	4	7
01:00 PM	0	3	0	0	0	0	0	1	0	0	0	0	0	0	4
02:00 PM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
03:00 PM	0	4	0	0	2	0	0	0	0	0	0	0	0	0	6
04:00 PM	0	0	2	0	0	0	0	1	0	0	0	0	0	0	3
05:00 PM	0	7	0	0	3	0	0	0	0	0	0	0	0	0	10
06:00 PM	1	0	1	0	1	0	0	0	0	0	0	0	0	0	3
07:00 PM	1	2	2	0	0	0	0	2	0	0	0	0	0	0	7
08:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	25	13	0	8	0	0	7	0	0	0	0	0	4	59
Percent	3.4%	42.4%	22%	0%	13.6%	0%	0%	11.9%	0%	0%	0%	0%	0%	6.8%	
ADT 59															
AM Peak Volume	12:00 AM	10:00 AM	4:00 AM	12:00 AM	7:00 AM	12:00 AM	12:00 AM	5:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	1	2	0	1	0	0	1	0	0	0	0	0	0	3
PM Peak Volume	6:00 PM	5:00 PM	12:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM
	1	7	2	0	3	0	0	2	0	0	0	0	0	4	10

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	1	0	0	0	0	2	0	0	0	0	0	0	3
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	2	0	1	0	0	1	0	0	0	0	0	0	4
09:00 AM	0	0	1	0	0	0	0	2	0	0	0	0	0	0	3
10:00 AM	1	1	1	0	0	0	0	1	0	0	0	0	0	0	4
11:00 AM	0	5	1	0	1	0	0	0	0	0	0	0	0	1	8
12:00 PM	0	3	1	0	0	0	0	1	0	0	0	0	0	0	5
01:00 PM	0	2	4	0	0	0	0	3	0	0	0	0	0	0	9
02:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	1	2	0	1	0	0	1	0	0	0	0	0	0	5
04:00 PM	0	0	2	0	1	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	2	1	0	0	0	0	2	0	0	0	0	0	0	5
06:00 PM	0	6	0	0	0	0	0	1	0	0	0	0	0	2	9
07:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	23	21	0	4	0	0	16	0	0	0	0	0	3	68
Percent	1.5%	33.8%	30.9%	0%	5.9%	0%	0%	23.5%	0%	0%	0%	0%	0%	4.4%	
ADT 68															
AM Peak Volume	10:00 AM	11:00 AM	8:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM
	1	5	2	0	1	0	0	2	0	0	0	0	0	1	8
PM Peak Volume	12:00 PM	6:00 PM	1:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM	1:00 PM
	0	6	4	0	1	0	0	3	0	0	0	0	0	2	9

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: EB, WB

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
07:00 AM	0	1	4	0	0	0	0	1	0	0	0	0	0	2	8
08:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
02:00 PM	0	2	3	0	0	0	0	2	0	0	0	0	0	0	7
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	1	4
05:00 PM	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8
06:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	2	0	0	2	0	0	1	0	0	0	0	0	0	5
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	19	11	0	2	0	0	8	0	0	0	0	0	3	43
Percent	0%	44.2%	25.6%	0%	4.7%	0%	0%	18.6%	0%	0%	0%	0%	0%	7%	
ADT 43															
AM Peak Volume	12:00 AM	10:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM	7:00 AM
	0	2	4	0	0	0	0	1	0	0	0	0	0	2	8
PM Peak Volume	12:00 PM	5:00 PM	2:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	5:00 PM
	0	8	3	0	2	0	0	2	0	0	0	0	0	1	8

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: EB, WB

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	2	0	0	0	0	0	0	0	0	0	3
07:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
08:00 AM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
09:00 AM	0	4	0	0	0	0	0	2	0	0	0	0	0	1	7
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	1	3
01:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	1	4
02:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
03:00 PM	0	3	0	0	1	0	0	0	0	0	0	0	0	0	4
04:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	1	2	0	1	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	3	1	0	0	0	0	2	0	0	0	0	0	0	6
07:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
08:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
Day Total	2	26	9	0	7	0	0	8	0	0	0	0	0	3	55
Percent	3.6%	47.3%	16.4%	0%	12.7%	0%	0%	14.5%	0%	0%	0%	0%	0%	5.5%	
ADT 55															
AM Peak Volume	12:00 AM	9:00 AM	7:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM	9:00 AM
	0	4	2	0	2	0	0	2	0	0	0	0	0	1	7
PM Peak Volume	12:00 PM	1:00 PM	4:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	6:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM
	2	3	2	0	2	0	0	2	0	0	0	0	0	1	6

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: EB, WB

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2
09:00 AM	0	1	2	0	2	0	0	0	0	0	0	0	0	0	5
10:00 AM	0	0	4	0	0	0	0	1	0	0	0	0	0	1	6
11:00 AM	0	3	1	0	0	0	0	1	0	0	0	0	0	0	5
12:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
02:00 PM	0	2	1	0	0	0	0	2	0	0	0	0	0	0	5
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	3	0	0	0	0	1	0	0	0	0	0	0	4
05:00 PM	0	1	1	0	0	0	0	2	0	0	0	0	0	0	4
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	4	1	0	0	0	0	1	0	0	0	0	0	0	6
08:00 PM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
Day Total	0	19	16	0	2	0	0	15	0	0	0	0	0	1	53
Percent	0%	35.8%	30.2%	0%	3.8%	0%	0%	28.3%	0%	0%	0%	0%	0%	1.9%	
ADT 53															
AM Peak Volume	12:00 AM	11:00 AM	10:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	10:00 AM
	0	3	4	0	2	0	0	2	0	0	0	0	0	1	6
PM Peak Volume	12:00 PM	7:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM
	0	4	3	0	0	0	0	2	0	0	0	0	0	0	6

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: EB, WB

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	5	0	0	0	0	0	0	5
07:00 AM	0	1	2	0	0	0	0	2	0	0	0	0	0	0	5
08:00 AM	0	1	1	0	1	0	0	1	0	0	0	0	0	0	4
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	1	3	0	0	0	0	3	0	0	0	0	0	0	7
11:00 AM	0	2	2	0	2	0	0	4	0	0	0	0	0	0	10
12:00 PM	4	5	2	0	0	0	0	4	0	0	0	0	0	0	15
01:00 PM	0	0	7	0	0	0	0	5	0	0	0	0	0	1	13
02:00 PM	8	4	0	0	1	0	0	2	0	0	0	0	0	2	17
03:00 PM	1	4	1	0	0	0	0	0	0	0	0	0	0	0	6
04:00 PM	0	5	0	0	1	0	0	0	0	0	0	0	0	0	6
05:00 PM	0	4	2	0	0	0	0	3	0	0	0	0	0	0	9
06:00 PM	0	5	4	0	1	0	0	7	0	0	0	0	0	0	17
07:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	2	2	0	0	0	0	1	0	0	0	0	0	0	5
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
11:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
Day Total	13	41	28	0	6	0	0	40	0	0	0	0	0	3	131
Percent	9.9%	31.3%	21.4%	0%	4.6%	0%	0%	30.5%	0%	0%	0%	0%	0%	2.3%	
ADT 131															
AM Peak Volume	12:00 AM 0	2:00 AM 2	10:00 AM 3	12:00 AM 0	11:00 AM 2	12:00 AM 0	12:00 AM 0	6:00 AM 5	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 10
PM Peak Volume	2:00 PM 8	12:00 PM 5	1:00 PM 7	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	6:00 PM 7	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 2	2:00 PM 17

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: EB, WB

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	4	0	0	0	0	0	0	4
07:00 AM	0	2	2	0	0	0	0	3	0	0	0	0	0	0	7
08:00 AM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
09:00 AM	0	3	0	0	0	0	0	1	0	0	0	0	0	0	4
10:00 AM	0	1	2	0	1	0	0	2	0	0	0	0	0	0	6
11:00 AM	0	3	0	0	0	0	0	1	0	0	0	0	0	0	4
12:00 PM	0	3	0	0	0	0	0	2	0	0	0	0	0	1	6
01:00 PM	0	10	3	0	1	0	0	1	0	0	0	0	0	0	15
02:00 PM	0	9	0	0	3	0	0	1	0	0	0	0	0	1	14
03:00 PM	0	7	2	0	3	0	0	1	0	0	0	0	0	1	14
04:00 PM	0	5	2	0	3	0	0	4	0	0	0	0	0	1	15
05:00 PM	0	2	3	0	2	0	0	1	0	0	0	0	0	0	8
06:00 PM	0	8	4	0	0	0	0	0	0	0	0	0	0	0	12
07:00 PM	1	5	1	0	1	0	0	1	0	0	0	0	0	1	10
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	63	19	0	14	0	0	24	0	0	0	0	0	5	126
Percent	0.8%	50%	15.1%	0%	11.1%	0%	0%	19%	0%	0%	0%	0%	0%	4%	
ADT 126															
AM Peak Volume	12:00 AM	8:00 AM	7:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM
	0	5	2	0	1	0	0	4	0	0	0	0	0	0	7
PM Peak Volume	7:00 PM	1:00 PM	6:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM
	1	10	4	0	3	0	0	4	0	0	0	0	0	1	15

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

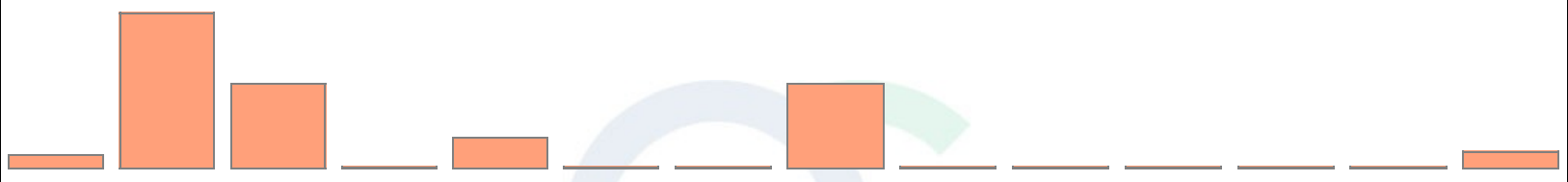
QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	19	216	117	0	43	0	0	118	0	0	0	0	0	22	535
Percent	3.6%	40.4%	21.9%	0%	8%	0%	0%	22.1%	0%	0%	0%	0%	0%	4.1%	
ADT 76															
Comments:															

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL										QC JOB #: 15889229 DIRECTION: EB, WB DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0	0	0	0	0	1	1	0	
01:00 AM	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	2	0	0	
03:00 AM	0	0	0	1	2	1	2	0	1	<div></div>
04:00 AM	2	3	0	0	0	1	0	0	1	<div></div>
05:00 AM	1	1	0	0	0	0	1	1	1	<div></div>
06:00 AM	0	1	1	3	2	1	5	4	2	<div></div>
07:00 AM	2	1	8	3	1	3	5	7	4	<div></div>
08:00 AM	0	4	2	3	2	2	4	5	3	<div></div>
09:00 AM	3	3	1	7	5	4	1	4	3	<div></div>
10:00 AM	1	4	2	2	6	3	7	6	4	<div></div>
11:00 AM	2	8	2	3	5	4	10	4	5	<div></div>
12:00 PM	7	5	0	3	2	3	15	6	5	<div></div>
01:00 PM	4	9	1	4	3	4	13	15	7	<div></div>
02:00 PM	6	3	7	2	5	5	17	14	8	<div></div>
03:00 PM	6	5	1	4	1	3	6	14	5	<div></div>
04:00 PM	3	3	4	3	4	3	6	15	5	<div></div>
05:00 PM	10	5	8	4	4	6	9	8	7	<div></div>
06:00 PM	3	9	1	6	0	4	17	12	7	<div></div>
07:00 PM	7	2	0	3	6	4	2	10	4	<div></div>
08:00 PM	2	0	5	2	3	2	5	0	2	<div></div>
09:00 PM	0	2	0	0	0	0	1	0	0	
10:00 PM	0	0	0	0	0	0	1	0	0	
11:00 PM	0	0	0	2	2	1	1	0	1	<div></div>
Day Total	59	68	43	55	53	54	131	126	75	
% Weekday Average	109.3%	125.9%	79.6%	101.9%	98.1%					
% Week Average	78.7%	90.7%	57.3%	73.3%	70.7%	72%	174.7%	168%		
AM Peak Volume	9:00 AM 3	11:00 AM 8	7:00 AM 8	9:00 AM 7	10:00 AM 6	9:00 AM 4	11:00 AM 10	7:00 AM 7	11:00 AM 5	
PM Peak Volume	5:00 PM 10	1:00 PM 9	5:00 PM 8	6:00 PM 6	7:00 PM 6	5:00 PM 6	2:00 PM 17	1:00 PM 15	2:00 PM 8	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 22 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
12:00 PM	2	0	1	0	0	1	0	0	0	0	0	0	0	0	4	1-10	1
01:00 PM	0	0	0	0	1	0	1	0	0	0	0	0	0	0	2	26-35	1
02:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
03:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
04:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
05:00 PM	0	2	1	2	0	1	0	0	0	0	0	0	0	0	6	16-25	3
06:00 PM	0	0	0	1	0	0	0	1	0	0	0	0	0	0	2	21-30	1
07:00 PM	0	1	1	2	1	0	0	0	0	0	0	0	0	0	5	26-35	3
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	3	3	11	6	3	1	1	0	0	0	0	0	0	30	26-35	17
Percent	6.7%	10%	10%	36.7%	20%	10%	3.3%	3.3%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	7:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	5:00 PM	12:00 PM	2:00 PM	1:00 PM	12:00 PM	1:00 PM	6:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM		
	2	2	1	2	1	1	1	1	0	0	0	0	0	0	6		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 23 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
10:00 AM	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	11-20	1
11:00 AM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
12:00 PM	0	0	0	1	3	0	0	0	0	0	0	0	0	0	4	26-35	4
01:00 PM	1	0	0	3	2	0	0	0	1	0	0	0	0	0	7	26-35	5
02:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
05:00 PM	0	1	0	0	0	2	0	0	0	0	0	0	0	0	3	31-40	2
06:00 PM	2	1	0	0	0	2	0	0	0	0	0	0	0	0	5	31-40	2
07:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	3	2	8	9	7	0	0	1	0	0	0	0	0	34	26-35	17
Percent	11.8%	8.8%	5.9%	23.5%	26.5%	20.6%	0%	0%	2.9%	0%	0%	0%	0%	0%			
AM Peak Volume	10:00 AM 1	10:00 AM 1	8:00 AM 1	11:00 AM 1	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3		
PM Peak Volume	6:00 PM 2	5:00 PM 1	12:00 PM 0	1:00 PM 3	12:00 PM 3	5:00 PM 2	12:00 PM 0	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 7		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 24 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	11-20	2
08:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2	11-20	1
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
11:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	1	1	1	2	0	0	0	0	0	0	0	0	5	31-40	3
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	1	0	1	0	1	0	0	0	0	0	0	0	0	0	3	16-25	1
05:00 PM	1	0	0	1	2	0	0	0	0	0	0	0	0	0	4	26-35	3
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	1	0	0	2	0	0	0	0	0	0	0	0	3	31-40	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	5	2	3	2	6	4	0	0	0	0	0	0	0	0	22	31-40	10
Percent	22.7%	9.1%	13.6%	9.1%	27.3%	18.2%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	7:00 AM 2	7:00 AM 1	12:00 AM 0	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 3		
PM Peak Volume	4:00 PM 1	12:00 PM 0	2:00 PM 1	2:00 PM 1	5:00 PM 2	2:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 5		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 25 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 AM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	3	26-35	2
09:00 AM	1	0	0	0	1	0	1	0	0	0	0	0	0	0	3	26-35	1
10:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
12:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
01:00 PM	2	0	0	0	1	0	0	0	0	0	0	0	0	0	3	1-10	1
02:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
05:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
06:00 PM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	26-35	3
07:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1-10	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
Day Total	5	2	1	5	10	4	1	0	0	0	0	0	0	0	28	26-35	15
Percent	17.9%	7.1%	3.6%	17.9%	35.7%	14.3%	3.6%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	9:00 AM 1	7:00 AM 1	8:00 AM 1	6:00 AM 1	8:00 AM 2	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	8:00 AM 3		
PM Peak Volume	1:00 PM 2	12:00 PM 0	12:00 PM 0	6:00 PM 2	1:00 PM 1	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 3		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 26 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	36-45	1
10:00 AM	1	0	1	1	1	1	0	0	0	0	0	0	0	0	5	21-30	2
11:00 AM	0	0	0	0	2	1	0	0	0	0	0	0	0	0	3	31-40	3
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
01:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
02:00 PM	0	0	1	0	2	1	0	0	0	0	0	0	0	0	4	31-40	3
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
05:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	3	26-35	2
08:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
Day Total	1	1	5	5	10	3	1	0	0	0	0	0	0	0	26	26-35	15
Percent	3.8%	3.8%	19.2%	19.2%	38.5%	11.5%	3.8%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	10:00 AM 1	12:00 AM 0	10:00 AM 1	10:00 AM 1	11:00 AM 2	10:00 AM 1	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 5		
PM Peak Volume	12:00 PM 0	4:00 PM 1	12:00 PM 1	5:00 PM 2	2:00 PM 2	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 4		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 27 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	2	36-45	2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	0	1	1	1	0	0	0	0	0	0	0	0	3	26-35	2
11:00 AM	0	0	0	1	2	2	0	0	0	0	0	0	0	0	5	31-40	4
12:00 PM	0	0	0	5	1	1	0	0	0	0	0	0	0	0	7	26-35	6
01:00 PM	0	0	1	1	2	1	0	1	0	0	0	0	0	0	6	31-40	3
02:00 PM	3	0	3	0	6	0	0	0	0	0	0	0	0	0	12	26-35	6
03:00 PM	0	0	1	0	1	1	0	0	0	0	0	0	0	0	3	31-40	2
04:00 PM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3	16-25	2
05:00 PM	0	0	0	2	0	0	1	0	0	0	0	0	0	0	3	21-30	2
06:00 PM	1	0	0	1	4	2	1	0	0	0	0	0	0	0	9	31-40	6
07:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 PM	0	0	0	3	1	0	0	0	0	0	0	0	0	0	4	26-35	4
09:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
Day Total	4	1	7	16	20	12	3	2	0	0	0	0	0	0	65	26-35	36
Percent	6.2%	1.5%	10.8%	24.6%	30.8%	18.5%	4.6%	3.1%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	3:00 AM	7:00 AM	11:00 AM	11:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	0	1	1	2	2	1	0	0	0	0	0	0	0	5		
PM Peak Volume	2:00 PM	4:00 PM	2:00 PM	12:00 PM	2:00 PM	6:00 PM	5:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	3	1	3	5	6	2	1	1	0	0	0	0	0	0	12		
Comments:																	

Report generated on 9/2/2022 10:19 AM

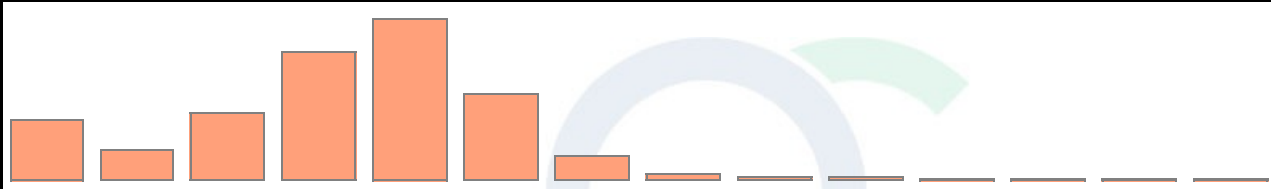
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889229 DIRECTION: WB DATE: Aug 28 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 AM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	16-25	1
08:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
09:00 AM	0	0	0	0	1	1	1	0	0	0	0	0	0	0	3	31-40	2
10:00 AM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	26-35	3
11:00 AM	1	0	0	2	0	0	0	0	0	0	0	0	0	0	3	21-30	2
12:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
01:00 PM	1	0	1	1	0	2	2	0	0	1	0	0	0	0	8	36-45	4
02:00 PM	1	0	0	1	2	1	1	0	0	0	0	0	0	0	6	31-40	3
03:00 PM	3	0	1	0	3	1	1	0	0	0	0	0	0	0	9	31-40	4
04:00 PM	1	0	2	2	2	0	0	0	0	0	0	0	0	0	7	21-30	4
05:00 PM	0	0	2	1	2	0	0	0	0	0	0	0	0	0	5	21-30	3
06:00 PM	0	1	2	2	2	0	0	0	0	0	0	0	0	0	7	21-30	4
07:00 PM	1	1	1	3	1	0	0	0	0	0	0	0	0	0	7	23-32	4
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	8	2	11	14	16	8	5	0	0	1	0	0	0	0	65	26-35	30
Percent	12.3%	3.1%	16.9%	21.5%	24.6%	12.3%	7.7%	0%	0%	1.5%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 1	12:00 AM 0	7:00 AM 1	10:00 AM 2	6:00 AM 1	12:00 AM 1	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 3		
PM Peak Volume	3:00 PM 3	6:00 PM 1	4:00 PM 2	7:00 PM 3	3:00 PM 3	1:00 PM 2	1:00 PM 2	12:00 PM 0	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 9		
Comments:																	

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Comfort Station Parking Area															QC JOB #: 15889229		
SPECIFIC LOCATION:															DIRECTION: WB		
CITY/STATE: Colbert, AL															DATE: Aug 22 2022 - Aug 28 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	29	14	32	61	77	41	11	3	1	1	0	0	0	0	270	26-35	138
Percent	10.7%	5.2%	11.9%	22.6%	28.5%	15.2%	4.1%	1.1%	0.4%	0.4%	0%	0%	0%	0%			
Cumulative Percent	10.7%	15.9%	27.8%	50.4%	78.9%	94.1%	98.1%	99.3%	99.6%	100%	100%	100%	100%	100%			
ADT 38															85th Percentile: 37 MPH Mean Speed(Average): 29 MPH Median: 29 MPH Mode: 33 MPH		
Comments:																	

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
12:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	0	2	4
01:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
02:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	4	0	0	2	0	0	0	0	0	0	0	0	0	6
06:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2
07:00 PM	1	1	1	0	0	0	0	2	0	0	0	0	0	0	5
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	11	6	0	5	0	0	5	0	0	0	0	0	2	30
Percent	3.3%	36.7%	20%	0%	16.7%	0%	0%	16.7%	0%	0%	0%	0%	0%	6.7%	
ADT 30															
AM Peak Volume	12:00 AM	11:00 AM	4:00 AM	12:00 AM	7:00 AM	12:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	1	1	0	1	0	0	1	0	0	0	0	0	0	2
PM Peak Volume	7:00 PM	5:00 PM	12:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM
	1	4	1	0	2	0	0	2	0	0	0	0	0	2	6

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 23 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
10:00 AM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
12:00 PM	0	2	1	0	0	0	0	1	0	0	0	0	0	0	4
01:00 PM	0	1	3	0	0	0	0	3	0	0	0	0	0	0	7
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
06:00 PM	0	3	0	0	0	0	0	1	0	0	0	0	0	1	5
07:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	11	10	0	3	0	0	8	0	0	0	0	0	1	34
Percent	2.9%	32.4%	29.4%	0%	8.8%	0%	0%	23.5%	0%	0%	0%	0%	0%	2.9%	
ADT 34															
AM Peak Volume	10:00 AM 1	11:00 AM 2	8:00 AM 1	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3
PM Peak Volume	12:00 PM 0	6:00 PM 3	1:00 PM 3	12:00 PM 0	4:00 PM 1	12:00 PM 0	12:00 PM 0	1:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 1	1:00 PM 7

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 24 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	1	3
08:00 AM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	1	2	0	0	0	0	2	0	0	0	0	0	0	5
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	1	3
05:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	1	0	0	1	0	0	1	0	0	0	0	0	0	3
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	9	6	0	1	0	0	4	0	0	0	0	0	2	22
Percent	0%	40.9%	27.3%	0%	4.5%	0%	0%	18.2%	0%	0%	0%	0%	0%	9.1%	
ADT 22															
AM Peak Volume	12:00 AM	8:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM	7:00 AM
	0	1	2	0	0	0	0	1	0	0	0	0	0	1	3
PM Peak Volume	12:00 PM	5:00 PM	2:00 PM	12:00 PM	8:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	2:00 PM
	0	4	2	0	1	0	0	2	0	0	0	0	0	1	5

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 25 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
09:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	1	3
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	1	3
02:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
07:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2
Day Total	1	13	4	0	4	0	0	4	0	0	0	0	0	2	28
Percent	3.6%	46.4%	14.3%	0%	14.3%	0%	0%	14.3%	0%	0%	0%	0%	0%	7.1%	
ADT 28															
AM Peak Volume	12:00 AM	8:00 AM	7:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM	8:00 AM
	0	2	1	0	1	0	0	1	0	0	0	0	0	1	3
PM Peak Volume	12:00 PM	1:00 PM	4:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	1:00 PM
	1	2	1	0	1	0	0	1	0	0	0	0	0	1	3

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 26 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	3	0	0	0	0	1	0	0	0	0	0	1	5
11:00 AM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	1	0	0	0	0	2	0	0	0	0	0	0	4
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
08:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
Day Total	0	9	7	0	1	0	0	8	0	0	0	0	0	1	26
Percent	0%	34.6%	26.9%	0%	3.8%	0%	0%	30.8%	0%	0%	0%	0%	0%	3.8%	
ADT 26															
AM Peak Volume	12:00 AM	7:00 AM	10:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	10:00 AM
	0	1	3	0	1	0	0	1	0	0	0	0	0	1	5
PM Peak Volume	12:00 PM	7:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM
	0	2	1	0	0	0	0	2	0	0	0	0	0	0	4

Comments:

Type of report: Tube Count - Vehicle Classification Data

LOCATION: (Off-Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889229

DIRECTION: WB

DATE: Aug 27 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
08:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	1	0	0	0	0	2	0	0	0	0	0	0	3
11:00 AM	0	1	0	0	1	0	0	3	0	0	0	0	0	0	5
12:00 PM	0	2	1	0	0	0	0	4	0	0	0	0	0	0	7
01:00 PM	0	0	4	0	0	0	0	2	0	0	0	0	0	0	6
02:00 PM	5	2	0	0	1	0	0	2	0	0	0	0	0	2	12
03:00 PM	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
06:00 PM	0	2	2	0	1	0	0	4	0	0	0	0	0	0	9
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	2	1	0	0	0	0	1	0	0	0	0	0	0	4
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
11:00 PM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
Day Total	6	20	12	0	4	0	0	21	0	0	0	0	0	2	65
Percent	9.2%	30.8%	18.5%	0%	6.2%	0%	0%	32.3%	0%	0%	0%	0%	0%	3.1%	
ADT 65															
AM Peak Volume	12:00 AM	2:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	1	1	0	1	0	0	3	0	0	0	0	0	0	5
PM Peak Volume	2:00 PM	4:00 PM	1:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	2:00 PM
	5	3	4	0	1	0	0	4	0	0	0	0	0	2	12

Comments:

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 28 2022

Start Time	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
10:00 AM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
11:00 AM	0	2	0	0	0	0	0	1	0	0	0	0	0	0	3
12:00 PM	0	1	0	0	0	0	0	1	0	0	0	0	0	0	2
01:00 PM	0	5	1	0	1	0	0	1	0	0	0	0	0	0	8
02:00 PM	0	3	0	0	1	0	0	1	0	0	0	0	0	1	6
03:00 PM	0	4	1	0	2	0	0	1	0	0	0	0	0	1	9
04:00 PM	0	2	1	0	1	0	0	2	0	0	0	0	0	1	7
05:00 PM	0	1	1	0	2	0	0	1	0	0	0	0	0	0	5
06:00 PM	0	5	2	0	0	0	0	0	0	0	0	0	0	0	7
07:00 PM	0	3	1	0	1	0	0	1	0	0	0	0	0	1	7
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	31	9	0	8	0	0	13	0	0	0	0	0	4	65
Percent	0%	47.7%	13.8%	0%	12.3%	0%	0%	20%	0%	0%	0%	0%	0%	6.2%	
ADT 65															
AM Peak Volume	12:00 AM	9:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	2	1	0	0	0	0	1	0	0	0	0	0	0	3
PM Peak Volume	12:00 PM	1:00 PM	6:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	3:00 PM
	0	5	2	0	2	0	0	2	0	0	0	0	0	1	9

Comments:

LOCATION: (Off-Peak) Comfort Station Parking Area

QC JOB #: 15889229

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Aug 22 2022 - Aug 28 2022

	Motorcycles	Cars & Trailer	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axle Double	5 Axle Double	>6 Axle Double	<6 Axle Multi	6 Axle Multi	>6 Axle Multi	Not Classified	Total
Grand Total	9	104	54	0	26	0	0	63	0	0	0	0	0	14	270
Percent	3.3%	38.5%	20%	0%	9.6%	0%	0%	23.3%	0%	0%	0%	0%	0%	5.2%	
ADT 38															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Off-Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL										QC JOB #: 15889229 DIRECTION: WB DATE: Aug 22 2022 - Aug 28 2022
Start Time	Mon 22 Aug 22	Tue 23 Aug 22	Wed 24 Aug 22	Thu 25 Aug 22	Fri 26 Aug 22	Average Weekday Hourly Traffic	Sat 27 Aug 22	Sun 28 Aug 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0	0	0	0	0	1	1	0	
01:00 AM	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	1	0	0	
03:00 AM	0	0	0	0	0	0	1	0	0	
04:00 AM	1	0	0	0	0	0	0	0	0	
05:00 AM	0	0	0	0	0	0	0	0	0	
06:00 AM	0	0	0	1	0	0	0	1	0	
07:00 AM	1	0	3	1	1	1	1	2	1	<div></div>
08:00 AM	0	2	2	3	0	1	2	1	1	<div></div>
09:00 AM	1	1	0	3	1	1	0	3	1	<div></div>
10:00 AM	0	2	1	1	5	2	3	3	2	<div></div>
11:00 AM	2	3	1	1	3	2	5	3	3	<div></div>
12:00 PM	4	4	0	1	1	2	7	2	3	<div></div>
01:00 PM	2	7	0	3	1	3	6	8	4	<div></div>
02:00 PM	2	1	5	1	4	3	12	6	4	<div></div>
03:00 PM	2	1	0	1	0	1	3	9	2	<div></div>
04:00 PM	1	2	3	2	2	2	3	7	3	<div></div>
05:00 PM	6	3	4	2	2	3	3	5	4	<div></div>
06:00 PM	2	5	0	3	0	2	9	7	4	<div></div>
07:00 PM	5	2	0	2	3	2	1	7	3	<div></div>
08:00 PM	1	0	3	1	2	1	4	0	2	<div></div>
09:00 PM	0	1	0	0	0	0	1	0	0	
10:00 PM	0	0	0	0	0	0	1	0	0	
11:00 PM	0	0	0	2	1	1	1	0	1	<div></div>
Day Total	30	34	22	28	26	27	65	65	38	
% Weekday Average	111.1%	125.9%	81.5%	103.7%	96.3%					
% Week Average	78.9%	89.5%	57.9%	73.7%	68.4%	71.1%	171.1%	171.1%		
AM Peak Volume	11:00 AM 2	11:00 AM 3	7:00 AM 3	8:00 AM 3	10:00 AM 5	10:00 AM 2	11:00 AM 5	9:00 AM 3	11:00 AM 3	
PM Peak Volume	5:00 PM 6	1:00 PM 7	2:00 PM 5	1:00 PM 3	2:00 PM 4	1:00 PM 3	2:00 PM 12	3:00 PM 9	1:00 PM 4	
Comments:										

Report generated on 9/2/2022 10:19 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB DATE: Nov 9 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	1	1	0	0	0	3	56-65	2
01:00 AM	0	0	0	0	0	0	0	1	0	2	0	0	0	0	3	51-60	2
02:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
03:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
04:00 AM	0	1	0	0	0	0	0	0	1	0	0	0	0	0	2	11-20	1
05:00 AM	0	0	0	0	0	0	0	0	3	5	1	1	0	0	10	51-60	8
06:00 AM	0	0	0	0	0	0	0	1	0	3	3	1	1	0	9	56-65	6
07:00 AM	0	0	0	0	0	0	0	2	9	7	4	0	2	0	24	51-60	16
08:00 AM	0	0	2	0	0	0	0	7	9	16	3	2	0	0	39	51-60	25
09:00 AM	1	0	0	0	0	1	1	3	4	6	2	0	0	0	18	51-60	10
10:00 AM	0	0	0	0	0	2	0	2	5	5	3	0	0	0	17	51-60	10
11:00 AM	1	0	0	1	2	2	2	8	8	6	1	1	0	0	32	46-55	16
12:00 PM	0	0	0	0	2	2	1	2	10	5	0	0	0	1	23	51-60	15
01:00 PM	0	0	0	0	1	3	2	8	10	3	1	1	0	0	29	46-55	18
02:00 PM	1	0	0	1	0	0	0	6	12	5	3	1	0	0	29	46-55	18
03:00 PM	1	0	0	0	3	4	2	5	12	11	2	0	1	0	41	51-60	23
04:00 PM	1	0	3	4	8	5	5	6	8	10	2	0	0	0	52	51-60	18
05:00 PM	1	0	0	0	1	2	1	8	16	17	4	4	0	0	54	51-60	33
06:00 PM	1	0	0	0	0	0	0	2	9	13	10	3	1	0	39	56-65	23
07:00 PM	0	0	0	0	0	0	0	0	3	3	4	0	0	0	10	56-65	7
08:00 PM	0	0	0	0	0	0	0	0	5	7	7	0	0	0	19	56-65	14
09:00 PM	0	0	0	0	0	0	0	3	1	1	2	2	0	0	9	46-55	4
10:00 PM	0	0	0	0	0	0	0	0	1	3	1	0	0	0	5	53-62	4
11:00 PM	0	0	0	0	0	0	0	0	3	2	0	1	0	0	6	51-60	5
Day Total	7	1	5	6	17	21	14	65	130	132	54	17	5	1	475	51-60	262
Percent	1.5%	0.2%	1.1%	1.3%	3.6%	4.4%	2.9%	13.7%	27.4%	27.8%	11.4%	3.6%	1.1%	0.2%			
AM Peak Volume	9:00 AM 1	4:00 AM 1	8:00 AM 2	11:00 AM 1	11:00 AM 2	10:00 AM 2	11:00 AM 2	11:00 AM 8	7:00 AM 9	8:00 AM 16	7:00 AM 4	8:00 AM 2	7:00 AM 2	12:00 AM 0	8:00 AM 39		
PM Peak Volume	2:00 PM 1	12:00 PM 0	4:00 PM 3	4:00 PM 4	4:00 PM 8	4:00 PM 5	4:00 PM 5	1:00 PM 8	5:00 PM 16	5:00 PM 17	6:00 PM 10	5:00 PM 4	3:00 PM 1	12:00 PM 1	5:00 PM 54		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB DATE: Nov 10 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	2	51-60	1
01:00 AM	0	0	0	0	0	0	0	0	1	1	0	1	0	0	3	51-60	2
02:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
03:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 AM	0	0	0	0	0	0	0	1	1	1	0	1	0	0	4	46-55	2
05:00 AM	0	0	0	0	0	0	0	0	3	1	1	1	0	0	6	51-60	4
06:00 AM	0	0	0	0	0	0	0	0	2	2	4	1	0	0	9	56-65	6
07:00 AM	0	0	0	0	0	0	0	2	8	11	2	0	1	0	24	51-60	19
08:00 AM	1	0	0	1	2	0	1	6	12	11	6	1	0	0	41	51-60	23
09:00 AM	0	0	2	0	1	2	1	7	5	4	0	0	1	0	23	46-55	12
10:00 AM	0	0	0	0	1	0	3	6	10	11	0	1	0	0	32	51-60	21
11:00 AM	0	3	1	0	2	3	2	6	5	5	2	0	0	0	29	46-55	11
12:00 PM	0	0	0	0	0	0	5	2	6	7	0	2	0	0	22	51-60	13
01:00 PM	0	0	0	1	0	0	4	7	3	8	3	1	0	1	28	41-50	11
02:00 PM	0	0	0	0	0	0	0	2	7	7	2	0	1	0	19	51-60	14
03:00 PM	0	0	0	1	0	1	3	6	14	8	5	1	0	0	39	51-60	22
04:00 PM	0	0	0	0	0	2	0	1	14	15	10	1	1	1	45	51-60	29
05:00 PM	0	0	0	0	0	0	1	4	13	11	12	4	1	0	46	51-60	24
06:00 PM	0	0	0	0	0	0	0	4	9	15	11	3	0	0	42	56-65	26
07:00 PM	0	0	0	0	0	0	1	2	3	4	3	1	1	0	15	51-60	7
08:00 PM	0	0	0	0	0	0	1	2	3	8	2	1	1	0	18	51-60	11
09:00 PM	0	0	0	0	0	0	0	3	4	2	0	0	0	0	9	46-55	7
10:00 PM	0	0	0	0	0	0	0	0	4	5	1	0	0	0	10	51-60	9
11:00 PM	0	0	0	0	0	0	0	0	0	0	3	0	0	0	3	56-65	3
Day Total	1	3	3	3	6	9	22	62	127	138	67	21	7	2	471	51-60	265
Percent	0.2%	0.6%	0.6%	0.6%	1.3%	1.9%	4.7%	13.2%	27%	29.3%	14.2%	4.5%	1.5%	0.4%			
AM Peak Volume	8:00 AM 1	11:00 AM 3	9:00 AM 2	8:00 AM 1	8:00 AM 2	11:00 AM 3	10:00 AM 3	9:00 AM 7	8:00 AM 12	7:00 AM 11	8:00 AM 6	12:00 AM 1	7:00 AM 1	12:00 AM 0	8:00 AM 41		
PM Peak Volume	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 1	12:00 PM 0	4:00 PM 2	12:00 PM 5	1:00 PM 7	3:00 PM 14	4:00 PM 15	5:00 PM 12	5:00 PM 4	2:00 PM 1	1:00 PM 1	5:00 PM 46		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889225 DIRECTION: EB DATE: Nov 11 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	1	0	1	0	0	3	41-50	1
01:00 AM	0	0	0	0	0	0	0	0	1	2	0	0	0	0	3	51-60	3
02:00 AM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	2	41-50	1
03:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	0	2	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	1	2	1	1	0	0	5	56-65	3
05:00 AM	0	0	0	0	0	0	0	1	3	3	2	0	1	0	10	51-60	6
06:00 AM	0	0	0	0	0	0	0	0	1	4	2	0	0	0	7	56-65	6
07:00 AM	0	0	0	0	0	0	2	1	2	7	4	0	0	0	16	56-65	11
08:00 AM	0	0	0	0	0	0	3	4	9	13	5	2	0	0	36	51-60	22
09:00 AM	1	0	0	0	0	0	0	5	4	0	2	3	0	0	15	46-55	9
10:00 AM	0	1	0	1	0	0	0	2	6	2	4	1	0	0	17	48-57	8
11:00 AM	0	0	0	0	1	0	1	3	7	7	7	0	0	0	26	51-60	14
12:00 PM	0	0	0	0	0	0	1	11	7	3	1	0	0	0	23	46-55	18
01:00 PM	0	0	0	0	0	1	5	5	10	9	2	2	1	0	35	51-60	19
02:00 PM	0	0	1	0	0	0	1	5	10	7	4	0	1	0	29	51-60	17
03:00 PM	0	0	0	0	0	0	4	3	11	8	5	1	1	0	33	51-60	19
04:00 PM	1	0	0	0	0	0	1	7	14	17	5	1	0	0	46	51-60	31
05:00 PM	0	0	0	0	1	0	1	15	14	14	7	4	1	1	58	46-55	29
06:00 PM	0	0	0	0	0	0	2	2	12	15	3	0	1	0	35	51-60	27
07:00 PM	0	0	0	0	0	0	0	5	7	5	3	0	0	0	20	46-55	12
08:00 PM	0	0	0	0	0	0	0	3	8	4	7	2	0	0	24	51-60	12
09:00 PM	0	0	0	0	0	0	0	1	7	0	5	3	1	0	17	46-55	8
10:00 PM	0	0	0	0	0	0	0	1	1	3	2	0	1	0	8	56-65	5
11:00 PM	0	0	0	0	0	0	0	1	1	0	1	2	0	0	5	61-70	3
Day Total	2	1	1	1	2	1	21	78	136	127	73	23	8	1	475	51-60	263
Percent	0.4%	0.2%	0.2%	0.2%	0.4%	0.2%	4.4%	16.4%	28.6%	26.7%	15.4%	4.8%	1.7%	0.2%			
AM Peak Volume	9:00 AM	10:00 AM	12:00 AM	10:00 AM	11:00 AM	12:00 AM	8:00 AM	9:00 AM	8:00 AM	8:00 AM	11:00 AM	9:00 AM	5:00 AM	12:00 AM	8:00 AM		
	1	1	0	1	1	0	3	5	9	13	7	3	1	0	36		
PM Peak Volume	4:00 PM	12:00 PM	2:00 PM	12:00 PM	5:00 PM	1:00 PM	1:00 PM	5:00 PM	4:00 PM	4:00 PM	5:00 PM	5:00 PM	1:00 PM	5:00 PM	5:00 PM		
	1	0	1	0	1	1	5	15	14	17	7	4	1	1	58		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB DATE: Nov 12 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
01:00 AM	0	0	0	0	0	1	0	0	1	4	0	0	0	0	6	51-60	5
02:00 AM	0	0	0	0	0	0	0	1	1	0	0	0	0	0	2	46-55	2
03:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1
05:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	0	3	51-60	3
06:00 AM	0	0	0	0	0	0	0	2	3	5	0	0	0	0	10	51-60	8
07:00 AM	0	0	0	0	0	0	0	1	0	3	0	1	0	0	5	51-60	3
08:00 AM	0	0	0	0	0	0	0	3	5	6	5	0	0	1	20	52-61	11
09:00 AM	0	0	0	0	0	0	0	3	6	4	3	1	0	0	17	51-60	10
10:00 AM	0	0	0	0	0	0	1	3	4	6	1	0	0	0	15	51-60	10
11:00 AM	0	0	0	0	1	0	3	4	9	10	4	0	0	0	31	51-60	19
12:00 PM	0	0	0	0	0	2	1	7	13	10	1	1	0	0	35	51-60	23
01:00 PM	0	0	0	0	0	0	0	4	11	6	5	0	0	0	26	51-60	17
02:00 PM	0	0	0	0	0	2	2	12	3	3	2	1	0	0	25	46-55	15
03:00 PM	0	0	0	0	0	0	0	5	11	8	1	0	0	0	25	51-60	19
04:00 PM	0	0	0	0	0	0	0	2	17	10	5	0	0	1	35	51-60	27
05:00 PM	0	0	0	0	0	0	0	5	12	8	1	2	0	0	28	51-60	20
06:00 PM	0	0	0	0	0	1	0	4	10	9	2	1	0	0	27	51-60	19
07:00 PM	0	0	0	0	0	0	0	5	5	7	4	1	0	0	22	51-60	12
08:00 PM	0	0	0	0	0	0	0	1	5	6	1	1	1	0	15	51-60	11
09:00 PM	0	0	0	0	0	0	1	1	1	1	1	0	1	0	6	41-50	2
10:00 PM	0	0	0	0	0	0	0	0	2	2	2	0	0	0	6	51-60	4
11:00 PM	0	0	0	0	0	1	2	0	0	1	0	0	0	0	4	36-45	3
Day Total	0	0	0	0	1	7	10	63	120	113	39	9	2	2	366	51-60	233
Percent	0%	0%	0%	0%	0.3%	1.9%	2.7%	17.2%	32.8%	30.9%	10.7%	2.5%	0.5%	0.5%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	1:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	8:00 AM	7:00 AM	12:00 AM	8:00 AM	11:00 AM		
	0	0	0	0	1	1	3	4	9	10	5	1	0	1	31		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	2:00 PM	4:00 PM	12:00 PM	1:00 PM	5:00 PM	8:00 PM	4:00 PM	12:00 PM		
	0	0	0	0	0	2	2	12	17	10	5	2	1	1	35		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB DATE: Nov 13 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	1	0	1	1	0	3	66-75	2
01:00 AM	0	0	0	0	0	0	0	0	1	2	0	0	0	0	3	51-60	3
02:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	2	46-55	1
05:00 AM	0	0	0	0	0	0	0	1	1	2	1	0	0	0	5	56-65	3
06:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	2	36-45	1
07:00 AM	0	0	0	0	0	0	0	1	1	0	3	1	1	0	7	61-70	4
08:00 AM	0	0	0	0	0	0	1	4	5	6	0	1	2	0	19	51-60	11
09:00 AM	0	0	0	0	0	0	0	2	4	2	1	1	0	0	10	51-60	6
10:00 AM	0	0	0	0	0	0	0	3	5	6	2	1	1	0	18	51-60	11
11:00 AM	0	0	0	0	0	1	1	2	14	5	4	0	0	0	27	51-60	19
12:00 PM	0	0	0	0	1	0	2	4	10	6	6	1	0	0	30	51-60	16
01:00 PM	0	0	0	0	0	3	2	5	8	8	5	3	0	0	34	51-60	16
02:00 PM	1	0	0	0	0	1	1	14	17	6	3	2	1	0	46	46-55	31
03:00 PM	1	0	0	0	0	0	1	9	8	11	2	0	0	0	32	51-60	19
04:00 PM	0	0	0	0	0	0	1	4	14	13	6	2	0	0	40	51-60	27
05:00 PM	0	0	0	0	1	0	3	5	19	6	4	2	0	0	40	51-60	25
06:00 PM	1	0	0	0	0	0	0	3	5	5	3	0	1	0	18	51-60	10
07:00 PM	0	0	0	0	0	0	0	5	3	5	5	2	0	0	20	56-65	10
08:00 PM	0	0	0	0	0	0	0	1	2	9	4	3	0	0	19	56-65	13
09:00 PM	0	0	0	0	0	1	0	2	3	3	2	0	0	0	11	51-60	6
10:00 PM	0	0	0	0	0	0	0	1	2	3	2	0	0	0	8	53-62	5
11:00 PM	0	0	0	0	0	0	0	0	3	0	1	0	0	0	4	46-55	3
Day Total	3	0	0	0	2	6	13	66	127	100	54	21	7	0	399	51-60	227
Percent	0.8%	0%	0%	0%	0.5%	1.5%	3.3%	16.5%	31.8%	25.1%	13.5%	5.3%	1.8%	0%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	6:00 AM	8:00 AM	11:00 AM	8:00 AM	11:00 AM	12:00 AM	8:00 AM	12:00 AM	11:00 AM		
	0	0	0	0	0	1	1	4	14	6	4	1	2	0	27		
PM Peak Volume	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	5:00 PM	2:00 PM	5:00 PM	4:00 PM	12:00 PM	1:00 PM	2:00 PM	12:00 PM	2:00 PM		
	1	0	0	0	1	3	3	14	19	13	6	3	1	0	46		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB DATE: Nov 14 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2	46-55	1
01:00 AM	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	46-55	2
02:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	2	51-60	2
05:00 AM	0	0	0	0	0	0	0	0	6	0	1	0	0	1	8	46-55	6
06:00 AM	0	0	0	0	0	0	0	2	0	1	3	1	0	0	7	58-67	4
07:00 AM	0	0	0	0	0	0	0	1	13	7	2	1	1	0	25	51-60	20
08:00 AM	1	0	0	0	0	0	0	5	13	12	8	1	0	0	40	51-60	25
09:00 AM	0	0	0	0	0	0	1	1	5	4	2	1	1	0	15	51-60	9
10:00 AM	0	0	0	0	0	0	2	2	6	7	4	0	1	0	22	51-60	13
11:00 AM	0	0	0	0	0	0	1	6	9	8	1	1	0	0	26	51-60	17
12:00 PM	0	0	0	1	0	0	1	2	5	3	1	0	0	0	13	51-60	8
01:00 PM	0	0	0	0	0	0	1	0	8	1	0	0	0	0	10	51-60	9
02:00 PM	0	0	0	0	0	0	1	4	4	4	4	3	0	0	20	46-55	8
03:00 PM	0	0	0	0	0	0	2	3	11	13	3	0	0	0	32	51-60	24
04:00 PM	0	0	0	0	0	0	2	2	15	14	5	4	1	0	43	51-60	29
05:00 PM	0	0	0	0	0	0	2	3	13	23	9	1	0	1	52	51-60	36
06:00 PM	0	0	0	0	0	0	0	5	8	19	9	2	2	1	46	56-65	28
07:00 PM	0	0	0	0	0	0	0	0	3	2	2	0	2	0	9	51-60	5
08:00 PM	0	0	0	0	0	0	0	1	4	5	5	1	0	0	16	56-65	10
09:00 PM	0	0	0	0	0	0	0	2	1	2	2	1	0	0	8	56-65	4
10:00 PM	0	0	0	0	0	0	0	0	1	1	1	1	0	0	4	51-60	2
11:00 PM	0	0	0	0	0	1	0	3	2	0	2	0	0	0	8	46-55	5
Day Total	1	0	0	1	0	1	13	42	132	127	65	18	8	3	411	51-60	259
Percent	0.2%	0%	0%	0.2%	0%	0.2%	3.2%	10.2%	32.1%	30.9%	15.8%	4.4%	1.9%	0.7%			
AM Peak Volume	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	11:00 AM	7:00 AM	8:00 AM	8:00 AM	6:00 AM	7:00 AM	5:00 AM	8:00 AM		
	1	0	0	0	0	0	2	6	13	12	8	1	1	1	40		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	11:00 PM	3:00 PM	6:00 PM	4:00 PM	5:00 PM	5:00 PM	4:00 PM	6:00 PM	5:00 PM	5:00 PM		
	0	0	0	1	0	1	2	5	15	23	9	4	2	1	52		
Comments:																	

Report generated on 11/29/2022 11:08 AM

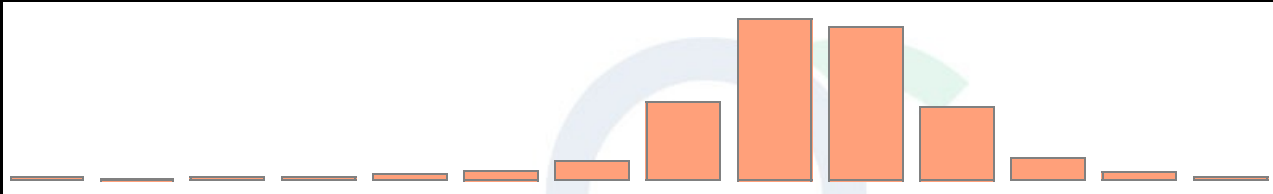
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB DATE: Nov 15 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	0	1	1	0	0	3	61-70	2
01:00 AM	0	0	0	0	0	0	0	0	1	2	0	0	0	0	3	51-60	3
02:00 AM	0	0	0	0	0	1	0	0	0	1	0	0	0	0	2	31-40	1
03:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	2	36-45	1
04:00 AM	0	0	0	0	0	0	0	1	1	1	0	0	0	0	3	46-55	2
05:00 AM	0	0	0	0	0	0	0	0	3	2	0	0	0	0	5	51-60	5
06:00 AM	0	0	0	0	0	0	1	0	4	5	1	0	0	0	11	51-60	9
07:00 AM	1	0	0	0	0	0	0	1	5	9	2	1	1	0	20	51-60	14
08:00 AM	0	0	0	0	0	0	0	6	14	13	5	1	1	0	40	51-60	27
09:00 AM	1	0	0	0	0	0	0	0	5	3	4	1	0	0	14	51-60	8
10:00 AM	0	0	0	0	2	0	1	2	6	8	1	2	0	0	22	51-60	14
11:00 AM	0	2	1	1	1	0	1	6	8	5	0	0	0	0	25	46-55	14
12:00 PM	0	0	0	0	1	2	0	6	5	3	2	0	0	0	19	46-55	11
01:00 PM	0	0	0	0	0	0	1	3	4	3	1	0	0	0	12	46-55	7
02:00 PM	0	0	0	1	0	0	1	9	8	6	2	1	0	0	28	46-55	17
03:00 PM	0	0	0	0	1	1	1	4	13	11	2	0	0	1	34	51-60	24
04:00 PM	0	0	0	0	0	2	3	11	15	14	5	1	0	0	51	51-60	29
05:00 PM	0	0	0	0	0	0	0	3	17	18	12	0	0	0	50	51-60	35
06:00 PM	0	0	0	0	0	0	1	6	19	15	8	1	1	0	51	51-60	34
07:00 PM	0	0	0	0	0	0	1	5	1	3	5	1	0	0	16	56-65	8
08:00 PM	0	0	0	1	0	0	0	4	6	7	1	1	0	0	20	51-60	13
09:00 PM	0	0	0	0	0	0	0	1	2	3	2	1	0	0	9	53-62	5
10:00 PM	0	0	0	0	0	0	0	0	0	0	3	0	0	0	3	56-65	3
11:00 PM	0	0	0	0	0	0	0	1	2	2	1	0	0	0	6	51-60	4
Day Total	2	2	1	3	5	6	12	70	139	135	58	12	3	1	449	51-60	274
Percent	0.4%	0.4%	0.2%	0.7%	1.1%	1.3%	2.7%	15.6%	31%	30.1%	12.9%	2.7%	0.7%	0.2%			
AM Peak Volume	7:00 AM 1	11:00 AM 2	11:00 AM 1	11:00 AM 1	10:00 AM 2	2:00 AM 1	3:00 AM 1	8:00 AM 6	8:00 AM 14	8:00 AM 13	8:00 AM 5	10:00 AM 2	7:00 AM 1	12:00 AM 0	8:00 AM 40		
PM Peak Volume	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	12:00 PM 1	12:00 PM 2	4:00 PM 3	4:00 PM 11	6:00 PM 19	5:00 PM 18	5:00 PM 12	2:00 PM 1	6:00 PM 1	3:00 PM 1	4:00 PM 51		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) John Coffee Memorial Bridge															QC JOB #: 15889225		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	16	7	10	14	33	51	105	446	911	872	410	121	40	10	3046	51-60	1783
Percent	0.5%	0.2%	0.3%	0.5%	1.1%	1.7%	3.4%	14.6%	29.9%	28.6%	13.5%	4%	1.3%	0.3%			
Cumulative Percent	0.5%	0.8%	1.1%	1.5%	2.6%	4.3%	7.7%	22.4%	52.3%	80.9%	94.4%	98.4%	99.7%	100%			
ADT 435															<div>85th Percentile: 61 MPH</div> <div>Mean Speed(Average): 54 MPH</div> <div>Median: 54 MPH</div> <div>Mode: 53 MPH</div>		
Comments:																	

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	6	4	0	0	0	0	0	0	0	0	0	0	0	10
06:00 AM	0	8	1	0	0	0	0	0	0	0	0	0	0	0	9
07:00 AM	0	13	10	0	1	0	0	0	0	0	0	0	0	0	24
08:00 AM	0	24	8	0	5	0	0	2	0	0	0	0	0	0	39
09:00 AM	0	9	3	0	4	2	0	0	0	0	0	0	0	0	18
10:00 AM	1	10	4	0	1	0	0	1	0	0	0	0	0	0	17
11:00 AM	1	16	6	0	3	4	0	1	0	0	0	0	0	1	32
12:00 PM	0	16	5	0	1	1	0	0	0	0	0	0	0	0	23
01:00 PM	0	16	6	0	4	3	0	0	0	0	0	0	0	0	29
02:00 PM	1	11	10	0	5	0	0	1	0	0	0	0	0	1	29
03:00 PM	0	17	13	0	5	4	0	1	0	0	0	0	0	1	41
04:00 PM	0	31	15	0	1	4	0	0	0	0	0	0	0	1	52
05:00 PM	0	27	16	0	5	2	0	3	0	0	0	0	0	1	54
06:00 PM	0	23	12	0	2	0	0	1	0	0	0	0	0	1	39
07:00 PM	0	8	1	0	1	0	0	0	0	0	0	0	0	0	10
08:00 PM	0	14	4	0	1	0	0	0	0	0	0	0	0	0	19
09:00 PM	0	3	4	0	2	0	0	0	0	0	0	0	0	0	9
10:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
11:00 PM	0	3	3	0	0	0	0	0	0	0	0	0	0	0	6
Day Total	3	265	130	0	41	20	0	10	0	0	0	0	0	6	475
Percent	0.6%	55.8%	27.4%	0%	8.6%	4.2%	0%	2.1%	0%	0%	0%	0%	0%	1.3%	
ADT 475															
AM Peak Volume	10:00 AM 1	8:00 AM 24	7:00 AM 10	12:00 AM 0	8:00 AM 5	11:00 AM 4	12:00 AM 0	8:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 1	8:00 AM 39
PM Peak Volume	2:00 PM 1	4:00 PM 31	5:00 PM 16	12:00 PM 0	2:00 PM 5	3:00 PM 4	12:00 PM 0	5:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	5:00 PM 54

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 10 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
05:00 AM	0	4	1	0	1	0	0	0	0	0	0	0	0	0	6
06:00 AM	0	5	4	0	0	0	0	0	0	0	0	0	0	0	9
07:00 AM	0	11	11	0	2	0	0	0	0	0	0	0	0	0	24
08:00 AM	0	23	10	0	4	0	0	3	0	0	0	0	0	1	41
09:00 AM	0	11	6	0	4	0	0	1	0	1	0	0	0	0	23
10:00 AM	0	14	13	0	4	1	0	0	0	0	0	0	0	0	32
11:00 AM	8	12	5	0	3	0	0	0	0	1	0	0	0	0	29
12:00 PM	0	12	7	1	2	0	0	0	0	0	0	0	0	0	22
01:00 PM	1	18	7	0	1	0	0	1	0	0	0	0	0	0	28
02:00 PM	0	14	3	0	1	0	0	1	0	0	0	0	0	0	19
03:00 PM	2	23	10	0	4	0	0	0	0	0	0	0	0	0	39
04:00 PM	0	29	12	0	2	0	0	2	0	0	0	0	0	0	45
05:00 PM	1	28	15	0	1	0	0	1	0	0	0	0	0	0	46
06:00 PM	0	22	16	0	4	0	0	0	0	0	0	0	0	0	42
07:00 PM	0	9	4	0	2	0	0	0	0	0	0	0	0	0	15
08:00 PM	0	12	6	0	0	0	0	0	0	0	0	0	0	0	18
09:00 PM	0	4	3	0	2	0	0	0	0	0	0	0	0	0	9
10:00 PM	0	5	4	0	1	0	0	0	0	0	0	0	0	0	10
11:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
Day Total	12	268	139	1	38	1	0	9	0	2	0	0	0	1	471
Percent	2.5%	56.9%	29.5%	0.2%	8.1%	0.2%	0%	1.9%	0%	0.4%	0%	0%	0%	0.2%	
ADT 471															
AM Peak Volume	11:00 AM 8	8:00 AM 23	10:00 AM 13	12:00 AM 0	8:00 AM 4	10:00 AM 1	12:00 AM 0	8:00 AM 3	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	8:00 AM 1	8:00 AM 41
PM Peak Volume	3:00 PM 2	4:00 PM 29	6:00 PM 16	12:00 PM 1	3:00 PM 4	12:00 PM 0	12:00 PM 0	4:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	5:00 PM 46

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 11 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5
05:00 AM	0	6	4	0	0	0	0	0	0	0	0	0	0	0	10
06:00 AM	0	3	3	0	1	0	0	0	0	0	0	0	0	0	7
07:00 AM	0	11	5	0	0	0	0	0	0	0	0	0	0	0	16
08:00 AM	0	25	10	0	1	0	0	0	0	0	0	0	0	0	36
09:00 AM	0	11	2	0	0	0	0	1	0	0	0	0	0	1	15
10:00 AM	1	12	3	0	0	0	0	1	0	0	0	0	0	0	17
11:00 AM	0	16	8	0	1	0	0	1	0	0	0	0	0	0	26
12:00 PM	0	15	7	0	1	0	0	0	0	0	0	0	0	0	23
01:00 PM	3	18	10	0	1	0	0	3	0	0	0	0	0	0	35
02:00 PM	2	20	3	0	3	0	0	1	0	0	0	0	0	0	29
03:00 PM	1	20	9	0	2	0	0	1	0	0	0	0	0	0	33
04:00 PM	0	31	11	0	2	0	0	1	0	0	0	0	0	1	46
05:00 PM	0	37	16	0	3	0	0	2	0	0	0	0	0	0	58
06:00 PM	0	24	8	0	2	0	0	1	0	0	0	0	0	0	35
07:00 PM	0	12	8	0	0	0	0	0	0	0	0	0	0	0	20
08:00 PM	0	16	6	0	1	0	0	1	0	0	0	0	0	0	24
09:00 PM	0	12	4	0	0	0	0	1	0	0	0	0	0	0	17
10:00 PM	0	6	0	0	1	0	0	1	0	0	0	0	0	0	8
11:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
Day Total	7	308	124	0	19	0	0	15	0	0	0	0	0	2	475
Percent	1.5%	64.8%	26.1%	0%	4%	0%	0%	3.2%	0%	0%	0%	0%	0%	0.4%	
ADT 475															
AM Peak Volume	10:00 AM 1	8:00 AM 25	8:00 AM 10	12:00 AM 0	6:00 AM 1	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	8:00 AM 36
PM Peak Volume	1:00 PM 3	5:00 PM 37	5:00 PM 16	12:00 PM 0	2:00 PM 3	12:00 PM 0	12:00 PM 0	1:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 1	5:00 PM 58

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
02:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
06:00 AM	0	2	7	0	1	0	0	0	0	0	0	0	0	0	10
07:00 AM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
08:00 AM	0	11	8	0	1	0	0	0	0	0	0	0	0	0	20
09:00 AM	0	13	4	0	0	0	0	0	0	0	0	0	0	0	17
10:00 AM	1	7	7	0	0	0	0	0	0	0	0	0	0	0	15
11:00 AM	0	22	7	0	1	0	0	1	0	0	0	0	0	0	31
12:00 PM	0	21	11	0	3	0	0	0	0	0	0	0	0	0	35
01:00 PM	0	15	8	0	3	0	0	0	0	0	0	0	0	0	26
02:00 PM	6	15	3	0	1	0	0	0	0	0	0	0	0	0	25
03:00 PM	0	19	3	0	3	0	0	0	0	0	0	0	0	0	25
04:00 PM	0	17	13	1	3	0	0	1	0	0	0	0	0	0	35
05:00 PM	0	19	7	0	1	0	0	1	0	0	0	0	0	0	28
06:00 PM	0	14	13	0	0	0	0	0	0	0	0	0	0	0	27
07:00 PM	0	16	4	0	2	0	0	0	0	0	0	0	0	0	22
08:00 PM	0	8	6	0	1	0	0	0	0	0	0	0	0	0	15
09:00 PM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
10:00 PM	0	3	3	0	0	0	0	0	0	0	0	0	0	0	6
11:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
Day Total	7	224	111	1	20	0	0	3	0	0	0	0	0	0	366
Percent	1.9%	61.2%	30.3%	0.3%	5.5%	0%	0%	0.8%	0%	0%	0%	0%	0%	0%	
ADT 366															
AM Peak Volume	10:00 AM 1	11:00 AM 22	8:00 AM 8	12:00 AM 0	6:00 AM 1	12:00 AM 0	12:00 AM 0	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 31
PM Peak Volume	2:00 PM 6	12:00 PM 21	4:00 PM 13	4:00 PM 1	12:00 PM 3	12:00 PM 0	12:00 PM 0	4:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 35

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 13 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	1	4	0	0	0	0	0	0	0	0	0	0	0	5
06:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	6	1	0	0	0	0	0	0	0	0	0	0	0	7
08:00 AM	0	7	8	0	3	0	0	1	0	0	0	0	0	0	19
09:00 AM	0	8	1	0	0	0	0	1	0	0	0	0	0	0	10
10:00 AM	0	11	4	0	1	0	0	2	0	0	0	0	0	0	18
11:00 AM	0	16	7	2	0	0	0	2	0	0	0	0	0	0	27
12:00 PM	0	16	11	0	1	0	0	2	0	0	0	0	0	0	30
01:00 PM	0	25	6	0	3	0	0	0	0	0	0	0	0	0	34
02:00 PM	0	25	14	0	4	0	0	2	0	0	0	0	0	1	46
03:00 PM	0	15	13	0	0	0	0	3	0	0	0	0	0	1	32
04:00 PM	0	26	10	1	2	0	0	1	0	0	0	0	0	0	40
05:00 PM	0	25	12	0	3	0	0	0	0	0	0	0	0	0	40
06:00 PM	0	11	5	0	1	0	0	0	0	0	0	0	0	1	18
07:00 PM	0	12	6	0	1	0	0	1	0	0	0	0	0	0	20
08:00 PM	0	13	5	0	0	0	0	1	0	0	0	0	0	0	19
09:00 PM	0	7	3	0	0	0	0	1	0	0	0	0	0	0	11
10:00 PM	0	6	1	0	1	0	0	0	0	0	0	0	0	0	8
11:00 PM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4
Day Total	0	237	119	3	20	0	0	17	0	0	0	0	0	3	399
Percent	0%	59.4%	29.8%	0.8%	5%	0%	0%	4.3%	0%	0%	0%	0%	0%	0.8%	
ADT 399															
AM Peak Volume	12:00 AM	11:00 AM	8:00 AM	11:00 AM	8:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	16	8	2	3	0	0	2	0	0	0	0	0	0	27
PM Peak Volume	12:00 PM	4:00 PM	2:00 PM	4:00 PM	2:00 PM	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	2:00 PM
	0	26	14	1	4	0	0	3	0	0	0	0	0	1	46

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 14 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	5	3	0	0	0	0	0	0	0	0	0	0	0	8
06:00 AM	0	4	2	0	0	0	0	1	0	0	0	0	0	0	7
07:00 AM	0	12	10	0	3	0	0	0	0	0	0	0	0	0	25
08:00 AM	0	26	12	0	0	0	0	1	0	0	0	0	0	1	40
09:00 AM	0	10	4	0	0	0	0	1	0	0	0	0	0	0	15
10:00 AM	0	13	7	0	1	0	0	1	0	0	0	0	0	0	22
11:00 AM	0	14	10	0	1	0	0	1	0	0	0	0	0	0	26
12:00 PM	0	8	4	0	1	0	0	0	0	0	0	0	0	0	13
01:00 PM	0	4	4	0	2	0	0	0	0	0	0	0	0	0	10
02:00 PM	0	12	7	0	1	0	0	0	0	0	0	0	0	0	20
03:00 PM	0	18	13	0	1	0	0	0	0	0	0	0	0	0	32
04:00 PM	0	25	15	0	2	0	0	1	0	0	0	0	0	0	43
05:00 PM	0	31	17	0	4	0	0	0	0	0	0	0	0	0	52
06:00 PM	0	28	18	0	0	0	0	0	0	0	0	0	0	0	46
07:00 PM	0	6	1	0	2	0	0	0	0	0	0	0	0	0	9
08:00 PM	0	7	9	0	0	0	0	0	0	0	0	0	0	0	16
09:00 PM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
10:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
11:00 PM	0	6	2	0	0	0	0	0	0	0	0	0	0	0	8
Day Total	0	241	144	0	19	0	0	6	0	0	0	0	0	1	411
Percent	0%	58.6%	35%	0%	4.6%	0%	0%	1.5%	0%	0%	0%	0%	0%	0.2%	
ADT 411															
AM Peak	12:00 AM	8:00 AM	8:00 AM	12:00 AM	7:00 AM	12:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	8:00 AM
Volume	0	26	12	0	3	0	0	1	0	0	0	0	0	1	40
PM Peak	12:00 PM	5:00 PM	6:00 PM	12:00 PM	5:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM
Volume	0	31	18	0	4	0	0	1	0	0	0	0	0	0	52

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
05:00 AM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
06:00 AM	0	5	5	0	0	0	0	1	0	0	0	0	0	0	11
07:00 AM	0	14	3	0	1	0	0	1	0	0	0	0	0	1	20
08:00 AM	0	29	9	0	2	0	0	0	0	0	0	0	0	0	40
09:00 AM	0	8	2	0	3	0	0	1	0	0	0	0	0	0	14
10:00 AM	0	12	9	1	0	0	0	0	0	0	0	0	0	0	22
11:00 AM	0	13	7	0	3	0	0	2	0	0	0	0	0	0	25
12:00 PM	0	9	8	1	1	0	0	0	0	0	0	0	0	0	19
01:00 PM	0	8	3	0	1	0	0	0	0	0	0	0	0	0	12
02:00 PM	0	17	9	0	1	0	0	1	0	0	0	0	0	0	28
03:00 PM	0	18	14	0	1	0	0	1	0	0	0	0	0	0	34
04:00 PM	0	29	19	0	3	0	0	0	0	0	0	0	0	0	51
05:00 PM	0	35	12	0	3	0	0	0	0	0	0	0	0	0	50
06:00 PM	0	36	14	0	1	0	0	0	0	0	0	0	0	0	51
07:00 PM	0	10	5	0	1	0	0	0	0	0	0	0	0	0	16
08:00 PM	0	11	8	0	1	0	0	0	0	0	0	0	0	0	20
09:00 PM	0	7	1	0	1	0	0	0	0	0	0	0	0	0	9
10:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
11:00 PM	0	3	3	0	0	0	0	0	0	0	0	0	0	0	6
Day Total	0	279	137	2	23	0	0	7	0	0	0	0	0	1	449
Percent	0%	62.1%	30.5%	0.4%	5.1%	0%	0%	1.6%	0%	0%	0%	0%	0%	0.2%	
ADT 449															
AM Peak Volume	12:00 AM 0	8:00 AM 29	8:00 AM 9	10:00 AM 1	9:00 AM 3	12:00 AM 0	12:00 AM 0	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 1	8:00 AM 40
PM Peak Volume	12:00 PM 0	6:00 PM 36	4:00 PM 19	12:00 PM 1	4:00 PM 3	12:00 PM 0	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 51

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	29	1822	904	7	180	21	0	67	0	2	0	0	0	14	3046
Percent	1%	59.8%	29.7%	0.2%	5.9%	0.7%	0%	2.2%	0%	0.1%	0%	0%	0%	0.5%	
ADT 435															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) John Coffee Memorial Bridge									QC JOB #: 15889225	
SPECIFIC LOCATION:									DIRECTION: EB	
CITY/STATE: Colbert, AL									DATE: Nov 9 2022 - Nov 15 2022	
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			3	2	3	3	1	3	2	<div><div></div></div>
01:00 AM			3	3	3	3	6	3	4	<div><div></div></div>
02:00 AM			1	1	2	1	2	1	1	<div><div></div></div>
03:00 AM			1	1	2	1	1	0	1	<div><div></div></div>
04:00 AM			2	4	5	4	1	2	3	<div><div></div></div>
05:00 AM			10	6	10	9	3	5	7	<div><div></div></div>
06:00 AM			9	9	7	8	10	2	7	<div><div></div></div>
07:00 AM			24	24	16	21	5	7	15	<div><div></div></div>
08:00 AM			39	41	36	39	20	19	31	<div><div></div></div>
09:00 AM			18	23	15	19	17	10	17	<div><div></div></div>
10:00 AM			17	32	17	22	15	18	20	<div><div></div></div>
11:00 AM			32	29	26	29	31	27	29	<div><div></div></div>
12:00 PM			23	22	23	23	35	30	27	<div><div></div></div>
01:00 PM			29	28	35	31	26	34	30	<div><div></div></div>
02:00 PM			29	19	29	26	25	46	30	<div><div></div></div>
03:00 PM			41	39	33	38	25	32	34	<div><div></div></div>
04:00 PM			52	45	46	48	35	40	44	<div><div></div></div>
05:00 PM			54	46	58	53	28	40	45	<div><div></div></div>
06:00 PM			39	42	35	39	27	18	32	<div><div></div></div>
07:00 PM			10	15	20	15	22	20	17	<div><div></div></div>
08:00 PM			19	18	24	20	15	19	19	<div><div></div></div>
09:00 PM			9	9	17	12	6	11	10	<div><div></div></div>
10:00 PM			5	10	8	8	6	8	7	<div><div></div></div>
11:00 PM			6	3	5	5	4	4	4	<div><div></div></div>
Day Total			475	471	475	477	366	399	436	
% Weekday Average			99.6%	98.7%	99.6%					
% Week Average			108.9%	108%	108.9%	109.4%	83.9%	91.5%		
AM Peak Volume			8:00 AM 39	8:00 AM 41	8:00 AM 36	8:00 AM 39	11:00 AM 31	11:00 AM 27	8:00 AM 31	
PM Peak Volume			5:00 PM 54	5:00 PM 46	5:00 PM 58	5:00 PM 53	12:00 PM 35	2:00 PM 46	5:00 PM 45	
Comments:										

Report generated on 11/29/2022 11:07 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) John Coffee Memorial Bridge							QC JOB #: 15889225			
SPECIFIC LOCATION:							DIRECTION: EB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	2	3				3			3	<div></div>
01:00 AM	2	3				3			3	<div></div>
02:00 AM	1	2				2			2	<div></div>
03:00 AM	0	2				1			1	<div></div>
04:00 AM	2	3				3			3	<div></div>
05:00 AM	8	5				7			7	<div></div>
06:00 AM	7	11				9			9	<div></div>
07:00 AM	25	20				23			23	<div></div>
08:00 AM	40	40				40			40	<div></div>
09:00 AM	15	14				15			15	<div></div>
10:00 AM	22	22				22			22	<div></div>
11:00 AM	26	25				26			26	<div></div>
12:00 PM	13	19				16			16	<div></div>
01:00 PM	10	12				11			11	<div></div>
02:00 PM	20	28				24			24	<div></div>
03:00 PM	32	34				33			33	<div></div>
04:00 PM	43	51				47			47	<div></div>
05:00 PM	52	50				51			51	<div></div>
06:00 PM	46	51				49			49	<div></div>
07:00 PM	9	16				13			13	<div></div>
08:00 PM	16	20				18			18	<div></div>
09:00 PM	8	9				9			9	<div></div>
10:00 PM	4	3				4			4	<div></div>
11:00 PM	8	6				7			7	<div></div>
Day Total	411	449				436			436	
% Weekday Average	94.3%	103%								
% Week Average	94.3%	103%				100%				
AM Peak Volume	8:00 AM 40	8:00 AM 40				8:00 AM 40			8:00 AM 40	
PM Peak Volume	5:00 PM 52	4:00 PM 51				5:00 PM 51			5:00 PM 51	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 9 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	1	0	2	0	1	1	0	0	0	5	41-50	2
01:00 AM	0	0	0	0	0	0	0	1	0	2	0	0	0	0	3	51-60	2
02:00 AM	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2	46-55	1
03:00 AM	0	0	0	0	0	0	0	1	0	1	2	0	0	0	4	56-65	3
04:00 AM	0	1	0	0	0	0	0	1	1	2	0	0	0	0	5	51-60	3
05:00 AM	0	0	0	0	0	0	0	0	4	12	1	1	0	0	18	51-60	16
06:00 AM	0	0	0	0	0	0	0	3	16	23	8	6	2	0	58	51-60	39
07:00 AM	0	0	0	0	0	0	0	6	22	21	13	7	2	1	72	51-60	43
08:00 AM	0	0	2	0	0	0	1	11	18	27	8	2	0	0	69	51-60	45
09:00 AM	1	0	0	0	0	1	2	6	13	13	3	0	0	0	39	51-60	26
10:00 AM	0	0	0	0	0	3	2	10	10	7	3	0	0	0	35	46-55	20
11:00 AM	6	0	0	1	3	4	4	10	15	11	2	1	0	0	57	51-60	26
12:00 PM	0	0	0	0	2	2	11	16	11	6	1	0	0	1	50	41-50	27
01:00 PM	2	0	0	0	2	10	5	20	15	4	1	1	0	0	60	46-55	35
02:00 PM	2	0	0	1	1	0	4	14	19	5	3	2	0	0	51	46-55	33
03:00 PM	7	1	0	1	3	7	8	17	20	12	2	1	1	0	80	46-55	37
04:00 PM	7	6	5	5	8	8	7	19	15	12	2	0	0	0	94	46-55	34
05:00 PM	1	0	0	0	1	7	7	24	22	21	4	5	0	0	92	46-55	46
06:00 PM	1	0	0	0	0	2	1	16	17	15	12	3	1	0	68	46-55	33
07:00 PM	0	0	0	0	0	0	1	3	13	14	5	0	0	1	37	51-60	27
08:00 PM	0	0	0	0	0	0	0	2	8	10	7	0	0	0	27	51-60	18
09:00 PM	0	0	0	0	0	1	0	7	2	6	3	2	0	0	21	46-55	9
10:00 PM	0	0	0	0	0	0	0	0	1	3	1	0	1	0	6	53-62	4
11:00 PM	0	0	0	0	0	0	0	1	4	2	0	1	0	0	8	51-60	6
Day Total	27	8	7	8	20	46	53	190	247	230	83	32	7	3	961	51-60	477
Percent	2.8%	0.8%	0.7%	0.8%	2.1%	4.8%	5.5%	19.8%	25.7%	23.9%	8.6%	3.3%	0.7%	0.3%			
AM Peak Volume	11:00 AM 6	4:00 AM 1	8:00 AM 2	11:00 AM 1	11:00 AM 3	11:00 AM 4	11:00 AM 4	8:00 AM 11	7:00 AM 22	8:00 AM 27	7:00 AM 13	7:00 AM 7	6:00 AM 2	7:00 AM 1	7:00 AM 72		
PM Peak Volume	3:00 PM 7	4:00 PM 6	4:00 PM 5	4:00 PM 5	4:00 PM 8	1:00 PM 10	12:00 PM 11	5:00 PM 24	5:00 PM 22	5:00 PM 21	6:00 PM 12	5:00 PM 5	3:00 PM 1	12:00 PM 1	4:00 PM 94		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 10 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	1	1	0	1	0	0	4	46-55	2
01:00 AM	0	0	0	0	0	0	0	1	1	1	0	1	0	0	4	46-55	2
02:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
03:00 AM	0	0	0	0	0	1	0	1	0	0	0	0	0	0	2	31-40	1
04:00 AM	0	0	0	0	0	0	0	2	2	1	0	2	0	0	7	46-55	4
05:00 AM	0	0	0	0	0	0	1	0	4	4	3	1	0	0	13	51-60	8
06:00 AM	0	0	0	0	0	0	0	3	17	18	14	6	1	0	59	51-60	35
07:00 AM	0	0	0	0	0	0	1	5	29	25	11	2	2	0	75	51-60	54
08:00 AM	1	0	0	1	2	0	2	12	21	23	11	2	0	0	75	51-60	44
09:00 AM	0	0	2	2	3	3	8	14	12	9	2	1	1	0	57	46-55	26
10:00 AM	2	0	0	0	1	1	4	9	13	13	1	1	0	0	45	51-60	26
11:00 AM	2	3	1	0	4	4	8	10	9	8	3	0	0	0	52	46-55	19
12:00 PM	1	5	0	0	1	0	5	5	14	10	2	2	1	0	46	51-60	24
01:00 PM	1	0	0	1	0	0	6	12	13	13	3	1	0	1	51	51-60	26
02:00 PM	0	0	0	0	0	0	2	12	14	11	4	0	1	0	44	46-55	26
03:00 PM	0	0	0	1	0	2	4	13	25	9	7	2	0	0	63	46-55	38
04:00 PM	1	0	0	0	0	2	0	11	26	21	11	2	1	1	76	51-60	47
05:00 PM	0	0	0	0	0	1	3	20	21	23	14	5	1	0	88	51-60	44
06:00 PM	0	0	0	0	0	1	1	11	21	24	14	4	0	0	76	51-60	45
07:00 PM	0	0	0	0	0	0	2	4	10	15	5	2	1	0	39	51-60	25
08:00 PM	0	0	0	0	0	0	2	9	8	14	3	1	1	0	38	51-60	22
09:00 PM	0	0	0	0	0	0	0	3	5	5	1	1	0	0	15	51-60	10
10:00 PM	0	0	0	0	0	0	2	3	5	5	1	1	0	0	17	51-60	10
11:00 PM	0	0	0	0	0	0	0	0	2	0	4	0	0	0	6	56-65	4
Day Total	8	8	3	5	11	15	51	162	273	253	114	38	10	2	953	51-60	526
Percent	0.8%	0.8%	0.3%	0.5%	1.2%	1.6%	5.4%	17%	28.6%	26.5%	12%	4%	1%	0.2%			
AM Peak Volume	10:00 AM 2	11:00 AM 3	9:00 AM 2	9:00 AM 2	11:00 AM 4	11:00 AM 4	9:00 AM 8	9:00 AM 14	7:00 AM 29	7:00 AM 25	6:00 AM 14	6:00 AM 6	7:00 AM 2	12:00 AM 0	7:00 AM 75		
PM Peak Volume	12:00 PM 1	12:00 PM 5	12:00 PM 0	1:00 PM 1	12:00 PM 1	3:00 PM 2	1:00 PM 6	5:00 PM 20	4:00 PM 26	6:00 PM 24	5:00 PM 14	5:00 PM 5	12:00 PM 1	1:00 PM 1	5:00 PM 88		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 11 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	1	0	1	0	0	3	41-50	1
01:00 AM	0	0	0	0	0	0	0	0	1	2	0	0	0	0	3	51-60	3
02:00 AM	0	0	0	0	0	0	0	1	1	1	0	0	0	0	3	46-55	2
03:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	0	2	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	1	2	2	2	0	0	7	56-65	4
05:00 AM	0	0	0	0	0	0	0	4	6	4	2	0	1	0	17	48-57	10
06:00 AM	0	0	0	0	0	0	2	3	7	17	12	4	0	1	46	56-65	29
07:00 AM	0	0	0	0	0	0	3	3	11	18	8	0	1	1	45	51-60	29
08:00 AM	0	0	0	0	0	0	5	9	18	22	5	3	0	0	62	51-60	40
09:00 AM	1	0	0	0	0	0	3	10	9	6	3	3	0	0	35	46-55	19
10:00 AM	0	1	0	1	0	0	3	6	12	6	4	1	0	0	34	46-55	18
11:00 AM	0	0	0	0	1	1	2	8	10	13	9	0	0	0	44	51-60	23
12:00 PM	0	0	0	0	1	0	3	15	16	5	2	0	0	0	42	46-55	31
01:00 PM	0	0	0	0	1	2	10	12	15	11	7	2	1	0	61	46-55	27
02:00 PM	1	0	1	0	0	1	5	12	22	10	5	1	1	0	59	46-55	34
03:00 PM	0	0	0	0	0	0	4	18	25	12	7	1	1	0	68	46-55	43
04:00 PM	1	0	0	0	0	2	6	25	35	23	8	1	0	0	101	46-55	60
05:00 PM	1	0	0	0	1	1	6	25	34	18	13	4	1	1	105	46-55	59
06:00 PM	0	0	0	0	1	0	4	6	24	20	4	1	2	0	62	51-60	44
07:00 PM	0	0	0	0	0	0	1	10	14	12	7	2	0	0	46	51-60	26
08:00 PM	0	0	0	0	0	0	2	5	12	10	7	3	0	0	39	51-60	22
09:00 PM	0	0	0	0	0	0	0	1	8	1	5	5	1	0	21	61-70	10
10:00 PM	0	0	0	0	0	1	0	3	3	5	4	1	1	0	18	56-65	9
11:00 PM	0	0	0	0	0	0	0	2	2	2	1	2	0	0	9	46-55	4
Day Total	4	1	1	1	5	8	59	180	286	221	116	37	10	3	932	51-60	507
Percent	0.4%	0.1%	0.1%	0.1%	0.5%	0.9%	6.3%	19.3%	30.7%	23.7%	12.4%	4%	1.1%	0.3%			
AM Peak Volume	9:00 AM 1	10:00 AM 1	12:00 AM 0	10:00 AM 1	11:00 AM 1	11:00 AM 1	8:00 AM 5	9:00 AM 10	8:00 AM 18	8:00 AM 22	6:00 AM 12	6:00 AM 4	5:00 AM 1	6:00 AM 1	8:00 AM 62		
PM Peak Volume	2:00 PM 1	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 1	1:00 PM 2	1:00 PM 10	4:00 PM 25	4:00 PM 35	4:00 PM 23	5:00 PM 13	9:00 PM 5	6:00 PM 2	5:00 PM 1	5:00 PM 105		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 12 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	2	51-60	2
01:00 AM	0	0	0	0	0	1	0	1	1	4	0	0	0	0	7	51-60	5
02:00 AM	0	0	0	0	0	0	1	2	2	0	0	0	0	0	5	46-55	4
03:00 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	51-60	2
04:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	0	2	41-50	1
05:00 AM	0	0	0	0	0	0	1	0	4	4	0	0	0	0	9	51-60	8
06:00 AM	0	0	0	0	0	0	0	5	5	12	1	1	0	0	24	51-60	17
07:00 AM	0	0	0	0	0	1	0	3	4	8	0	1	0	0	17	51-60	12
08:00 AM	0	0	0	0	0	0	0	5	11	8	5	0	0	1	30	51-60	19
09:00 AM	0	0	0	0	0	0	3	8	9	12	4	1	0	0	37	51-60	21
10:00 AM	0	0	0	0	0	0	1	12	7	8	2	0	0	0	30	46-55	19
11:00 AM	0	0	0	0	1	0	10	23	23	15	4	0	0	0	76	46-55	46
12:00 PM	0	0	0	0	0	4	3	17	22	15	2	1	0	0	64	46-55	39
01:00 PM	0	0	0	0	0	1	1	15	14	12	7	0	0	0	50	46-55	29
02:00 PM	0	0	0	0	0	2	2	25	13	10	4	1	0	0	57	46-55	38
03:00 PM	0	0	0	0	0	2	1	16	23	14	2	0	0	0	58	46-55	39
04:00 PM	0	0	0	0	0	1	0	9	28	15	9	0	0	1	63	51-60	43
05:00 PM	0	0	0	0	0	0	0	12	21	17	2	2	0	0	54	51-60	38
06:00 PM	0	0	0	0	0	1	1	5	17	15	2	2	0	0	43	51-60	32
07:00 PM	0	0	0	0	0	0	0	7	17	14	6	3	0	0	47	51-60	31
08:00 PM	0	0	0	0	0	0	0	3	8	9	3	3	1	0	27	51-60	17
09:00 PM	0	0	0	0	1	0	2	3	3	3	2	0	1	0	15	46-55	6
10:00 PM	0	0	0	0	0	0	1	0	3	5	2	0	0	0	11	51-60	8
11:00 PM	0	0	0	0	0	1	2	1	1	2	0	0	0	0	7	41-50	3
Day Total	0	0	0	0	2	14	29	173	237	205	58	15	2	2	737	51-60	442
Percent	0%	0%	0%	0%	0.3%	1.9%	3.9%	23.5%	32.2%	27.8%	7.9%	2%	0.3%	0.3%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	1:00 AM	11:00 AM	11:00 AM	11:00 AM	11:00 AM	8:00 AM	6:00 AM	12:00 AM	8:00 AM	11:00 AM		
	0	0	0	0	1	1	10	23	23	15	5	1	0	1	76		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	12:00 PM	9:00 PM	12:00 PM	12:00 PM	2:00 PM	4:00 PM	5:00 PM	4:00 PM	7:00 PM	8:00 PM	4:00 PM	12:00 PM		
	0	0	0	0	1	4	3	25	28	17	9	3	1	1	64		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 13 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	1	2	1	1	1	0	6	56-65	3
01:00 AM	0	0	0	0	0	0	0	0	1	2	0	0	0	0	3	51-60	3
02:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	2	46-55	1
05:00 AM	0	0	0	0	0	0	0	1	1	3	1	2	0	0	8	53-62	4
06:00 AM	0	0	0	0	0	0	1	0	1	4	2	0	0	0	8	56-65	6
07:00 AM	0	0	0	0	0	0	0	1	3	3	4	1	1	0	13	56-65	7
08:00 AM	0	0	0	1	0	1	1	7	6	10	0	1	2	0	29	51-60	16
09:00 AM	0	0	0	0	0	0	2	5	8	3	1	1	0	0	20	46-55	13
10:00 AM	0	0	0	0	0	0	0	6	10	8	4	3	1	0	32	51-60	18
11:00 AM	0	0	0	0	0	2	2	4	20	7	4	1	0	0	40	51-60	27
12:00 PM	0	0	0	0	1	0	3	12	22	17	6	2	0	0	63	51-60	39
01:00 PM	0	0	0	0	0	3	3	15	17	20	6	3	0	0	67	51-60	37
02:00 PM	1	0	0	0	0	2	4	24	32	13	5	3	1	1	86	46-55	56
03:00 PM	2	0	0	0	0	0	1	18	20	18	3	1	0	0	63	50-59	38
04:00 PM	0	0	0	0	0	1	8	11	23	16	10	3	0	0	72	51-60	39
05:00 PM	0	0	0	0	2	1	4	16	28	11	9	3	1	0	75	46-55	44
06:00 PM	2	0	0	0	1	0	0	7	12	11	7	5	1	0	46	51-60	23
07:00 PM	0	0	0	0	0	0	0	6	7	12	9	6	0	0	40	56-65	21
08:00 PM	0	0	0	0	0	0	0	3	7	12	4	3	0	0	29	51-60	19
09:00 PM	0	0	0	0	0	1	0	4	10	3	3	0	0	0	21	46-55	14
10:00 PM	0	0	0	0	0	0	0	1	3	6	3	1	0	0	14	51-60	9
11:00 PM	0	0	0	0	0	0	0	0	4	1	2	0	0	0	7	51-60	5
Day Total	5	0	0	1	4	11	29	142	238	182	84	41	8	1	746	51-60	420
Percent	0.7%	0%	0%	0.1%	0.5%	1.5%	3.9%	19%	31.9%	24.4%	11.3%	5.5%	1.1%	0.1%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	11:00 AM	9:00 AM	8:00 AM	11:00 AM	8:00 AM	7:00 AM	10:00 AM	8:00 AM	12:00 AM	11:00 AM		
	0	0	0	1	0	2	2	7	20	10	4	3	2	0	40		
PM Peak Volume	3:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM	1:00 PM	4:00 PM	2:00 PM	2:00 PM	1:00 PM	4:00 PM	7:00 PM	2:00 PM	2:00 PM	2:00 PM		
	2	0	0	0	2	3	8	24	32	20	10	6	1	1	86		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL																QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 14 2022	
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	0	1	0	1	0	0	0	3	36-45	1
01:00 AM	0	0	0	0	0	0	0	0	2	0	1	0	0	0	3	46-55	2
02:00 AM	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2	46-55	1
03:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	0	2	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	1	1	1	0	0	0	3	51-60	2
05:00 AM	0	0	0	0	0	0	1	0	9	1	4	1	0	1	17	51-60	10
06:00 AM	0	0	0	0	0	0	0	2	6	11	16	7	2	6	50	56-65	27
07:00 AM	0	0	0	0	0	0	0	7	28	22	11	5	2	2	77	51-60	50
08:00 AM	1	0	0	0	0	0	1	5	30	23	13	4	0	1	78	51-60	53
09:00 AM	0	0	0	0	0	0	1	3	10	9	3	1	1	0	28	51-60	19
10:00 AM	0	0	0	1	0	1	2	6	11	12	5	0	1	0	39	51-60	23
11:00 AM	0	0	0	0	0	0	2	8	12	12	3	2	0	0	39	51-60	24
12:00 PM	0	0	0	1	0	0	5	4	19	7	2	0	0	0	38	51-60	26
01:00 PM	0	0	0	0	0	1	3	4	16	4	3	1	0	0	32	50-59	20
02:00 PM	0	0	0	0	0	0	2	10	8	6	5	3	0	1	35	46-55	18
03:00 PM	0	0	0	0	3	0	4	5	17	18	4	0	0	0	51	51-60	35
04:00 PM	0	0	0	0	0	0	4	6	27	19	7	5	1	0	69	51-60	46
05:00 PM	0	0	0	0	0	0	3	12	20	30	9	3	2	1	80	51-60	50
06:00 PM	0	0	0	0	0	0	1	14	26	28	10	3	2	1	85	51-60	54
07:00 PM	0	0	0	0	0	0	3	1	7	3	7	2	3	0	26	55-64	10
08:00 PM	0	0	0	0	0	0	0	4	9	9	6	2	0	0	30	51-60	18
09:00 PM	0	0	0	0	0	0	0	2	4	6	2	2	0	0	16	51-60	10
10:00 PM	0	0	0	0	0	0	1	0	6	2	2	1	0	0	12	51-60	8
11:00 PM	0	0	0	0	0	1	0	5	2	2	2	0	0	0	12	46-55	7
Day Total	1	0	0	2	3	3	34	99	272	225	119	42	14	13	827	51-60	497
Percent	0.1%	0%	0%	0.2%	0.4%	0.4%	4.1%	12%	32.9%	27.2%	14.4%	5.1%	1.7%	1.6%			
AM Peak Volume	8:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	10:00 AM	10:00 AM	11:00 AM	8:00 AM	8:00 AM	6:00 AM	6:00 AM	6:00 AM	6:00 AM	8:00 AM		
	1	0	0	1	0	1	2	8	30	23	16	7	2	6	78		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM	1:00 PM	12:00 PM	6:00 PM	4:00 PM	5:00 PM	6:00 PM	4:00 PM	7:00 PM	2:00 PM	6:00 PM		
	0	0	0	1	3	1	5	14	27	30	10	5	3	1	85		
Comments:																	

Report generated on 11/29/2022 11:08 AM

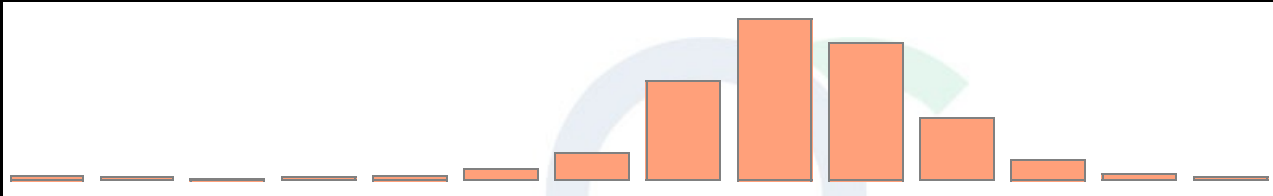
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: EB, WB DATE: Nov 15 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	0	1	1	1	0	0	4	56-65	2
01:00 AM	0	0	0	0	0	0	0	0	1	3	0	0	0	0	4	51-60	4
02:00 AM	0	0	0	0	0	1	1	1	0	1	0	0	0	0	4	36-45	2
03:00 AM	0	0	0	0	0	0	1	1	0	1	0	1	0	0	4	41-50	2
04:00 AM	0	0	0	0	0	0	0	1	2	1	0	1	0	0	5	51-60	3
05:00 AM	0	0	0	0	0	0	0	0	6	4	1	0	1	0	12	51-60	10
06:00 AM	0	0	0	0	0	0	1	0	6	22	18	3	0	4	54	56-65	40
07:00 AM	2	0	0	0	0	0	1	2	12	22	12	9	1	0	61	51-60	34
08:00 AM	0	0	0	0	0	0	0	15	22	27	11	1	2	0	78	51-60	49
09:00 AM	1	0	0	0	0	0	1	5	10	8	8	1	1	0	35	51-60	18
10:00 AM	0	0	0	0	2	0	3	7	11	12	3	2	0	0	40	51-60	23
11:00 AM	0	2	1	1	1	0	2	11	12	6	0	0	0	0	36	46-55	23
12:00 PM	0	2	0	0	3	2	4	9	10	8	2	0	0	0	40	46-55	19
01:00 PM	0	0	0	0	0	1	3	9	11	6	2	0	0	0	32	46-55	20
02:00 PM	0	0	0	1	0	0	4	17	14	9	3	2	0	0	50	46-55	31
03:00 PM	0	0	1	0	1	4	7	15	28	16	2	0	0	1	75	51-60	44
04:00 PM	0	0	0	0	0	4	7	21	25	17	5	1	1	0	81	46-55	46
05:00 PM	0	0	0	0	0	1	3	9	25	27	13	0	0	0	78	51-60	52
06:00 PM	0	0	0	0	0	0	2	12	32	22	10	2	1	0	81	51-60	54
07:00 PM	0	0	0	0	0	0	1	9	8	7	7	2	0	0	34	46-55	17
08:00 PM	0	0	0	1	0	0	4	7	11	9	4	1	0	0	37	51-60	20
09:00 PM	0	0	0	0	0	0	0	5	5	4	2	1	0	0	17	46-55	10
10:00 PM	0	0	0	0	1	0	0	2	4	3	8	1	1	0	20	56-65	11
11:00 PM	0	0	0	0	0	0	0	1	3	2	2	0	0	0	8	51-60	5
Day Total	3	4	2	3	8	13	45	160	258	238	114	29	8	5	890	51-60	496
Percent	0.3%	0.4%	0.2%	0.3%	0.9%	1.5%	5.1%	18%	29%	26.7%	12.8%	3.3%	0.9%	0.6%			
AM Peak Volume	7:00 AM 2	11:00 AM 2	11:00 AM 1	11:00 AM 1	10:00 AM 2	2:00 AM 1	10:00 AM 3	8:00 AM 15	8:00 AM 22	8:00 AM 27	6:00 AM 18	7:00 AM 9	8:00 AM 2	6:00 AM 4	8:00 AM 78		
PM Peak Volume	12:00 PM 0	12:00 PM 2	3:00 PM 1	2:00 PM 1	12:00 PM 3	3:00 PM 4	3:00 PM 7	4:00 PM 21	6:00 PM 32	5:00 PM 27	5:00 PM 13	2:00 PM 2	4:00 PM 1	3:00 PM 1	4:00 PM 81		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) John Coffee Memorial Bridge														QC JOB #: 15889225			
SPECIFIC LOCATION:														DIRECTION: EB, WB			
CITY/STATE: Colbert, AL														DATE: Nov 9 2022 - Nov 15 2022			
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	48	21	13	20	53	110	300	1106	1811	1554	688	234	59	29	6046	51-60	3365
Percent	0.8%	0.3%	0.2%	0.3%	0.9%	1.8%	5%	18.3%	30%	25.7%	11.4%	3.9%	1%	0.5%			
Cumulative Percent	0.8%	1.1%	1.4%	1.7%	2.6%	4.4%	9.3%	27.6%	57.6%	83.3%	94.7%	98.5%	99.5%	100%			
ADT 863															85th Percentile: 60 MPH Mean Speed(Average): 53 MPH Median: 53 MPH Mode: 53 MPH		
Comments:																	

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	1	4	0	0	0	0	0	0	0	0	0	0	0	5
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
04:00 AM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5
05:00 AM	0	9	9	0	0	0	0	0	0	0	0	0	0	0	18
06:00 AM	0	41	15	0	2	0	0	0	0	0	0	0	0	0	58
07:00 AM	0	42	25	0	4	0	0	1	0	0	0	0	0	0	72
08:00 AM	0	48	12	0	7	0	0	2	0	0	0	0	0	0	69
09:00 AM	0	20	11	0	6	2	0	0	0	0	0	0	0	0	39
10:00 AM	2	17	11	0	2	0	0	3	0	0	0	0	0	0	35
11:00 AM	2	26	12	0	4	6	0	2	0	0	0	0	0	5	57
12:00 PM	0	30	11	0	2	3	0	4	0	0	0	0	0	0	50
01:00 PM	2	29	12	0	11	5	0	0	0	0	0	0	0	1	60
02:00 PM	1	20	18	0	7	2	0	1	0	0	0	0	0	2	51
03:00 PM	2	34	25	0	9	6	0	1	0	0	0	0	0	3	80
04:00 PM	4	42	28	1	9	4	0	1	0	0	0	0	0	5	94
05:00 PM	2	50	24	0	7	5	0	3	0	0	0	0	0	1	92
06:00 PM	1	41	18	0	4	1	0	2	0	0	0	0	0	1	68
07:00 PM	0	22	11	0	4	0	0	0	0	0	0	0	0	0	37
08:00 PM	0	20	6	0	1	0	0	0	0	0	0	0	0	0	27
09:00 PM	0	11	8	0	2	0	0	0	0	0	0	0	0	0	21
10:00 PM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
11:00 PM	0	5	3	0	0	0	0	0	0	0	0	0	0	0	8
Day Total	16	522	269	1	81	34	0	20	0	0	0	0	0	18	961
Percent	1.7%	54.3%	28%	0.1%	8.4%	3.5%	0%	2.1%	0%	0%	0%	0%	0%	1.9%	
ADT 961															
AM Peak Volume	10:00 AM 2	8:00 AM 48	7:00 AM 25	12:00 AM 0	8:00 AM 7	11:00 AM 6	12:00 AM 0	10:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 5	7:00 AM 72
PM Peak Volume	4:00 PM 4	5:00 PM 50	4:00 PM 28	4:00 PM 1	1:00 PM 11	3:00 PM 6	12:00 PM 0	12:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 5	4:00 PM 94

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889225

DIRECTION: EB, WB

DATE: Nov 10 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
01:00 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	5	2	0	0	0	0	0	0	0	0	0	0	0	7
05:00 AM	0	7	5	0	1	0	0	0	0	0	0	0	0	0	13
06:00 AM	0	32	25	0	2	0	0	0	0	0	0	0	0	0	59
07:00 AM	0	40	28	0	7	0	0	0	0	0	0	0	0	0	75
08:00 AM	0	40	19	0	7	0	0	8	0	0	0	0	0	1	75
09:00 AM	1	27	15	0	10	1	0	2	0	1	0	0	0	0	57
10:00 AM	0	21	16	0	5	1	0	0	0	0	0	0	0	2	45
11:00 AM	10	20	10	0	9	0	0	1	0	1	0	0	0	1	52
12:00 PM	3	24	13	1	2	0	0	2	0	0	0	0	0	1	46
01:00 PM	3	24	14	1	4	0	0	4	0	0	0	0	0	1	51
02:00 PM	2	22	15	0	4	0	0	1	0	0	0	0	0	0	44
03:00 PM	3	40	15	0	4	0	0	1	0	0	0	0	0	0	63
04:00 PM	2	47	19	0	4	0	0	3	0	0	0	0	0	1	76
05:00 PM	2	50	30	0	2	0	0	4	0	0	0	0	0	0	88
06:00 PM	0	39	31	0	6	0	0	0	0	0	0	0	0	0	76
07:00 PM	0	25	11	0	3	0	0	0	0	0	0	0	0	0	39
08:00 PM	1	25	10	0	1	0	0	1	0	0	0	0	0	0	38
09:00 PM	0	4	8	0	3	0	0	0	0	0	0	0	0	0	15
10:00 PM	0	12	4	0	1	0	0	0	0	0	0	0	0	0	17
11:00 PM	0	5	0	0	1	0	0	0	0	0	0	0	0	0	6
Day Total	27	519	291	2	76	2	0	27	0	2	0	0	0	7	953
Percent	2.8%	54.5%	30.5%	0.2%	8%	0.2%	0%	2.8%	0%	0.2%	0%	0%	0%	0.7%	
ADT 953															
AM Peak Volume	11:00 AM	7:00 AM	7:00 AM	12:00 AM	9:00 AM	9:00 AM	12:00 AM	8:00 AM	12:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	7:00 AM
	10	40	28	0	10	1	0	8	0	1	0	0	0	2	75
PM Peak Volume	12:00 PM	5:00 PM	6:00 PM	12:00 PM	6:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM
	3	50	31	1	6	0	0	4	0	0	0	0	0	1	88

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 11 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
05:00 AM	0	9	8	0	0	0	0	0	0	0	0	0	0	0	17
06:00 AM	0	28	14	0	4	0	0	0	0	0	0	0	0	0	46
07:00 AM	0	26	16	0	3	0	0	0	0	0	0	0	0	0	45
08:00 AM	0	43	15	0	3	0	0	1	0	0	0	0	0	0	62
09:00 AM	0	22	8	0	3	0	0	1	0	0	0	0	0	1	35
10:00 AM	1	22	7	0	1	0	0	3	0	0	0	0	0	0	34
11:00 AM	0	19	13	2	6	0	0	4	0	0	0	0	0	0	44
12:00 PM	0	27	14	0	1	0	0	0	0	0	0	0	0	0	42
01:00 PM	7	34	16	0	1	0	0	3	0	0	0	0	0	0	61
02:00 PM	3	35	13	0	5	0	0	2	0	0	0	0	0	1	59
03:00 PM	1	45	15	0	3	0	0	4	0	0	0	0	0	0	68
04:00 PM	4	71	21	0	2	0	0	2	0	0	0	0	0	1	101
05:00 PM	1	63	32	0	4	0	0	4	0	0	0	0	0	1	105
06:00 PM	0	39	18	0	4	0	0	1	0	0	0	0	0	0	62
07:00 PM	0	31	14	0	1	0	0	0	0	0	0	0	0	0	46
08:00 PM	0	25	11	0	2	0	0	1	0	0	0	0	0	0	39
09:00 PM	0	16	4	0	0	0	0	1	0	0	0	0	0	0	21
10:00 PM	0	15	1	0	1	0	0	1	0	0	0	0	0	0	18
11:00 PM	0	6	3	0	0	0	0	0	0	0	0	0	0	0	9
Day Total	17	588	249	2	44	0	0	28	0	0	0	0	0	4	932
Percent	1.8%	63.1%	26.7%	0.2%	4.7%	0%	0%	3%	0%	0%	0%	0%	0%	0.4%	
ADT 932															
AM Peak Volume	10:00 AM 1	8:00 AM 43	7:00 AM 16	11:00 AM 2	11:00 AM 6	12:00 AM 0	12:00 AM 0	11:00 AM 4	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	8:00 AM 62
PM Peak Volume	1:00 PM 7	4:00 PM 71	5:00 PM 32	12:00 PM 0	2:00 PM 5	12:00 PM 0	12:00 PM 0	3:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	5:00 PM 105

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	6	1	0	0	0	0	0	0	0	0	0	0	0	7
02:00 AM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	5	4	0	0	0	0	0	0	0	0	0	0	0	9
06:00 AM	0	11	11	0	2	0	0	0	0	0	0	0	0	0	24
07:00 AM	0	14	2	0	0	0	0	1	0	0	0	0	0	0	17
08:00 AM	0	18	10	0	1	0	0	1	0	0	0	0	0	0	30
09:00 AM	0	28	9	0	0	0	0	0	0	0	0	0	0	0	37
10:00 AM	1	18	11	0	0	0	0	0	0	0	0	0	0	0	30
11:00 AM	16	36	18	0	4	0	0	2	0	0	0	0	0	0	76
12:00 PM	0	37	21	0	5	0	0	1	0	0	0	0	0	0	64
01:00 PM	0	25	18	0	5	0	0	2	0	0	0	0	0	0	50
02:00 PM	6	39	10	0	2	0	0	0	0	0	0	0	0	0	57
03:00 PM	0	44	11	0	3	0	0	0	0	0	0	0	0	0	58
04:00 PM	0	32	24	1	4	0	0	2	0	0	0	0	0	0	63
05:00 PM	0	36	15	0	2	0	0	1	0	0	0	0	0	0	54
06:00 PM	0	24	17	0	2	0	0	0	0	0	0	0	0	0	43
07:00 PM	0	30	15	0	2	0	0	0	0	0	0	0	0	0	47
08:00 PM	0	15	10	0	2	0	0	0	0	0	0	0	0	0	27
09:00 PM	0	12	2	0	1	0	0	0	0	0	0	0	0	0	15
10:00 PM	0	6	5	0	0	0	0	0	0	0	0	0	0	0	11
11:00 PM	0	4	3	0	0	0	0	0	0	0	0	0	0	0	7
Day Total	23	448	220	1	35	0	0	10	0	0	0	0	0	0	737
Percent	3.1%	60.8%	29.9%	0.1%	4.7%	0%	0%	1.4%	0%	0%	0%	0%	0%	0%	
ADT 737															
AM Peak Volume	11:00 AM	11:00 AM	11:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	16	36	18	0	4	0	0	2	0	0	0	0	0	0	76
PM Peak Volume	2:00 PM	3:00 PM	4:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	6	44	24	1	5	0	0	2	0	0	0	0	0	0	64

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 13 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
01:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
06:00 AM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
07:00 AM	0	11	2	0	0	0	0	0	0	0	0	0	0	0	13
08:00 AM	0	14	10	0	4	0	0	1	0	0	0	0	0	0	29
09:00 AM	0	12	6	0	0	0	0	2	0	0	0	0	0	0	20
10:00 AM	0	17	10	0	2	0	0	3	0	0	0	0	0	0	32
11:00 AM	0	23	9	2	3	0	0	3	0	0	0	0	0	0	40
12:00 PM	0	35	22	0	2	0	0	4	0	0	0	0	0	0	63
01:00 PM	0	48	11	0	8	0	0	0	0	0	0	0	0	0	67
02:00 PM	0	49	25	1	7	0	0	3	0	0	0	0	0	1	86
03:00 PM	0	36	20	0	2	0	0	3	0	0	0	0	0	2	63
04:00 PM	0	47	18	1	3	0	0	3	0	0	0	0	0	0	72
05:00 PM	0	47	23	0	4	0	0	1	0	0	0	0	0	0	75
06:00 PM	0	25	13	0	6	0	0	0	0	0	0	0	0	2	46
07:00 PM	0	25	12	0	2	0	0	1	0	0	0	0	0	0	40
08:00 PM	0	22	5	0	1	0	0	1	0	0	0	0	0	0	29
09:00 PM	0	12	8	0	0	0	0	1	0	0	0	0	0	0	21
10:00 PM	0	10	2	0	1	0	0	1	0	0	0	0	0	0	14
11:00 PM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
Day Total	0	454	211	4	45	0	0	27	0	0	0	0	0	5	746
Percent	0%	60.9%	28.3%	0.5%	6%	0%	0%	3.6%	0%	0%	0%	0%	0%	0.7%	
ADT 746															
AM Peak Volume	12:00 AM 0	11:00 AM 23	8:00 AM 10	11:00 AM 2	8:00 AM 4	12:00 AM 0	12:00 AM 0	10:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 40
PM Peak Volume	12:00 PM 0	2:00 PM 49	2:00 PM 25	2:00 PM 1	1:00 PM 8	12:00 PM 0	12:00 PM 0	12:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 2	2:00 PM 86

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889225

DIRECTION: EB, WB

DATE: Nov 14 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
05:00 AM	0	10	7	0	0	0	0	0	0	0	0	0	0	0	17
06:00 AM	0	24	17	0	8	0	0	1	0	0	0	0	0	0	50
07:00 AM	0	38	32	0	7	0	0	0	0	0	0	0	0	0	77
08:00 AM	0	49	22	0	4	0	0	2	0	0	0	0	0	1	78
09:00 AM	0	15	10	0	1	0	0	2	0	0	0	0	0	0	28
10:00 AM	0	19	17	0	1	0	0	2	0	0	0	0	0	0	39
11:00 AM	0	19	16	0	2	0	0	2	0	0	0	0	0	0	39
12:00 PM	0	21	14	0	3	0	0	0	0	0	0	0	0	0	38
01:00 PM	0	16	12	0	4	0	0	0	0	0	0	0	0	0	32
02:00 PM	0	22	10	0	1	0	0	2	0	0	0	0	0	0	35
03:00 PM	0	31	17	0	2	0	0	1	0	0	0	0	0	0	51
04:00 PM	0	44	20	0	4	0	0	1	0	0	0	0	0	0	69
05:00 PM	0	50	26	0	4	0	0	0	0	0	0	0	0	0	80
06:00 PM	0	52	33	0	0	0	0	0	0	0	0	0	0	0	85
07:00 PM	0	17	7	0	2	0	0	0	0	0	0	0	0	0	26
08:00 PM	0	18	12	0	0	0	0	0	0	0	0	0	0	0	30
09:00 PM	0	9	7	0	0	0	0	0	0	0	0	0	0	0	16
10:00 PM	0	7	5	0	0	0	0	0	0	0	0	0	0	0	12
11:00 PM	0	8	4	0	0	0	0	0	0	0	0	0	0	0	12
Day Total	0	478	291	0	44	0	0	13	0	0	0	0	0	1	827
Percent	0%	57.8%	35.2%	0%	5.3%	0%	0%	1.6%	0%	0%	0%	0%	0%	0.1%	
ADT 827															
AM Peak Volume	12:00 AM	8:00 AM	7:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	8:00 AM
	0	49	32	0	8	0	0	2	0	0	0	0	0	1	78
PM Peak Volume	12:00 PM	6:00 PM	6:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	6:00 PM
	0	52	33	0	4	0	0	2	0	0	0	0	0	0	85

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4
01:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
02:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
03:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
04:00 AM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
05:00 AM	0	7	5	0	0	0	0	0	0	0	0	0	0	0	12
06:00 AM	0	26	17	0	9	0	0	2	0	0	0	0	0	0	54
07:00 AM	0	35	16	0	6	0	0	2	0	0	0	0	0	2	61
08:00 AM	0	57	19	0	2	0	0	0	0	0	0	0	0	0	78
09:00 AM	0	19	7	0	7	0	0	2	0	0	0	0	0	0	35
10:00 AM	0	21	15	1	1	0	0	2	0	0	0	0	0	0	40
11:00 AM	0	20	10	0	4	0	0	2	0	0	0	0	0	0	36
12:00 PM	1	18	14	2	4	0	0	1	0	0	0	0	0	0	40
01:00 PM	0	16	13	0	2	0	0	1	0	0	0	0	0	0	32
02:00 PM	0	32	15	0	2	0	0	1	0	0	0	0	0	0	50
03:00 PM	0	38	26	0	7	0	0	4	0	0	0	0	0	0	75
04:00 PM	0	47	28	0	4	0	0	2	0	0	0	0	0	0	81
05:00 PM	0	53	21	1	3	0	0	0	0	0	0	0	0	0	78
06:00 PM	0	56	23	0	2	0	0	0	0	0	0	0	0	0	81
07:00 PM	0	21	12	0	1	0	0	0	0	0	0	0	0	0	34
08:00 PM	0	23	13	0	1	0	0	0	0	0	0	0	0	0	37
09:00 PM	0	11	5	0	1	0	0	0	0	0	0	0	0	0	17
10:00 PM	0	16	3	0	1	0	0	0	0	0	0	0	0	0	20
11:00 PM	0	5	3	0	0	0	0	0	0	0	0	0	0	0	8
Day Total	1	534	273	4	57	0	0	19	0	0	0	0	0	2	890
Percent	0.1%	60%	30.7%	0.4%	6.4%	0%	0%	2.1%	0%	0%	0%	0%	0%	0.2%	
ADT 890															
AM Peak Volume	12:00 AM 0	8:00 AM 57	8:00 AM 19	10:00 AM 1	6:00 AM 9	12:00 AM 0	12:00 AM 0	6:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 2	8:00 AM 78
PM Peak Volume	12:00 PM 1	6:00 PM 56	4:00 PM 28	12:00 PM 2	3:00 PM 7	12:00 PM 0	12:00 PM 0	3:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 81

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge


QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	84	3543	1804	14	382	36	0	144	0	2	0	0	0	37	6046
Percent	1.4%	58.6%	29.8%	0.2%	6.3%	0.6%	0%	2.4%	0%	0%	0%	0%	0%	0.6%	
ADT 863															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) John Coffee Memorial Bridge									QC JOB #: 15889225	
SPECIFIC LOCATION:									DIRECTION: EB, WB	
CITY/STATE: Colbert, AL									DATE: Nov 9 2022 - Nov 15 2022	
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			5	4	3	4	2	6	4	<div><div></div></div>
01:00 AM			3	4	3	3	7	3	4	<div><div></div></div>
02:00 AM			2	1	3	2	5	1	2	<div><div></div></div>
03:00 AM			4	2	2	3	2	1	2	<div><div></div></div>
04:00 AM			5	7	7	6	2	2	5	<div><div></div></div>
05:00 AM			18	13	17	16	9	8	13	<div><div></div></div>
06:00 AM			58	59	46	54	24	8	39	<div><div></div></div>
07:00 AM			72	75	45	64	17	13	44	<div><div></div></div>
08:00 AM			69	75	62	69	30	29	53	<div><div></div></div>
09:00 AM			39	57	35	44	37	20	38	<div><div></div></div>
10:00 AM			35	45	34	38	30	32	35	<div><div></div></div>
11:00 AM			57	52	44	51	76	40	54	<div><div></div></div>
12:00 PM			50	46	42	46	64	63	53	<div><div></div></div>
01:00 PM			60	51	61	57	50	67	58	<div><div></div></div>
02:00 PM			51	44	59	51	57	86	59	<div><div></div></div>
03:00 PM			80	63	68	70	58	63	66	<div><div></div></div>
04:00 PM			94	76	101	90	63	72	81	<div><div></div></div>
05:00 PM			92	88	105	95	54	75	83	<div><div></div></div>
06:00 PM			68	76	62	69	43	46	59	<div><div></div></div>
07:00 PM			37	39	46	41	47	40	42	<div><div></div></div>
08:00 PM			27	38	39	35	27	29	32	<div><div></div></div>
09:00 PM			21	15	21	19	15	21	19	<div><div></div></div>
10:00 PM			6	17	18	14	11	14	13	<div><div></div></div>
11:00 PM			8	6	9	8	7	7	7	<div><div></div></div>
Day Total			961	953	932	949	737	746	865	
% Weekday Average			101.3%	100.4%	98.2%					
% Week Average			111.1%	110.2%	107.7%	109.7%	85.2%	86.2%		
AM Peak Volume			7:00 AM 72	7:00 AM 75	8:00 AM 62	8:00 AM 69	11:00 AM 76	11:00 AM 40	11:00 AM 54	
PM Peak Volume			4:00 PM 94	5:00 PM 88	5:00 PM 105	5:00 PM 95	12:00 PM 64	2:00 PM 86	5:00 PM 83	
Comments:										

Report generated on 11/29/2022 11:07 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) John Coffee Memorial Bridge										QC JOB #: 15889225	
SPECIFIC LOCATION:										DIRECTION: EB, WB	
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022	
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile	
12:00 AM	3	4				4			4	<div></div>	
01:00 AM	3	4				4			4	<div></div>	
02:00 AM	2	4				3			3	<div></div>	
03:00 AM	2	4				3			3	<div></div>	
04:00 AM	3	5				4			4	<div></div>	
05:00 AM	17	12				15			15	<div></div>	
06:00 AM	50	54				52			52	<div></div>	
07:00 AM	77	61				69			69	<div></div>	
08:00 AM	78	78				78			78	<div></div>	
09:00 AM	28	35				32			32	<div></div>	
10:00 AM	39	40				40			40	<div></div>	
11:00 AM	39	36				38			38	<div></div>	
12:00 PM	38	40				39			39	<div></div>	
01:00 PM	32	32				32			32	<div></div>	
02:00 PM	35	50				43			43	<div></div>	
03:00 PM	51	75				63			63	<div></div>	
04:00 PM	69	81				75			75	<div></div>	
05:00 PM	80	78				79			79	<div></div>	
06:00 PM	85	81				83			83	<div></div>	
07:00 PM	26	34				30			30	<div></div>	
08:00 PM	30	37				34			34	<div></div>	
09:00 PM	16	17				17			17	<div></div>	
10:00 PM	12	20				16			16	<div></div>	
11:00 PM	12	8				10			10	<div></div>	
Day Total	827	890				863			863		
% Weekday Average	95.8%	103.1%									
% Week Average	95.8%	103.1%				100%					
AM Peak Volume	8:00 AM 78	8:00 AM 78				8:00 AM 78			8:00 AM 78		
PM Peak Volume	6:00 PM 85	4:00 PM 81				6:00 PM 83			6:00 PM 83		
Comments:											

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: WB DATE: Nov 9 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	1	0	1	0	0	0	0	0	0	2	31-40	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1
03:00 AM	0	0	0	0	0	0	0	1	0	0	2	0	0	0	3	56-65	2
04:00 AM	0	0	0	0	0	0	0	1	0	2	0	0	0	0	3	51-60	2
05:00 AM	0	0	0	0	0	0	0	0	1	7	0	0	0	0	8	51-60	8
06:00 AM	0	0	0	0	0	0	0	2	16	20	5	5	1	0	49	51-60	36
07:00 AM	0	0	0	0	0	0	0	4	13	14	9	7	0	1	48	51-60	27
08:00 AM	0	0	0	0	0	0	1	4	9	11	5	0	0	0	30	51-60	20
09:00 AM	0	0	0	0	0	0	1	3	9	7	1	0	0	0	21	51-60	16
10:00 AM	0	0	0	0	0	1	2	8	5	2	0	0	0	0	18	46-55	13
11:00 AM	5	0	0	0	1	2	2	2	7	5	1	0	0	0	25	51-60	12
12:00 PM	0	0	0	0	0	0	10	14	1	1	1	0	0	0	27	41-50	24
01:00 PM	2	0	0	0	1	7	3	12	5	1	0	0	0	0	31	46-55	17
02:00 PM	1	0	0	0	1	0	4	8	7	0	0	1	0	0	22	46-55	15
03:00 PM	6	1	0	1	0	3	6	12	8	1	0	1	0	0	39	46-55	20
04:00 PM	6	6	2	1	0	3	2	13	7	2	0	0	0	0	42	46-55	20
05:00 PM	0	0	0	0	0	5	6	16	6	4	0	1	0	0	38	41-50	22
06:00 PM	0	0	0	0	0	2	1	14	8	2	2	0	0	0	29	46-55	22
07:00 PM	0	0	0	0	0	0	1	3	10	11	1	0	0	1	27	51-60	21
08:00 PM	0	0	0	0	0	0	0	2	3	3	0	0	0	0	8	51-60	6
09:00 PM	0	0	0	0	0	1	0	4	1	5	1	0	0	0	12	54-63	6
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	66-75	1
11:00 PM	0	0	0	0	0	0	0	1	1	0	0	0	0	0	2	46-55	2
Day Total	20	7	2	2	3	25	39	125	117	98	29	15	2	2	486	46-55	242
Percent	4.1%	1.4%	0.4%	0.4%	0.6%	5.1%	8%	25.7%	24.1%	20.2%	6%	3.1%	0.4%	0.4%			
AM Peak Volume	11:00 AM 5	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 1	11:00 AM 2	10:00 AM 2	10:00 AM 8	9:00 AM 16	8:00 AM 20	7:00 AM 9	7:00 AM 7	6:00 AM 1	5:00 AM 1	49		
PM Peak Volume	3:00 PM 6	4:00 PM 6	4:00 PM 2	3:00 PM 1	1:00 PM 1	1:00 PM 7	12:00 PM 10	5:00 PM 16	7:00 PM 10	7:00 PM 11	6:00 PM 2	2:00 PM 1	10:00 PM 1	7:00 PM 1	42		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: WB DATE: Nov 10 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	1	1	0	0	0	0	0	2	46-55	2
01:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
04:00 AM	0	0	0	0	0	0	0	1	1	0	0	1	0	0	3	46-55	2
05:00 AM	0	0	0	0	0	0	1	0	1	3	2	0	0	0	7	56-65	5
06:00 AM	0	0	0	0	0	0	0	3	15	16	10	5	1	0	50	51-60	31
07:00 AM	0	0	0	0	0	0	1	3	21	14	9	2	1	0	51	51-60	35
08:00 AM	0	0	0	0	0	0	1	6	9	12	5	1	0	0	34	51-60	21
09:00 AM	0	0	0	2	2	1	7	7	7	5	2	1	0	0	34	41-50	14
10:00 AM	2	0	0	0	0	1	1	3	3	2	1	0	0	0	13	46-55	6
11:00 AM	2	0	0	0	2	1	6	4	4	3	1	0	0	0	23	41-50	10
12:00 PM	1	5	0	0	1	0	0	3	8	3	2	0	1	0	24	46-55	11
01:00 PM	1	0	0	0	0	0	2	5	10	5	0	0	0	0	23	46-55	15
02:00 PM	0	0	0	0	0	0	2	10	7	4	2	0	0	0	25	46-55	17
03:00 PM	0	0	0	0	0	1	1	7	11	1	2	1	0	0	24	46-55	18
04:00 PM	1	0	0	0	0	0	0	10	12	6	1	1	0	0	31	46-55	22
05:00 PM	0	0	0	0	0	1	2	16	8	12	2	1	0	0	42	46-55	24
06:00 PM	0	0	0	0	0	1	1	7	12	9	3	1	0	0	34	51-60	21
07:00 PM	0	0	0	0	0	0	1	2	7	11	2	1	0	0	24	51-60	18
08:00 PM	0	0	0	0	0	0	1	7	5	6	1	0	0	0	20	46-55	12
09:00 PM	0	0	0	0	0	0	0	0	1	3	1	1	0	0	6	53-62	4
10:00 PM	0	0	0	0	0	0	2	3	1	0	0	1	0	0	7	41-50	5
11:00 PM	0	0	0	0	0	0	0	0	2	0	1	0	0	0	3	46-55	2
Day Total	7	5	0	2	5	6	29	100	146	115	47	17	3	0	482	51-60	261
Percent	1.5%	1%	0%	0.4%	1%	1.2%	6%	20.7%	30.3%	23.9%	9.8%	3.5%	0.6%	0%			
AM Peak Volume	10:00 AM 2	12:00 AM 0	12:00 AM 0	9:00 AM 2	9:00 AM 2	9:00 AM 1	9:00 AM 7	9:00 AM 7	7:00 AM 21	6:00 AM 16	6:00 AM 10	6:00 AM 5	6:00 AM 1	12:00 AM 0	7:00 AM 51		
PM Peak Volume	12:00 PM 1	12:00 PM 5	12:00 PM 0	12:00 PM 0	12:00 PM 1	3:00 PM 1	1:00 PM 2	5:00 PM 16	4:00 PM 12	5:00 PM 12	6:00 PM 3	3:00 PM 1	12:00 PM 1	12:00 PM 0	5:00 PM 42		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: WB DATE: Nov 11 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	46-55	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	2	61-70	2
05:00 AM	0	0	0	0	0	0	0	3	3	1	0	0	0	0	7	46-55	6
06:00 AM	0	0	0	0	0	0	2	3	6	13	10	4	0	1	39	56-65	23
07:00 AM	0	0	0	0	0	0	1	2	9	11	4	0	1	1	29	51-60	20
08:00 AM	0	0	0	0	0	0	2	5	9	9	0	1	0	0	26	51-60	18
09:00 AM	0	0	0	0	0	0	3	5	5	6	1	0	0	0	20	51-60	11
10:00 AM	0	0	0	0	0	0	3	4	6	4	0	0	0	0	17	48-57	10
11:00 AM	0	0	0	0	0	1	1	5	3	6	2	0	0	0	18	51-60	9
12:00 PM	0	0	0	0	1	0	2	4	9	2	1	0	0	0	19	46-55	13
01:00 PM	0	0	0	0	1	1	5	7	5	2	5	0	0	0	26	41-50	12
02:00 PM	1	0	0	0	0	1	4	7	12	3	1	1	0	0	30	46-55	19
03:00 PM	0	0	0	0	0	0	0	15	14	4	2	0	0	0	35	46-55	29
04:00 PM	0	0	0	0	0	2	5	18	21	6	3	0	0	0	55	46-55	39
05:00 PM	1	0	0	0	0	1	5	10	20	4	6	0	0	0	47	46-55	30
06:00 PM	0	0	0	0	1	0	2	4	12	5	1	1	1	0	27	51-60	17
07:00 PM	0	0	0	0	0	0	1	5	7	7	4	2	0	0	26	51-60	14
08:00 PM	0	0	0	0	0	0	2	2	4	6	0	1	0	0	15	51-60	10
09:00 PM	0	0	0	0	0	0	0	0	1	1	0	2	0	0	4	61-70	2
10:00 PM	0	0	0	0	0	1	0	2	2	2	2	1	0	0	10	46-55	4
11:00 PM	0	0	0	0	0	0	0	1	1	2	0	0	0	0	4	51-60	3
Day Total	2	0	0	0	3	7	38	102	150	94	43	14	2	2	457	46-55	252
Percent	0.4%	0%	0%	0%	0.7%	1.5%	8.3%	22.3%	32.8%	20.6%	9.4%	3.1%	0.4%	0.4%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	9:00 AM	8:00 AM	7:00 AM	6:00 AM	6:00 AM	6:00 AM	7:00 AM	6:00 AM	6:00 AM		
	0	0	0	0	0	1	3	5	9	13	10	4	1	1	39		
PM Peak Volume	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	1:00 PM	4:00 PM	4:00 PM	7:00 PM	5:00 PM	7:00 PM	6:00 PM	12:00 PM	4:00 PM		
	1	0	0	0	1	2	5	18	21	7	6	2	1	0	55		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: WB DATE: Nov 12 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
01:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
02:00 AM	0	0	0	0	0	0	1	1	1	0	0	0	0	0	3	41-50	2
03:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
04:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
05:00 AM	0	0	0	0	0	0	1	0	4	1	0	0	0	0	6	51-60	5
06:00 AM	0	0	0	0	0	0	0	3	2	7	1	1	0	0	14	51-60	9
07:00 AM	0	0	0	0	0	1	0	2	4	5	0	0	0	0	12	51-60	9
08:00 AM	0	0	0	0	0	0	0	2	6	2	0	0	0	0	10	48-57	8
09:00 AM	0	0	0	0	0	0	3	5	3	8	1	0	0	0	20	51-60	11
10:00 AM	0	0	0	0	0	0	0	9	3	2	1	0	0	0	15	46-55	12
11:00 AM	0	0	0	0	0	0	7	19	14	5	0	0	0	0	45	46-55	33
12:00 PM	0	0	0	0	0	2	2	10	9	5	1	0	0	0	29	46-55	19
01:00 PM	0	0	0	0	0	1	1	11	3	6	2	0	0	0	24	46-55	14
02:00 PM	0	0	0	0	0	0	0	13	10	7	2	0	0	0	32	46-55	23
03:00 PM	0	0	0	0	0	2	1	11	12	6	1	0	0	0	33	46-55	23
04:00 PM	0	0	0	0	0	1	0	7	11	5	4	0	0	0	28	46-55	18
05:00 PM	0	0	0	0	0	0	0	7	9	9	1	0	0	0	26	51-60	18
06:00 PM	0	0	0	0	0	0	1	1	7	6	0	1	0	0	16	51-60	13
07:00 PM	0	0	0	0	0	0	0	2	12	7	2	2	0	0	25	51-60	19
08:00 PM	0	0	0	0	0	0	0	2	3	3	2	2	0	0	12	51-60	6
09:00 PM	0	0	0	0	1	0	1	2	2	2	1	0	0	0	9	46-55	4
10:00 PM	0	0	0	0	0	0	1	0	1	3	0	0	0	0	5	51-60	4
11:00 PM	0	0	0	0	0	0	0	1	1	1	0	0	0	0	3	46-55	2
Day Total	0	0	0	0	1	7	19	110	117	92	19	6	0	0	371	46-55	227
Percent	0%	0%	0%	0%	0.3%	1.9%	5.1%	29.6%	31.5%	24.8%	5.1%	1.6%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM	11:00 AM	11:00 AM	11:00 AM	9:00 AM	6:00 AM	6:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	0	0	0	0	1	7	19	14	8	1	1	0	0	45		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	12:00 PM	9:00 PM	12:00 PM	12:00 PM	2:00 PM	3:00 PM	5:00 PM	4:00 PM	7:00 PM	12:00 PM	12:00 PM	3:00 PM		
	0	0	0	0	1	2	2	13	12	9	4	2	0	0	33		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: WB DATE: Nov 13 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	1	1	1	0	0	0	3	51-60	2
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	1	0	2	0	0	3	61-70	2
06:00 AM	0	0	0	0	0	0	0	0	1	3	2	0	0	0	6	56-65	5
07:00 AM	0	0	0	0	0	0	0	0	2	3	1	0	0	0	6	51-60	5
08:00 AM	0	0	0	1	0	1	0	3	1	4	0	0	0	0	10	51-60	5
09:00 AM	0	0	0	0	0	0	2	3	4	1	0	0	0	0	10	46-55	7
10:00 AM	0	0	0	0	0	0	0	3	5	2	2	2	0	0	14	46-55	8
11:00 AM	0	0	0	0	0	1	1	2	6	2	0	1	0	0	13	48-57	8
12:00 PM	0	0	0	0	0	0	1	8	12	11	0	1	0	0	33	51-60	23
01:00 PM	0	0	0	0	0	0	1	10	9	12	1	0	0	0	33	51-60	21
02:00 PM	0	0	0	0	0	1	3	10	15	7	2	1	0	1	40	46-55	25
03:00 PM	1	0	0	0	0	0	0	9	12	7	1	1	0	0	31	46-55	21
04:00 PM	0	0	0	0	0	1	7	7	9	3	4	1	0	0	32	46-55	16
05:00 PM	0	0	0	0	1	1	1	11	9	5	5	1	1	0	35	46-55	20
06:00 PM	1	0	0	0	1	0	0	4	7	6	4	5	0	0	28	51-60	13
07:00 PM	0	0	0	0	0	0	0	1	4	7	4	4	0	0	20	54-63	11
08:00 PM	0	0	0	0	0	0	0	2	5	3	0	0	0	0	10	51-60	8
09:00 PM	0	0	0	0	0	0	0	2	7	0	1	0	0	0	10	46-55	9
10:00 PM	0	0	0	0	0	0	0	0	1	3	1	1	0	0	6	53-62	4
11:00 PM	0	0	0	0	0	0	0	0	1	1	1	0	0	0	3	51-60	2
Day Total	2	0	0	1	2	5	16	76	111	82	30	20	1	1	347	51-60	193
Percent	0.6%	0%	0%	0.3%	0.6%	1.4%	4.6%	21.9%	32%	23.6%	8.6%	5.8%	0.3%	0.3%			
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	8:00 AM	12:00 AM	8:00 AM	9:00 AM	8:00 AM	11:00 AM	8:00 AM	6:00 AM	5:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	0	0	1	0	1	2	3	6	4	2	2	0	0	14		
PM Peak Volume	3:00 PM	12:00 PM	12:00 PM	12:00 PM	5:00 PM	2:00 PM	4:00 PM	5:00 PM	2:00 PM	1:00 PM	5:00 PM	6:00 PM	5:00 PM	2:00 PM	2:00 PM		
	1	0	0	0	1	1	7	11	15	12	5	5	1	1	40		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889225

DIRECTION: WB

DATE: Nov 14 2022

Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	36-45	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1
02:00 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1
03:00 AM	0	0	0	0	0	0	0	1	0	0	1	0	0	0	2	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	56-65	1
05:00 AM	0	0	0	0	0	0	1	0	3	1	3	1	0	0	9	51-60	4
06:00 AM	0	0	0	0	0	0	0	0	6	10	13	6	2	6	43	56-65	23
07:00 AM	0	0	0	0	0	0	0	6	15	15	9	4	1	2	52	51-60	30
08:00 AM	0	0	0	0	0	0	1	0	17	11	5	3	0	1	38	51-60	28
09:00 AM	0	0	0	0	0	0	0	2	5	5	1	0	0	0	13	51-60	10
10:00 AM	0	0	0	1	0	1	0	4	5	5	1	0	0	0	17	51-60	10
11:00 AM	0	0	0	0	0	0	1	2	3	4	2	1	0	0	13	51-60	7
12:00 PM	0	0	0	0	0	0	4	2	14	4	1	0	0	0	25	51-60	18
01:00 PM	0	0	0	0	0	1	2	4	8	3	3	1	0	0	22	46-55	12
02:00 PM	0	0	0	0	0	0	1	6	4	2	1	0	0	1	15	46-55	10
03:00 PM	0	0	0	0	3	0	2	2	6	5	1	0	0	0	19	51-60	11
04:00 PM	0	0	0	0	0	0	2	4	12	5	2	1	0	0	26	51-60	17
05:00 PM	0	0	0	0	0	0	1	9	7	7	0	2	2	0	28	46-55	16
06:00 PM	0	0	0	0	0	0	1	9	18	9	1	1	0	0	39	46-55	27
07:00 PM	0	0	0	0	0	0	3	1	4	1	5	2	1	0	17	61-70	7
08:00 PM	0	0	0	0	0	0	0	3	5	4	1	1	0	0	14	51-60	9
09:00 PM	0	0	0	0	0	0	0	0	3	4	0	1	0	0	8	51-60	7
10:00 PM	0	0	0	0	0	0	1	0	5	1	1	0	0	0	8	51-60	6
11:00 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	4	41-50	2
Day Total Percent	0 0%	0 0%	0 0%	1 0.2%	3 0.7%	2 0.5%	21 5%	57 13.7%	140 33.7%	98 23.6%	54 13%	24 5.8%	6 1.4%	10 2.4%	416	51-60	238
AM Peak Volume	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 1	12:00 AM 0	10:00 AM 1	12:00 AM 1	7:00 AM 6	8:00 AM 17	7:00 AM 15	6:00 AM 13	6:00 AM 6	6:00 AM 2	6:00 AM 6	7:00 AM 52		
PM Peak Volume	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 3	1:00 PM 1	12:00 PM 4	5:00 PM 9	6:00 PM 18	6:00 PM 9	7:00 PM 5	5:00 PM 2	5:00 PM 2	2:00 PM 1	6:00 PM 39		

Comments:

Report generated on 11/29/2022 11:08 AM

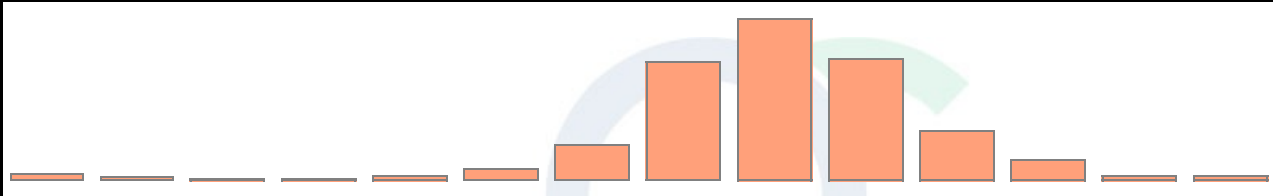
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) John Coffee Memorial Bridge SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889225 DIRECTION: WB DATE: Nov 15 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
01:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	51-60	1
02:00 AM	0	0	0	0	0	0	1	1	0	0	0	0	0	0	2	41-50	2
03:00 AM	0	0	0	0	0	0	0	1	0	0	0	1	0	0	2	41-50	1
04:00 AM	0	0	0	0	0	0	0	0	1	0	0	1	0	0	2	46-55	1
05:00 AM	0	0	0	0	0	0	0	0	3	2	1	0	1	0	7	51-60	5
06:00 AM	0	0	0	0	0	0	0	0	2	17	17	3	0	4	43	56-65	34
07:00 AM	1	0	0	0	0	0	1	1	7	13	10	8	0	0	41	56-65	23
08:00 AM	0	0	0	0	0	0	0	9	8	14	6	0	1	0	38	51-60	22
09:00 AM	0	0	0	0	0	0	1	5	5	5	4	0	1	0	21	46-55	10
10:00 AM	0	0	0	0	0	0	2	5	5	4	2	0	0	0	18	46-55	10
11:00 AM	0	0	0	0	0	0	1	5	4	1	0	0	0	0	11	46-55	9
12:00 PM	0	2	0	0	2	0	4	3	5	5	0	0	0	0	21	51-60	10
01:00 PM	0	0	0	0	0	1	2	6	7	3	1	0	0	0	20	46-55	13
02:00 PM	0	0	0	0	0	0	3	8	6	3	1	1	0	0	22	46-55	14
03:00 PM	0	0	1	0	0	3	6	11	15	5	0	0	0	0	41	46-55	26
04:00 PM	0	0	0	0	0	2	4	10	10	3	0	0	1	0	30	46-55	20
05:00 PM	0	0	0	0	0	1	3	6	8	9	1	0	0	0	28	51-60	17
06:00 PM	0	0	0	0	0	0	1	6	13	7	2	1	0	0	30	51-60	20
07:00 PM	0	0	0	0	0	0	0	4	7	4	2	1	0	0	18	49-58	11
08:00 PM	0	0	0	0	0	0	4	3	5	2	3	0	0	0	17	46-55	8
09:00 PM	0	0	0	0	0	0	0	4	3	1	0	0	0	0	8	46-55	7
10:00 PM	0	0	0	0	1	0	0	2	4	3	5	1	1	0	17	56-65	8
11:00 PM	0	0	0	0	0	0	0	0	1	0	1	0	0	0	2	46-55	1
Day Total	1	2	1	0	3	7	33	90	119	103	56	17	5	4	441	51-60	222
Percent	0.2%	0.5%	0.2%	0%	0.7%	1.6%	7.5%	20.4%	27%	23.4%	12.7%	3.9%	1.1%	0.9%			
AM Peak Volume	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM	8:00 AM	8:00 AM	6:00 AM	6:00 AM	7:00 AM	5:00 AM	6:00 AM	6:00 AM		
	1	0	0	0	0	0	2	9	8	17	17	8	1	4	43		
PM Peak Volume	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	3:00 PM	3:00 PM	3:00 PM	3:00 PM	5:00 PM	10:00 PM	2:00 PM	4:00 PM	12:00 PM	3:00 PM		
	0	2	1	0	2	3	6	11	15	9	5	1	1	0	41		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) John Coffee Memorial Bridge															QC JOB #: 15889225		
SPECIFIC LOCATION:															DIRECTION: WB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	32	14	3	6	20	59	195	660	900	682	278	113	19	19	3000	51-60	1582
Percent	1.1%	0.5%	0.1%	0.2%	0.7%	2%	6.5%	22%	30%	22.7%	9.3%	3.8%	0.6%	0.6%			
Cumulative Percent	1.1%	1.5%	1.6%	1.8%	2.5%	4.5%	11%	33%	63%	85.7%	95%	98.7%	99.4%	100%			
ADT 428															85th Percentile: 59 MPH Mean Speed(Average): 52 MPH Median: 52 MPH Mode: 53 MPH		
Comments:																	

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
04:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
05:00 AM	0	3	5	0	0	0	0	0	0	0	0	0	0	0	8
06:00 AM	0	33	14	0	2	0	0	0	0	0	0	0	0	0	49
07:00 AM	0	29	15	0	3	0	0	1	0	0	0	0	0	0	48
08:00 AM	0	24	4	0	2	0	0	0	0	0	0	0	0	0	30
09:00 AM	0	11	8	0	2	0	0	0	0	0	0	0	0	0	21
10:00 AM	1	7	7	0	1	0	0	2	0	0	0	0	0	0	18
11:00 AM	1	10	6	0	1	2	0	1	0	0	0	0	0	4	25
12:00 PM	0	14	6	0	1	2	0	4	0	0	0	0	0	0	27
01:00 PM	2	13	6	0	7	2	0	0	0	0	0	0	0	1	31
02:00 PM	0	9	8	0	2	2	0	0	0	0	0	0	0	1	22
03:00 PM	2	17	12	0	4	2	0	0	0	0	0	0	0	2	39
04:00 PM	4	11	13	1	8	0	0	1	0	0	0	0	0	4	42
05:00 PM	2	23	8	0	2	3	0	0	0	0	0	0	0	0	38
06:00 PM	1	18	6	0	2	1	0	1	0	0	0	0	0	0	29
07:00 PM	0	14	10	0	3	0	0	0	0	0	0	0	0	0	27
08:00 PM	0	6	2	0	0	0	0	0	0	0	0	0	0	0	8
09:00 PM	0	8	4	0	0	0	0	0	0	0	0	0	0	0	12
10:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Day Total	13	257	139	1	40	14	0	10	0	0	0	0	0	12	486
Percent	2.7%	52.9%	28.6%	0.2%	8.2%	2.9%	0%	2.1%	0%	0%	0%	0%	0%	2.5%	
ADT 486															
AM Peak Volume	10:00 AM 1	6:00 AM 33	7:00 AM 15	12:00 AM 0	7:00 AM 3	11:00 AM 2	12:00 AM 0	10:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 4	6:00 AM 49
PM Peak Volume	4:00 PM 4	5:00 PM 23	4:00 PM 13	4:00 PM 1	4:00 PM 8	5:00 PM 3	12:00 PM 0	12:00 PM 4	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 4	4:00 PM 42

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 10 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
05:00 AM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
06:00 AM	0	27	21	0	2	0	0	0	0	0	0	0	0	0	50
07:00 AM	0	29	17	0	5	0	0	0	0	0	0	0	0	0	51
08:00 AM	0	17	9	0	3	0	0	5	0	0	0	0	0	0	34
09:00 AM	1	16	9	0	6	1	0	1	0	0	0	0	0	0	34
10:00 AM	0	7	3	0	1	0	0	0	0	0	0	0	0	2	13
11:00 AM	2	8	5	0	6	0	0	1	0	0	0	0	0	1	23
12:00 PM	3	12	6	0	0	0	0	2	0	0	0	0	0	1	24
01:00 PM	2	6	7	1	3	0	0	3	0	0	0	0	0	1	23
02:00 PM	2	8	12	0	3	0	0	0	0	0	0	0	0	0	25
03:00 PM	1	17	5	0	0	0	0	1	0	0	0	0	0	0	24
04:00 PM	2	18	7	0	2	0	0	1	0	0	0	0	0	1	31
05:00 PM	1	22	15	0	1	0	0	3	0	0	0	0	0	0	42
06:00 PM	0	17	15	0	2	0	0	0	0	0	0	0	0	0	34
07:00 PM	0	16	7	0	1	0	0	0	0	0	0	0	0	0	24
08:00 PM	1	13	4	0	1	0	0	1	0	0	0	0	0	0	20
09:00 PM	0	0	5	0	1	0	0	0	0	0	0	0	0	0	6
10:00 PM	0	7	0	0	0	0	0	0	0	0	0	0	0	0	7
11:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
Day Total	15	251	152	1	38	1	0	18	0	0	0	0	0	6	482
Percent	3.1%	52.1%	31.5%	0.2%	7.9%	0.2%	0%	3.7%	0%	0%	0%	0%	0%	1.2%	
ADT 482															
AM Peak Volume	11:00 AM 2	7:00 AM 29	6:00 AM 21	12:00 AM 0	9:00 AM 6	9:00 AM 1	12:00 AM 0	8:00 AM 5	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 2	7:00 AM 51
PM Peak Volume	12:00 PM 3	5:00 PM 22	5:00 PM 15	1:00 PM 1	1:00 PM 3	12:00 PM 0	12:00 PM 0	1:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 1	5:00 PM 42

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 11 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
06:00 AM	0	25	11	0	3	0	0	0	0	0	0	0	0	0	39
07:00 AM	0	15	11	0	3	0	0	0	0	0	0	0	0	0	29
08:00 AM	0	18	5	0	2	0	0	1	0	0	0	0	0	0	26
09:00 AM	0	11	6	0	3	0	0	0	0	0	0	0	0	0	20
10:00 AM	0	10	4	0	1	0	0	2	0	0	0	0	0	0	17
11:00 AM	0	3	5	2	5	0	0	3	0	0	0	0	0	0	18
12:00 PM	0	12	7	0	0	0	0	0	0	0	0	0	0	0	19
01:00 PM	4	16	6	0	0	0	0	0	0	0	0	0	0	0	26
02:00 PM	1	15	10	0	2	0	0	1	0	0	0	0	0	1	30
03:00 PM	0	25	6	0	1	0	0	3	0	0	0	0	0	0	35
04:00 PM	4	40	10	0	0	0	0	1	0	0	0	0	0	0	55
05:00 PM	1	26	16	0	1	0	0	2	0	0	0	0	0	1	47
06:00 PM	0	15	10	0	2	0	0	0	0	0	0	0	0	0	27
07:00 PM	0	19	6	0	1	0	0	0	0	0	0	0	0	0	26
08:00 PM	0	9	5	0	1	0	0	0	0	0	0	0	0	0	15
09:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
10:00 PM	0	9	1	0	0	0	0	0	0	0	0	0	0	0	10
11:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
Day Total	10	280	125	2	25	0	0	13	0	0	0	0	0	2	457
Percent	2.2%	61.3%	27.4%	0.4%	5.5%	0%	0%	2.8%	0%	0%	0%	0%	0%	0.4%	
ADT 457															
AM Peak Volume	12:00 AM 0	6:00 AM 25	6:00 AM 11	11:00 AM 2	11:00 AM 5	12:00 AM 0	12:00 AM 0	11:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 39
PM Peak Volume	1:00 PM 4	4:00 PM 40	5:00 PM 16	12:00 PM 0	2:00 PM 2	12:00 PM 0	12:00 PM 0	3:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	4:00 PM 55

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	3	3	0	0	0	0	0	0	0	0	0	0	0	6
06:00 AM	0	9	4	0	1	0	0	0	0	0	0	0	0	0	14
07:00 AM	0	9	2	0	0	0	0	1	0	0	0	0	0	0	12
08:00 AM	0	7	2	0	0	0	0	1	0	0	0	0	0	0	10
09:00 AM	0	15	5	0	0	0	0	0	0	0	0	0	0	0	20
10:00 AM	0	11	4	0	0	0	0	0	0	0	0	0	0	0	15
11:00 AM	16	14	11	0	3	0	0	1	0	0	0	0	0	0	45
12:00 PM	0	16	10	0	2	0	0	1	0	0	0	0	0	0	29
01:00 PM	0	10	10	0	2	0	0	2	0	0	0	0	0	0	24
02:00 PM	0	24	7	0	1	0	0	0	0	0	0	0	0	0	32
03:00 PM	0	25	8	0	0	0	0	0	0	0	0	0	0	0	33
04:00 PM	0	15	11	0	1	0	0	1	0	0	0	0	0	0	28
05:00 PM	0	17	8	0	1	0	0	0	0	0	0	0	0	0	26
06:00 PM	0	10	4	0	2	0	0	0	0	0	0	0	0	0	16
07:00 PM	0	14	11	0	0	0	0	0	0	0	0	0	0	0	25
08:00 PM	0	7	4	0	1	0	0	0	0	0	0	0	0	0	12
09:00 PM	0	7	1	0	1	0	0	0	0	0	0	0	0	0	9
10:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
11:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
Day Total	16	224	109	0	15	0	0	7	0	0	0	0	0	0	371
Percent	4.3%	60.4%	29.4%	0%	4%	0%	0%	1.9%	0%	0%	0%	0%	0%	0%	
ADT 371															
AM Peak Volume	11:00 AM 16	9:00 AM 15	11:00 AM 11	12:00 AM 0	11:00 AM 3	12:00 AM 0	12:00 AM 0	7:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 45
PM Peak Volume	12:00 PM 0	3:00 PM 25	4:00 PM 11	12:00 PM 0	12:00 PM 2	12:00 PM 0	12:00 PM 0	1:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 33

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 13 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
06:00 AM	0	4	2	0	0	0	0	0	0	0	0	0	0	0	6
07:00 AM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
08:00 AM	0	7	2	0	1	0	0	0	0	0	0	0	0	0	10
09:00 AM	0	4	5	0	0	0	0	1	0	0	0	0	0	0	10
10:00 AM	0	6	6	0	1	0	0	1	0	0	0	0	0	0	14
11:00 AM	0	7	2	0	3	0	0	1	0	0	0	0	0	0	13
12:00 PM	0	19	11	0	1	0	0	2	0	0	0	0	0	0	33
01:00 PM	0	23	5	0	5	0	0	0	0	0	0	0	0	0	33
02:00 PM	0	24	11	1	3	0	0	1	0	0	0	0	0	0	40
03:00 PM	0	21	7	0	2	0	0	0	0	0	0	0	0	1	31
04:00 PM	0	21	8	0	1	0	0	2	0	0	0	0	0	0	32
05:00 PM	0	22	11	0	1	0	0	1	0	0	0	0	0	0	35
06:00 PM	0	14	8	0	5	0	0	0	0	0	0	0	0	1	28
07:00 PM	0	13	6	0	1	0	0	0	0	0	0	0	0	0	20
08:00 PM	0	9	0	0	1	0	0	0	0	0	0	0	0	0	10
09:00 PM	0	5	5	0	0	0	0	0	0	0	0	0	0	0	10
10:00 PM	0	4	1	0	0	0	0	1	0	0	0	0	0	0	6
11:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
Day Total	0	217	92	1	25	0	0	10	0	0	0	0	0	2	347
Percent	0%	62.5%	26.5%	0.3%	7.2%	0%	0%	2.9%	0%	0%	0%	0%	0%	0.6%	
ADT 347															
AM Peak Volume	12:00 AM 0	8:00 AM 7	10:00 AM 6	12:00 AM 0	11:00 AM 3	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 14
PM Peak Volume	12:00 PM 0	2:00 PM 24	12:00 PM 11	2:00 PM 1	1:00 PM 5	12:00 PM 0	12:00 PM 0	12:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 1	2:00 PM 40

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 14 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	5	4	0	0	0	0	0	0	0	0	0	0	0	9
06:00 AM	0	20	15	0	8	0	0	0	0	0	0	0	0	0	43
07:00 AM	0	26	22	0	4	0	0	0	0	0	0	0	0	0	52
08:00 AM	0	23	10	0	4	0	0	1	0	0	0	0	0	0	38
09:00 AM	0	5	6	0	1	0	0	1	0	0	0	0	0	0	13
10:00 AM	0	6	10	0	0	0	0	1	0	0	0	0	0	0	17
11:00 AM	0	5	6	0	1	0	0	1	0	0	0	0	0	0	13
12:00 PM	0	13	10	0	2	0	0	0	0	0	0	0	0	0	25
01:00 PM	0	12	8	0	2	0	0	0	0	0	0	0	0	0	22
02:00 PM	0	10	3	0	0	0	0	2	0	0	0	0	0	0	15
03:00 PM	0	13	4	0	1	0	0	1	0	0	0	0	0	0	19
04:00 PM	0	19	5	0	2	0	0	0	0	0	0	0	0	0	26
05:00 PM	0	19	9	0	0	0	0	0	0	0	0	0	0	0	28
06:00 PM	0	24	15	0	0	0	0	0	0	0	0	0	0	0	39
07:00 PM	0	11	6	0	0	0	0	0	0	0	0	0	0	0	17
08:00 PM	0	11	3	0	0	0	0	0	0	0	0	0	0	0	14
09:00 PM	0	5	3	0	0	0	0	0	0	0	0	0	0	0	8
10:00 PM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
11:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
Day Total	0	237	147	0	25	0	0	7	0	0	0	0	0	0	416
Percent	0%	57%	35.3%	0%	6%	0%	0%	1.7%	0%	0%	0%	0%	0%	0%	
ADT 416															
AM Peak Volume	12:00 AM 0	7:00 AM 26	7:00 AM 22	12:00 AM 0	6:00 AM 8	12:00 AM 0	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 52
PM Peak Volume	12:00 PM 0	6:00 PM 24	6:00 PM 15	12:00 PM 0	12:00 PM 2	12:00 PM 0	12:00 PM 0	2:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	6:00 PM 39

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 AM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
06:00 AM	0	21	12	0	9	0	0	1	0	0	0	0	0	0	43
07:00 AM	0	21	13	0	5	0	0	1	0	0	0	0	0	1	41
08:00 AM	0	28	10	0	0	0	0	0	0	0	0	0	0	0	38
09:00 AM	0	11	5	0	4	0	0	1	0	0	0	0	0	0	21
10:00 AM	0	9	6	0	1	0	0	2	0	0	0	0	0	0	18
11:00 AM	0	7	3	0	1	0	0	0	0	0	0	0	0	0	11
12:00 PM	1	9	6	1	3	0	0	1	0	0	0	0	0	0	21
01:00 PM	0	8	10	0	1	0	0	1	0	0	0	0	0	0	20
02:00 PM	0	15	6	0	1	0	0	0	0	0	0	0	0	0	22
03:00 PM	0	20	12	0	6	0	0	3	0	0	0	0	0	0	41
04:00 PM	0	18	9	0	1	0	0	2	0	0	0	0	0	0	30
05:00 PM	0	18	9	1	0	0	0	0	0	0	0	0	0	0	28
06:00 PM	0	20	9	0	1	0	0	0	0	0	0	0	0	0	30
07:00 PM	0	11	7	0	0	0	0	0	0	0	0	0	0	0	18
08:00 PM	0	12	5	0	0	0	0	0	0	0	0	0	0	0	17
09:00 PM	0	4	4	0	0	0	0	0	0	0	0	0	0	0	8
10:00 PM	0	13	3	0	1	0	0	0	0	0	0	0	0	0	17
11:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Day Total	1	255	136	2	34	0	0	12	0	0	0	0	0	1	441
Percent	0.2%	57.8%	30.8%	0.5%	7.7%	0%	0%	2.7%	0%	0%	0%	0%	0%	0.2%	
ADT 441															
AM Peak	12:00 AM	8:00 AM	7:00 AM	12:00 AM	6:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	7:00 AM	6:00 AM
Volume	0	28	13	0	9	0	0	2	0	0	0	0	0	1	43
PM Peak	12:00 PM	3:00 PM	3:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM
Volume	1	20	12	1	6	0	0	3	0	0	0	0	0	0	41

Comments:

LOCATION: (Peak) John Coffee Memorial Bridge

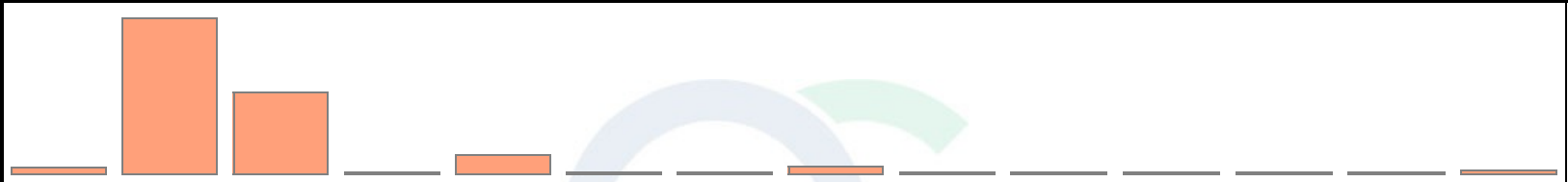
QC JOB #: 15889225

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	55	1721	900	7	202	15	0	77	0	0	0	0	0	23	3000
Percent	1.8%	57.4%	30%	0.2%	6.7%	0.5%	0%	2.6%	0%	0%	0%	0%	0%	0.8%	
ADT 428															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) John Coffee Memorial Bridge										QC JOB #: 15889225
SPECIFIC LOCATION:										DIRECTION: WB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday	Sat	Sun	Average Week	Average Week Profile
			9 Nov 22	10 Nov 22	11 Nov 22	Hourly Traffic	12 Nov 22	13 Nov 22	Hourly Traffic	
12:00 AM			2	2	0	1	1	3	2	
01:00 AM			0	1	0	0	1	0	0	
02:00 AM			1	0	1	1	3	0	1	
03:00 AM			3	1	0	1	1	1	1	
04:00 AM			3	3	2	3	1	0	2	
05:00 AM			8	7	7	7	6	3	6	
06:00 AM			49	50	39	46	14	6	32	
07:00 AM			48	51	29	43	12	6	29	
08:00 AM			30	34	26	30	10	10	22	
09:00 AM			21	34	20	25	20	10	21	
10:00 AM			18	13	17	16	15	14	15	
11:00 AM			25	23	18	22	45	13	25	
12:00 PM			27	24	19	23	29	33	26	
01:00 PM			31	23	26	27	24	33	27	
02:00 PM			22	25	30	26	32	40	30	
03:00 PM			39	24	35	33	33	31	32	
04:00 PM			42	31	55	43	28	32	38	
05:00 PM			38	42	47	42	26	35	38	
06:00 PM			29	34	27	30	16	28	27	
07:00 PM			27	24	26	26	25	20	24	
08:00 PM			8	20	15	14	12	10	13	
09:00 PM			12	6	4	7	9	10	8	
10:00 PM			1	7	10	6	5	6	6	
11:00 PM			2	3	4	3	3	3	3	
Day Total			486	482	457	475	371	347	428	
% Weekday Average			102.3%	101.5%	96.2%					
% Week Average			113.6%	112.6%	106.8%	111%	86.7%	81.1%		
AM Peak Volume			6:00 AM 49	7:00 AM 51	6:00 AM 39	6:00 AM 46	11:00 AM 45	10:00 AM 14	6:00 AM 32	
PM Peak Volume			4:00 PM 42	5:00 PM 42	4:00 PM 55	4:00 PM 43	3:00 PM 33	2:00 PM 40	4:00 PM 38	
Comments:										

Report generated on 11/29/2022 11:07 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) John Coffee Memorial Bridge							QC JOB #: 15889225			
SPECIFIC LOCATION:							DIRECTION: WB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	1	1				1			1	<div></div>
01:00 AM	1	1				1			1	<div></div>
02:00 AM	1	2				2			2	<div></div>
03:00 AM	2	2				2			2	<div></div>
04:00 AM	1	2				2			2	<div></div>
05:00 AM	9	7				8			8	<div></div>
06:00 AM	43	43				43			43	<div></div>
07:00 AM	52	41				47			47	<div></div>
08:00 AM	38	38				38			38	<div></div>
09:00 AM	13	21				17			17	<div></div>
10:00 AM	17	18				18			18	<div></div>
11:00 AM	13	11				12			12	<div></div>
12:00 PM	25	21				23			23	<div></div>
01:00 PM	22	20				21			21	<div></div>
02:00 PM	15	22				19			19	<div></div>
03:00 PM	19	41				30			30	<div></div>
04:00 PM	26	30				28			28	<div></div>
05:00 PM	28	28				28			28	<div></div>
06:00 PM	39	30				35			35	<div></div>
07:00 PM	17	18				18			18	<div></div>
08:00 PM	14	17				16			16	<div></div>
09:00 PM	8	8				8			8	<div></div>
10:00 PM	8	17				13			13	<div></div>
11:00 PM	4	2				3			3	<div></div>
Day Total	416	441				433			433	
% Weekday Average	96.1%	101.8%								
% Week Average	96.1%	101.8%				100%				
AM Peak Volume	7:00 AM 52	6:00 AM 43				7:00 AM 47			7:00 AM 47	
PM Peak Volume	6:00 PM 39	3:00 PM 41				6:00 PM 35			6:00 PM 35	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 9 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
03:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
09:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
10:00 AM	0	1	0	1	2	0	0	0	0	0	0	0	0	0	4	26-35	3
11:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
02:00 PM	0	0	0	0	2	0	0	1	0	0	0	0	0	0	3	26-35	2
03:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
04:00 PM	0	0	2	0	1	0	0	0	0	0	0	0	0	0	3	16-25	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	1	4	9	10	0	0	1	0	0	0	0	0	0	25	26-35	19
Percent	0%	4%	16%	36%	40%	0%	0%	4%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	10:00 AM	8:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	1	1	2	2	0	0	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	12:00 PM	4:00 PM	3:00 PM	2:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	0	0	2	2	2	0	0	1	0	0	0	0	0	0	3		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 10 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1-10	1
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
10:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
11:00 AM	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	21-30	3
12:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
01:00 PM	0	0	0	1	2	0	0	0	0	0	0	0	0	0	3	26-35	3
02:00 PM	0	0	1	1	0	1	0	0	0	0	0	0	0	0	3	21-30	2
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2	21-30	1
05:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	0	3	8	5	2	0	0	0	0	0	0	0	0	22	26-35	13
Percent	18.2%	0%	13.6%	36.4%	22.7%	9.1%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	8:00 AM 2	12:00 AM 0	10:00 AM 1	11:00 AM 2	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3		
PM Peak Volume	4:00 PM 1	12:00 PM 0	2:00 PM 1	1:00 PM 1	1:00 PM 2	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 3		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 11 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
08:00 AM	0	0	1	0	3	0	0	0	0	0	0	0	0	0	4	26-35	3
09:00 AM	0	0	2	0	1	1	0	0	0	0	0	0	0	0	4	16-25	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
12:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
01:00 PM	0	0	0	1	1	0	1	0	0	0	0	0	0	0	3	26-35	2
02:00 PM	0	0	0	2	0	1	0	0	0	0	0	0	0	0	3	21-30	2
03:00 PM	0	0	1	2	1	0	0	0	0	0	0	0	0	0	4	26-35	3
04:00 PM	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	1-10	2
05:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	2	4	9	7	4	1	0	0	0	0	0	0	0	30	26-35	16
Percent	10%	6.7%	13.3%	30%	23.3%	13.3%	3.3%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	7:00 AM 1	9:00 AM 2	7:00 AM 1	8:00 AM 3	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	8:00 AM 4		
PM Peak Volume	4:00 PM 3	7:00 PM 1	3:00 PM 1	2:00 PM 2	1:00 PM 1	2:00 PM 1	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 4		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 12 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
11:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	21-30	3
12:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
02:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	3	0	0	0	0	0	0	0	0	0	3	26-35	3
08:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
09:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	1	6	2	5	0	0	0	0	0	0	0	0	0	14	21-30	8
Percent	0%	7.1%	42.9%	14.3%	35.7%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	11:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	0	2	1	1	0	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	1:00 PM	12:00 PM	2:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM		
	0	1	2	1	3	0	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 13 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
06:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	26-35	3
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
11:00 AM	2	1	1	2	0	0	0	0	0	0	0	0	0	0	6	21-30	3
12:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
01:00 PM	0	0	2	1	2	1	0	0	0	0	0	0	0	0	6	21-30	3
02:00 PM	1	0	0	1	2	1	0	0	0	0	0	0	0	0	5	31-40	3
03:00 PM	0	0	0	2	0	1	0	0	0	0	0	0	0	0	3	21-30	2
04:00 PM	1	1	0	1	0	1	1	0	0	0	0	0	0	0	5	36-45	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	2	4	10	6	6	1	0	0	0	0	0	0	0	33	26-35	16
Percent	12.1%	6.1%	12.1%	30.3%	18.2%	18.2%	3%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	11:00 AM 1	10:00 AM 1	9:00 AM 2	9:00 AM 1	5:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 6		
PM Peak Volume	2:00 PM 1	4:00 PM 1	1:00 PM 2	3:00 PM 2	1:00 PM 2	12:00 PM 1	4:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 6		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 14 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	36-45	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	16-25	1
10:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
11:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
02:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
03:00 PM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	16-25	1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
Day Total	0	1	3	4	0	3	1	0	0	0	0	0	0	0	12	21-30	7
Percent	0%	8.3%	25%	33.3%	0%	25%	8.3%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	9:00 AM	10:00 AM	12:00 AM	9:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM		
	0	0	1	2	0	1	1	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	11:00 PM	1:00 PM	2:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	0	1	1	1	0	1	0	0	0	0	0	0	0	0	2		
Comments:																	

Report generated on 11/29/2022 11:08 AM

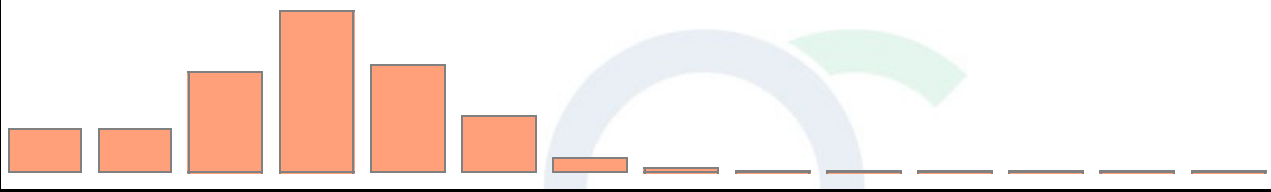
SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB DATE: Nov 15 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
03:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
09:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
10:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 AM	1	2	1	0	0	0	0	0	0	0	0	0	0	0	4	16-25	3
12:00 PM	0	0	1	2	0	1	1	0	0	0	0	0	0	0	5	21-30	3
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	1	0	3	1	0	0	0	0	0	0	0	0	0	0	5	21-30	4
03:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 PM	0	3	0	1	0	0	0	0	0	0	0	0	0	0	4	11-20	3
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	6	7	8	0	2	1	0	0	0	0	0	0	0	26	21-30	15
Percent	7.7%	23.1%	26.9%	30.8%	0%	7.7%	3.8%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 1	11:00 AM 2	3:00 AM 1	2:00 AM 1	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 4		
PM Peak Volume	2:00 PM 1	4:00 PM 3	2:00 PM 3	12:00 PM 2	12:00 PM 0	12:00 PM 1	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 5		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Comfort Station Parking Area															QC JOB #: 15889226		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	13	13	31	50	33	17	4	1	0	0	0	0	0	0	162	26-35	83
Percent	8%	8%	19.1%	30.9%	20.4%	10.5%	2.5%	0.6%	0%	0%	0%	0%	0%	0%			
Cumulative Percent	8%	16%	35.2%	66%	86.4%	96.9%	99.4%	100%	100%	100%	100%	100%	100%	100%			
ADT 23															85th Percentile: 34 MPH Mean Speed(Average): 27 MPH Median: 27 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	18	5	0	0	0	0	0	0	0	0	0	0	0	25
Percent	8%	72%	20%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 25															
AM Peak Volume	12:00 AM 0	10:00 AM 4	7:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 4
PM Peak Volume	4:00 PM 2	2:00 PM 2	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 3
Comments:															

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 10 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3
02:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	13	4	0	1	0	0	0	0	0	0	0	0	2	22
Percent	9.1%	59.1%	18.2%	0%	4.5%	0%	0%	0%	0%	0%	0%	0%	0%	9.1%	
ADT 22															
AM Peak Volume	12:00 AM	11:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	11:00 AM
	0	3	1	0	0	0	0	0	0	0	0	0	0	2	3
PM Peak Volume	12:00 PM	2:00 PM	1:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM
	1	2	1	0	1	0	0	0	0	0	0	0	0	0	3

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 11 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	3	0	0	1	0	0	0	0	0	0	0	0	0	4
09:00 AM	0	3	0	0	1	0	0	0	0	0	0	0	0	0	4
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3
02:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	1	3	0	0	0	0	0	0	0	0	0	0	0	0	4
04:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	1	3
05:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	19	5	0	2	0	0	0	0	0	0	0	0	1	30
Percent	10%	63.3%	16.7%	0%	6.7%	0%	0%	0%	0%	0%	0%	0%	0%	3.3%	
ADT 30															
AM Peak Volume	12:00 AM	8:00 AM	7:00 AM	12:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM
	0	3	1	0	1	0	0	0	0	0	0	0	0	0	4
PM Peak Volume	1:00 PM	3:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	3:00 PM
	1	3	2	0	0	0	0	0	0	0	0	0	0	1	4

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	8	5	0	1	0	0	0	0	0	0	0	0	0	14
Percent	0%	57.1%	35.7%	0%	7.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 14															
AM Peak Volume	12:00 AM	10:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	2	2	0	0	0	0	0	0	0	0	0	0	0	3
PM Peak Volume	12:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM
	0	1	3	0	1	0	0	0	0	0	0	0	0	0	3

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 13 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	1	3	1	0	0	0	0	0	0	0	0	0	0	1	6
12:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	4	2	0	0	0	0	0	0	0	0	0	0	0	6
02:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
03:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	1	5
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	23	7	0	0	0	0	0	0	0	0	0	0	2	33
Percent	3%	69.7%	21.2%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	6.1%	
ADT 33															
AM Peak Volume	11:00 AM	9:00 AM	5:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM
	1	3	1	0	0	0	0	0	0	0	0	0	0	1	6
PM Peak Volume	12:00 PM	1:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	1:00 PM
	0	4	2	0	0	0	0	0	0	0	0	0	0	1	6

Comments:

LOCATION: (Peak) Comfort Station Parking Area

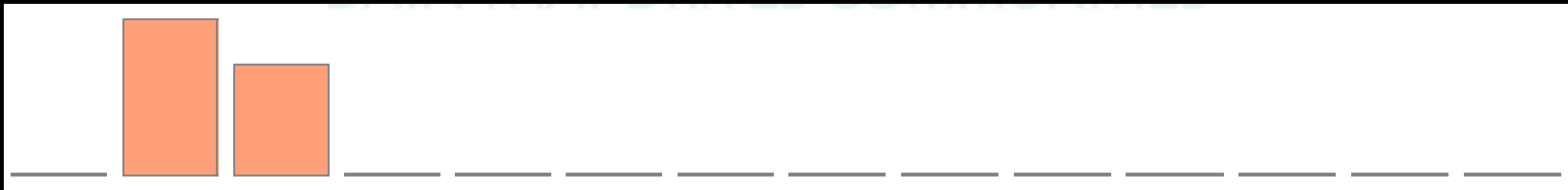
QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 14 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total	
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
04:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
09:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	
03:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	
Day Total	0	7	5	0	0	0	0	0	0	0	0	0	0	0	12	
Percent	0%	58.3%	41.7%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%		
ADT 12																
AM Peak Volume	12:00 AM 0	4:00 AM 1	10:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 2	
PM Peak Volume	12:00 PM 0	1:00 PM 1	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 2	

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
09:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	1	0	0	0	1	0	0	1	1	0	0	0	0	0	4
12:00 PM	0	2	1	0	1	0	0	0	1	0	0	0	0	0	5
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	4	0	0	0	0	0	1	0	0	0	0	0	0	5
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	1	1	0	1	0	0	1	0	0	0	0	0	0	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	10	6	0	3	0	0	4	2	0	0	0	0	0	26
Percent	3.8%	38.5%	23.1%	0%	11.5%	0%	0%	15.4%	7.7%	0%	0%	0%	0%	0%	
ADT 26															
AM Peak Volume	11:00 AM	2:00 AM	9:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	8:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	1	1	2	0	1	0	0	1	1	0	0	0	0	0	4
PM Peak Volume	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	0	4	1	0	1	0	0	1	1	0	0	0	0	0	5

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	9	98	37	0	7	0	0	4	2	0	0	0	0	5	162
Percent	5.6%	60.5%	22.8%	0%	4.3%	0%	0%	2.5%	1.2%	0%	0%	0%	0%	3.1%	
ADT 23															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Comfort Station Parking Area										QC JOB #: 15889226
SPECIFIC LOCATION:										DIRECTION: EB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			0	1	0	0	0	0	0	
01:00 AM			0	0	0	0	0	0	0	
02:00 AM			1	1	0	1	0	0	0	
03:00 AM			1	1	0	1	0	0	0	
04:00 AM			0	0	0	0	0	0	0	
05:00 AM			0	0	0	0	0	1	0	
06:00 AM			1	0	1	1	0	1	1	<div></div>
07:00 AM			1	0	2	1	0	0	1	<div></div>
08:00 AM			2	2	4	3	0	0	2	<div></div>
09:00 AM			2	1	4	2	0	3	2	<div></div>
10:00 AM			4	2	0	2	2	1	2	<div></div>
11:00 AM			2	3	2	2	3	6	3	<div></div>
12:00 PM			0	2	1	1	2	2	1	<div></div>
01:00 PM			1	3	3	2	1	6	3	<div></div>
02:00 PM			3	3	3	3	1	5	3	<div></div>
03:00 PM			2	0	4	2	0	3	2	<div></div>
04:00 PM			3	2	3	3	0	5	3	<div></div>
05:00 PM			0	1	1	1	0	0	0	
06:00 PM			0	0	0	0	0	0	0	
07:00 PM			1	0	1	1	3	0	1	<div></div>
08:00 PM			1	0	0	0	1	0	0	
09:00 PM			0	0	1	0	1	0	0	
10:00 PM			0	0	0	0	0	0	0	
11:00 PM			0	0	0	0	0	0	0	
Day Total			25	22	30	26	14	33	24	
% Weekday Average			96.2%	84.6%	115.4%					
% Week Average			104.2%	91.7%	125%	108.3%	58.3%	137.5%		
AM Peak Volume			10:00 AM 4	11:00 AM 3	8:00 AM 4	8:00 AM 3	11:00 AM 3	11:00 AM 6	11:00 AM 3	
PM Peak Volume			2:00 PM 3	1:00 PM 3	3:00 PM 4	2:00 PM 3	7:00 PM 3	1:00 PM 6	1:00 PM 3	
Comments:										

Report generated on 11/29/2022 11:07 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Comfort Station Parking Area							QC JOB #: 15889226			
SPECIFIC LOCATION:							DIRECTION: EB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0				0			0	
01:00 AM	0	0				0			0	
02:00 AM	0	1				1			1	<div></div>
03:00 AM	0	1				1			1	<div></div>
04:00 AM	1	0				1			1	<div></div>
05:00 AM	0	0				0			0	
06:00 AM	0	0				0			0	
07:00 AM	1	0				1			1	<div></div>
08:00 AM	0	1				1			1	<div></div>
09:00 AM	2	2				2			2	<div></div>
10:00 AM	2	1				2			2	<div></div>
11:00 AM	1	4				3			3	<div></div>
12:00 PM	0	5				3			3	<div></div>
01:00 PM	1	0				1			1	<div></div>
02:00 PM	1	5				3			3	<div></div>
03:00 PM	2	1				2			2	<div></div>
04:00 PM	0	4				2			2	<div></div>
05:00 PM	0	0				0			0	
06:00 PM	0	0				0			0	
07:00 PM	0	0				0			0	
08:00 PM	0	0				0			0	
09:00 PM	0	1				1			1	<div></div>
10:00 PM	0	0				0			0	
11:00 PM	1	0				1			1	<div></div>
Day Total	12	26				25			25	
% Weekday Average	48%	104%								
% Week Average	48%	104%				100%				
AM Peak Volume	9:00 AM 2	11:00 AM 4				11:00 AM 3			11:00 AM 3	
PM Peak Volume	3:00 PM 2	12:00 PM 5				12:00 PM 3			12:00 PM 3	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 9 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
03:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 AM	0	1	0	0	1	0	1	0	0	0	0	0	0	0	3	11-20	1
08:00 AM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3	16-25	2
09:00 AM	0	0	3	1	0	0	0	0	0	0	0	0	0	0	4	21-30	4
10:00 AM	0	1	0	1	4	0	0	0	0	0	0	0	0	0	6	26-35	5
11:00 AM	0	0	2	2	1	0	0	0	0	0	0	0	0	0	5	21-30	4
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
01:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
02:00 PM	0	0	0	1	3	0	0	1	0	0	0	0	0	0	5	26-35	4
03:00 PM	1	0	1	2	0	0	0	0	0	0	0	0	0	0	4	21-30	3
04:00 PM	0	0	2	2	3	0	0	0	0	0	0	0	0	0	7	26-35	5
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
06:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
08:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	3	12	12	19	1	1	1	0	0	0	0	0	0	50	26-35	31
Percent	2%	6%	24%	24%	38%	2%	2%	2%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	7:00 AM	9:00 AM	11:00 AM	10:00 AM	3:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	1	3	2	4	1	1	0	0	0	0	0	0	0	6		
PM Peak Volume	3:00 PM	12:00 PM	4:00 PM	3:00 PM	2:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM		
	1	0	2	2	3	0	0	1	0	0	0	0	0	0	7		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 10 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
03:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
08:00 AM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	1-10	1
09:00 AM	1	0	0	0	2	0	0	0	0	0	0	0	0	0	3	26-35	2
10:00 AM	0	0	1	3	0	0	0	0	0	0	0	0	0	0	4	21-30	4
11:00 AM	0	0	1	3	1	0	0	0	0	0	0	0	0	0	5	23-32	4
12:00 PM	1	0	0	0	1	2	0	0	0	0	0	0	0	0	4	31-40	3
01:00 PM	0	0	1	2	3	0	0	0	0	0	0	0	0	0	6	26-35	5
02:00 PM	1	0	1	3	0	2	0	0	0	0	0	0	0	0	7	21-30	4
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2	21-30	1
05:00 PM	2	0	0	0	1	0	0	0	0	0	0	0	0	0	3	1-10	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	8	0	4	16	11	4	0	0	0	0	0	0	0	0	43	26-35	27
Percent	18.6%	0%	9.3%	37.2%	25.6%	9.3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	8:00 AM 2	12:00 AM 0	10:00 AM 1	10:00 AM 3	2:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 5		
PM Peak Volume	5:00 PM 2	12:00 PM 0	1:00 PM 1	2:00 PM 3	1:00 PM 3	12:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 7		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 11 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 AM	0	1	2	1	0	0	0	0	0	0	0	0	0	0	4	21-30	3
08:00 AM	0	0	1	0	5	1	0	0	0	0	0	0	0	0	7	31-40	6
09:00 AM	0	2	2	1	1	1	0	0	0	0	0	0	0	0	7	16-25	4
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	2	0	1	1	1	1	0	0	0	0	0	0	0	0	6	21-30	2
12:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
01:00 PM	0	0	0	1	2	1	1	0	0	0	0	0	0	0	5	31-40	3
02:00 PM	0	0	0	2	1	2	0	0	0	0	0	0	0	0	5	26-35	3
03:00 PM	0	1	2	6	1	0	0	0	0	0	0	0	0	0	10	21-30	8
04:00 PM	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	1-10	2
05:00 PM	1	0	0	0	1	1	0	0	0	0	0	0	0	0	3	31-40	2
06:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	6	5	9	15	13	8	1	0	0	0	0	0	0	0	57	26-35	28
Percent	10.5%	8.8%	15.8%	26.3%	22.8%	14%	1.8%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	9:00 AM 2	7:00 AM 2	7:00 AM 1	8:00 AM 5	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	8:00 AM 7		
PM Peak Volume	4:00 PM 3	3:00 PM 1	3:00 PM 2	3:00 PM 6	1:00 PM 2	2:00 PM 2	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 10		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 12 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3	16-25	2
11:00 AM	0	1	2	4	0	0	0	0	0	0	0	0	0	0	7	21-30	6
12:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3	16-25	3
01:00 PM	0	2	0	1	0	0	0	0	0	0	0	0	0	0	3	11-20	2
02:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	5	0	0	0	0	0	0	0	0	0	5	26-35	5
08:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
09:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	4	8	8	9	0	0	0	0	0	0	0	0	0	29	26-35	17
Percent	0%	13.8%	27.6%	27.6%	31%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	10:00 AM	11:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	2	4	1	0	0	0	0	0	0	0	0	0	7		
PM Peak Volume	12:00 PM	1:00 PM	12:00 PM	2:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM		
	0	2	3	2	5	0	0	0	0	0	0	0	0	0	5		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 13 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
06:00 AM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	2	3	0	1	0	0	0	0	0	0	0	6	26-35	5
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
11:00 AM	2	1	1	3	1	0	0	0	0	0	0	0	0	0	8	23-32	4
12:00 PM	1	1	0	0	2	2	0	0	0	0	0	0	0	0	6	31-40	4
01:00 PM	0	0	2	2	2	5	0	0	0	0	0	0	0	0	11	31-40	7
02:00 PM	1	1	1	4	3	2	0	0	0	0	0	0	0	0	12	26-35	7
03:00 PM	0	0	0	3	0	2	0	0	0	0	0	0	0	0	5	21-30	3
04:00 PM	1	1	1	2	0	2	1	0	0	0	0	0	0	0	8	36-45	3
05:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	6	5	7	17	12	16	2	0	0	0	0	0	0	0	65	26-35	29
Percent	9.2%	7.7%	10.8%	26.2%	18.5%	24.6%	3.1%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	7:00 AM 1	10:00 AM 2	11:00 AM 3	9:00 AM 3	5:00 AM 1	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 8		
PM Peak Volume	12:00 PM 1	12:00 PM 1	1:00 PM 2	2:00 PM 4	2:00 PM 3	1:00 PM 5	4:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 12		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 14 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	36-45	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
08:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 AM	0	0	2	0	1	1	0	0	0	0	0	0	0	0	4	16-25	2
10:00 AM	0	0	0	4	0	0	0	0	0	0	0	0	0	0	4	21-30	4
11:00 AM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
02:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
03:00 PM	0	0	2	0	0	2	0	0	0	0	0	0	0	0	4	16-25	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
Day Total	0	1	7	7	3	4	1	0	0	0	0	0	0	0	23	21-30	14
Percent	0%	4.3%	30.4%	30.4%	13%	17.4%	4.3%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	9:00 AM	10:00 AM	8:00 AM	9:00 AM	4:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM		
	0	0	2	4	1	1	1	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	11:00 PM	1:00 PM	2:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	0	1	2	1	0	2	0	0	0	0	0	0	0	0	4		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: EB, WB DATE: Nov 15 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
03:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
09:00 AM	0	0	2	2	0	0	0	0	0	0	0	0	0	0	4	21-30	4
10:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
11:00 AM	1	2	3	1	0	0	0	0	0	0	0	0	0	0	7	16-25	5
12:00 PM	1	2	2	2	0	3	1	1	0	0	0	0	0	0	12	36-45	4
01:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
02:00 PM	1	0	3	1	0	1	0	0	0	0	0	0	0	0	6	21-30	4
03:00 PM	0	0	2	3	1	0	0	0	0	0	0	0	0	0	6	21-30	5
04:00 PM	0	3	2	2	0	1	0	0	0	0	0	0	0	0	8	16-25	5
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	8	15	16	2	6	1	1	0	0	0	0	0	0	52	21-30	31
Percent	5.8%	15.4%	28.8%	30.8%	3.8%	11.5%	1.9%	1.9%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 1	11:00 AM 2	11:00 AM 3	9:00 AM 2	2:00 AM 1	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 7		
PM Peak Volume	12:00 PM 1	4:00 PM 3	2:00 PM 3	3:00 PM 3	3:00 PM 1	12:00 PM 3	12:00 PM 1	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 12		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Comfort Station Parking Area															QC JOB #: 15889226		
SPECIFIC LOCATION:															DIRECTION: EB, WB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	24	26	62	91	69	39	6	2	0	0	0	0	0	0	319	26-35	160
Percent	7.5%	8.2%	19.4%	28.5%	21.6%	12.2%	1.9%	0.6%	0%	0%	0%	0%	0%	0%			
Cumulative Percent	7.5%	15.7%	35.1%	63.6%	85.3%	97.5%	99.4%	100%	100%	100%	100%	100%	100%	100%			
ADT 45															85th Percentile: 34 MPH Mean Speed(Average): 27 MPH Median: 27 MPH Mode: 28 MPH		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Quality Counts

DATA THAT DRIVES COMMUNITIES

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
08:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
09:00 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4
10:00 AM	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
11:00 AM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
12:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
03:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	1	4
04:00 PM	4	2	1	0	0	0	0	0	0	0	0	0	0	0	7
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	4	35	9	0	1	0	0	0	0	0	0	0	0	1	50
Percent	8%	70%	18%	0%	2%	0%	0%	0%	0%	0%	0%	0%	0%	2%	
ADT 50															
AM Peak Volume	12:00 AM	10:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM
	0	6	2	0	0	0	0	0	0	0	0	0	0	0	6
PM Peak Volume	4:00 PM	2:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM	4:00 PM
	4	4	2	0	1	0	0	0	0	0	0	0	0	1	7

Comments:

LOCATION: (Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889226

DIRECTION: EB, WB

DATE: Nov 10 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2
09:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	1	3
10:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
11:00 AM	0	5	0	0	0	0	0	0	0	0	0	0	0	0	5
12:00 PM	1	2	1	0	0	0	0	0	0	0	0	0	0	0	4
01:00 PM	0	2	2	0	2	0	0	0	0	0	0	0	0	0	6
02:00 PM	0	4	2	0	0	0	0	0	0	0	0	0	0	1	7
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	26	9	0	2	0	0	0	0	0	0	0	0	4	43
Percent	4.7%	60.5%	20.9%	0%	4.7%	0%	0%	0%	0%	0%	0%	0%	0%	9.3%	
ADT 43															
AM Peak Volume	12:00 AM	11:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	8:00 AM	11:00 AM
	0	5	2	0	0	0	0	0	0	0	0	0	0	2	5
PM Peak Volume	12:00 PM	2:00 PM	1:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM	2:00 PM
	1	4	2	0	2	0	0	0	0	0	0	0	0	1	7

Comments:

LOCATION: (Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889226

DIRECTION: EB, WB

DATE: Nov 11 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
08:00 AM	0	5	0	0	2	0	0	0	0	0	0	0	0	0	7
09:00 AM	0	5	0	0	2	0	0	0	0	0	0	0	0	0	7
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	2	6
12:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	2	3	0	0	0	0	0	0	0	0	0	0	0	0	5
02:00 PM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5
03:00 PM	2	7	0	0	0	0	0	1	0	0	0	0	0	0	10
04:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	1	3
05:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	5	36	8	0	4	0	0	1	0	0	0	0	0	3	57
Percent	8.8%	63.2%	14%	0%	7%	0%	0%	1.8%	0%	0%	0%	0%	0%	5.3%	
ADT 57															
AM Peak Volume	12:00 AM 0	8:00 AM 5	7:00 AM 2	12:00 AM 0	8:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 2	8:00 AM 7
PM Peak Volume	1:00 PM 2	3:00 PM 7	2:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	4:00 PM 1	3:00 PM 10

Comments:

LOCATION: (Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889226

DIRECTION: EB, WB

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
11:00 AM	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
12:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
01:00 PM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	5	0	0	0	0	0	0	0	0	0	0	0	5
08:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
09:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	16	11	0	2	0	0	0	0	0	0	0	0	0	29
Percent	0%	55.2%	37.9%	0%	6.9%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 29															
AM Peak Volume	12:00 AM	10:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	3	4	0	0	0	0	0	0	0	0	0	0	0	7
PM Peak Volume	12:00 PM	12:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	7:00 PM
	0	2	5	0	1	0	0	0	0	0	0	0	0	0	5

Comments:

LOCATION: (Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889226

DIRECTION: EB, WB

DATE: Nov 13 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
10:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	1	4	2	0	0	0	0	0	0	0	0	0	0	1	8
12:00 PM	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
01:00 PM	0	8	3	0	0	0	0	0	0	0	0	0	0	0	11
02:00 PM	0	9	3	0	0	0	0	0	0	0	0	0	0	0	12
03:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
04:00 PM	0	5	2	0	0	0	0	0	0	0	0	0	0	1	8
05:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	46	16	0	0	0	0	0	0	0	0	0	0	2	65
Percent	1.5%	70.8%	24.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	3.1%	
ADT 65															
AM Peak Volume	11:00 AM	9:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM
	1	6	2	0	0	0	0	0	0	0	0	0	0	1	8
PM Peak Volume	12:00 PM	2:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	2:00 PM
	0	9	3	0	0	0	0	0	0	0	0	0	0	1	12

Comments:

LOCATION: (Peak) Comfort Station Parking Area


SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889226

DIRECTION: EB, WB

DATE: Nov 14 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
10:00 AM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
Day Total	0	13	10	0	0	0	0	0	0	0	0	0	0	0	23
Percent	0%	56.5%	43.5%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 23															
AM Peak Volume	12:00 AM	9:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	2	4	0	0	0	0	0	0	0	0	0	0	0	4
PM Peak Volume	12:00 PM	1:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM
	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4

Comments:

LOCATION: (Peak) Comfort Station Parking Area

SPECIFIC LOCATION:

CITY/STATE: Colbert, AL

QC JOB #: 15889226

DIRECTION: EB, WB

DATE: Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
09:00 AM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	1	1	0	0	1	0	0	2	2	0	0	0	0	0	7
12:00 PM	1	3	1	0	4	0	0	1	2	0	0	0	0	0	12
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	5	0	0	0	0	0	1	0	0	0	0	0	0	6
03:00 PM	0	3	2	0	0	0	0	1	0	0	0	0	0	0	6
04:00 PM	0	2	2	0	2	0	0	2	0	0	0	0	0	0	8
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	20	11	0	7	0	0	8	4	0	0	0	0	0	52
Percent	3.8%	38.5%	21.2%	0%	13.5%	0%	0%	15.4%	7.7%	0%	0%	0%	0%	0%	
ADT 52															
AM Peak Volume	11:00 AM	2:00 AM	9:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	1	2	4	0	1	0	0	2	2	0	0	0	0	0	7
PM Peak Volume	12:00 PM	2:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	1	5	2	0	4	0	0	2	2	0	0	0	0	0	12

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	14	192	74	0	16	0	0	9	4	0	0	0	0	10	319
Percent	4.4%	60.2%	23.2%	0%	5%	0%	0%	2.8%	1.3%	0%	0%	0%	0%	3.1%	
ADT 45															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Comfort Station Parking Area										QC JOB #: 15889226
SPECIFIC LOCATION:										DIRECTION: EB, WB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			0	1	0	0	0	0	0	
01:00 AM			0	0	0	0	0	0	0	
02:00 AM			2	2	0	1	0	0	1	<div></div>
03:00 AM			2	2	0	1	0	0	1	<div></div>
04:00 AM			0	0	0	0	0	0	0	
05:00 AM			0	0	0	0	1	1	0	
06:00 AM			1	0	1	1	0	2	1	<div></div>
07:00 AM			3	2	4	3	0	1	2	<div></div>
08:00 AM			3	2	7	4	0	0	2	<div></div>
09:00 AM			4	3	7	5	0	6	4	<div></div>
10:00 AM			6	4	0	3	3	2	3	<div></div>
11:00 AM			5	5	6	5	7	8	6	<div></div>
12:00 PM			1	4	2	2	3	6	3	<div></div>
01:00 PM			2	6	5	4	3	11	5	<div></div>
02:00 PM			5	7	5	6	2	12	6	<div></div>
03:00 PM			4	0	10	5	0	5	4	<div></div>
04:00 PM			7	2	3	4	0	8	4	<div></div>
05:00 PM			1	3	3	2	0	1	2	<div></div>
06:00 PM			1	0	1	1	0	0	0	
07:00 PM			2	0	2	1	5	0	2	<div></div>
08:00 PM			1	0	0	0	3	2	1	<div></div>
09:00 PM			0	0	1	0	2	0	1	<div></div>
10:00 PM			0	0	0	0	0	0	0	
11:00 PM			0	0	0	0	0	0	0	
Day Total			50	43	57	48	29	65	48	
% Weekday Average			104.2%	89.6%	118.8%					
% Week Average			104.2%	89.6%	118.8%	100%	60.4%	135.4%		
AM Peak Volume			10:00 AM 6	11:00 AM 5	8:00 AM 7	9:00 AM 5	11:00 AM 7	11:00 AM 8	11:00 AM 6	
PM Peak Volume			4:00 PM 7	2:00 PM 7	3:00 PM 10	2:00 PM 6	7:00 PM 5	2:00 PM 12	2:00 PM 6	
Comments:										

Report generated on 11/29/2022 11:07 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Comfort Station Parking Area							QC JOB #: 15889226			
SPECIFIC LOCATION:							DIRECTION: EB, WB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0				0			0	
01:00 AM	0	0				0			0	
02:00 AM	0	2				1			1	<div></div>
03:00 AM	0	2				1			1	<div></div>
04:00 AM	1	0				1			1	<div></div>
05:00 AM	0	0				0			0	
06:00 AM	0	0				0			0	
07:00 AM	2	0				1			1	<div></div>
08:00 AM	1	1				1			1	<div></div>
09:00 AM	4	4				4			4	<div></div>
10:00 AM	4	1				3			3	<div></div>
11:00 AM	2	7				5			5	<div></div>
12:00 PM	0	12				6			6	<div></div>
01:00 PM	2	1				2			2	<div></div>
02:00 PM	2	6				4			4	<div></div>
03:00 PM	4	6				5			5	<div></div>
04:00 PM	0	8				4			4	<div></div>
05:00 PM	0	0				0			0	
06:00 PM	0	0				0			0	
07:00 PM	0	0				0			0	
08:00 PM	0	0				0			0	
09:00 PM	0	2				1			1	<div></div>
10:00 PM	0	0				0			0	
11:00 PM	1	0				1			1	<div></div>
Day Total	23	52				40			40	
% Weekday Average	57.5%	130%								
% Week Average	57.5%	130%				100%				
AM Peak Volume	9:00 AM 4	11:00 AM 7				11:00 AM 5			11:00 AM 5	
PM Peak Volume	3:00 PM 4	12:00 PM 12				12:00 PM 6			12:00 PM 6	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 9 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	1	0	0	0	0	1	0	0	0	0	0	0	0	2	11-20	1
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
09:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
10:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
11:00 AM	0	0	2	0	1	0	0	0	0	0	0	0	0	0	3	16-25	2
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
02:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
03:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	16-25	1
04:00 PM	0	0	0	2	2	0	0	0	0	0	0	0	0	0	4	26-35	4
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
06:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
07:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	2	8	3	9	1	1	0	0	0	0	0	0	0	25	26-35	12
Percent	4%	8%	32%	12%	36%	4%	4%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	7:00 AM	9:00 AM	12:00 AM	10:00 AM	3:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	2	0	2	1	1	0	0	0	0	0	0	0	3		
PM Peak Volume	3:00 PM	12:00 PM	12:00 PM	4:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM		
	1	0	1	2	2	0	0	0	0	0	0	0	0	0	4		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 10 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	26-35	1
10:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
11:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
12:00 PM	1	0	0	0	0	1	0	0	0	0	0	0	0	0	2	31-40	1
01:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	21-30	2
02:00 PM	1	0	0	2	0	1	0	0	0	0	0	0	0	0	4	21-30	2
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	26-35	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	0	1	8	6	2	0	0	0	0	0	0	0	0	21	26-35	14
Percent	19%	0%	4.8%	38.1%	28.6%	9.5%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	9:00 AM 1	12:00 AM 0	12:00 AM 0	10:00 AM 2	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 2		
PM Peak Volume	12:00 PM 1	12:00 PM 0	1:00 PM 1	2:00 PM 2	1:00 PM 1	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 4		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 11 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	16-25	2
08:00 AM	0	0	0	0	2	1	0	0	0	0	0	0	0	0	3	31-40	3
09:00 AM	0	2	0	1	0	0	0	0	0	0	0	0	0	0	3	11-20	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	2	0	1	0	1	0	0	0	0	0	0	0	0	0	4	1-10	1
12:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
01:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
02:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
03:00 PM	0	1	1	4	0	0	0	0	0	0	0	0	0	0	6	21-30	5
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	26-35	1
06:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
07:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	3	5	6	6	4	0	0	0	0	0	0	0	0	27	26-35	12
Percent	11.1%	11.1%	18.5%	22.2%	22.2%	14.8%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	9:00 AM 2	7:00 AM 2	9:00 AM 1	8:00 AM 2	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 4		
PM Peak Volume	5:00 PM 1	3:00 PM 1	3:00 PM 1	3:00 PM 4	1:00 PM 1	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 6		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 12 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
11:00 AM	0	1	0	3	0	0	0	0	0	0	0	0	0	0	4	21-30	3
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
01:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	11-20	1
02:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	26-35	2
08:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
09:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	3	2	6	4	0	0	0	0	0	0	0	0	0	15	26-35	10
Percent	0%	20%	13.3%	40%	26.7%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	10:00 AM	5:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	1	3	0	0	0	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	1:00 PM	12:00 PM	1:00 PM	7:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM		
	0	1	1	1	2	0	0	0	0	0	0	0	0	0	2		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 13 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
07:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	11-20	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	2	0	1	0	0	0	0	0	0	0	3	26-35	2
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
11:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	26-35	2
12:00 PM	1	1	0	0	1	1	0	0	0	0	0	0	0	0	4	31-40	2
01:00 PM	0	0	0	1	0	4	0	0	0	0	0	0	0	0	5	31-40	4
02:00 PM	0	1	1	3	1	1	0	0	0	0	0	0	0	0	7	23-32	4
03:00 PM	0	0	0	1	0	1	0	0	0	0	0	0	0	0	2	21-30	1
04:00 PM	0	0	1	1	0	1	0	0	0	0	0	0	0	0	3	21-30	2
05:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2	31-40	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	3	3	7	6	10	1	0	0	0	0	0	0	0	32	31-40	16
Percent	6.3%	9.4%	9.4%	21.9%	18.8%	31.3%	3.1%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	7:00 AM	10:00 AM	11:00 AM	9:00 AM	6:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM		
	0	1	1	1	2	1	1	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	12:00 PM	2:00 PM	2:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	1	1	1	3	1	4	0	0	0	0	0	0	0	0	7		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 14 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
08:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
09:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	16-25	1
10:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	21-30	2
11:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	16-25	1
03:00 PM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2	16-25	1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	0	4	3	3	1	0	0	0	0	0	0	0	0	11	21-30	7
Percent	0%	0%	36.4%	27.3%	27.3%	9.1%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	9:00 AM	10:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM		
	0	0	1	2	1	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	0	0	1	0	0	1	0	0	0	0	0	0	0	0	2		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Comfort Station Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889226 DIRECTION: WB DATE: Nov 15 2022		
Start Time	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	26-35	1
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	21-30	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	21-30	3
12:00 PM	1	2	1	0	0	2	0	1	0	0	0	0	0	0	7	16-25	3
01:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
02:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	31-40	1
03:00 PM	0	0	2	2	1	0	0	0	0	0	0	0	0	0	5	21-30	4
04:00 PM	0	0	2	1	0	1	0	0	0	0	0	0	0	0	4	21-30	3
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	21-30	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	2	8	8	2	4	0	1	0	0	0	0	0	0	26	21-30	16
Percent	3.8%	7.7%	30.8%	30.8%	7.7%	15.4%	0%	3.8%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	12:00 AM 0	11:00 AM 2	3:00 AM 1	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3		
PM Peak Volume	12:00 PM 1	12:00 PM 2	3:00 PM 2	3:00 PM 2	3:00 PM 1	12:00 PM 2	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 7		
Comments:																	

Report generated on 11/29/2022 11:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Comfort Station Parking Area															QC JOB #: 15889226		
SPECIFIC LOCATION:															DIRECTION: WB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	1 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total Percent	11 7%	13 8.3%	31 19.7%	41 26.1%	36 22.9%	22 14%	2 1.3%	1 0.6%	0 0%	0 0%	0 0%	0 0%	0 0%	0 0%	157	26-35	77
Cumulative Percent	7%	15.3%	35%	61.1%	84.1%	98.1%	99.4%	100%	100%	100%	100%	100%	100%	100%			
ADT 22															85th Percentile: 35 MPH Mean Speed(Average): 27 MPH Median: 27 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	1	2
04:00 PM	2	1	1	0	0	0	0	0	0	0	0	0	0	0	4
05:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	17	4	0	1	0	0	0	0	0	0	0	0	1	25
Percent	8%	68%	16%	0%	4%	0%	0%	0%	0%	0%	0%	0%	0%	4%	
ADT 25															
AM Peak Volume	12:00 AM	11:00 AM	7:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	3	1	0	0	0	0	0	0	0	0	0	0	0	3
PM Peak Volume	4:00 PM	2:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM	4:00 PM
	2	2	1	0	1	0	0	0	0	0	0	0	0	1	4

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 10 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	2
10:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3
02:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	1	4
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	13	5	0	1	0	0	0	0	0	0	0	0	2	21
Percent	0%	61.9%	23.8%	0%	4.8%	0%	0%	0%	0%	0%	0%	0%	0%	9.5%	
ADT 21															
AM Peak Volume	12:00 AM 0	7:00 AM 2	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	7:00 AM 2
PM Peak Volume	12:00 PM 0	2:00 PM 2	12:00 PM 1	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	2:00 PM 4

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 11 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
09:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	2	4
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	1	4	0	0	0	0	0	1	0	0	0	0	0	0	6
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	17	3	0	2	0	0	1	0	0	0	0	0	2	27
Percent	7.4%	63%	11.1%	0%	7.4%	0%	0%	3.7%	0%	0%	0%	0%	0%	7.4%	
ADT 27															
AM Peak Volume	12:00 AM 0	8:00 AM 2	7:00 AM 1	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 2	11:00 AM 4
PM Peak Volume	1:00 PM 1	3:00 PM 4	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 6

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
08:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	8	6	0	1	0	0	0	0	0	0	0	0	0	15
Percent	0%	53.3%	40%	0%	6.7%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 15															
AM Peak Volume	12:00 AM	11:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
PM Peak Volume	12:00 PM	12:00 PM	7:00 PM	12:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM
	0	1	2	0	1	0	0	0	0	0	0	0	0	0	2

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 13 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classified	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
12:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
01:00 PM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5
02:00 PM	0	5	2	0	0	0	0	0	0	0	0	0	0	0	7
03:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
05:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	23	9	0	0	0	0	0	0	0	0	0	0	0	32
Percent	0%	71.9%	28.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 32															
AM Peak Volume	12:00 AM	9:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	3	1	0	0	0	0	0	0	0	0	0	0	0	3
PM Peak Volume	12:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM
	0	5	2	0	0	0	0	0	0	0	0	0	0	0	7

Comments:

LOCATION: (Peak) Comfort Station Parking Area


QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 14 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	6	5	0	0	0	0	0	0	0	0	0	0	0	11
Percent	0%	54.5%	45.5%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 11															
AM Peak Volume	12:00 AM	8:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM
	0	1	2	0	0	0	0	0	0	0	0	0	0	0	2
PM Peak Volume	12:00 PM	1:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM
	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	1	0	0	0	0	0	1	1	0	0	0	0	0	3
12:00 PM	1	1	0	0	3	0	0	1	1	0	0	0	0	0	7
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	3	1	0	0	0	0	1	0	0	0	0	0	0	5
04:00 PM	0	1	1	0	1	0	0	1	0	0	0	0	0	0	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	10	5	0	4	0	0	4	2	0	0	0	0	0	26
Percent	3.8%	38.5%	19.2%	0%	15.4%	0%	0%	15.4%	7.7%	0%	0%	0%	0%	0%	
ADT 26															
AM Peak Volume	12:00 AM	2:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	1	2	0	0	0	0	1	1	0	0	0	0	0	3
PM Peak Volume	12:00 PM	3:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	1	3	1	0	3	0	0	1	1	0	0	0	0	0	7

Comments:

LOCATION: (Peak) Comfort Station Parking Area

QC JOB #: 15889226

SPECIFIC LOCATION:

DIRECTION: WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	5	94	37	0	9	0	0	5	2	0	0	0	0	5	157
Percent	3.2%	59.9%	23.6%	0%	5.7%	0%	0%	3.2%	1.3%	0%	0%	0%	0%	3.2%	
ADT 22															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Comfort Station Parking Area										QC JOB #: 15889226
SPECIFIC LOCATION:										DIRECTION: WB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			0	0	0	0	0	0	0	
01:00 AM			0	0	0	0	0	0	0	
02:00 AM			1	1	0	1	0	0	0	
03:00 AM			1	1	0	1	0	0	0	
04:00 AM			0	0	0	0	0	0	0	
05:00 AM			0	0	0	0	1	0	0	
06:00 AM			0	0	0	0	0	1	0	
07:00 AM			2	2	2	2	0	1	1	<div></div>
08:00 AM			1	0	3	1	0	0	1	<div></div>
09:00 AM			2	2	3	2	0	3	2	<div></div>
10:00 AM			2	2	0	1	1	1	1	<div></div>
11:00 AM			3	2	4	3	4	2	3	<div></div>
12:00 PM			1	2	1	1	1	4	2	<div></div>
01:00 PM			1	3	2	2	2	5	3	<div></div>
02:00 PM			2	4	2	3	1	7	3	<div></div>
03:00 PM			2	0	6	3	0	2	2	<div></div>
04:00 PM			4	0	0	1	0	3	1	<div></div>
05:00 PM			1	2	2	2	0	1	1	<div></div>
06:00 PM			1	0	1	1	0	0	0	
07:00 PM			1	0	1	1	2	0	1	<div></div>
08:00 PM			0	0	0	0	2	2	1	<div></div>
09:00 PM			0	0	0	0	1	0	0	
10:00 PM			0	0	0	0	0	0	0	
11:00 PM			0	0	0	0	0	0	0	
Day Total			25	21	27	25	15	32	22	
% Weekday Average			100%	84%	108%					
% Week Average			113.6%	95.5%	122.7%	113.6%	68.2%	145.5%		
AM Peak Volume			11:00 AM 3	7:00 AM 2	11:00 AM 4	11:00 AM 3	11:00 AM 4	9:00 AM 3	11:00 AM 3	
PM Peak Volume			4:00 PM 4	2:00 PM 4	3:00 PM 6	2:00 PM 3	1:00 PM 2	2:00 PM 7	1:00 PM 3	
Comments:										

Report generated on 11/29/2022 11:07 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Comfort Station Parking Area							QC JOB #: 15889226			
SPECIFIC LOCATION:							DIRECTION: WB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0				0			0	
01:00 AM	0	0				0			0	
02:00 AM	0	1				1			1	<div></div>
03:00 AM	0	1				1			1	<div></div>
04:00 AM	0	0				0			0	
05:00 AM	0	0				0			0	
06:00 AM	0	0				0			0	
07:00 AM	1	0				1			1	<div></div>
08:00 AM	1	0				1			1	<div></div>
09:00 AM	2	2				2			2	<div></div>
10:00 AM	2	0				1			1	<div></div>
11:00 AM	1	3				2			2	<div></div>
12:00 PM	0	7				4			4	<div></div>
01:00 PM	1	1				1			1	<div></div>
02:00 PM	1	1				1			1	<div></div>
03:00 PM	2	5				4			4	<div></div>
04:00 PM	0	4				2			2	<div></div>
05:00 PM	0	0				0			0	
06:00 PM	0	0				0			0	
07:00 PM	0	0				0			0	
08:00 PM	0	0				0			0	
09:00 PM	0	1				1			1	<div></div>
10:00 PM	0	0				0			0	
11:00 PM	0	0				0			0	
Day Total	11	26				22			22	
% Weekday Average	50%	118.2%								
% Week Average	50%	118.2%				100%				
AM Peak Volume	9:00 AM 2	11:00 AM 3				9:00 AM 2			9:00 AM 2	
PM Peak Volume	3:00 PM 2	12:00 PM 7				12:00 PM 4			12:00 PM 4	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 9 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
11:00 AM	2	1	2	0	0	0	0	0	0	0	0	0	0	0	5	17-26	3
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
01:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	17-26	1
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	2	7	1	2	0	0	0	0	0	0	0	0	0	14	17-26	9
Percent	14.3%	14.3%	50%	7.1%	14.3%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	11:00 AM 1	11:00 AM 2	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 5		
PM Peak Volume	12:00 PM 0	12:00 PM 1	3:00 PM 2	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 2		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 10 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
09:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	12-21	2
10:00 AM	1	1	1	1	0	0	0	0	0	0	0	0	0	0	4	17-26	2
11:00 AM	0	0	0	3	0	0	0	0	0	0	0	0	0	0	3	22-31	3
12:00 PM	0	1	0	1	1	0	0	0	0	0	0	0	0	0	3	27-36	2
01:00 PM	2	1	1	2	0	0	0	0	0	0	0	0	0	0	6	22-31	3
02:00 PM	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	22-31	3
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	1
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	6	8	9	1	0	0	0	0	0	0	0	0	0	28	22-31	17
Percent	14.3%	21.4%	28.6%	32.1%	3.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	10:00 AM 1	9:00 AM 2	2:00 AM 2	11:00 AM 3	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 4		
PM Peak Volume	1:00 PM 2	12:00 PM 1	1:00 PM 1	1:00 PM 2	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 6		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 11 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
08:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	22-31	2
12:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	22-31	2
01:00 PM	0	1	0	2	1	0	0	0	0	0	0	0	0	0	4	27-36	3
02:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
03:00 PM	0	0	3	3	0	0	0	0	0	0	0	0	0	0	6	22-31	6
04:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
05:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	1	8	14	3	0	0	0	0	0	0	0	0	0	27	22-31	22
Percent	3.7%	3.7%	29.6%	51.9%	11.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	12:00 AM 0	11:00 AM 1	7:00 AM 2	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3		
PM Peak Volume	7:00 PM 1	1:00 PM 1	3:00 PM 3	3:00 PM 3	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 6		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 12 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
09:00 AM	0	0	0	3	0	0	0	0	0	0	0	0	0	0	3	22-31	3
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
11:00 AM	0	0	1	2	2	0	0	0	0	0	0	0	0	0	5	27-36	4
12:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
02:00 PM	0	0	0	1	2	1	0	0	0	0	0	0	0	0	4	32-41	3
03:00 PM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	12-21	1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	4	3	8	4	1	0	0	0	0	0	0	0	0	20	27-36	12
Percent	0%	20%	15%	40%	20%	5%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	8:00 AM	11:00 AM	9:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	1	3	2	0	0	0	0	0	0	0	0	0	5		
PM Peak Volume	12:00 PM	1:00 PM	8:00 PM	12:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	0	1	2	1	2	1	0	0	0	0	0	0	0	0	4		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 13 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
10:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
11:00 AM	0	0	1	1	0	1	0	0	0	0	0	0	0	0	3	22-31	2
12:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	27-36	2
01:00 PM	0	1	1	1	4	0	0	0	0	0	0	0	0	0	7	27-36	5
02:00 PM	0	0	2	2	0	0	0	0	0	0	0	0	0	0	4	22-31	4
03:00 PM	0	0	0	0	1	2	0	3	1	0	0	0	0	0	7	47-56	4
04:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	2	5	10	6	3	0	3	1	0	0	0	0	0	30	27-36	16
Percent	0%	6.7%	16.7%	33.3%	20%	10%	0%	10%	3.3%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	6:00 AM	11:00 AM	9:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	1	2	0	1	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	1:00 PM	2:00 PM	2:00 PM	1:00 PM	3:00 PM	12:00 PM	3:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM		
	0	1	2	2	4	2	0	3	1	0	0	0	0	0	7		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 14 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
10:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
03:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	17-26	1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	0	4	2	2	0	0	0	0	0	0	0	0	0	8	22-31	6
Percent	0%	0%	50%	25%	25%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 2	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 2		
PM Peak Volume	12:00 PM 0	12:00 PM 0	12:00 PM 1	12:00 PM 0	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 2		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB DATE: Nov 15 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
10:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
11:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	12-21	2
12:00 PM	0	1	2	3	0	0	0	0	0	0	0	0	0	0	6	22-31	5
01:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
02:00 PM	0	2	0	1	0	1	0	0	0	0	0	0	0	0	4	12-21	2
03:00 PM	0	0	4	2	0	0	0	0	0	0	0	0	0	0	6	22-31	6
04:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4	17-26	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1-10	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	8	13	10	0	1	0	0	0	0	0	0	0	0	33	22-31	23
Percent	3%	24.2%	39.4%	30.3%	0%	3%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	2:00 AM	8:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	2:00 AM		
	0	2	2	2	0	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	9:00 PM	2:00 PM	3:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	1	2	4	3	0	1	0	0	0	0	0	0	0	0	6		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Overlook Rd Parking Area															QC JOB #: 15889227		
SPECIFIC LOCATION:															DIRECTION: EB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	8	23	48	54	18	5	0	3	1	0	0	0	0	0	160	22-31	102
Percent	5%	14.4%	30%	33.8%	11.3%	3.1%	0%	1.9%	0.6%	0%	0%	0%	0%	0%			
Cumulative Percent	5%	19.4%	49.4%	83.1%	94.4%	97.5%	97.5%	99.4%	100%	100%	100%	100%	100%	100%			
ADT 22															85th Percentile: 30 MPH Mean Speed(Average): 25 MPH Median: 25 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 9 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
11:00 AM	3	0	0	0	0	1	0	0	0	0	0	0	0	1	5
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	5	1	0	0	0	2	0	0	0	0	0	0	0	6	14
Percent	35.7%	7.1%	0%	0%	0%	14.3%	0%	0%	0%	0%	0%	0%	0%	42.9%	
ADT 14															
AM Peak Volume	11:00 AM 3	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 1	11:00 AM 5
PM Peak Volume	3:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	9:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 2	1:00 PM 2

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 10 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
09:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	1	2
10:00 AM	1	1	0	0	0	0	0	0	0	0	0	0	0	2	4
11:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	2	3
12:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	2	3
01:00 PM	0	3	1	0	0	2	0	0	0	0	0	0	0	0	6
02:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	2	3
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	7	1	0	0	5	0	0	0	0	0	0	0	12	28
Percent	10.7%	25%	3.6%	0%	0%	17.9%	0%	0%	0%	0%	0%	0%	0%	42.9%	
ADT 28															
AM Peak Volume	10:00 AM 1	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 2	10:00 AM 4
PM Peak Volume	4:00 PM 2	1:00 PM 3	1:00 PM 1	12:00 PM 0	12:00 PM 0	1:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 2	1:00 PM 6


Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 11 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	2
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	2
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	3	3
12:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	1	3
01:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	2	4
02:00 PM	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	1	0	0	0	2	0	0	0	0	0	0	0	3	6
04:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	1	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	5	2	0	0	4	0	0	0	0	0	0	0	13	27
Percent	11.1%	18.5%	7.4%	0%	0%	14.8%	0%	0%	0%	0%	0%	0%	0%	48.1%	
ADT 27															
AM Peak Volume	12:00 AM 0	9:00 AM 1	7:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3	11:00 AM 3
PM Peak Volume	12:00 PM 1	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 3	3:00 PM 6

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 12 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	1	1	0	0	0	0	0	0	0	0	0	0	0	1	3
10:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
11:00 AM	0	3	0	0	0	1	0	0	0	0	0	0	0	1	5
12:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
02:00 PM	0	2	1	0	0	1	0	0	0	0	0	0	0	0	4
03:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	7	3	0	0	4	0	0	0	0	0	0	0	5	20
Percent	5%	35%	15%	0%	0%	20%	0%	0%	0%	0%	0%	0%	0%	25%	
ADT 20															
AM Peak Volume	9:00 AM	11:00 AM	8:00 AM	12:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM	11:00 AM
	1	3	1	0	0	1	0	0	0	0	0	0	0	1	5
PM Peak Volume	12:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	8:00 PM	2:00 PM
	0	2	1	0	0	1	0	0	0	0	0	0	0	2	4

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 13 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	1	0	0	0	0	0	0	0	1	0	0	0	0	2
11:00 AM	1	1	0	0	0	0	0	0	0	0	0	0	0	1	3
12:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	1	5	1	0	0	0	0	0	0	0	0	0	0	0	7
02:00 PM	1	2	0	0	0	0	0	0	0	0	0	0	0	1	4
03:00 PM	0	7	0	0	0	0	0	0	0	0	0	0	0	0	7
04:00 PM	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	19	2	0	0	2	0	0	0	1	0	0	0	3	30
Percent	10%	63.3%	6.7%	0%	0%	6.7%	0%	0%	0%	3.3%	0%	0%	0%	10%	
ADT 30															
AM Peak Volume	11:00 AM 1	9:00 AM 1	9:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1	11:00 AM 3
PM Peak Volume	1:00 PM 1	3:00 PM 7	1:00 PM 1	12:00 PM 0	12:00 PM 0	4:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	1:00 PM 7

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 14 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	1	2
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	2	1	0	0	2	0	0	0	0	0	0	0	3	8
Percent	0%	25%	12.5%	0%	0%	25%	0%	0%	0%	0%	0%	0%	0%	37.5%	
ADT 8															
AM Peak Volume	12:00 AM 0	12:00 AM 0	9:00 AM 1	12:00 AM 0	12:00 AM 0	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 1	10:00 AM 2
PM Peak Volume	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 1	3:00 PM 2

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB**CITY/STATE:** Colbert, AL**DATE:** Nov 15 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	1	0	0	0	0	1	0	0	0	0	0	0	0	0	2
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	2	0	0	0	0	0	0	0	0	2
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	2	2
12:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	4	6
01:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2
02:00 PM	1	0	1	0	0	1	0	0	0	0	0	0	0	1	4
03:00 PM	2	0	0	0	1	2	0	0	0	0	0	0	0	1	6
04:00 PM	0	1	0	0	0	1	0	0	0	1	0	0	0	1	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	8	2	1	0	2	10	0	0	0	1	0	0	0	9	33
Percent	24.2%	6.1%	3%	0%	6.1%	30.3%	0%	0%	0%	3%	0%	0%	0%	27.3%	
ADT 33															
AM Peak Volume	2:00 AM	12:00 AM	12:00 AM	12:00 AM	9:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	2:00 AM
	2	0	0	0	1	2	0	0	0	0	0	0	0	2	2
PM Peak Volume	12:00 PM	1:00 PM	2:00 PM	12:00 PM	3:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM
	2	1	1	0	1	2	0	0	0	1	0	0	0	4	6

Comments:

LOCATION: (Peak) Overlook Rd Parking Area

QC JOB #: 15889227

SPECIFIC LOCATION:

DIRECTION: EB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
Grand Total	23	43	10	0	2	29	0	0	0	2	0	0	0	51	160
Percent	14.4%	26.9%	6.3%	0%	1.3%	18.1%	0%	0%	0%	1.3%	0%	0%	0%	31.9%	
ADT 22															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Overlook Rd Parking Area										QC JOB #: 15889227
SPECIFIC LOCATION:										DIRECTION: EB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			0	0	0	0	0	0	0	
01:00 AM			0	0	0	0	0	0	0	
02:00 AM			1	2	0	1	0	0	1	<div></div>
03:00 AM			0	1	0	0	0	0	0	
04:00 AM			0	0	0	0	0	0	0	
05:00 AM			0	0	0	0	0	0	0	
06:00 AM			0	0	0	0	0	1	0	
07:00 AM			1	1	2	1	0	0	1	<div></div>
08:00 AM			0	1	2	1	1	0	1	<div></div>
09:00 AM			0	2	1	1	3	2	2	<div></div>
10:00 AM			1	4	0	2	1	2	2	<div></div>
11:00 AM			5	3	3	4	5	3	4	<div></div>
12:00 PM			1	3	3	2	1	2	2	<div></div>
01:00 PM			2	6	4	4	1	7	4	<div></div>
02:00 PM			0	3	2	2	4	4	3	<div></div>
03:00 PM			2	0	6	3	2	7	3	<div></div>
04:00 PM			0	2	2	1	0	2	1	<div></div>
05:00 PM			0	0	1	0	0	0	0	
06:00 PM			0	0	0	0	0	0	0	
07:00 PM			0	0	1	0	0	0	0	
08:00 PM			0	0	0	0	2	0	0	
09:00 PM			1	0	0	0	0	0	0	
10:00 PM			0	0	0	0	0	0	0	
11:00 PM			0	0	0	0	0	0	0	
Day Total			14	28	27	22	20	30	24	
% Weekday Average			63.6%	127.3%	122.7%					
% Week Average			58.3%	116.7%	112.5%	91.7%	83.3%	125%		
AM Peak Volume			11:00 AM 5	10:00 AM 4	11:00 AM 3	11:00 AM 4	11:00 AM 5	11:00 AM 3	11:00 AM 4	
PM Peak Volume			1:00 PM 2	1:00 PM 6	3:00 PM 6	1:00 PM 4	2:00 PM 4	1:00 PM 7	1:00 PM 4	
Comments:										

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Overlook Rd Parking Area							QC JOB #: 15889227			
SPECIFIC LOCATION:							DIRECTION: EB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0				0			0	
01:00 AM	0	0				0			0	
02:00 AM	0	2				1			1	<div></div>
03:00 AM	0	1				1			1	<div></div>
04:00 AM	0	0				0			0	
05:00 AM	0	0				0			0	
06:00 AM	0	0				0			0	
07:00 AM	0	0				0			0	
08:00 AM	0	2				1			1	<div></div>
09:00 AM	1	1				1			1	<div></div>
10:00 AM	2	2				2			2	<div></div>
11:00 AM	0	2				1			1	<div></div>
12:00 PM	1	6				4			4	<div></div>
01:00 PM	0	2				1			1	<div></div>
02:00 PM	1	4				3			3	<div></div>
03:00 PM	2	6				4			4	<div></div>
04:00 PM	0	4				2			2	<div></div>
05:00 PM	0	0				0			0	
06:00 PM	0	0				0			0	
07:00 PM	0	0				0			0	
08:00 PM	0	0				0			0	
09:00 PM	0	1				1			1	<div></div>
10:00 PM	1	0				1			1	<div></div>
11:00 PM	0	0				0			0	
Day Total	8	33				23			23	
% Weekday Average	34.8%	143.5%								
% Week Average	34.8%	143.5%				100%				
AM Peak Volume	10:00 AM 2	2:00 AM 2				10:00 AM 2			10:00 AM 2	
PM Peak Volume	3:00 PM 2	12:00 PM 6				12:00 PM 4			12:00 PM 4	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 9 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
11:00 AM	2	2	4	0	0	0	0	0	0	0	0	0	0	0	8	17-26	6
12:00 PM	2	1	1	0	0	0	0	0	0	0	0	0	0	0	4	17-26	2
01:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	22-31	2
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
03:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
04:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	3	14	5	2	0	0	0	0	0	0	0	0	0	28	22-31	19
Percent	14.3%	10.7%	50%	17.9%	7.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	11:00 AM 2	11:00 AM 2	11:00 AM 4	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 8		
PM Peak Volume	12:00 PM 2	12:00 PM 1	3:00 PM 2	1:00 PM 1	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 4		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 10 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	22-31	3
03:00 AM	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3	17-26	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
08:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	22-31	3
09:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	17-26	3
10:00 AM	2	2	2	1	1	0	0	0	0	0	0	0	0	0	8	17-26	4
11:00 AM	0	1	0	6	0	0	0	0	0	0	0	0	0	0	7	22-31	6
12:00 PM	1	1	0	2	2	0	0	0	0	0	0	0	0	0	6	27-36	4
01:00 PM	2	2	1	5	0	0	0	0	0	0	0	0	0	0	10	22-31	6
02:00 PM	0	2	1	5	0	0	0	0	0	0	0	0	0	0	8	22-31	6
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	2	0	1	1	0	0	0	0	0	0	0	0	0	0	4	22-31	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	8	11	12	22	3	0	0	0	0	0	0	0	0	0	56	22-31	34
Percent	14.3%	19.6%	21.4%	39.3%	5.4%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	10:00 AM 2	9:00 AM 2	2:00 AM 2	11:00 AM 6	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 8		
PM Peak Volume	1:00 PM 2	1:00 PM 2	1:00 PM 1	1:00 PM 5	12:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 10		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 11 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	3	0	0	0	0	0	0	0	0	0	0	4	22-31	4
08:00 AM	0	0	0	3	1	0	0	0	0	0	0	0	0	0	4	27-36	4
09:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	1	3	2	0	0	0	0	0	0	0	0	0	6	27-36	5
12:00 PM	0	1	2	1	2	0	0	0	0	0	0	0	0	0	6	22-31	3
01:00 PM	0	1	0	3	2	1	0	0	0	0	0	0	0	0	7	27-36	5
02:00 PM	1	0	2	2	0	0	0	0	0	0	0	0	0	0	5	22-31	4
03:00 PM	1	0	4	4	1	0	0	0	0	0	0	0	0	0	10	22-31	8
04:00 PM	1	0	3	2	0	0	0	0	0	0	0	0	0	0	6	22-31	5
05:00 PM	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	22-31	3
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	2	15	25	8	1	0	0	0	0	0	0	0	0	55	22-31	40
Percent	7.3%	3.6%	27.3%	45.5%	14.5%	1.8%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	12:00 AM 0	7:00 AM 1	7:00 AM 3	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 6		
PM Peak Volume	2:00 PM 1	12:00 PM 1	3:00 PM 4	3:00 PM 4	12:00 PM 2	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 10		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 12 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
09:00 AM	0	1	0	5	0	0	0	0	0	0	0	0	0	0	6	22-31	5
10:00 AM	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3	12-21	2
11:00 AM	0	0	1	5	3	0	0	0	0	0	0	0	0	0	9	27-36	8
12:00 PM	0	0	0	3	0	0	0	0	0	0	0	0	0	0	3	22-31	3
01:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	12-21	2
02:00 PM	0	0	1	1	3	2	0	0	0	0	0	0	0	0	7	32-41	5
03:00 PM	0	2	0	2	0	1	0	0	0	0	0	0	0	0	5	12-21	2
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4	17-26	4
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	9	5	16	7	3	0	0	0	0	0	0	0	0	40	27-36	23
Percent	0%	22.5%	12.5%	40%	17.5%	7.5%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	10:00 AM	11:00 AM	9:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	2	1	5	3	0	0	0	0	0	0	0	0	0	9		
PM Peak Volume	12:00 PM	1:00 PM	8:00 PM	12:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	0	2	3	3	3	2	0	0	0	0	0	0	0	0	7		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 13 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	1	0	3	0	0	0	0	0	0	0	0	0	0	4	22-31	3
10:00 AM	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	22-31	3
11:00 AM	0	0	2	3	0	1	0	0	0	0	0	0	0	0	6	22-31	5
12:00 PM	0	0	0	2	2	0	0	0	0	0	0	0	0	0	4	27-36	4
01:00 PM	0	1	2	3	7	0	0	0	0	0	0	0	0	0	13	27-36	10
02:00 PM	0	0	5	5	0	0	0	0	0	0	0	0	0	0	10	22-31	10
03:00 PM	0	0	0	0	1	2	0	3	1	0	0	0	0	0	7	47-56	4
04:00 PM	0	0	2	1	4	3	1	0	0	0	0	0	0	0	11	32-41	7
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	3	13	19	14	6	1	3	1	0	0	0	0	0	60	27-36	33
Percent	0%	5%	21.7%	31.7%	23.3%	10%	1.7%	5%	1.7%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	6:00 AM	11:00 AM	9:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	2	3	0	1	0	0	0	0	0	0	0	0	6		
PM Peak Volume	12:00 PM	1:00 PM	2:00 PM	2:00 PM	1:00 PM	4:00 PM	4:00 PM	3:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM		
	0	1	5	5	7	3	1	3	1	0	0	0	0	0	13		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 14 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	27-36	2
10:00 AM	0	0	1	3	0	0	0	0	0	0	0	0	0	0	4	22-31	4
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	17-26	1
03:00 PM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	3	27-36	2
04:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	0	6	4	6	0	0	0	0	0	0	0	0	0	16	22-31	10
Percent	0%	0%	37.5%	25%	37.5%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	10:00 AM	10:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	0	1	3	2	0	0	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	10:00 PM	3:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	3:00 PM		
	0	0	2	1	2	0	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 15 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	3	0	1	0	0	0	0	0	0	0	0	0	4	17-26	3
03:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	4	0	0	0	0	0	0	0	0	0	0	4	22-31	4
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
10:00 AM	0	0	3	2	0	0	0	0	0	0	0	0	0	0	5	22-31	5
11:00 AM	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5	17-26	5
12:00 PM	0	1	3	4	1	0	0	0	0	0	0	0	0	0	9	22-31	7
01:00 PM	0	0	4	2	1	0	0	0	0	0	0	0	0	0	7	22-31	6
02:00 PM	0	2	0	3	0	1	0	0	0	0	0	0	0	0	6	22-31	3
03:00 PM	0	1	4	2	0	0	0	0	0	0	0	0	0	0	7	22-31	6
04:00 PM	0	2	3	3	0	0	0	0	0	0	0	0	0	0	8	22-31	6
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	1	11	24	20	3	1	0	0	0	0	0	0	0	0	60	22-31	44
Percent	1.7%	18.3%	40%	33.3%	5%	1.7%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	2:00 AM	8:00 AM	2:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	4	3	4	1	0	0	0	0	0	0	0	0	0	5		
PM Peak Volume	9:00 PM	2:00 PM	1:00 PM	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	1	2	4	4	1	1	0	0	0	0	0	0	0	0	9		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Overlook Rd Parking Area															QC JOB #: 15889227		
SPECIFIC LOCATION:															DIRECTION: EB, WB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	17	39	89	111	43	11	1	3	1	0	0	0	0	0	315	22-31	200
Percent	5.4%	12.4%	28.3%	35.2%	13.7%	3.5%	0.3%	1%	0.3%	0%	0%	0%	0%	0%			
Cumulative Percent	5.4%	17.8%	46%	81.3%	94.9%	98.4%	98.7%	99.7%	100%	100%	100%	100%	100%	100%			
ADT 45															85th Percentile: 31 MPH Mean Speed(Average): 25 MPH Median: 25 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Peak) Overlook Rd Parking Area

QC JOB #: 15889227

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	2
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	1	2
11:00 AM	3	2	1	0	0	1	0	0	0	0	0	0	0	1	8
12:00 PM	2	1	0	0	0	0	0	0	0	0	0	0	0	1	4
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	2	3
02:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	9	6	4	0	1	2	0	0	0	0	0	0	0	6	28
Percent	32.1%	21.4%	14.3%	0%	3.6%	7.1%	0%	0%	0%	0%	0%	0%	0%	21.4%	
ADT 28															
AM Peak Volume	11:00 AM 3	11:00 AM 2	7:00 AM 1	12:00 AM 0	12:00 AM 0	11:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	7:00 AM 1	11:00 AM 8
PM Peak Volume	12:00 PM 2	12:00 PM 1	1:00 PM 1	12:00 PM 0	2:00 PM 1	9:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 2	12:00 PM 4

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Nov 10 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	1	3
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1
08:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	1	3
09:00 AM	0	0	1	0	0	1	0	0	0	0	0	0	0	1	3
10:00 AM	2	3	1	0	0	0	0	0	0	0	0	0	0	2	8
11:00 AM	0	3	1	0	1	0	0	0	0	0	0	0	0	2	7
12:00 PM	0	3	1	0	0	0	0	0	0	0	0	0	0	2	6
01:00 PM	0	4	3	0	1	2	0	0	0	0	0	0	0	0	10
02:00 PM	0	3	2	0	0	1	0	0	0	0	0	0	0	2	8
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	4	0	0	0	0	0	0	0	0	0	0	0	0	0	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	6	21	9	0	3	5	0	0	0	0	0	0	0	12	56
Percent	10.7%	37.5%	16.1%	0%	5.4%	8.9%	0%	0%	0%	0%	0%	0%	0%	21.4%	
ADT 56															
AM Peak Volume	10:00 AM 2	10:00 AM 3	9:00 AM 1	12:00 AM 0	8:00 AM 1	2:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 2	10:00 AM 8
PM Peak Volume	4:00 PM 4	1:00 PM 4	1:00 PM 3	12:00 PM 0	1:00 PM 1	1:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 2	1:00 PM 10

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Nov 11 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	1	4
08:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	1	4
09:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	3	6
12:00 PM	1	4	0	0	0	0	0	0	0	0	0	0	0	1	6
01:00 PM	2	2	1	0	0	0	0	0	0	0	0	0	0	2	7
02:00 PM	2	2	0	0	1	0	0	0	0	0	0	0	0	0	5
03:00 PM	1	4	0	0	0	2	0	0	0	0	0	0	0	3	10
04:00 PM	0	2	2	0	0	1	0	0	0	0	0	0	0	1	6
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	1	3
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	6	21	10	0	1	4	0	0	0	0	0	0	0	13	55
Percent	10.9%	38.2%	18.2%	0%	1.8%	7.3%	0%	0%	0%	0%	0%	0%	0%	23.6%	
ADT 55															
AM Peak Volume	12:00 AM 0	9:00 AM 2	7:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3	11:00 AM 6
PM Peak Volume	1:00 PM 2	12:00 PM 4	4:00 PM 2	12:00 PM 0	2:00 PM 1	3:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 3	3:00 PM 10

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Nov 12 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
09:00 AM	2	2	1	0	0	0	0	0	0	0	0	0	0	1	6
10:00 AM	0	1	0	0	1	1	0	0	0	0	0	0	0	0	3
11:00 AM	0	7	0	0	0	1	0	0	0	0	0	0	0	1	9
12:00 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	1	2
02:00 PM	0	3	3	0	0	1	0	0	0	0	0	0	0	0	7
03:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	2	4
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	2	20	8	0	1	4	0	0	0	0	0	0	0	5	40
Percent	5%	50%	20%	0%	2.5%	10%	0%	0%	0%	0%	0%	0%	0%	12.5%	
ADT 40															
AM Peak Volume	9:00 AM 2	11:00 AM 7	8:00 AM 1	12:00 AM 0	10:00 AM 1	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	9:00 AM 1	11:00 AM 9
PM Peak Volume	12:00 PM 0	2:00 PM 3	2:00 PM 3	12:00 PM 0	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	8:00 PM 2	2:00 PM 7

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Nov 13 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	1	2
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
10:00 AM	0	2	0	0	0	0	0	0	0	1	0	0	0	0	3
11:00 AM	1	2	1	0	0	0	0	1	0	0	0	0	0	1	6
12:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
01:00 PM	1	10	2	0	0	0	0	0	0	0	0	0	0	0	13
02:00 PM	1	8	0	0	0	0	0	0	0	0	0	0	0	1	10
03:00 PM	0	7	0	0	0	0	0	0	0	0	0	0	0	0	7
04:00 PM	0	7	2	0	0	2	0	0	0	0	0	0	0	0	11
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	42	8	0	0	2	0	1	0	1	0	0	0	3	60
Percent	5%	70%	13.3%	0%	0%	3.3%	0%	1.7%	0%	1.7%	0%	0%	0%	5%	
ADT 60															
AM Peak Volume	11:00 AM 1	9:00 AM 2	9:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 1	12:00 AM 0	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	6:00 AM 1	11:00 AM 6
PM Peak Volume	1:00 PM 1	1:00 PM 10	1:00 PM 2	12:00 PM 0	12:00 PM 0	4:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 1	1:00 PM 13

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Nov 14 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	0	2	0	0	1	0	0	0	0	0	0	0	1	4
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	1	2
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3
04:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	1	2
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	6	4	0	1	2	0	0	0	0	0	0	0	3	16
Percent	0%	37.5%	25%	0%	6.3%	12.5%	0%	0%	0%	0%	0%	0%	0%	18.8%	
ADT 16															
AM Peak Volume	12:00 AM 0	12:00 AM 0	9:00 AM 2	12:00 AM 0	12:00 AM 0	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 1	10:00 AM 4
PM Peak Volume	12:00 PM 0	2:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 1	3:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 1	3:00 PM 3

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** EB, WB**CITY/STATE:** Colbert, AL**DATE:** Nov 15 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	2	2	0	0	0	0	0	0	0	0	0	0	0	0	4
03:00 AM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	2	0	0	0	1	1	0	0	0	0	0	0	0	0	4
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	1	1	0	1	2	0	0	0	0	0	0	0	0	5
11:00 AM	3	0	0	0	0	0	0	0	0	0	0	0	0	2	5
12:00 PM	2	2	1	0	0	0	0	0	0	0	0	0	0	4	9
01:00 PM	3	3	0	0	0	1	0	0	0	0	0	0	0	0	7
02:00 PM	1	1	2	0	0	1	0	0	0	0	0	0	0	1	6
03:00 PM	2	0	1	0	1	2	0	0	0	0	0	0	0	1	7
04:00 PM	0	2	0	0	2	1	0	1	0	1	0	0	0	1	8
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	15	12	5	0	7	10	0	1	0	1	0	0	0	9	60
Percent	25%	20%	8.3%	0%	11.7%	16.7%	0%	1.7%	0%	1.7%	0%	0%	0%	15%	
ADT 60															
AM Peak Volume	11:00 AM 3	2:00 AM 2	10:00 AM 1	12:00 AM 0	8:00 AM 1	10:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 2	10:00 AM 5
PM Peak Volume	1:00 PM 3	1:00 PM 3	2:00 PM 2	12:00 PM 0	4:00 PM 2	3:00 PM 2	12:00 PM 0	4:00 PM 1	12:00 PM 0	4:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 4	12:00 PM 9

Comments:

LOCATION: (Peak) Overlook Rd Parking Area


QC JOB #: 15889227

SPECIFIC LOCATION:

DIRECTION: EB, WB

CITY/STATE: Colbert, AL

DATE: Nov 9 2022 - Nov 15 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
Grand Total	41	128	48	0	14	29	0	2	0	2	0	0	0	51	315
Percent	13%	40.6%	15.2%	0%	4.4%	9.2%	0%	0.6%	0%	0.6%	0%	0%	0%	16.2%	
ADT 45															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL										QC JOB #: 15889227 DIRECTION: EB, WB DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			0	0	0	0	0	0	0	
01:00 AM			0	0	0	0	0	0	0	
02:00 AM			1	3	0	1	0	0	1	<div></div>
03:00 AM			1	3	0	1	0	0	1	<div></div>
04:00 AM			0	0	0	0	0	0	0	
05:00 AM			0	0	0	0	0	0	0	
06:00 AM			0	0	0	0	0	2	0	
07:00 AM			2	1	4	2	0	0	1	<div></div>
08:00 AM			0	3	4	2	1	0	2	<div></div>
09:00 AM			0	3	2	2	6	4	3	<div></div>
10:00 AM			2	8	0	3	3	3	3	<div></div>
11:00 AM			8	7	6	7	9	6	7	<div></div>
12:00 PM			4	6	6	5	3	4	5	<div></div>
01:00 PM			3	10	7	7	2	13	7	<div></div>
02:00 PM			1	8	5	5	7	10	6	<div></div>
03:00 PM			2	0	10	4	5	7	5	<div></div>
04:00 PM			2	4	6	4	0	11	5	<div></div>
05:00 PM			0	0	3	1	0	0	1	<div></div>
06:00 PM			0	0	0	0	0	0	0	
07:00 PM			0	0	2	1	0	0	0	
08:00 PM			0	0	0	0	4	0	1	<div></div>
09:00 PM			1	0	0	0	0	0	0	
10:00 PM			1	0	0	0	0	0	0	
11:00 PM			0	0	0	0	0	0	0	
Day Total			28	56	55	45	40	60	48	
% Weekday Average			62.2%	124.4%	122.2%					
% Week Average			58.3%	116.7%	114.6%	93.8%	83.3%	125%		
AM Peak Volume			11:00 AM 8	10:00 AM 8	11:00 AM 6	11:00 AM 7	11:00 AM 9	11:00 AM 6	11:00 AM 7	
PM Peak Volume			12:00 PM 4	1:00 PM 10	3:00 PM 10	1:00 PM 7	2:00 PM 7	1:00 PM 13	1:00 PM 7	
Comments:										

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Overlook Rd Parking Area							QC JOB #: 15889227			
SPECIFIC LOCATION:							DIRECTION: EB, WB			
CITY/STATE: Colbert, AL							DATE: Nov 9 2022 - Nov 15 2022			
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0				0			0	
01:00 AM	0	0				0			0	
02:00 AM	0	4				2			2	<div></div>
03:00 AM	0	2				1			1	<div></div>
04:00 AM	0	0				0			0	
05:00 AM	0	0				0			0	
06:00 AM	0	0				0			0	
07:00 AM	0	0				0			0	
08:00 AM	0	4				2			2	<div></div>
09:00 AM	2	1				2			2	<div></div>
10:00 AM	4	5				5			5	<div></div>
11:00 AM	0	5				3			3	<div></div>
12:00 PM	2	9				6			6	<div></div>
01:00 PM	0	7				4			4	<div></div>
02:00 PM	2	6				4			4	<div></div>
03:00 PM	3	7				5			5	<div></div>
04:00 PM	1	8				5			5	<div></div>
05:00 PM	0	0				0			0	
06:00 PM	0	0				0			0	
07:00 PM	0	0				0			0	
08:00 PM	0	0				0			0	
09:00 PM	0	2				1			1	<div></div>
10:00 PM	2	0				1			1	<div></div>
11:00 PM	0	0				0			0	
Day Total	16	60				41			41	
% Weekday Average	39%	146.3%								
% Week Average	39%	146.3%				100%				
AM Peak Volume	10:00 AM 4	10:00 AM 5				10:00 AM 5			10:00 AM 5	
PM Peak Volume	3:00 PM 3	12:00 PM 9				12:00 PM 6			12:00 PM 6	
Comments:										

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 9 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
11:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	17-26	3
12:00 PM	2	0	1	0	0	0	0	0	0	0	0	0	0	0	3	1-10	1
01:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
02:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	2	1	7	4	0	0	0	0	0	0	0	0	0	0	14	22-31	11
Percent	14.3%	7.1%	50%	28.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	11:00 AM	3:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	2	1	0	0	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	12:00 PM	4:00 PM	1:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	2	0	2	1	0	0	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 10 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
03:00 AM	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
10:00 AM	1	1	1	0	1	0	0	0	0	0	0	0	0	0	4	17-26	2
11:00 AM	0	1	0	3	0	0	0	0	0	0	0	0	0	0	4	22-31	3
12:00 PM	1	0	0	1	1	0	0	0	0	0	0	0	0	0	3	27-36	2
01:00 PM	0	1	0	3	0	0	0	0	0	0	0	0	0	0	4	22-31	3
02:00 PM	0	2	0	3	0	0	0	0	0	0	0	0	0	0	5	22-31	3
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2	22-31	1
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	4	5	4	13	2	0	0	0	0	0	0	0	0	0	28	22-31	17
Percent	14.3%	17.9%	14.3%	46.4%	7.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	3:00 AM 1	10:00 AM 1	3:00 AM 1	11:00 AM 3	10:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 4		
PM Peak Volume	12:00 PM 1	2:00 PM 2	12:00 PM 0	1:00 PM 3	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 5		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 11 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
08:00 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	27-36	2
09:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 AM	0	0	0	2	1	0	0	0	0	0	0	0	0	0	3	27-36	3
12:00 PM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3	17-26	2
01:00 PM	0	0	0	1	1	1	0	0	0	0	0	0	0	0	3	27-36	2
02:00 PM	1	0	1	1	0	0	0	0	0	0	0	0	0	0	3	22-31	2
03:00 PM	1	0	1	1	1	0	0	0	0	0	0	0	0	0	4	22-31	2
04:00 PM	1	0	1	2	0	0	0	0	0	0	0	0	0	0	4	22-31	3
05:00 PM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	3	1	7	11	5	1	0	0	0	0	0	0	0	0	28	22-31	18
Percent	10.7%	3.6%	25%	39.3%	17.9%	3.6%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM 0	12:00 AM 0	7:00 AM 1	11:00 AM 2	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3		
PM Peak Volume	2:00 PM 1	12:00 PM 1	12:00 PM 1	4:00 PM 2	12:00 PM 1	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 4		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 12 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	1	0	2	0	0	0	0	0	0	0	0	0	0	3	22-31	2
10:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2	12-21	1
11:00 AM	0	0	0	3	1	0	0	0	0	0	0	0	0	0	4	27-36	4
12:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
02:00 PM	0	0	1	0	1	1	0	0	0	0	0	0	0	0	3	32-41	2
03:00 PM	0	1	0	1	0	1	0	0	0	0	0	0	0	0	3	12-21	1
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	17-26	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	5	2	8	3	2	0	0	0	0	0	0	0	0	20	27-36	11
Percent	0%	25%	10%	40%	15%	10%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	9:00 AM	12:00 AM	11:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	0	3	1	0	0	0	0	0	0	0	0	0	4		
PM Peak Volume	12:00 PM	1:00 PM	2:00 PM	12:00 PM	2:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM		
	0	1	1	2	1	1	0	0	0	0	0	0	0	0	3		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 13 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	12-21	1
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
11:00 AM	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	22-31	3
12:00 PM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	2	27-36	2
01:00 PM	0	0	1	2	3	0	0	0	0	0	0	0	0	0	6	27-36	5
02:00 PM	0	0	3	3	0	0	0	0	0	0	0	0	0	0	6	22-31	6
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 PM	0	0	1	0	4	3	1	0	0	0	0	0	0	0	9	32-41	7
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	1	8	9	8	3	1	0	0	0	0	0	0	0	30	22-31	17
Percent	0%	3.3%	26.7%	30%	26.7%	10%	3.3%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	9:00 AM	6:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM		
	0	1	1	2	0	0	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	12:00 PM	2:00 PM	2:00 PM	4:00 PM	4:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM		
	0	0	3	3	4	3	1	0	0	0	0	0	0	0	9		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 14 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
10:00 AM	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	22-31	2
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
12:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
03:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
04:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	27-36	1
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 PM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	22-31	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	0	2	2	4	0	0	0	0	0	0	0	0	0	8	27-36	6
Percent	0%	0%	25%	25%	50%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	12:00 AM	10:00 AM	10:00 AM	9:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	0	1	1	1	0	0	0	0	0	0	0	0	0	2		
PM Peak Volume	12:00 PM	12:00 PM	12:00 PM	10:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM		
	0	0	1	1	1	0	0	0	0	0	0	0	0	0	1		
Comments:																	

Report generated on 12/9/2022 1:13 PM


SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Speed Data

LOCATION: (Peak) Overlook Rd Parking Area SPECIFIC LOCATION: CITY/STATE: Colbert, AL															QC JOB #: 15889227 DIRECTION: WB DATE: Nov 15 2022		
Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
02:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	17-26	1
03:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 AM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
10:00 AM	0	0	2	1	0	0	0	0	0	0	0	0	0	0	3	22-31	3
11:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	17-26	3
12:00 PM	0	0	1	1	1	0	0	0	0	0	0	0	0	0	3	22-31	2
01:00 PM	0	0	3	1	1	0	0	0	0	0	0	0	0	0	5	22-31	4
02:00 PM	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	22-31	2
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	12-21	1
04:00 PM	0	0	1	3	0	0	0	0	0	0	0	0	0	0	4	22-31	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
09:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	17-26	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1-10	0
Day Total	0	3	11	10	3	0	0	0	0	0	0	0	0	0	27	22-31	21
Percent	0%	11.1%	40.7%	37%	11.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%			
AM Peak Volume	12:00 AM	11:00 AM	10:00 AM	8:00 AM	2:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	10:00 AM		
	0	2	2	2	1	0	0	0	0	0	0	0	0	0	3		
PM Peak Volume	12:00 PM	3:00 PM	1:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	1:00 PM		
	0	1	3	3	1	0	0	0	0	0	0	0	0	0	5		
Comments:																	

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Overlook Rd Parking Area															QC JOB #: 15889227		
SPECIFIC LOCATION:															DIRECTION: WB		
CITY/STATE: Colbert, AL															DATE: Nov 9 2022 - Nov 15 2022		
Speed Range	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 999	Total	Pace Speed	Number in Pace
Grand Total	9	16	41	57	25	6	1	0	0	0	0	0	0	0	155	22-31	98
Percent	5.8%	10.3%	26.5%	36.8%	16.1%	3.9%	0.6%	0%	0%	0%	0%	0%	0%	0%			
Cumulative Percent	5.8%	16.1%	42.6%	79.4%	95.5%	99.4%	100%	100%	100%	100%	100%	100%	100%	100%			
ADT 22															85th Percentile: 31 MPH Mean Speed(Average): 26 MPH Median: 26 MPH Mode: 28 MPH		
Comments:																	

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 9 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3
01:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	4	5	4	0	1	0	0	0	0	0	0	0	0	0	14
Percent	28.6%	35.7%	28.6%	0%	7.1%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 14															
AM Peak Volume	12:00 AM 0	11:00 AM 2	7:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3
PM Peak Volume	12:00 PM 2	12:00 PM 1	1:00 PM 1	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 3

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 10 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	1	2	1	0	0	0	0	0	0	0	0	0	0	0	4
11:00 AM	0	2	1	0	1	0	0	0	0	0	0	0	0	0	4
12:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 PM	0	1	2	0	1	0	0	0	0	0	0	0	0	0	4
02:00 PM	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	14	8	0	3	0	0	0	0	0	0	0	0	0	28
Percent	10.7%	50%	28.6%	0%	10.7%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 28															
AM Peak Volume	10:00 AM 1	3:00 AM 2	9:00 AM 1	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 4
PM Peak Volume	4:00 PM 2	2:00 PM 3	1:00 PM 2	12:00 PM 0	1:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	2:00 PM 5

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 11 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
08:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3
01:00 PM	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3
02:00 PM	1	1	0	0	1	0	0	0	0	0	0	0	0	0	3
03:00 PM	1	3	0	0	0	0	0	0	0	0	0	0	0	0	4
04:00 PM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4
05:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	3	16	8	0	1	0	0	0	0	0	0	0	0	0	28
Percent	10.7%	57.1%	28.6%	0%	3.6%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 28															
AM Peak Volume	12:00 AM 0	7:00 AM 1	11:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	11:00 AM 3
PM Peak Volume	1:00 PM 1	12:00 PM 3	4:00 PM 2	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	3:00 PM 4

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 12 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3
10:00 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
12:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
02:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3
03:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	1	13	5	0	1	0	0	0	0	0	0	0	0	0	20
Percent	5%	65%	25%	0%	5%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 20															
AM Peak Volume	9:00 AM	11:00 AM	9:00 AM	12:00 AM	10:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	1	4	1	0	1	0	0	0	0	0	0	0	0	0	4
PM Peak Volume	12:00 PM	12:00 PM	2:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	2:00 PM
	0	2	2	0	0	0	0	0	0	0	0	0	0	0	3

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 13 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
10:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 AM	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
12:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
01:00 PM	0	5	1	0	0	0	0	0	0	0	0	0	0	0	6
02:00 PM	0	6	0	0	0	0	0	0	0	0	0	0	0	0	6
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	7	2	0	0	0	0	0	0	0	0	0	0	0	9
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	23	6	0	0	0	0	1	0	0	0	0	0	0	30
Percent	0%	76.7%	20%	0%	0%	0%	0%	3.3%	0%	0%	0%	0%	0%	0%	
ADT 30															
AM Peak Volume	12:00 AM	9:00 AM	6:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	11:00 AM
	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3
PM Peak Volume	12:00 PM	4:00 PM	4:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	4:00 PM
	0	7	2	0	0	0	0	0	0	0	0	0	0	0	9

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 14 2022


Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
10:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
03:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	4	3	0	1	0	0	0	0	0	0	0	0	0	8
Percent	0%	50%	37.5%	0%	12.5%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 8															
AM Peak Volume	12:00 AM 0	12:00 AM 0	10:00 AM 2	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 2
PM Peak Volume	12:00 PM 0	2:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 1

Comments:

LOCATION: (Peak) Overlook Rd Parking Area**QC JOB #:** 15889227**SPECIFIC LOCATION:****DIRECTION:** WB**CITY/STATE:** Colbert, AL**DATE:** Nov 15 2022

Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
03:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3
11:00 AM	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3
12:00 PM	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3
01:00 PM	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5
02:00 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2
03:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1
04:00 PM	0	1	0	0	2	0	0	1	0	0	0	0	0	0	4
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 PM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	7	10	4	0	5	0	0	1	0	0	0	0	0	0	27
Percent	25.9%	37%	14.8%	0%	18.5%	0%	0%	3.7%	0%	0%	0%	0%	0%	0%	
ADT 27															
AM Peak Volume	11:00 AM 3	2:00 AM 2	10:00 AM 1	12:00 AM 0	8:00 AM 1	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	10:00 AM 3
PM Peak Volume	1:00 PM 3	12:00 PM 2	12:00 PM 1	12:00 PM 0	4:00 PM 2	12:00 PM 0	12:00 PM 0	4:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	1:00 PM 5

Comments:

LOCATION: (Peak) Overlook Rd Parking Area														QC JOB #: 15889227	
SPECIFIC LOCATION:														DIRECTION: WB	
CITY/STATE: Colbert, AL														DATE: Nov 9 2022 - Nov 15 2022	
Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
Grand Total	18	85	38	0	12	0	0	2	0	0	0	0	0	0	155
Percent	11.6%	54.8%	24.5%	0%	7.7%	0%	0%	1.3%	0%	0%	0%	0%	0%	0%	
ADT 22															
Comments:															

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Overlook Rd Parking Area										QC JOB #: 15889227
SPECIFIC LOCATION:										DIRECTION: WB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon	Tue	Wed 9 Nov 22	Thu 10 Nov 22	Fri 11 Nov 22	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun 13 Nov 22	Average Week Hourly Traffic	Average Week Profile
12:00 AM			0	0	0	0	0	0	0	
01:00 AM			0	0	0	0	0	0	0	
02:00 AM			0	1	0	0	0	0	0	
03:00 AM			1	2	0	1	0	0	1	<div></div>
04:00 AM			0	0	0	0	0	0	0	
05:00 AM			0	0	0	0	0	0	0	
06:00 AM			0	0	0	0	0	1	0	
07:00 AM			1	0	2	1	0	0	1	<div></div>
08:00 AM			0	2	2	1	0	0	1	<div></div>
09:00 AM			0	1	1	1	3	2	1	<div></div>
10:00 AM			1	4	0	2	2	1	2	<div></div>
11:00 AM			3	4	3	3	4	3	3	<div></div>
12:00 PM			3	3	3	3	2	2	3	<div></div>
01:00 PM			1	4	3	3	1	6	3	<div></div>
02:00 PM			1	5	3	3	3	6	4	<div></div>
03:00 PM			0	0	4	1	3	0	1	<div></div>
04:00 PM			2	2	4	3	0	9	3	<div></div>
05:00 PM			0	0	2	1	0	0	0	
06:00 PM			0	0	0	0	0	0	0	
07:00 PM			0	0	1	0	0	0	0	
08:00 PM			0	0	0	0	2	0	0	
09:00 PM			0	0	0	0	0	0	0	
10:00 PM			1	0	0	0	0	0	0	
11:00 PM			0	0	0	0	0	0	0	
Day Total			14	28	28	23	20	30	23	
% Weekday Average			60.9%	121.7%	121.7%					
% Week Average			60.9%	121.7%	121.7%	100%	87%	130.4%		
AM Peak Volume			11:00 AM 3	10:00 AM 4	11:00 AM 3	11:00 AM 3	11:00 AM 4	11:00 AM 3	11:00 AM 3	
PM Peak Volume			12:00 PM 3	2:00 PM 5	3:00 PM 4	12:00 PM 3	2:00 PM 3	4:00 PM 9	2:00 PM 4	
Comments:										

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Overlook Rd Parking Area										QC JOB #: 15889227
SPECIFIC LOCATION:										DIRECTION: WB
CITY/STATE: Colbert, AL										DATE: Nov 9 2022 - Nov 15 2022
Start Time	Mon 14 Nov 22	Tue 15 Nov 22	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM	0	0				0			0	
01:00 AM	0	0				0			0	
02:00 AM	0	2				1			1	<div></div>
03:00 AM	0	1				1			1	<div></div>
04:00 AM	0	0				0			0	
05:00 AM	0	0				0			0	
06:00 AM	0	0				0			0	
07:00 AM	0	0				0			0	
08:00 AM	0	2				1			1	<div></div>
09:00 AM	1	0				1			1	<div></div>
10:00 AM	2	3				3			3	<div></div>
11:00 AM	0	3				2			2	<div></div>
12:00 PM	1	3				2			2	<div></div>
01:00 PM	0	5				3			3	<div></div>
02:00 PM	1	2				2			2	<div></div>
03:00 PM	1	1				1			1	<div></div>
04:00 PM	1	4				3			3	<div></div>
05:00 PM	0	0				0			0	
06:00 PM	0	0				0			0	
07:00 PM	0	0				0			0	
08:00 PM	0	0				0			0	
09:00 PM	0	1				1			1	<div></div>
10:00 PM	1	0				1			1	<div></div>
11:00 PM	0	0				0			0	
Day Total	8	27				22			22	
% Weekday Average	36.4%	122.7%								
% Week Average	36.4%	122.7%				100%				
AM Peak Volume	10:00 AM 2	10:00 AM 3				10:00 AM 3			10:00 AM 3	
PM Peak Volume	12:00 PM 1	1:00 PM 5				1:00 PM 3			1:00 PM 3	
Comments:										

Report generated on 12/9/2022 1:13 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>)

LOCATION: (Peak) Colbert Ferry boat ramp

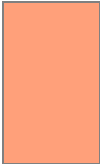
QC JOB #: 15889231

SPECIFIC LOCATION:

DIRECTION: NB

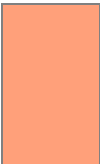
CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Percent	0%	100%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 2															
AM Peak Volume	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM	12:00 AM
PM Peak Volume	12:00 PM	8:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	12:00 PM	8:00 PM

Comments:

LOCATION: (Peak) Colbert Ferry boat ramp**QC JOB #:** 15889231**SPECIFIC LOCATION:****DIRECTION:** NB**CITY/STATE:** Colbert, AL**DATE:** Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Percent	0%	100%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 2															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Colbert Ferry boat ramp						QC JOB #: 15889231				
SPECIFIC LOCATION:						DIRECTION: NB				
CITY/STATE: Colbert, AL						DATE: Nov 12 2022 - Nov 12 2022				
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM							0		0	<div></div> <div></div>
01:00 AM							0		0	
02:00 AM							0		0	
03:00 AM							0		0	
04:00 AM							0		0	
05:00 AM							0		0	
06:00 AM							0		0	
07:00 AM							0		0	
08:00 AM							0		0	
09:00 AM							0		0	
10:00 AM							0		0	
11:00 AM							0		0	
12:00 PM							0		0	
01:00 PM							0		0	
02:00 PM							0		0	
03:00 PM							0		0	
04:00 PM							0		0	
05:00 PM	0		0							
06:00 PM	0		0							
07:00 PM	0		0							
08:00 PM						1		1		
09:00 PM						1		1		
10:00 PM						0		0		
11:00 PM						0		0		
Day Total							2		2	
% Weekday Average										
% Week Average						0%	100%			
AM Peak Volume							12:00 AM 0		12:00 AM 0	
PM Peak Volume							8:00 PM 1		8:00 PM 1	
Comments:										

LOCATION: (Peak) Colbert Ferry boat ramp

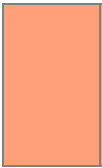
QC JOB #: 15889231

SPECIFIC LOCATION:

DIRECTION: NB, SB


CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
09:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
Percent	0%	100%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 4															
AM Peak Volume	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0
PM Peak Volume	12:00 PM 0	8:00 PM 2	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	8:00 PM 2

Comments:

LOCATION: (Peak) Colbert Ferry boat ramp**QC JOB #:** 15889231**SPECIFIC LOCATION:****DIRECTION:** NB, SB**CITY/STATE:** Colbert, AL**DATE:** Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	0	4	0	0	0	0	0	0	0	0	0	0	0	0	4
Percent	0%	100%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 4															

Comments:

Type of report: Tube Count - Volume Data

LOCATION: (Peak) Colbert Ferry boat ramp						QC JOB #: 15889231				
SPECIFIC LOCATION:						DIRECTION: NB, SB				
CITY/STATE: Colbert, AL						DATE: Nov 12 2022 - Nov 12 2022				
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM							0		0	
01:00 AM							0		0	
02:00 AM							0		0	
03:00 AM							0		0	
04:00 AM							0		0	
05:00 AM							0		0	
06:00 AM							0		0	
07:00 AM							0		0	
08:00 AM							0		0	
09:00 AM							0		0	
10:00 AM							0		0	
11:00 AM							0		0	
12:00 PM							0		0	
01:00 PM							0		0	
02:00 PM							0		0	
03:00 PM							0		0	
04:00 PM	0	0								
05:00 PM	0	0								
06:00 PM	0	0								
07:00 PM	0	0								
08:00 PM						2			2	<div></div>
09:00 PM						2			2	<div></div>
10:00 PM						0			0	
11:00 PM						0			0	
Day Total							4		4	
% Weekday Average										
% Week Average						0%	100%			
AM Peak Volume							12:00 AM 0		12:00 AM 0	
PM Peak Volume							8:00 PM 2		8:00 PM 2	
Comments:										

LOCATION: (Peak) Colbert Ferry boat ramp

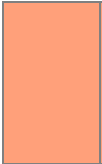
QC JOB #: 15889231

SPECIFIC LOCATION:

DIRECTION: SB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
12:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
09:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
01:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
02:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
03:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
06:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
07:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
09:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1
10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Day Total	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Percent	0%	100%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 2															
AM Peak Volume	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0	12:00 AM 0
PM Peak Volume	12:00 PM 0	8:00 PM 1	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	12:00 PM 0	8:00 PM 1

Comments:

LOCATION: (Peak) Colbert Ferry boat ramp

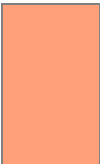
QC JOB #: 15889231

SPECIFIC LOCATION:

DIRECTION: SB

CITY/STATE: Colbert, AL

DATE: Nov 12 2022

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
Grand Total	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2
Percent	0%	100%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
ADT 2															

Comments:

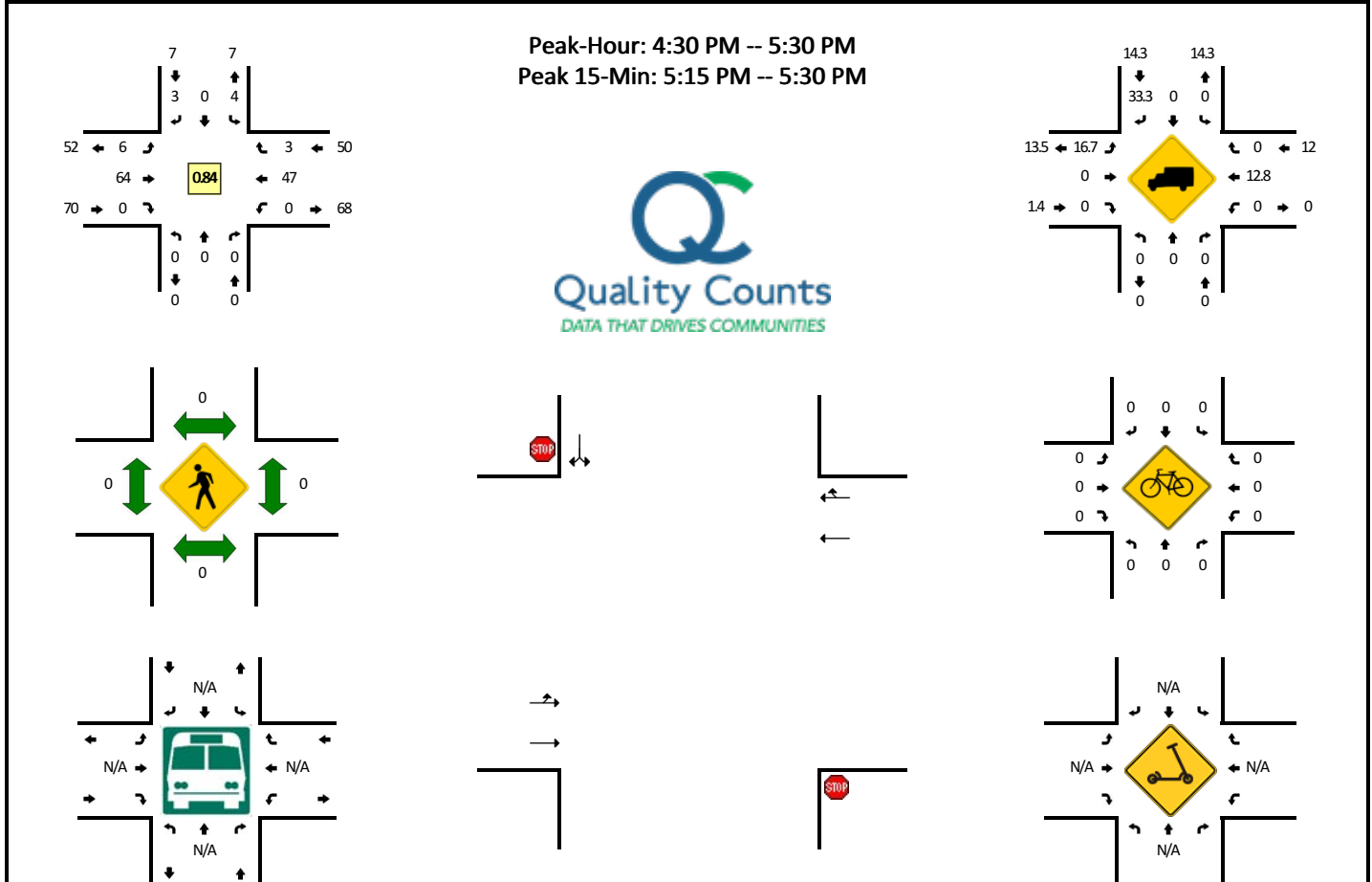
Type of report: Tube Count - Volume Data

LOCATION: (Peak) Colbert Ferry boat ramp						QC JOB #: 15889231				
SPECIFIC LOCATION:						DIRECTION: SB				
CITY/STATE: Colbert, AL						DATE: Nov 12 2022 - Nov 12 2022				
Start Time	Mon	Tue	Wed	Thu	Fri	Average Weekday Hourly Traffic	Sat 12 Nov 22	Sun	Average Week Hourly Traffic	Average Week Profile
12:00 AM							0		0	<div></div> <div></div>
01:00 AM							0	0		
02:00 AM							0	0		
03:00 AM							0	0		
04:00 AM							0	0		
05:00 AM							0	0		
06:00 AM							0	0		
07:00 AM							0	0		
08:00 AM							0	0		
09:00 AM							0	0		
10:00 AM							0	0		
11:00 AM							0	0		
12:00 PM							0	0		
01:00 PM							0	0		
02:00 PM							0	0		
03:00 PM							0	0		
04:00 PM							0	0		
05:00 PM	0	0								
06:00 PM	0	0								
07:00 PM	0	0								
08:00 PM						1		1		
09:00 PM						1		1		
10:00 PM						0		0		
11:00 PM						0		0		
Day Total							2		2	
% Weekday Average										
% Week Average						0%	100%			
AM Peak Volume							12:00 AM 0		12:00 AM 0	
PM Peak Volume							8:00 PM 1		8:00 PM 1	
Comments:										

Turning Movement Count Data

LOCATION: (Off-Peak) Colbert Ferry Rd -- Natchez Trace Pkwy
CITY/STATE: Colbert, AL

QC JOB #: 15889201
DATE: Tue, Aug 23 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Colbert Ferry Rd (Northbound)					(Off-Peak) Colbert Ferry Rd (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	9	0	0	0	13	71 85 93 97 90 75 51 47 39 43 50 57 63 56 53 44 43 52 53 55 54 49 49 56 62 63 66 59 56 56 54 61 65 70 70
6:15 AM	0	0	0	0	0	0	0	1	0	0	0	7	0	0	0	0	16	0	0	0	24		
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	7	0	0	0	13		
6:45 AM	0	0	0	0	0	0	0	1	0	0	0	4	0	0	0	0	14	2	0	0	21		
7:00 AM	0	0	0	0	0	0	0	0	0	0	3	12	0	0	0	0	12	0	0	0	27		
7:15 AM	0	0	0	0	0	0	0	0	0	0	1	21	0	0	0	0	10	0	0	0	32		
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	8	0	0	0	17		
7:45 AM	0	0	0	0	0	1	0	0	0	0	0	8	0	0	0	0	5	0	0	0	14		
8:00 AM	0	0	0	0	0	1	0	0	0	0	0	6	0	0	0	0	5	0	0	0	12		
8:15 AM	0	0	0	0	0	0	0	1	0	0	1	5	0	0	0	0	1	0	0	0	8		
8:30 AM	0	0	0	0	0	0	0	0	0	0	2	6	0	0	0	0	4	1	0	0	13		
8:45 AM	0	0	0	0	0	0	0	1	0	0	0	2	0	0	0	0	3	0	0	0	6		
9:00 AM	0	0	0	0	0	1	0	0	0	0	0	8	0	0	0	0	6	1	0	0	16		
9:15 AM	0	0	0	0	0	1	0	0	1	0	0	6	0	0	0	0	7	0	0	0	15		
9:30 AM	0	0	0	0	0	0	0	1	0	0	1	7	0	0	0	0	9	2	0	0	20		
9:45 AM	0	0	0	0	0	1	0	2	0	0	0	4	0	0	0	0	4	1	0	0	12		
10:00 AM	0	0	0	0	0	0	0	0	0	0	1	5	0	0	0	0	3	0	0	0	9		
10:15 AM	0	0	0	0	0	0	0	1	0	0	0	6	0	0	0	0	5	0	0	0	12		
10:30 AM	0	0	0	0	0	1	0	0	0	0	0	3	0	0	0	0	7	0	0	0	11		
10:45 AM	0	0	0	0	0	0	0	0	0	0	2	5	0	0	0	0	4	0	0	0	11		
11:00 AM	0	0	0	0	0	1	0	0	0	0	1	8	0	0	0	0	7	1	0	0	18		
11:15 AM	0	0	0	0	0	0	0	1	0	0	1	6	0	0	0	0	5	0	0	0	13		
11:30 AM	0	0	0	0	0	3	0	1	0	0	2	3	0	0	0	0	4	0	0	0	13		
11:45 AM	0	0	0	0	0	0	0	0	0	0	1	3	0	0	0	0	6	0	0	0	10		
12:00 PM	0	0	0	0	0	1	0	1	0	0	0	3	0	0	0	0	6	2	0	0	13		
12:15 PM	0	0	0	0	0	0	0	2	0	0	0	4	0	0	0	0	6	1	0	0	13		
12:30 PM	0	0	0	0	0	1	0	1	0	0	0	9	0	1	0	0	7	1	0	0	20		
12:45 PM	0	0	0	0	0	1	0	0	0	0	0	5	0	0	0	0	10	0	0	0	16		
1:00 PM	0	0	0	0	0	0	0	0	0	0	1	4	0	0	0	0	8	1	0	0	14		
1:15 PM	0	0	0	0	0	1	0	4	0	0	0	6	0	0	0	0	5	0	0	0	16		
1:30 PM	0	0	0	0	0	2	0	0	0	0	0	7	0	0	0	0	3	1	0	0	13		
1:45 PM	0	0	0	0	0	0	0	1	0	0	0	8	0	0	0	0	4	0	0	0	13		
2:00 PM	0	0	0	0	0	1	0	0	0	0	1	2	0	0	0	0	9	1	0	0	14		
2:15 PM	0	0	0	0	0	1	0	1	0	0	1	7	0	1	0	0	2	1	0	0	14		
2:30 PM	0	0	0	0	0	0	0	1	0	0	0	13	0	0	0	0	4	2	0	0	20		
2:45 PM	0	0	0	0	0	1	0	1	0	0	1	10	0	0	0	0	4	0	0	0	17		
3:00 PM	0	0	0	0	0	0	0	0	0	0	0	14	0	0	0	0	4	1	0	0	19		
3:15 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	6	1	0	0	14		

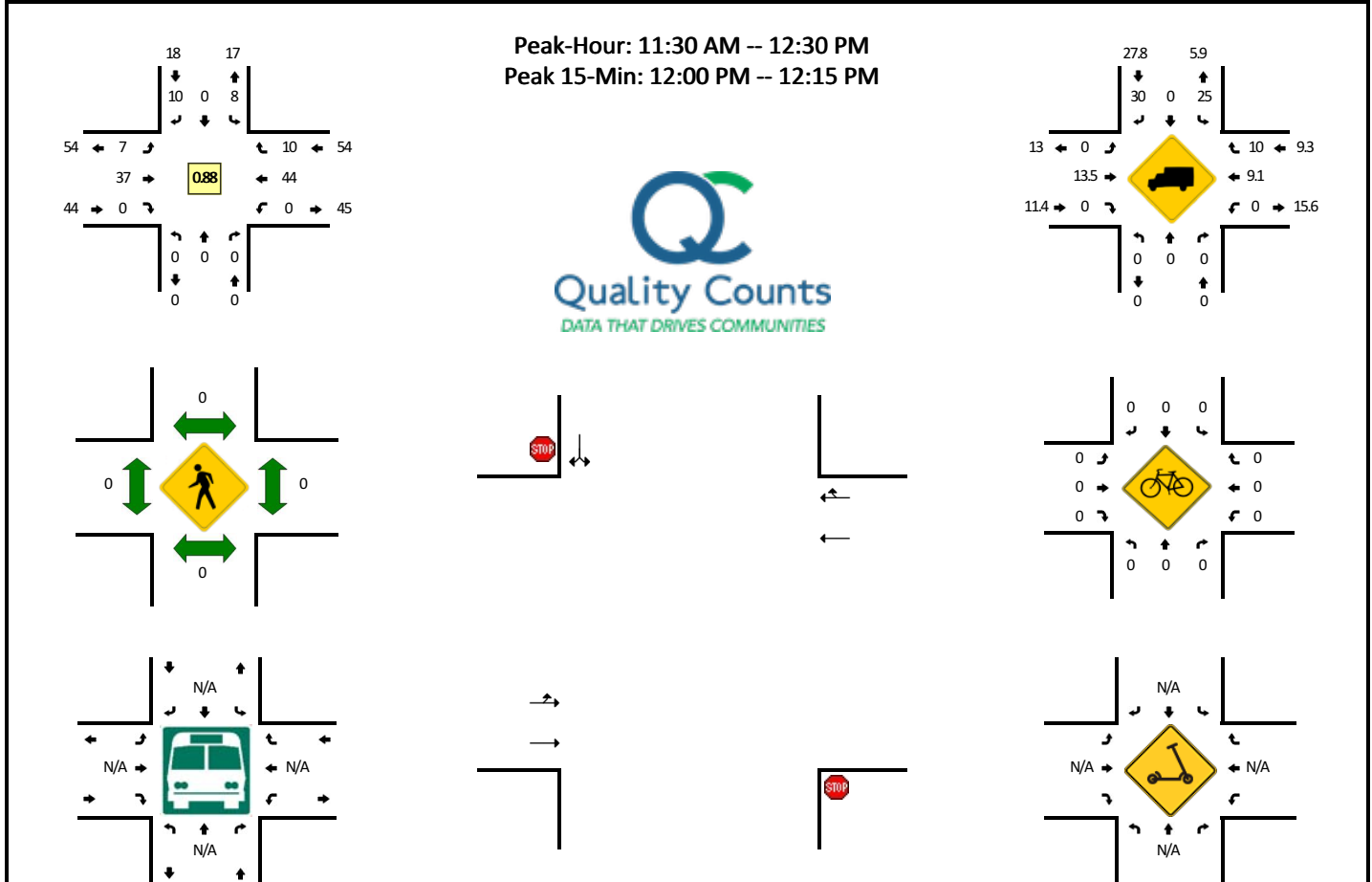
15-Min Count Period Beginning At	(Off-Peak) Colbert Ferry Rd (Northbound)					(Off-Peak) Colbert Ferry Rd (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
3:30 PM	0	0	0	0	0	0	0	1	0	0	0	8	0	0	0	0	0	13	1	0	0	23	73
3:45 PM	0	0	0	0	0	0	0	2	0	0	0	1	12	0	0	0	0	8	2	0	0	25	81
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	10	0	0	0	14	76
4:15 PM	0	0	0	0	0	1	0	0	0	0	0	0	14	0	0	0	0	14	0	0	0	29	91
4:30 PM	0	0	0	0	0	1	0	1	0	0	0	1	16	0	1	0	0	6	0	0	0	26	94
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	17	0	1	0	0	10	1	0	0	29	98
5:00 PM	0	0	0	0	0	0	0	1	0	0	0	1	21	0	0	0	0	10	1	0	0	34	118
5:15 PM	0	0	0	0	0	3	0	1	0	0	0	2	10	0	0	0	0	21	1	0	0	38	127
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	9	0	0	0	17	118
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	7	1	0	0	16	105
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total		
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
All Vehicles	0	0	0	0	0	12	0	4	0	0	8	40	0	0	0	0	0	84	4	0	0	152	
Heavy Trucks	0	0	0			0	0	4			0	0	0				0	20	0			24	
Buses																							
Pedestrians		0					0					0						0				0	
Bicycles	0	0	0			0	0	0			0	0	0				0	0	0			0	
Scooters																							
Comments:																							

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Colbert Ferry Rd -- Natchez Trace Pkwy
CITY/STATE: Colbert, AL

QC JOB #: 15889202
DATE: Sat, Aug 27 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Colbert Ferry Rd (Northbound)					(Off-Peak) Colbert Ferry Rd (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	8	1	0	0	11	
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	1	0	0	4	
6:30 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	2	1	0	0	5	
6:45 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	3	1	0	0	6	26
7:00 AM	0	0	0	0	0	1	0	0	0	0	2	4	0	0	0	0	1	0	0	0	8	23
7:15 AM	0	0	0	0	0	0	0	1	0	0	1	10	0	0	0	0	2	1	0	0	15	34
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	5	1	0	0	11	40
7:45 AM	0	0	0	0	0	0	0	1	0	0	1	4	0	0	0	0	6	0	0	0	12	46
8:00 AM	0	0	0	0	0	1	0	0	0	0	1	5	0	0	0	0	6	0	0	0	13	51
8:15 AM	0	0	0	0	0	0	0	0	0	0	1	5	0	0	0	0	5	0	0	0	11	47
8:30 AM	0	0	0	0	0	1	0	0	0	0	0	4	0	0	0	0	7	0	0	0	12	48
8:45 AM	0	0	0	0	0	1	0	0	0	0	2	5	0	0	0	0	4	1	0	0	13	49
9:00 AM	0	0	0	0	0	1	0	2	0	0	0	5	0	0	0	0	4	0	0	0	12	48
9:15 AM	0	0	0	0	0	0	0	0	0	0	1	6	0	0	0	0	5	0	0	0	12	49
9:30 AM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	2	3	0	0	15	52
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	11	2	0	0	18	57
10:00 AM	0	0	0	0	0	0	0	5	0	0	2	9	0	0	0	0	4	0	0	0	20	65
10:15 AM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	6	0	0	0	16	69
10:30 AM	0	0	0	0	0	0	0	0	0	0	3	13	0	0	0	0	7	2	0	0	25	79
10:45 AM	0	0	0	0	0	3	0	3	0	0	1	11	0	0	0	0	17	0	0	0	35	96
11:00 AM	0	0	0	0	0	1	0	0	0	0	2	9	0	0	0	0	12	0	0	0	24	100
11:15 AM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	11	2	0	0	21	105
11:30 AM	0	0	0	0	0	3	0	4	0	0	1	4	0	0	0	0	11	4	0	0	27	107
11:45 AM	0	0	0	0	0	3	0	0	0	0	1	13	0	0	0	0	11	0	0	0	28	100
12:00 PM	0	0	0	0	0	1	0	2	0	0	1	13	0	0	0	0	10	6	0	0	33	109
12:15 PM	0	0	0	0	0	1	0	4	0	0	4	7	0	0	0	0	12	0	0	0	28	116
12:30 PM	0	0	0	0	0	1	0	1	0	0	1	4	0	0	0	0	9	3	0	0	19	108
12:45 PM	0	0	0	0	0	1	0	2	0	0	2	5	0	0	0	0	13	1	0	0	24	104
1:00 PM	0	0	0	0	0	0	0	1	0	0	0	14	0	0	0	0	9	1	0	0	25	96
1:15 PM	0	0	0	0	0	1	0	4	0	0	0	8	0	0	0	0	12	3	0	0	28	96
1:30 PM	0	0	0	0	0	0	0	0	0	0	1	12	0	0	0	0	6	0	0	0	19	96
1:45 PM	0	0	0	0	0	1	0	3	0	0	6	6	0	0	0	0	12	1	0	0	29	101
2:00 PM	0	0	0	0	0	4	0	1	0	0	0	14	0	0	0	0	9	1	0	0	29	105
2:15 PM	0	0	0	0	0	2	0	5	0	0	2	8	0	0	0	0	5	1	0	0	23	100
2:30 PM	0	0	0	0	0	1	0	1	0	0	0	18	0	0	0	0	8	1	0	0	29	110
2:45 PM	0	0	0	0	0	2	0	1	0	0	1	10	0	0	0	0	10	1	0	0	25	106
3:00 PM	0	0	0	0	0	2	0	1	0	0	3	16	0	0	0	0	5	0	0	0	27	104

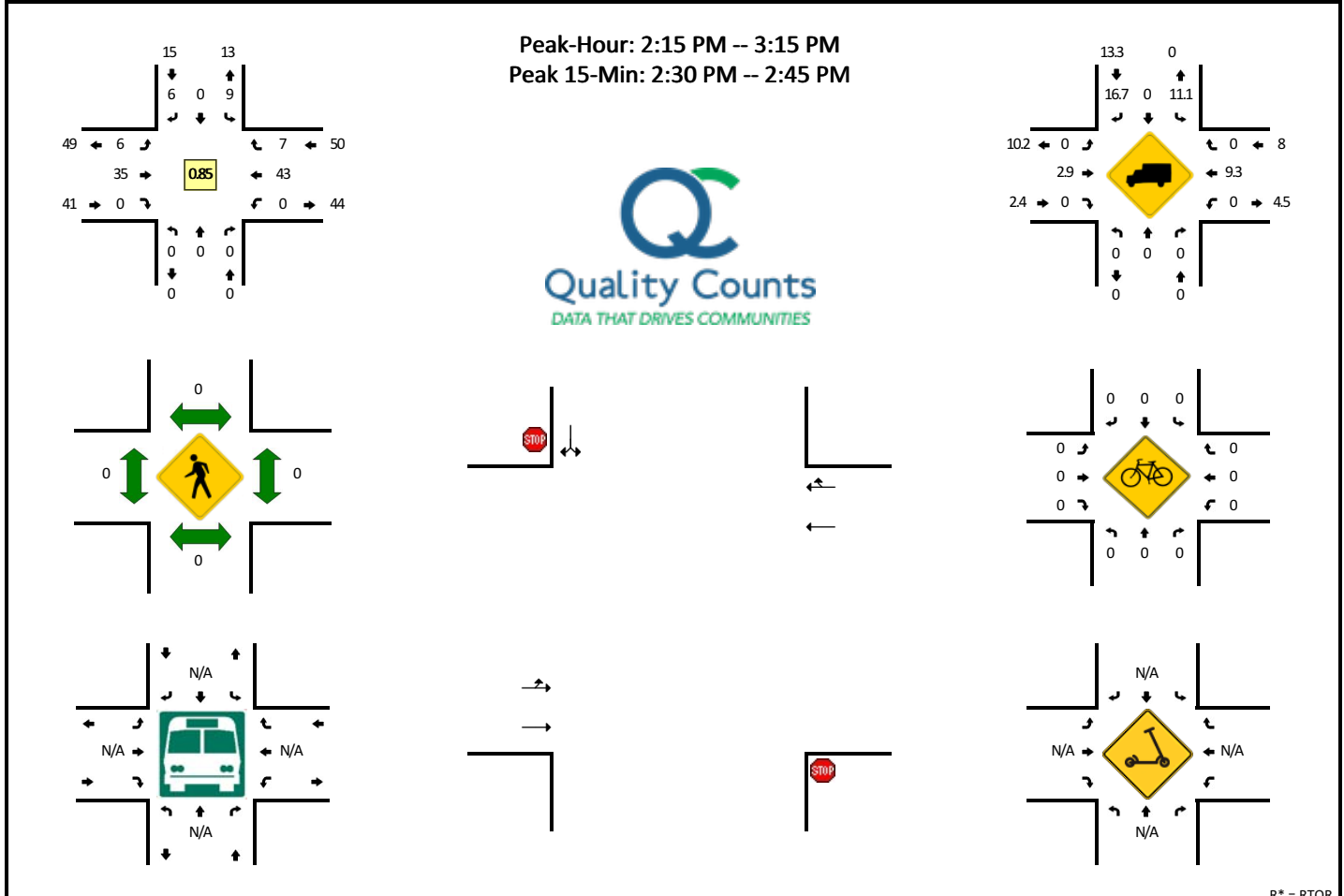
15-Min Count Period Beginning At	(Off-Peak) Colbert Ferry Rd (Northbound)					(Off-Peak) Colbert Ferry Rd (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
3:15 PM	0	0	0	0	0	3	0	0	0	0	1	10	0	0	0	0	20	1	0	0	35	116
3:30 PM	0	0	0	0	0	0	0	0	0	0	1	7	0	0	0	0	13	1	0	0	22	109
3:45 PM	0	0	0	0	0	3	0	0	0	0	1	7	0	0	0	0	10	0	0	0	21	105
4:00 PM	0	0	0	0	0	1	0	2	0	0	3	7	0	0	0	0	15	1	0	0	29	107
4:15 PM	0	0	0	0	0	2	0	0	0	0	1	17	0	0	0	0	10	0	0	0	30	102
4:30 PM	0	0	0	0	0	1	0	1	0	0	1	7	0	0	0	0	8	0	0	0	18	98
4:45 PM	0	0	0	0	0	1	0	0	0	0	0	7	0	0	0	0	17	1	0	0	26	103
5:00 PM	0	0	0	0	0	1	0	1	0	0	1	8	0	0	0	0	9	0	0	0	20	94
5:15 PM	0	0	0	0	0	0	0	0	0	0	1	16	0	0	0	0	10	2	0	0	29	93
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	13	0	0	0	0	17	1	0	0	31	106
5:45 PM	0	0	0	0	0	2	0	0	0	0	2	4	0	0	0	0	10	1	0	0	19	99
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
All Vehicles	0	0	0	0	0	4	0	8	0	0	4	52	0	0	0	0	40	24	0	0	132	
Heavy Trucks	0	0	0			0	0	0			0	16	0			0	4	0			20	
Buses																						
Pedestrians		0					0					0					0				0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	
Scooters																						
<i>Comments:</i>																						

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Colbert Ferry Rd -- Natchez Trace Pkwy
CITY/STATE: Colbert, AL

QC JOB #: 15889203
DATE: Sun, Aug 28 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Colbert Ferry Rd (Northbound)					(Off-Peak) Colbert Ferry Rd (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	6	0	0	0	9	24 23 27 30 30 34 39 45 45 46 48 39 45 51 59 65 66 67 61 69 77 82 96 92 92 90 89 91 90 102 103 106
6:15 AM	0	0	0	0	0	1	0	0	0	0	2	0	0	0	0	0	0	5	0	0	0	8	
6:30 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	2	0	0	0	3	
6:45 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	2	0	0	0	4	
7:00 AM	0	0	0	0	0	0	0	0	0	0	1	3	0	0	0	0	0	4	0	0	0	8	
7:15 AM	0	0	0	0	0	1	0	0	0	0	1	5	0	0	0	0	0	3	2	0	0	12	
7:30 AM	0	0	0	0	0	1	0	0	0	0	1	3	0	0	0	0	0	1	0	0	0	6	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	0	0	0	4	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	0	3	0	0	0	12	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	6	0	0	0	10	
8:30 AM	0	0	0	0	0	0	0	1	0	0	0	5	0	0	0	0	0	1	1	0	0	8	
8:45 AM	0	0	0	0	0	0	0	0	0	0	1	4	0	0	0	0	0	3	1	0	0	9	
9:00 AM	0	0	0	0	0	1	0	1	0	0	2	8	0	0	0	0	0	6	0	0	0	18	
9:15 AM	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	0	8	0	0	0	10	
9:30 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	3	1	0	0	9	
9:45 AM	0	0	0	0	0	0	0	1	0	0	1	4	0	0	0	0	0	4	1	0	0	11	
10:00 AM	0	0	0	0	0	2	0	0	0	0	3	2	0	0	0	0	0	2	0	0	0	9	
10:15 AM	0	0	0	0	0	1	0	2	0	0	1	2	0	0	0	0	0	8	2	0	0	16	
10:30 AM	0	0	0	0	0	3	0	1	0	0	0	3	0	0	0	0	0	8	0	0	0	15	
10:45 AM	0	0	0	0	0	1	0	0	0	0	0	7	0	0	0	0	0	7	4	0	0	19	
11:00 AM	0	0	0	0	0	1	0	2	0	0	1	9	0	0	0	0	0	2	0	0	0	15	
11:15 AM	0	0	0	0	0	0	0	0	0	0	1	8	0	0	0	0	0	8	0	0	0	17	
11:30 AM	0	0	0	0	0	0	0	2	0	0	0	8	0	0	0	0	0	5	0	1	0	16	
11:45 AM	0	0	0	0	0	0	0	0	0	0	1	6	0	0	0	0	0	5	1	0	0	13	
12:00 PM	0	0	0	0	0	3	0	1	0	0	1	7	0	0	0	0	0	11	0	0	0	23	
12:15 PM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	0	14	1	0	0	25	
12:30 PM	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	16	3	0	0	21	
12:45 PM	0	0	0	0	0	1	0	2	0	0	0	10	0	0	0	0	0	12	2	0	0	27	
1:00 PM	0	0	0	0	0	0	0	1	0	0	1	6	0	0	0	0	0	9	2	0	0	19	
1:15 PM	0	0	0	0	0	2	0	2	0	0	1	7	0	0	0	0	0	9	4	0	0	25	
1:30 PM	0	0	0	0	0	3	0	0	0	0	0	13	0	0	0	0	0	3	0	0	0	19	
1:45 PM	0	0	0	0	0	0	0	2	0	0	1	8	0	0	0	0	0	14	1	0	0	26	
2:00 PM	0	0	0	0	0	1	0	1	0	0	0	8	0	0	0	0	0	9	2	0	0	21	
2:15 PM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	0	10	4	0	0	24	
2:30 PM	0	0	0	0	0	3	0	3	0	0	3	8	0	0	0	0	0	13	1	0	0	31	
2:45 PM	0	0	0	0	0	3	0	1	0	0	2	10	0	0	0	0	0	10	1	0	0	27	
3:00 PM	0	0	0	0	0	3	0	2	0	0	1	7	0	0	0	0	0	10	1	0	0	24	

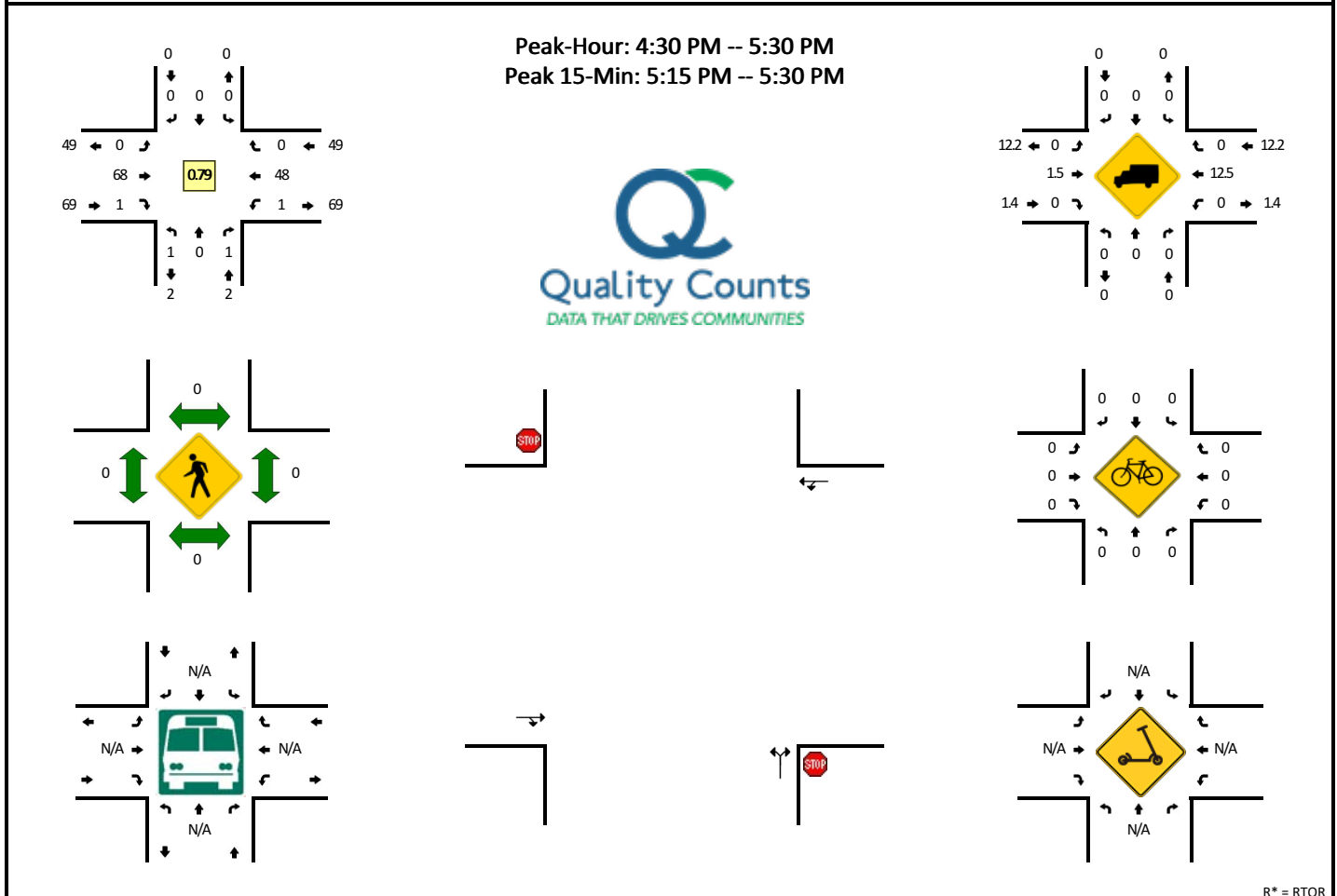
15-Min Count Period Beginning At	(Off-Peak) Colbert Ferry Rd (Northbound)					(Off-Peak) Colbert Ferry Rd (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
3:15 PM	0	0	0	0	0	1	0	2	0	0	0	6	0	0	0	0	14	0	0	0	0	23	105
3:30 PM	0	0	0	0	0	0	0	1	0	0	0	9	0	0	0	0	10	3	0	0	0	23	97
3:45 PM	0	0	0	0	0	0	0	2	0	0	0	6	0	0	0	0	16	3	0	0	0	27	97
4:00 PM	0	0	0	0	0	0	0	1	0	0	0	6	0	0	0	0	7	3	0	0	0	19	92
4:15 PM	0	0	0	0	0	2	0	2	0	0	0	13	0	0	0	0	8	1	0	0	0	28	97
4:30 PM	0	0	0	0	0	1	0	0	0	0	0	11	0	0	0	0	16	0	0	0	0	31	105
4:45 PM	0	0	0	0	0	5	0	1	0	0	0	6	0	0	0	0	9	0	0	0	0	23	101
5:00 PM	0	0	0	0	0	2	0	1	0	0	0	9	0	0	0	0	6	1	0	0	0	19	101
5:15 PM	0	0	0	0	0	1	0	1	0	0	0	10	0	0	0	0	13	3	0	0	0	28	101
5:30 PM	0	0	0	0	0	1	0	5	0	0	0	4	0	0	0	0	7	0	0	0	0	18	88
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	15	2	0	0	0	28	93
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total		
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
All Vehicles	0	0	0	0	0	12	0	12	0	0	12	32	0	0	0	0	52	4	0	0	0	124	
Heavy Trucks	0	0	0			0	0	4			0	0	0			0	8	0				12	
Buses																							
Pedestrians		0					0					0					0					0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0				0	
Scooters																							
Comments:																							

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Parking Area -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889210
DATE: Tue, Aug 23 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Parking Area (Northbound)					(Off-Peak) Parking Area (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	11	0	0	0	14	
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	15	0	0	0	23	
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	7	0	0	0	13	
6:45 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	17	0	0	0	20	70
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	13	0	0	0	0	11	0	0	0	24	80
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	18	1	0	0	0	10	0	0	0	29	86
7:30 AM	0	0	1	0	0	0	0	0	0	0	0	9	1	0	0	0	7	0	0	0	18	91
7:45 AM	0	0	1	0	0	0	0	0	0	0	0	8	0	0	0	0	6	0	0	0	15	86
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	7	1	0	0	0	4	0	0	0	12	74
8:15 AM	0	0	1	0	0	0	0	0	0	0	0	5	0	0	0	0	2	0	0	0	8	53
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	5	0	0	0	12	47
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	3	0	0	0	5	37
9:00 AM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	6	0	0	0	13	38
9:15 AM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	1	8	0	0	0	17	47
9:30 AM	1	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	11	0	0	0	21	56
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	4	0	0	0	8	59
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	3	0	0	0	9	55
10:15 AM	0	0	1	0	0	0	0	0	0	0	0	4	1	0	0	0	6	0	0	0	12	50
10:30 AM	0	0	1	0	0	0	0	0	0	0	0	5	0	0	0	1	8	0	0	0	15	44
10:45 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	4	0	0	0	9	45
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	5	0	0	0	13	49
11:15 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	5	0	0	0	10	47
11:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	4	0	0	0	10	42
11:45 AM	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	8	0	0	0	12	45
12:00 PM	1	0	0	0	0	0	0	0	0	0	0	4	0	0	0	1	6	0	0	0	12	44
12:15 PM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	6	0	0	0	9	43
12:30 PM	1	0	0	0	0	0	0	0	0	0	0	9	0	0	0	1	7	0	0	0	18	51
12:45 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	10	0	0	0	17	56
1:00 PM	0	0	1	0	0	0	0	0	0	0	0	5	0	0	0	1	10	0	0	0	17	61
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	1	4	0	0	0	11	63
1:30 PM	1	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	3	0	0	0	14	59
1:45 PM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	6	0	0	0	14	56
2:00 PM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	1	8	0	0	0	12	51
2:15 PM	0	0	1	0	0	0	0	0	0	0	0	8	0	0	0	1	3	0	0	0	13	53
2:30 PM	1	0	0	0	0	0	0	0	0	0	0	12	1	0	0	1	6	0	0	0	21	60
2:45 PM	0	0	1	0	0	0	0	0	0	0	0	9	0	0	0	0	4	0	0	0	14	60
3:00 PM	1	0	1	0	0	0	0	0	0	0	0	16	0	0	0	2	3	0	0	0	23	71
3:15 PM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	8	0	0	0	16	74

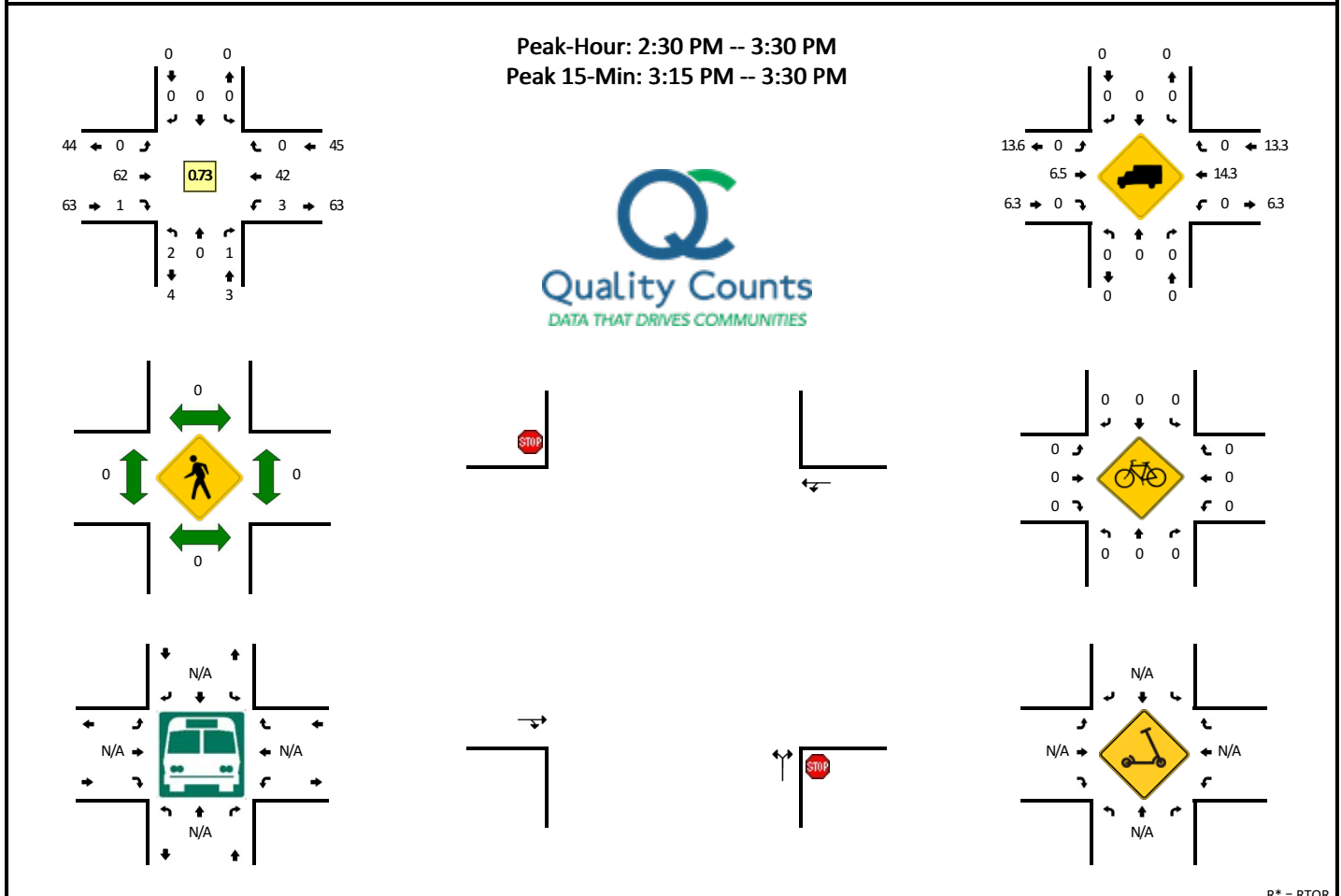
15-Min Count Period Beginning At	(Off-Peak) Parking Area (Northbound)					(Off-Peak) Parking Area (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
3:30 PM	1	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	12	0	0	0	20	73
3:45 PM	0	0	0	0	0	0	0	0	0	0	0	13	0	0	0	0	0	13	0	0	0	26	85
4:00 PM	1	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	1	8	0	0	0	14	76
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	14	0	0	0	0	0	13	0	0	0	27	87
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	17	0	0	0	0	1	6	0	0	0	24	91
4:45 PM	1	0	0	0	0	0	0	0	0	0	0	18	0	0	0	0	0	10	0	0	0	29	94
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	18	0	0	0	0	0	11	0	0	0	29	109
5:15 PM	0	0	1	0	0	0	0	0	0	0	0	15	1	0	0	0	0	21	0	0	0	38	120
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	10	0	0	0	18	114
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	10	0	0	0	18	103
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total		
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
All Vehicles	0	0	4	0	0	0	0	0	0	0	0	60	4	0	0	0	0	84	0	0	0	152	
Heavy Trucks	0	0	0			0	0	0			0	0	0			0	20	0			20		
Buses																							
Pedestrians		0					0					0					0				0		
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0		
Scooters																							
Comments:																							

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Parking Area -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889211
DATE: Sat, Aug 27 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Parking Area (Northbound)					(Off-Peak) Parking Area (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	9	0	0	0	11	25 19 30 39 41 49 50 43 43 41 43 50 64 65 75 88 98 104 102 97 102 99 96 92 88 88 86 90 85 91 96 88 111
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	3	0	0	0	6	
6:45 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	4	0	0	0	6	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	0	1	0	0	0	5	
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	0	3	0	0	0	13	
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	0	6	0	0	0	15	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	6	0	0	0	8	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	6	0	0	0	13	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	8	0	0	0	14	
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	3	0	0	0	8	
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	5	0	0	0	8	
9:00 AM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	4	0	0	0	11	
9:15 AM	0	0	0	0	0	0	0	0	0	0	0	10	1	0	0	0	0	5	0	0	0	16	
9:30 AM	0	0	1	0	0	0	0	0	0	0	0	9	0	0	0	0	0	5	0	0	0	15	
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	13	0	0	0	18	
10:00 AM	2	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	2	2	0	0	0	15	
10:15 AM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	1	6	0	0	0	17	
10:30 AM	0	0	0	0	0	0	0	0	0	0	0	13	1	0	0	0	1	10	0	0	0	25	
10:45 AM	0	0	1	0	0	0	0	0	0	0	0	13	0	0	0	0	1	16	0	0	0	31	
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	13	0	0	0	0	0	12	0	0	0	25	
11:15 AM	1	0	1	0	0	0	0	0	0	0	0	8	0	0	0	0	0	13	0	0	0	23	
11:30 AM	1	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	1	13	0	0	0	23	
11:45 AM	0	0	0	0	0	0	0	0	0	0	0	11	1	0	0	0	0	14	0	0	0	26	
12:00 PM	1	0	1	0	0	0	0	0	0	0	0	13	0	0	0	0	2	13	0	0	0	30	
12:15 PM	1	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	0	9	0	0	0	20	
12:30 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	13	0	0	0	20	
12:45 PM	0	0	1	0	0	0	0	0	0	0	0	7	0	0	0	0	0	14	0	0	0	22	
1:00 PM	0	0	0	0	0	0	0	0	0	0	0	13	0	0	0	0	0	13	0	0	0	26	
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	8	1	0	0	0	0	11	0	0	0	20	
1:30 PM	0	0	1	0	0	0	0	0	0	0	0	12	0	0	0	0	1	6	0	0	0	20	
1:45 PM	2	0	0	0	0	0	0	0	0	0	0	4	1	0	0	0	0	13	0	0	0	20	
2:00 PM	0	0	0	0	0	0	0	0	0	0	0	19	1	0	0	0	0	10	0	0	0	30	
2:15 PM	0	0	1	0	0	0	0	0	0	0	0	9	0	0	0	0	0	5	0	0	0	15	
2:30 PM	0	0	0	0	0	0	0	0	0	0	0	18	0	0	0	0	0	8	0	0	0	26	
2:45 PM	1	0	0	0	0	0	0	0	0	0	0	12	1	0	0	0	1	10	0	0	0	25	
3:00 PM	1	0	1	0	0	0	0	0	0	0	0	16	0	0	0	0	1	3	0	0	0	22	
3:15 PM	0	0	0	0	0	0	0	0	0	0	0	16	0	0	0	0	1	21	0	0	0	38	

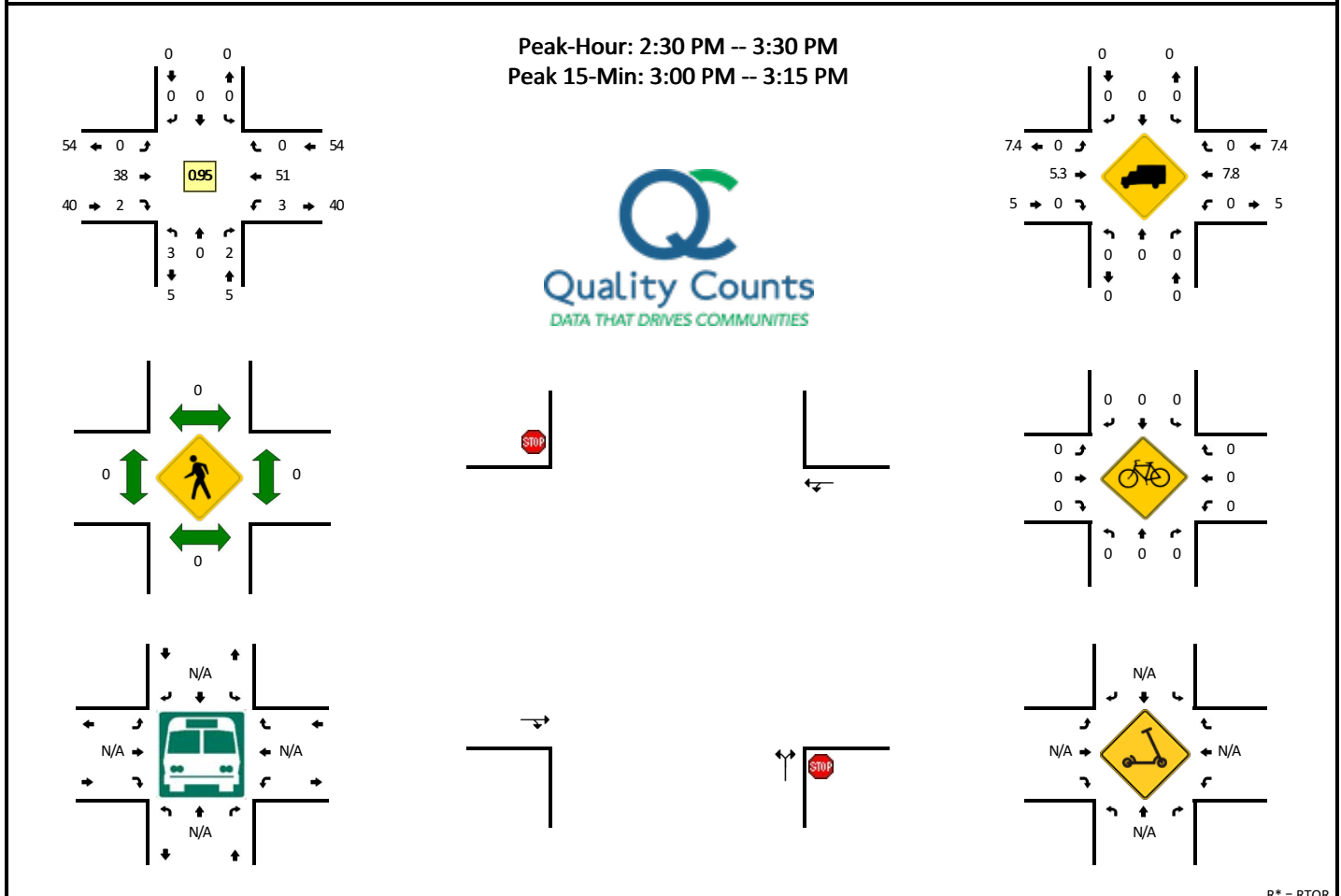
15-Min Count Period Beginning At	(Off-Peak) Parking Area (Northbound)					(Off-Peak) Parking Area (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
3:30 PM	1	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	16	0	0	0	24	109
3:45 PM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	1	8	0	0	0	18	102
4:00 PM	0	0	1	0	0	0	0	0	0	0	0	9	1	0	0	0	0	15	0	0	0	26	106
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	17	0	0	0	0	0	11	0	0	0	28	96
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	0	7	0	0	0	16	88
4:45 PM	1	0	2	0	0	0	0	0	0	0	0	7	2	0	0	0	0	18	0	0	0	30	100
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	8	0	0	0	15	89
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	17	0	0	0	0	0	10	0	0	0	27	88
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	12	0	0	0	0	0	19	0	0	0	31	103
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	4	0	1	0	0	0	8	0	0	0	13	86
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total		
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
All Vehicles	0	0	0	0	0	0	0	0	0	0	0	64	0	0	0	0	4	84	0	0	0	152	
Heavy Trucks	0	0	0			0	0	0			0	4	0				0	0	0			4	
Buses																							
Pedestrians		0					0					0						0				0	
Bicycles	0	0	0			0	0	0			0	0	0				0	0	0			0	
Scooters																							
Comments:																							

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Parking Area -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889212
DATE: Sun, Aug 28 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Parking Area (Northbound)					(Off-Peak) Parking Area (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	7	0	0	0	9	20 17 23 28 29 34 35 35 39 43 42 44 43 36 36 44 53 57 63 58 57 72 78 92 98 90 87 87 90 95 93 91 91 93 99
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	4	0	0	0	6	
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	
6:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	2	0	0	0	3	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	4	0	0	0	6	
7:15 AM	0	0	1	0	0	0	0	0	0	0	0	5	1	0	0	0	0	5	0	0	0	12	
7:30 AM	0	0	1	0	0	0	0	0	0	0	0	4	1	0	0	0	0	1	0	0	0	7	
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	2	0	0	0	4	
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	1	3	0	0	0	11	
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	0	6	0	0	0	13	
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	2	0	0	0	7	
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	3	1	0	0	0	0	4	0	0	0	8	
9:00 AM	2	0	0	0	0	0	0	0	0	0	0	7	1	0	0	0	1	4	0	0	0	15	
9:15 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	9	0	0	0	12	
9:30 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	4	0	0	0	9	
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	4	0	0	0	7	
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	0	3	0	0	0	8	
10:15 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	9	0	0	0	12	
10:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	11	0	0	0	17	
10:45 AM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	8	0	0	0	16	
11:00 AM	1	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	1	2	0	0	0	12	
11:15 AM	0	0	0	0	0	0	0	0	0	0	0	11	0	0	0	0	0	7	0	0	0	18	
11:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	6	0	0	0	12	
11:45 AM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	0	6	0	0	0	15	
12:00 PM	3	0	1	0	0	0	0	0	0	0	0	9	1	0	0	0	4	9	0	0	0	27	
12:15 PM	1	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	1	13	0	0	0	24	
12:30 PM	1	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	23	0	0	0	26	
12:45 PM	1	0	1	0	0	0	0	0	0	0	0	10	0	0	0	0	1	8	0	0	0	21	
1:00 PM	0	0	1	0	0	0	0	0	0	0	0	6	1	0	0	0	0	11	0	0	0	19	
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	8	0	0	0	0	0	11	0	2	0	21	
1:30 PM	0	0	2	0	0	0	0	0	0	0	0	15	0	0	0	0	3	6	0	0	0	26	
1:45 PM	1	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	1	13	0	0	0	24	
2:00 PM	0	0	0	0	0	0	0	0	0	0	0	10	0	0	0	0	0	14	0	0	0	24	
2:15 PM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	0	10	0	0	0	19	
2:30 PM	0	0	0	0	0	0	0	0	0	0	0	11	0	0	0	0	0	13	0	0	0	24	
2:45 PM	1	0	0	0	0	0	0	0	0	0	0	11	1	0	0	0	0	11	0	0	0	24	
3:00 PM	0	0	2	0	0	0	0	0	0	0	0	9	0	0	0	0	3	12	0	0	0	26	
3:15 PM	2	0	0	0	0	0	0	0	0	0	0	7	1	0	0	0	0	15	0	0	0	25	

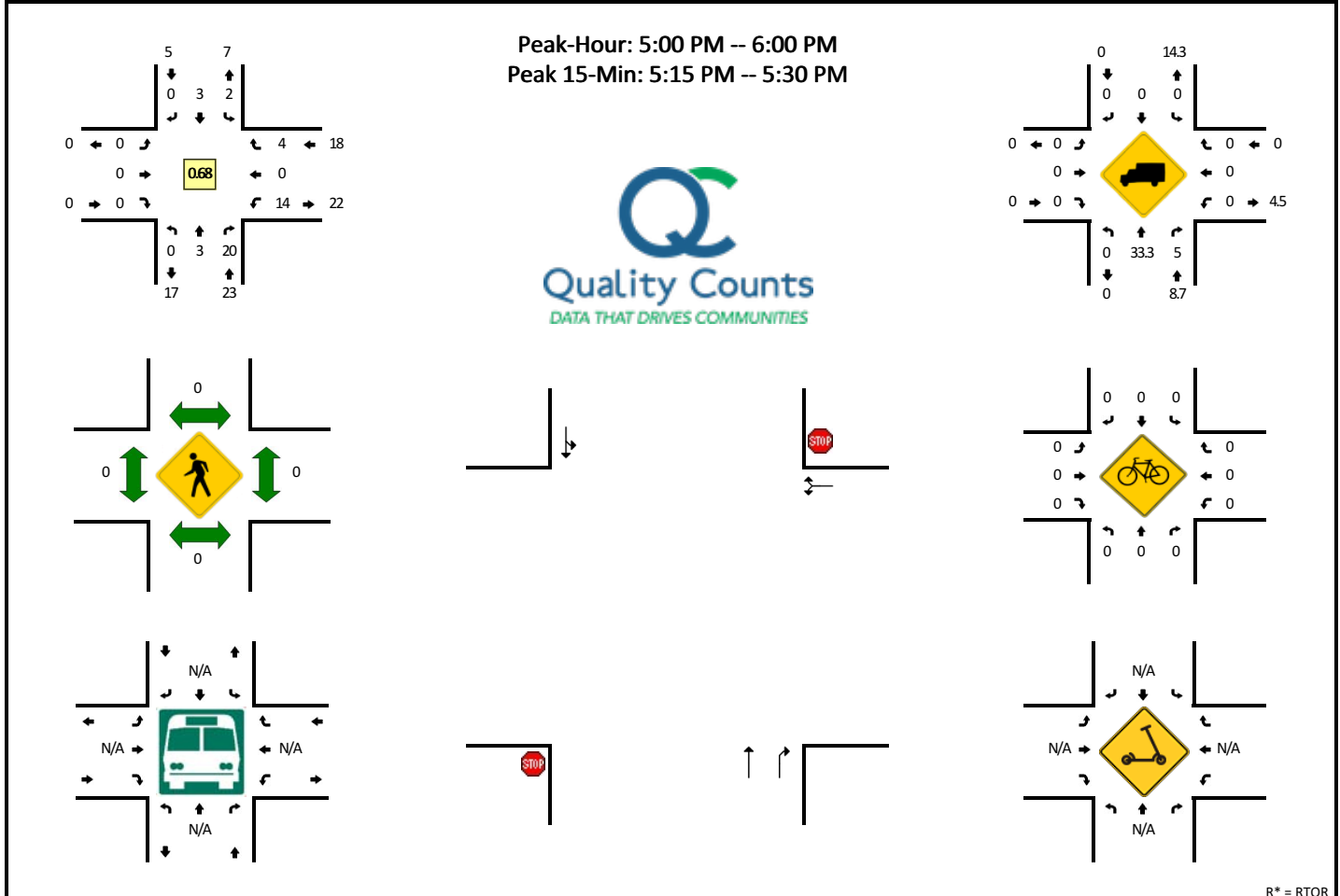
15-Min Count Period Beginning At	(Off-Peak) Parking Area (Northbound)					(Off-Peak) Parking Area (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
3:30 PM	1	0	1	0	0	0	0	0	0	0	0	10	0	0	0	1	10	0	0	0	23	98
3:45 PM	1	0	0	0	0	0	0	0	0	0	0	4	0	0	0	1	17	0	0	0	23	97
4:00 PM	1	0	1	0	0	0	0	0	0	0	0	8	0	0	0	2	10	0	0	0	22	93
4:15 PM	0	0	1	0	0	0	0	0	0	0	0	12	0	0	0	1	11	0	0	0	25	93
4:30 PM	1	0	0	0	0	0	0	0	0	0	0	13	0	0	0	1	11	0	0	0	26	96
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	12	0	0	0	0	9	0	0	0	21	94
5:00 PM	1	0	0	0	0	0	0	0	0	0	0	10	0	0	0	1	10	0	0	0	22	94
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	11	0	0	0	0	14	0	0	0	25	94
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	1	5	0	0	0	13	81
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	7	0	0	0	0	19	0	0	0	26	86
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
All Vehicles	0	0	8	0	0	0	0	0	0	0	0	36	0	0	0	12	48	0	0	0	104	
Heavy Trucks	0	0	0			0	0	0			0	4	0			0	0	0			4	
Buses																						
Pedestrians		0					0					0					0				0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	
Scooters																						
Comments:																						

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Lauderdale County 2 -- Ramp to Natchez Trace Pkwy (west)
CITY/STATE: Lauderdale, AL

QC JOB #: 15889207
DATE: Tue, Aug 23 2022



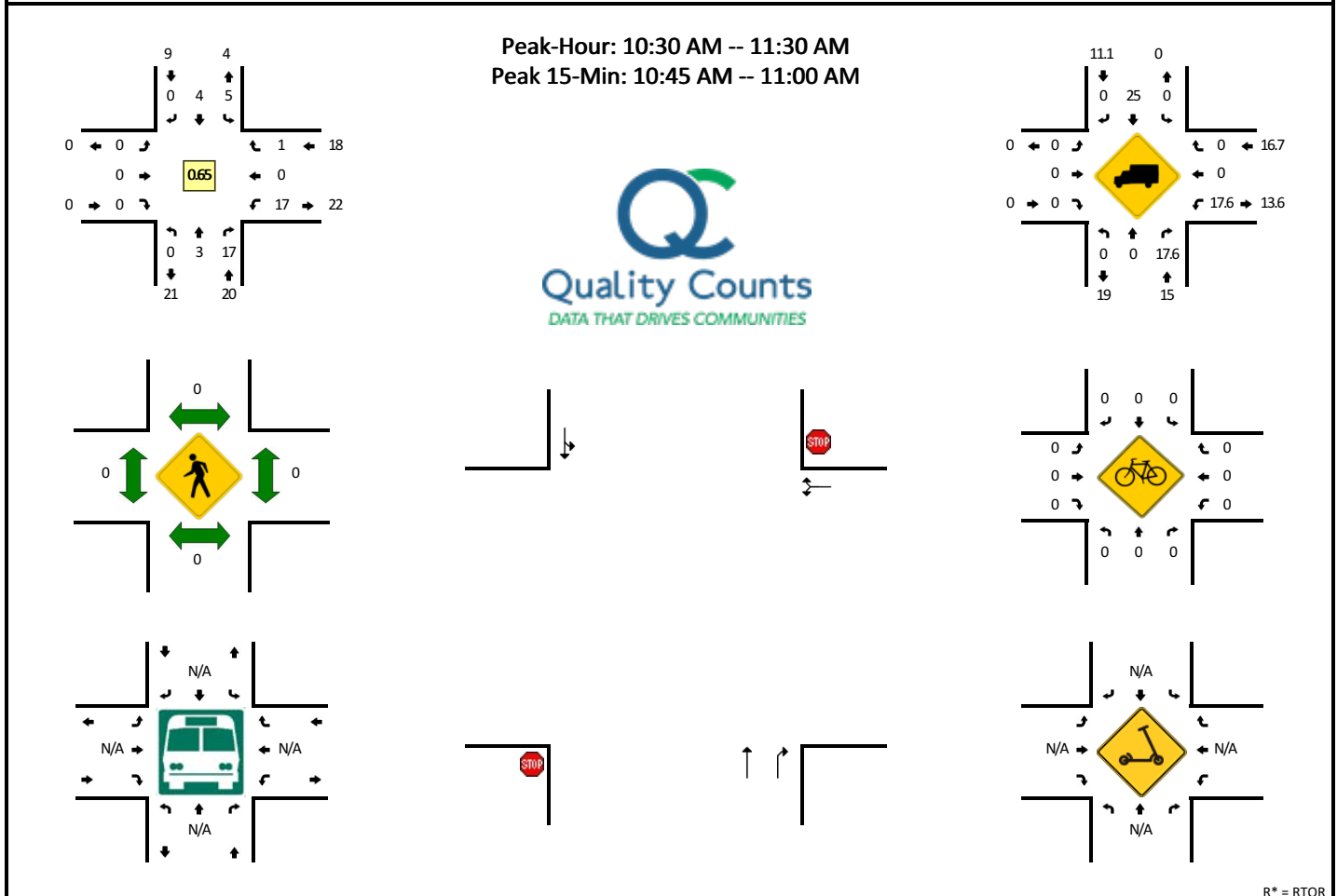
R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Lauderdale County 2 (Northbound)					(Off-Peak) Lauderdale County 2 (Southbound)					Ramp to Natchez Trace Pkwy (west) (Eastbound)					Ramp to Natchez Trace Pkwy (west) (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
6:15 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
6:30 AM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	
6:45 AM	0	0	2	0	0	0	3	0	0	0	0	0	0	0	0	1	0	0	0	0	6	12
7:00 AM	0	1	6	0	0	0	1	0	0	0	0	0	0	0	0	4	0	0	0	0	12	24
7:15 AM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	9	32
7:30 AM	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	9	36
7:45 AM	0	0	2	0	0	0	2	0	0	0	0	0	0	0	0	1	0	0	0	0	5	35
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	4	27
8:15 AM	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	1	0	0	0	0	3	21
8:30 AM	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	4	16
8:45 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	3	14
9:00 AM	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	0	0	5	15
9:15 AM	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	0	5	17
9:30 AM	0	0	4	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	7	20
9:45 AM	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	7	24
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	2	21
10:15 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	3	19
10:30 AM	0	3	5	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	10	22
10:45 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	17
11:00 AM	0	1	1	0	0	2	1	0	0	0	0	0	0	0	0	3	0	0	0	0	8	23
11:15 AM	0	1	2	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	6	26
11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16
11:45 AM	0	1	5	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	8	22
12:00 PM	0	0	3	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	5	19
12:15 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	5	18
12:30 PM	0	0	2	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	6	24
12:45 PM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	7	23
1:00 PM	0	0	3	0	0	1	2	0	0	0	0	0	0	0	0	3	0	1	0	0	10	28
1:15 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	0	2	0	1	0	0	6	29
1:30 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	0	1	0	0	4	27
1:45 PM	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	0	6	26
2:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	4	20
2:15 PM	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	4	0	1	1	0	9	23
2:30 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	4	0	2	0	0	9	28
2:45 PM	0	1	2	0	0	0	1	0	0	0	0	0	0	0	0	4	0	1	0	0	9	31

15-Min Count Period Beginning At	(Off-Peak) Lauderdale County 2 (Northbound)					(Off-Peak) Lauderdale County 2 (Southbound)					Ramp to Natchez Trace Pkwy (west) (Eastbound)					Ramp to Natchez Trace Pkwy (west) (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
3:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	5	0	0	0	0	7	34
3:15 PM	0	0	6	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	8	33
3:30 PM	0	4	2	0	0	0	2	0	0	0	0	0	0	0	0	2	0	0	0	0	10	34
3:45 PM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	9	34
4:00 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	4	31
4:15 PM	0	2	4	0	0	0	1	0	0	0	0	0	0	0	0	3	0	0	0	0	10	33
4:30 PM	0	0	3	0	0	1	1	0	0	0	0	0	0	0	0	2	0	1	0	0	8	31
4:45 PM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	6	28
5:00 PM	0	1	3	0	0	0	1	0	0	0	0	0	0	0	0	4	0	2	0	0	11	35
5:15 PM	0	1	8	0	0	1	1	0	0	0	0	0	0	0	0	5	0	1	0	0	17	42
5:30 PM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	3	0	1	0	0	8	42
5:45 PM	0	1	5	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	10	46
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
All Vehicles	0	4	32	0	0	4	4	0	0	0	0	0	0	0	0	20	0	4	0	0	68	
Heavy Trucks	0	0	4			0	0	0			0	0	0			0	0	0			4	
Buses																						
Pedestrians		0					0					0					0				0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	
Scooters																						
<i>Comments:</i>																						

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Lauderdale County 2 -- Ramp to Natchez Trace Pkwy (west)**QC JOB #:** 15889208**CITY/STATE:** Lauderdale, AL**DATE:** Sat, Aug 27 2022

R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Lauderdale County 2 (Northbound)					(Off-Peak) Lauderdale County 2 (Southbound)					Ramp to Natchez Trace Pkwy (west) (Eastbound)					Ramp to Natchez Trace Pkwy (west) (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1	
6:15 AM	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	
6:30 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	2	
6:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	4	9
7:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	2	10
7:15 AM	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	5	13
7:30 AM	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	3	14
7:45 AM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	13
8:00 AM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	7	18
8:15 AM	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	6	19
8:30 AM	0	1	3	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	4	20
8:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	18
9:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	3	14
9:15 AM	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	5	13
9:30 AM	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	0	1	0	0	4	13
9:45 AM	0	2	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	16
10:00 AM	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	0	5	18
10:15 AM	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	2	0	3	0	0	8	21
10:30 AM	0	1	3	0	0	1	1	0	0	0	0	0	0	0	0	4	0	0	0	0	10	27
10:45 AM	0	1	8	0	0	2	1	0	0	0	0	0	0	0	0	6	0	0	0	0	18	41
11:00 AM	0	1	3	0	0	1	1	0	0	0	0	0	0	0	0	3	0	0	0	0	9	45
11:15 AM	0	0	3	0	0	1	1	0	0	0	0	0	0	0	0	4	0	1	0	0	10	47
11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	1	0	0	3	40
11:45 AM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	0	1	0	0	5	27
12:00 PM	0	0	5	0	0	0	2	0	0	0	0	0	0	0	0	6	0	0	0	0	13	31
12:15 PM	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	3	0	1	0	0	6	27
12:30 PM	0	1	3	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	6	30
12:45 PM	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	1	0	1	0	0	6	31
1:00 PM	0	0	3	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	23
1:15 PM	0	1	3	0	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	0	7	24
1:30 PM	0	2	2	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	7	25
1:45 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	4	23
2:00 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	5	0	1	0	0	9	27
2:15 PM	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	2	0	0	0	0	5	25
2:30 PM	0	0	1	0	0	2	2	0	0	0	0	0	0	0	0	5	0	0	0	0	10	28
2:45 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	0	5	29

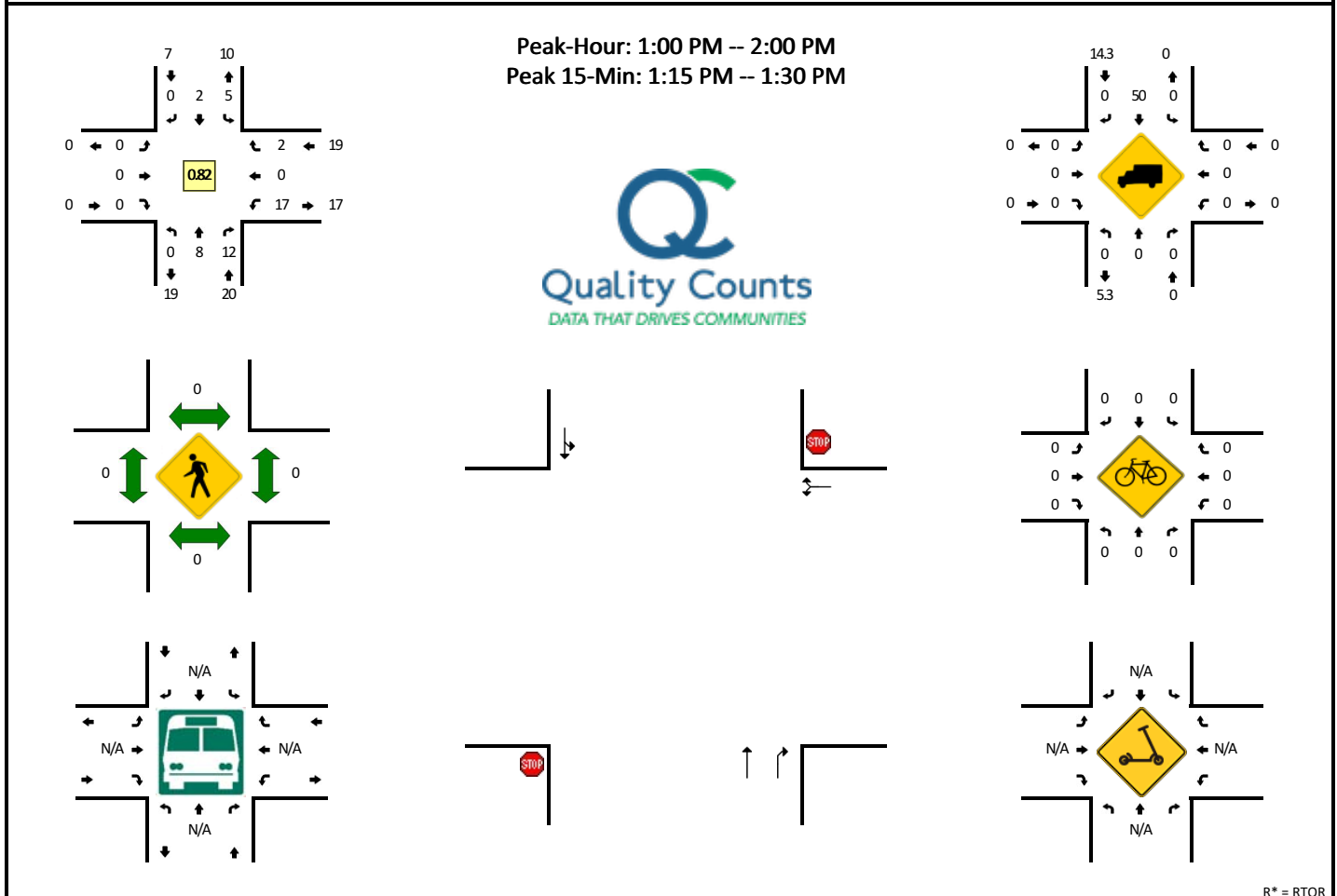
15-Min Count Period Beginning At	(Off-Peak) Lauderdale County 2 (Northbound)					(Off-Peak) Lauderdale County 2 (Southbound)					Ramp to Natchez Trace Pkwy (west) (Eastbound)					Ramp to Natchez Trace Pkwy (west) (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
3:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	5	25
3:15 PM	0	2	2	0	0	1	1	0	0	0	0	0	0	0	0	3	0	1	0	0	10	30
3:30 PM	0	0	3	0	0	2	1	0	0	0	0	0	0	0	0	0	0	1	0	0	7	27
3:45 PM	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	3	0	1	0	0	8	30
4:00 PM	0	3	2	0	0	3	1	0	0	0	0	0	0	0	0	3	0	1	0	0	13	38
4:15 PM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	7	0	2	0	0	12	40
4:30 PM	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	5	0	0	0	0	8	41
4:45 PM	0	0	1	0	0	8	0	0	0	0	0	0	0	0	0	3	0	1	0	0	13	46
5:00 PM	0	0	4	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2	0	0	7	40
5:15 PM	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	4	0	2	0	0	9	37
5:30 PM	0	0	4	0	0	2	3	0	0	0	0	0	0	0	0	3	0	0	0	0	12	41
5:45 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
All Vehicles	0	4	32	0	0	8	4	0	0	0	0	0	0	0	0	24	0	0	0	0	72	
Heavy Trucks	0	0	4			0	0	0			0	0	0			4	0	0			8	
Buses																						
Pedestrians		0					0					0					0				0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	
Scooters																						
<i>Comments:</i>																						

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Lauderdale County 2 -- Ramp to Natchez Trace Pkwy (west)
CITY/STATE: Lauderdale, AL

QC JOB #: 15889209
DATE: Sun, Aug 28 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Lauderdale County 2 (Northbound)					(Off-Peak) Lauderdale County 2 (Southbound)					Ramp to Natchez Trace Pkwy (west) (Eastbound)					Ramp to Natchez Trace Pkwy (west) (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	2	
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	
6:30 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	2	
6:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	6
7:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	6
7:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	6
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	5
7:45 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	5
8:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	3	6
8:15 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	3	8
8:30 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	3	10
8:45 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	5	14
9:00 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	3	14
9:15 AM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	13
9:30 AM	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	5	15
9:45 AM	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	13
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	2	12
10:15 AM	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	12
10:30 AM	0	0	5	0	0	1	0	0	0	0	0	0	0	0	0	2	0	0	0	0	8	15
10:45 AM	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	0	5	17
11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	5	20
11:15 AM	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	9	27
11:30 AM	0	1	2	0	0	0	2	0	0	0	0	0	0	0	0	5	0	0	0	0	10	29
11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24
12:00 PM	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	2	0	1	0	0	8	27
12:15 PM	0	0	4	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	8	26
12:30 PM	0	0	0	0	0	4	1	0	0	0	0	0	0	0	0	1	0	0	0	0	6	22
12:45 PM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	4	26
1:00 PM	0	3	2	0	0	0	1	0	0	0	0	0	0	0	0	1	0	1	0	0	8	26
1:15 PM	0	1	5	0	0	2	0	0	0	0	0	0	0	0	0	6	0	0	0	0	14	32
1:30 PM	0	3	0	0	0	2	1	0	0	0	0	0	0	0	0	6	0	0	0	0	12	38
1:45 PM	0	1	5	0	0	1	0	0	0	0	0	0	0	0	0	4	0	1	0	0	12	46
2:00 PM	0	0	3	0	0	2	0	0	0	0	0	0	0	0	0	2	0	0	0	0	7	45
2:15 PM	0	1	2	0	0	1	1	0	0	0	0	0	0	0	0	2	0	0	0	0	7	38
2:30 PM	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	0	1	0	0	6	32
2:45 PM	0	4	4	0	0	0	1	0	0	0	0	0	0	0	0	3	0	0	0	0	12	32

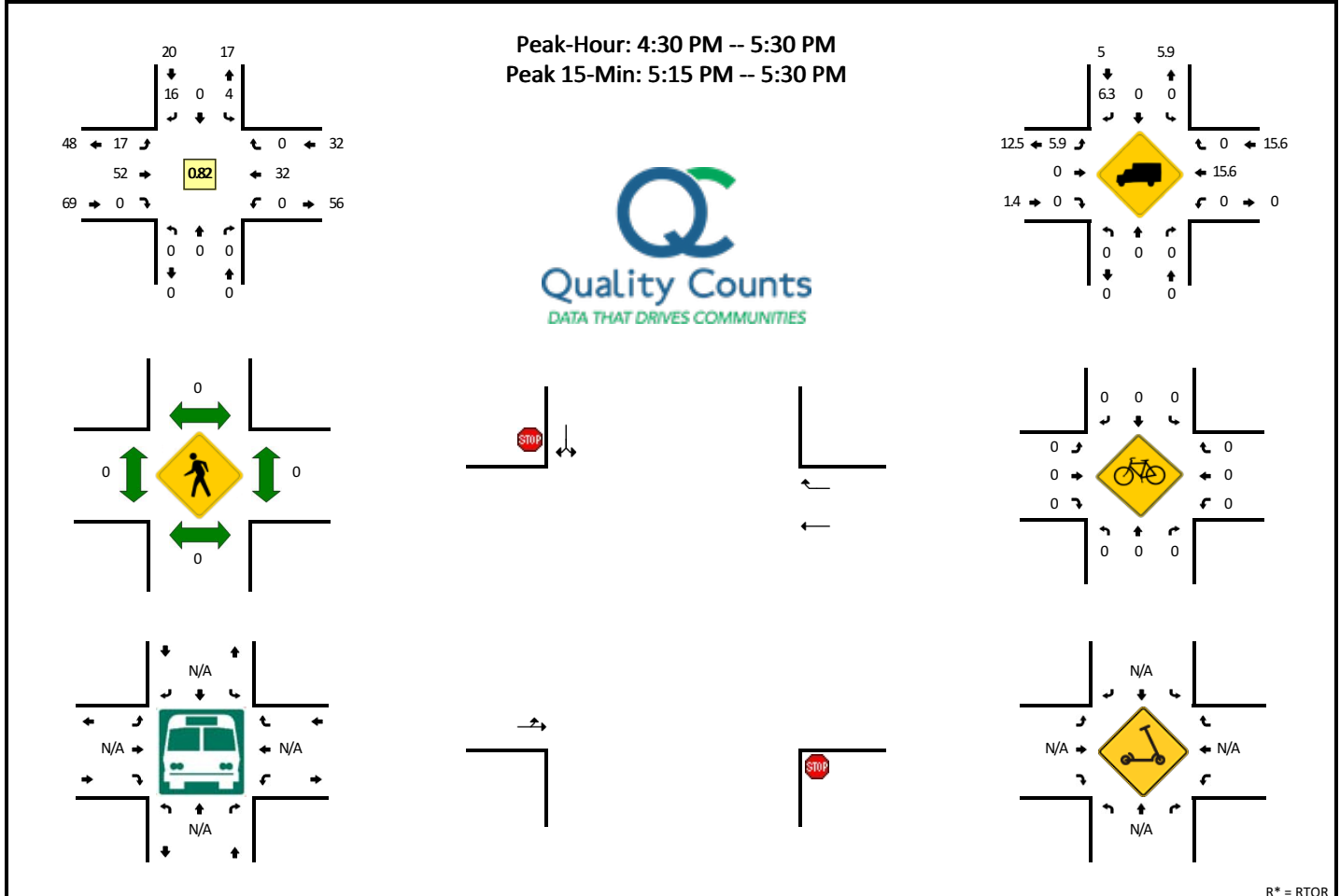
15-Min Count Period Beginning At	(Off-Peak) Lauderdale County 2 (Northbound)					(Off-Peak) Lauderdale County 2 (Southbound)					Ramp to Natchez Trace Pkwy (west) (Eastbound)					Ramp to Natchez Trace Pkwy (west) (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
3:00 PM	0	0	4	0	0	0	1	0	0	0	0	0	0	0	0	2	0	1	0	0	8	33
3:15 PM	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	5	0	1	0	0	8	34
3:30 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	0	0	7	35
3:45 PM	0	2	5	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	31
4:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	2	0	0	5	28
4:15 PM	0	2	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	1	0	0	6	26
4:30 PM	0	0	3	0	0	0	1	0	0	0	0	0	0	0	0	3	0	2	0	0	9	28
4:45 PM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	5	0	0	0	0	8	28
5:00 PM	0	0	2	0	0	0	2	0	0	0	0	0	0	0	0	1	0	1	0	0	6	29
5:15 PM	0	1	1	0	0	0	1	0	0	0	0	0	0	0	0	2	0	1	0	0	6	29
5:30 PM	0	0	5	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	0	7	27
5:45 PM	0	0	6	0	0	0	1	0	0	0	0	0	0	0	0	3	0	0	0	0	10	29
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
All Vehicles	0	4	20	0	0	8	0	0	0	0	0	0	0	0	0	24	0	0	0	0	56	
Heavy Trucks	0	0	0			0	0	0			0	0	0			0	0	0			0	
Buses																						
Pedestrians		0					0					0					0				0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	
Scooters																						
<i>Comments:</i>																						

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Ramp to Lauderdale County Rd 2 (East) -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889204
DATE: Tue, Aug 23 2022



15-Min Count Period Beginning At	(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)					(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	11	0	0	0	14	71 81 88 94 84 72 52 45 38 38 47 56 60 57 51 45 45 43 47 44 43 49 60 57 56 52 58 60 52 55 63 62 72
6:15 AM	0	0	0	0	0	0	0	1	0	0	0	7	0	0	0	0	0	13	0	0	0	21	
6:30 AM	0	0	0	0	0	1	0	2	0	0	0	6	0	0	0	0	0	5	0	0	0	14	
6:45 AM	0	0	0	0	0	0	0	2	0	0	1	3	0	0	0	0	0	16	0	0	0	22	
7:00 AM	0	0	0	0	0	1	0	5	0	0	4	9	0	0	0	0	0	5	0	0	0	24	
7:15 AM	0	0	0	0	0	0	0	4	0	0	5	13	0	0	0	0	0	6	0	0	0	28	
7:30 AM	0	0	0	0	0	1	0	3	0	0	4	6	0	0	0	0	0	6	0	0	0	20	
7:45 AM	0	0	0	0	0	0	0	2	0	0	1	7	0	0	0	0	0	2	0	0	0	12	
8:00 AM	0	0	0	0	0	0	0	0	0	0	4	4	0	0	0	0	0	4	0	0	0	12	
8:15 AM	0	0	0	0	0	0	0	0	0	0	1	5	0	0	0	0	0	2	0	0	0	8	
8:30 AM	0	0	0	0	0	0	0	1	0	0	1	6	0	0	0	0	0	4	1	0	0	13	
8:45 AM	0	0	0	0	0	0	0	1	0	0	1	1	0	0	0	0	0	2	0	0	0	5	
9:00 AM	0	0	0	0	0	1	0	1	0	0	2	3	0	0	0	0	0	5	0	0	0	12	
9:15 AM	0	0	0	0	0	0	0	3	0	0	2	6	0	0	0	0	0	6	0	0	0	17	
9:30 AM	0	0	0	0	0	0	0	5	0	0	2	8	0	1	0	0	0	6	0	0	0	22	
9:45 AM	0	0	0	0	0	1	0	0	0	0	3	1	0	0	0	0	0	4	0	0	0	9	
10:00 AM	0	0	0	0	0	0	0	0	0	0	2	4	0	0	0	0	0	3	0	0	0	9	
10:15 AM	0	0	0	0	0	0	0	1	0	0	1	4	0	0	0	0	0	5	0	0	0	11	
10:30 AM	0	0	0	0	0	1	0	4	0	0	2	4	0	0	0	0	0	5	0	0	0	16	
10:45 AM	0	0	0	0	0	0	0	2	0	0	0	5	0	0	0	0	0	2	0	0	0	9	
11:00 AM	0	0	0	0	0	1	0	2	0	0	3	5	0	0	0	0	0	3	0	0	0	14	
11:15 AM	0	0	0	0	0	0	0	2	0	0	2	3	0	0	0	0	0	3	0	0	0	10	
11:30 AM	0	0	0	0	0	0	0	0	0	0	0	6	0	0	0	0	0	4	0	0	0	10	
11:45 AM	0	0	0	0	0	1	0	4	0	0	2	2	0	0	0	0	0	4	0	0	0	13	
12:00 PM	0	0	0	0	0	0	0	3	0	0	1	3	0	0	0	0	0	4	0	0	0	11	
12:15 PM	0	0	0	0	0	0	0	2	0	0	2	1	0	0	0	0	0	4	0	0	0	9	
12:30 PM	0	0	0	0	0	0	0	3	0	0	2	6	0	0	0	0	0	5	0	0	0	16	
12:45 PM	0	0	0	0	0	0	0	2	0	0	3	3	0	1	0	0	0	7	0	0	0	16	
1:00 PM	0	0	0	0	0	1	0	4	0	0	3	1	0	1	0	0	0	6	1	0	0	17	
1:15 PM	0	0	0	0	0	0	0	1	0	0	3	4	0	0	0	0	0	3	0	0	0	11	
1:30 PM	0	0	0	0	0	0	0	1	0	0	3	7	0	0	0	0	0	2	0	0	0	13	
1:45 PM	0	0	0	0	0	1	0	2	0	0	2	6	0	0	0	0	0	4	0	0	0	15	
2:00 PM	0	0	0	0	0	0	0	3	0	0	1	2	0	0	0	0	0	6	1	0	0	13	
2:15 PM	0	0	0	0	0	0	0	3	0	0	4	4	0	0	0	0	0	1	2	0	0	14	
2:30 PM	0	0	0	0	0	0	0	2	0	0	5	8	0	0	0	0	0	5	1	0	0	21	
2:45 PM	0	0	0	0	0	0	0	2	0	0	4	4	0	0	0	0	0	3	1	0	0	14	
3:00 PM	0	0	0	0	0	0	0	1	0	0	5	14	0	0	0	0	0	3	0	0	0	23	

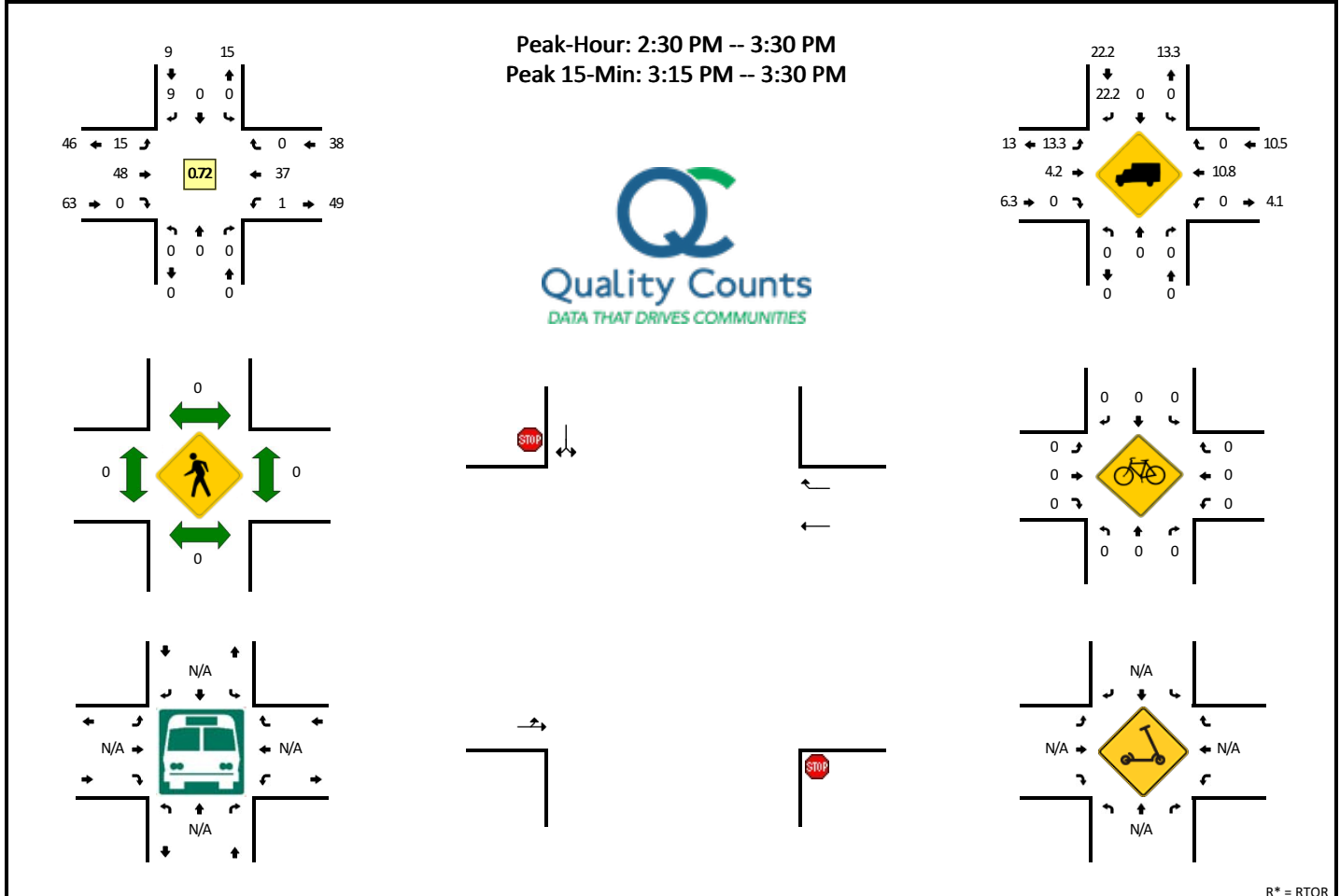
15-Min Count Period Beginning At	(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)					(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
3:15 PM	0	0	0	0	0	0	0	6	0	0	2	6	0	0	0	0	2	0	0	0	0	16	74
3:30 PM	0	0	0	0	0	0	0	2	0	0	1	6	0	0	0	0	10	1	0	0	0	20	73
3:45 PM	0	0	0	0	0	0	0	4	0	0	5	8	0	0	0	0	9	0	0	0	0	26	85
4:00 PM	0	0	0	0	0	0	0	3	0	0	1	3	0	0	0	0	6	0	0	0	0	13	75
4:15 PM	0	0	0	0	0	0	0	4	0	0	3	11	0	0	0	0	10	0	0	0	0	28	87
4:30 PM	0	0	0	0	0	1	0	3	0	0	3	13	0	0	0	0	4	0	0	0	0	24	91
4:45 PM	0	0	0	0	0	2	0	2	0	0	3	16	0	0	0	0	7	0	0	0	0	30	95
5:00 PM	0	0	0	0	0	1	0	2	0	0	5	13	0	0	0	0	9	0	0	0	0	30	112
5:15 PM	0	0	0	0	0	0	0	9	0	0	6	10	0	0	0	0	12	0	0	0	0	37	121
5:30 PM	0	0	0	0	0	0	0	4	0	0	4	4	0	0	0	0	6	0	0	0	0	18	115
5:45 PM	0	0	0	0	0	1	0	5	0	0	1	7	0	0	0	0	5	1	0	0	0	20	105
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total		
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
All Vehicles	0	0	0	0	0	0	0	36	0	0	24	40	0	0	0	0	48	0	0	0	0	148	
Heavy Trucks	0	0	0			0	0	4			0	0	0			0	16	0			0	20	
Buses																							
Pedestrians		0					0					0					0					0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	0	
Scooters																							
Comments:																							

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Ramp to Lauderdale County Rd 2 (East) -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889205
DATE: Sat, Aug 27 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)					(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	8	0	0	0	10	24 19 31 39 41 49 49 43 44 42 43 50 59 62 65 75 88 99 104 100 94 102 99 96 95 88 88 91 82 87 82 87 94 87
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	
6:30 AM	0	0	0	0	0	0	0	1	0	0	2	1	0	0	0	0	2	0	0	0	6	
6:45 AM	0	0	0	0	0	0	0	1	0	0	2	0	0	0	0	0	3	0	0	0	6	
7:00 AM	0	0	0	0	0	0	0	1	0	0	1	3	0	0	0	0	0	0	0	0	5	
7:15 AM	0	0	0	0	0	0	0	2	0	0	2	8	0	0	0	0	2	0	0	0	14	
7:30 AM	0	0	0	0	0	0	0	1	0	0	1	7	0	0	0	0	5	0	0	0	14	
7:45 AM	0	0	0	0	0	0	0	3	0	0	0	3	0	0	0	0	2	0	0	0	8	
8:00 AM	0	0	0	0	0	0	0	3	0	0	3	4	0	0	0	0	3	0	0	0	13	
8:15 AM	0	0	0	0	0	1	0	2	0	0	2	3	0	0	0	0	6	0	0	0	14	
8:30 AM	0	0	0	0	0	0	0	0	0	0	2	3	0	0	0	0	3	0	0	0	8	
8:45 AM	0	0	0	0	0	0	0	1	0	0	0	4	0	0	0	0	4	0	0	0	9	
9:00 AM	0	0	0	0	0	0	0	1	0	0	1	6	0	0	0	0	3	0	0	0	11	
9:15 AM	0	0	0	0	0	0	0	2	0	0	2	8	0	0	0	0	3	0	0	0	15	
9:30 AM	0	0	0	0	0	0	0	1	0	0	2	8	0	0	0	0	4	0	0	0	15	
9:45 AM	0	0	0	0	0	0	0	1	0	0	0	5	0	0	0	0	12	0	0	0	18	
10:00 AM	0	0	0	0	0	1	0	3	0	0	1	8	0	0	0	0	1	0	0	0	14	
10:15 AM	0	0	0	0	0	1	0	1	0	0	5	5	0	0	0	0	6	0	0	0	18	
10:30 AM	0	0	0	0	0	0	0	4	0	0	4	8	0	0	0	0	9	0	0	0	25	
10:45 AM	0	0	0	0	0	0	0	10	0	0	6	9	0	0	0	0	6	0	0	0	31	
11:00 AM	0	0	0	0	0	0	0	4	0	0	2	11	0	0	0	0	7	1	0	0	25	
11:15 AM	0	0	0	0	0	0	0	4	0	0	5	4	0	0	0	0	10	0	0	0	23	
11:30 AM	0	0	0	0	0	0	0	0	0	0	3	5	0	0	0	0	13	0	0	0	21	
11:45 AM	0	0	0	0	0	0	0	1	0	0	2	8	0	0	0	0	13	1	0	0	25	
12:00 PM	0	0	0	0	0	0	0	5	1	0	6	9	0	0	0	0	11	1	0	0	33	
12:15 PM	0	0	0	0	0	0	0	0	0	0	3	7	0	0	0	0	10	0	0	0	20	
12:30 PM	0	0	0	0	0	0	0	3	0	0	1	5	0	0	0	0	9	0	0	0	18	
12:45 PM	0	0	0	0	0	1	0	2	0	0	2	7	0	0	0	0	12	0	0	0	24	
1:00 PM	0	0	0	0	0	0	0	5	0	0	0	13	0	0	0	0	8	0	0	0	26	
1:15 PM	0	0	0	0	0	1	0	3	0	0	2	6	0	0	0	0	8	0	0	0	20	
1:30 PM	0	0	0	0	0	1	0	1	0	0	3	10	0	0	0	0	6	0	0	0	21	
1:45 PM	0	0	0	0	0	0	0	3	0	0	1	1	0	0	0	0	10	0	0	0	15	
2:00 PM	0	0	0	0	0	0	0	2	0	0	6	15	0	0	0	0	8	0	0	0	31	
2:15 PM	0	0	0	0	0	0	0	2	0	0	2	8	0	0	0	0	3	0	0	0	15	
2:30 PM	0	0	0	0	0	0	0	3	0	0	5	13	0	0	0	0	5	0	0	0	26	
2:45 PM	0	0	0	0	0	0	0	1	0	0	2	9	0	0	0	0	10	0	0	0	22	
3:00 PM	0	0	0	0	0	0	0	1	0	0	4	14	0	0	0	0	5	0	0	0	24	

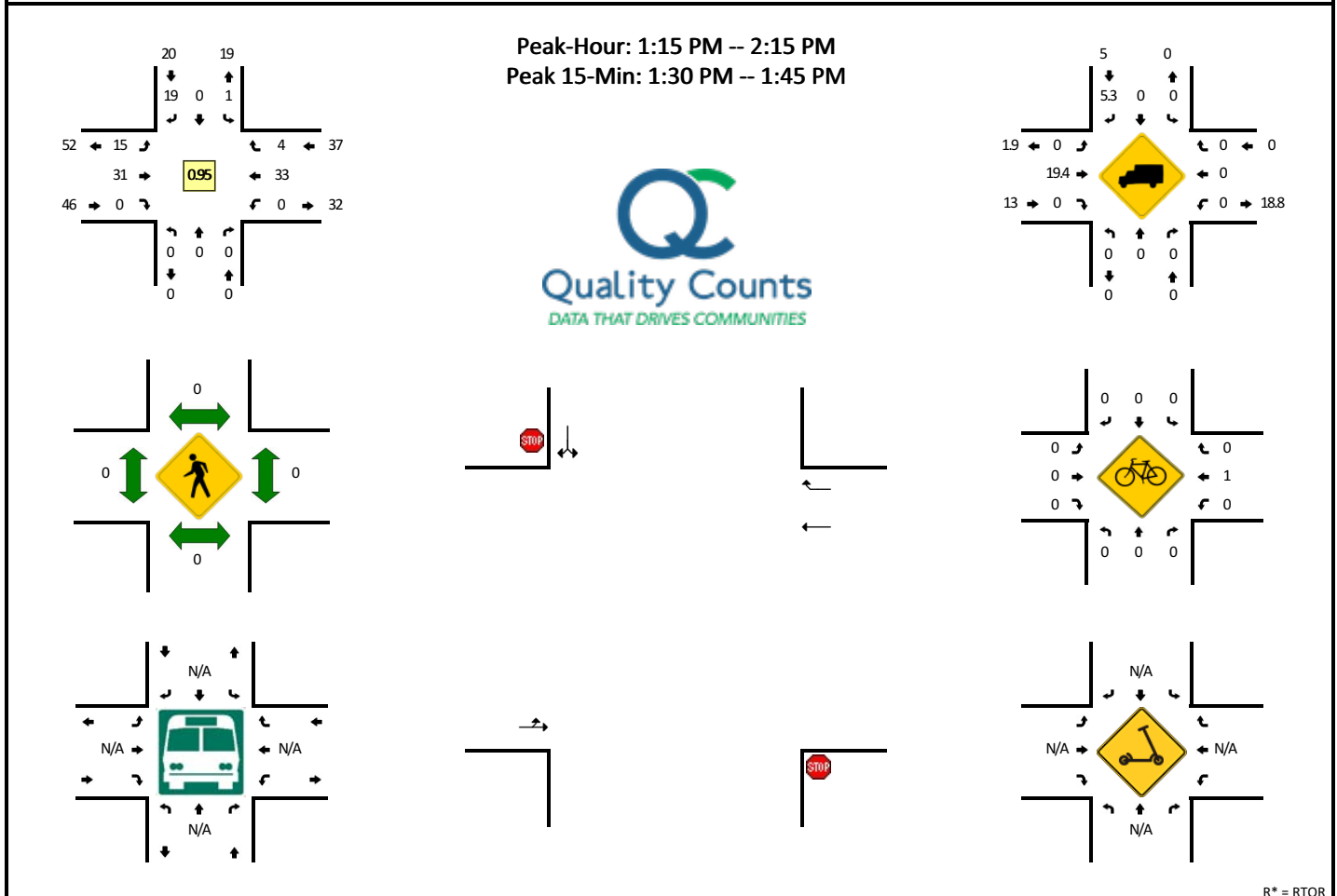
15-Min Count Period Beginning At	(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)					(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
3:15 PM	0	0	0	0	0	0	0	4	0	0	4	12	0	0	0	0	17	0	1	0	38	110
3:30 PM	0	0	0	0	0	1	0	4	0	0	1	5	0	0	0	0	11	0	0	0	22	106
3:45 PM	0	0	0	0	0	0	0	3	0	0	4	6	0	0	0	0	7	0	0	0	20	104
4:00 PM	0	0	0	0	0	0	0	6	0	0	3	7	0	0	0	0	8	2	0	0	26	106
4:15 PM	0	0	0	0	0	0	0	2	0	0	8	9	0	0	0	0	9	0	0	0	28	96
4:30 PM	0	0	0	0	0	0	0	3	0	0	4	5	0	0	0	0	6	1	0	0	19	93
4:45 PM	0	0	0	0	0	0	0	9	0	0	4	3	0	0	0	0	7	0	0	0	23	96
5:00 PM	0	0	0	0	0	1	0	3	0	0	3	6	0	0	0	0	7	0	0	0	20	90
5:15 PM	0	0	0	0	0	0	0	2	0	0	5	10	0	0	0	0	7	0	0	0	24	86
5:30 PM	0	0	0	0	0	0	0	6	0	0	3	12	0	0	0	0	13	0	0	0	34	101
5:45 PM	0	0	0	0	0	2	0	1	0	0	0	4	0	0	0	0	8	0	0	0	15	93
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
All Vehicles	0	0	0	0	0	0	0	16	0	0	16	48	0	0	0	0	68	0	4	0	152	
Heavy Trucks	0	0	0			0	0	0			0	4	0			0	0	0			4	
Buses																						
Pedestrians		0					0					0					0				0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0	
Scooters																						
<i>Comments:</i>																						

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Off-Peak) Ramp to Lauderdale County Rd 2 (East) -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889206
DATE: Sun, Aug 28 2022



R* = RTOR

15-Min Count Period Beginning At	(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)					(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*		
6:00 AM	0	0	0	0	0	0	0	1	0	0	1	1	0	0	0	0	6	0	0	0	9	
6:15 AM	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	4	0	0	0	6	
6:30 AM	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0	0	2	
6:45 AM	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	1	0	0	0	3	20
7:00 AM	0	0	0	0	0	0	0	2	0	0	0	2	0	0	0	0	2	0	0	0	6	17
7:15 AM	0	0	0	0	0	0	0	1	0	0	0	6	0	0	0	0	5	0	0	0	12	23
7:30 AM	0	0	0	0	0	0	0	0	0	0	1	4	0	0	0	0	0	0	0	0	5	26
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	0	2	0	0	0	4	27
8:00 AM	0	0	0	0	0	0	0	1	0	0	2	5	0	0	0	0	3	0	0	0	11	32
8:15 AM	0	0	0	0	0	0	0	2	0	0	0	7	0	0	0	0	4	1	0	0	14	34
8:30 AM	0	0	0	0	0	0	0	2	0	0	2	2	0	0	0	0	0	0	0	0	6	35
8:45 AM	0	0	0	0	0	0	0	2	0	0	2	2	0	0	0	0	2	1	0	0	9	40
9:00 AM	0	0	0	0	0	0	0	1	0	0	1	6	0	0	0	0	4	0	0	0	12	41
9:15 AM	0	0	0	0	0	0	0	1	0	0	0	3	0	0	0	0	8	0	0	0	12	39
9:30 AM	0	0	0	0	0	0	0	2	0	0	2	3	0	0	0	0	2	0	0	0	9	42
9:45 AM	0	0	0	0	0	0	0	1	0	0	0	3	0	0	0	0	3	0	0	0	7	40
10:00 AM	0	0	0	0	0	0	0	1	0	0	1	4	0	0	0	0	2	0	0	0	8	36
10:15 AM	0	0	0	0	0	0	0	2	0	0	0	3	0	0	0	0	7	0	0	0	12	36
10:30 AM	0	0	0	0	0	1	0	5	0	0	2	4	0	0	0	0	6	0	0	0	18	45
10:45 AM	0	0	0	0	0	0	0	4	0	0	1	6	0	0	0	0	4	0	0	0	15	53
11:00 AM	0	0	0	0	0	0	0	0	0	0	5	4	0	0	0	0	3	0	0	0	12	57
11:15 AM	0	0	0	0	0	0	0	2	0	0	5	6	0	0	0	0	5	0	0	0	18	63
11:30 AM	0	0	0	0	0	0	0	2	0	0	5	1	0	0	0	0	4	0	0	0	12	57
11:45 AM	0	0	0	0	0	0	0	0	0	0	0	9	0	0	0	0	7	0	0	0	16	58
12:00 PM	0	0	0	0	0	1	0	2	0	0	3	6	0	0	0	0	12	0	0	0	24	70
12:15 PM	0	0	0	0	0	1	0	4	0	0	2	8	0	0	0	0	8	0	0	0	23	75
12:30 PM	0	0	0	0	0	0	0	4	0	0	1	1	0	0	0	0	19	0	0	0	25	88
12:45 PM	0	0	0	0	0	1	0	0	0	0	3	8	0	0	0	0	9	0	0	0	21	93
1:00 PM	0	0	0	0	0	0	0	2	0	0	2	5	0	0	0	0	11	0	0	0	20	89
1:15 PM	0	0	0	0	0	0	0	7	0	0	3	6	0	0	0	0	6	3	0	0	25	91
1:30 PM	0	0	0	0	0	0	0	2	0	0	6	12	0	0	0	0	7	0	0	0	27	93
1:45 PM	0	0	0	0	0	1	0	5	0	0	4	5	0	0	0	0	9	1	0	0	25	97
2:00 PM	0	0	0	0	0	0	0	5	0	0	2	8	0	0	0	0	11	0	0	0	26	103
2:15 PM	0	0	0	0	0	1	0	2	0	0	2	7	0	0	0	0	6	0	0	0	18	96
2:30 PM	0	0	0	0	0	0	0	1	0	0	4	7	0	0	0	0	12	0	0	0	24	93
2:45 PM	0	0	0	0	0	0	0	4	0	0	3	8	0	0	0	0	8	0	0	0	23	91

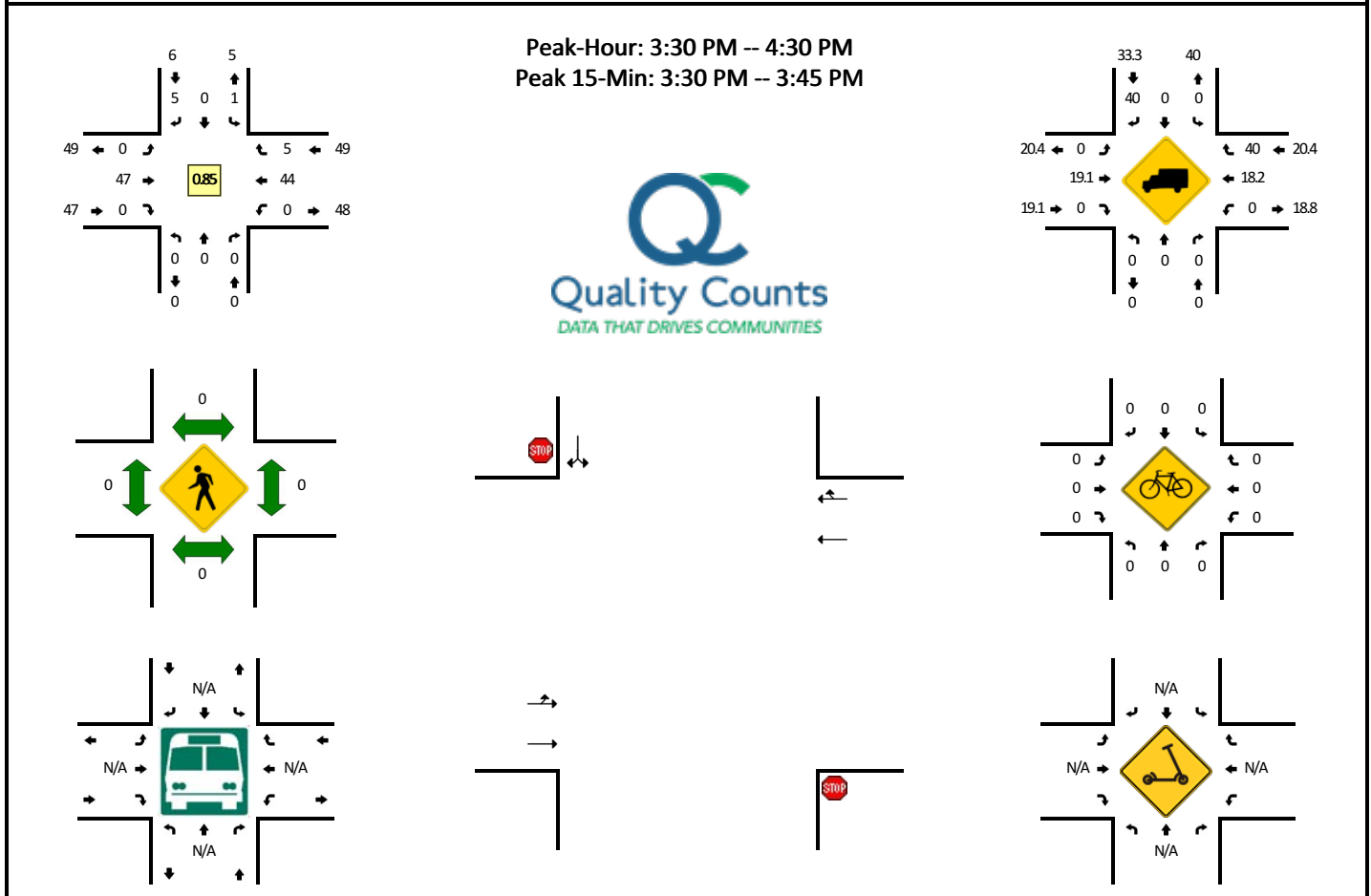
15-Min Count Period Beginning At	(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)					(Off-Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)					Natchez Trace Pkwy (Eastbound)					Natchez Trace Pkwy (Westbound)					Total	Hourly Totals	
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
3:00 PM	0	0	0	0	0	0	0	4	0	0	3	8	0	0	0	0	10	0	0	0	0	25	90
3:15 PM	0	0	0	0	0	0	0	1	0	0	5	2	0	0	0	0	14	1	0	0	0	23	95
3:30 PM	0	0	0	0	0	0	0	3	0	0	3	8	0	0	0	0	8	1	0	0	0	23	94
3:45 PM	0	0	0	0	0	0	0	6	0	0	0	4	0	0	0	0	12	0	0	0	0	22	93
4:00 PM	0	0	0	0	0	0	0	0	0	0	3	6	0	0	0	0	12	1	0	0	0	22	90
4:15 PM	0	0	0	0	0	0	0	2	0	0	1	12	0	0	0	0	11	0	0	0	0	26	93
4:30 PM	0	0	0	0	0	0	0	3	0	0	5	8	0	0	0	0	8	0	0	0	0	24	94
4:45 PM	0	0	0	0	0	0	0	3	0	0	5	6	0	0	0	0	6	0	0	0	0	20	92
5:00 PM	0	0	0	0	0	1	0	1	0	0	2	9	0	0	0	0	10	0	0	0	0	23	93
5:15 PM	0	0	0	0	0	0	0	1	0	0	3	8	0	0	0	0	13	0	0	0	0	25	92
5:30 PM	0	0	0	0	0	2	0	3	0	0	1	6	0	0	0	0	5	0	0	0	0	17	85
5:45 PM	0	0	0	0	0	1	0	5	0	0	4	3	0	0	0	0	13	0	0	0	0	26	91
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total		
	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*	Left	Thru	Right	U	R*			
All Vehicles	0	0	0	0	0	0	0	8	0	0	24	48	0	0	0	0	28	0	0	0	0	108	
Heavy Trucks	0	0	0			0	0	0			0	8	0			0	0	0			8		
Buses																							
Pedestrians		0					0					0					0					0	
Bicycles	0	0	0			0	0	0			0	0	0			0	0	0			0		
Scooters																							
Comments:																							

Report generated on 9/15/2022 9:08 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Colbert Ferry Rd -- Natchez Trace Pkwy
CITY/STATE: Colbert, AL

QC JOB #: 15889213
DATE: Wed, Nov 9 2022



15-Min Count Period Beginning At	(Peak) Colbert Ferry Rd (Northbound)				(Peak) Colbert Ferry Rd (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	1	0	0	4	0	0	0	9	0	0	14	
6:15 AM	0	0	0	0	0	0	0	0	1	7	0	0	0	14	0	0	22	
6:30 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	11	0	0	19	
6:45 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	11	0	0	15	70
7:00 AM	0	0	0	0	0	0	0	0	1	11	0	0	0	7	0	0	19	75
7:15 AM	0	0	0	0	2	0	1	0	0	16	0	0	0	11	0	0	30	83
7:30 AM	0	0	0	0	0	0	1	0	0	4	0	0	0	9	1	0	15	79
7:45 AM	0	0	0	0	0	0	1	0	0	5	0	0	0	2	0	0	8	72
8:00 AM	0	0	0	0	1	0	0	0	1	2	0	0	0	10	1	0	15	68
8:15 AM	0	0	0	0	0	0	0	0	0	6	0	0	0	2	0	0	8	46
8:30 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	3	1	0	9	40
8:45 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	3	0	0	6	38
9:00 AM	0	0	0	0	0	0	2	0	1	4	0	0	0	2	0	0	9	32
9:15 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	3	0	0	5	29
9:30 AM	0	0	0	0	2	0	0	0	1	4	0	0	0	6	2	0	15	35
9:45 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	4	0	1	7	36
10:00 AM	0	0	0	0	0	0	0	0	0	7	0	0	0	3	0	1	11	38
10:15 AM	0	0	0	0	2	0	0	0	1	2	0	0	0	3	1	0	9	42
10:30 AM	0	0	0	0	0	0	2	0	1	10	0	0	0	4	1	0	18	45
10:45 AM	0	0	0	0	0	0	0	0	1	11	0	0	0	9	0	0	21	59
11:00 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	4	1	0	10	58
11:15 AM	0	0	0	0	1	0	1	0	2	3	0	0	0	6	0	0	13	62
11:30 AM	0	0	0	0	1	0	0	0	0	5	0	0	0	5	0	0	11	55
11:45 AM	0	0	0	0	1	0	1	0	1	7	0	0	0	9	0	0	19	53
12:00 PM	0	0	0	0	2	0	0	0	0	6	0	0	0	4	1	0	13	56
12:15 PM	0	0	0	0	1	0	0	0	1	5	0	0	0	8	0	0	15	58
12:30 PM	0	0	0	0	1	0	0	0	0	8	0	0	0	5	2	0	16	63
12:45 PM	0	0	0	0	0	0	1	0	0	7	0	0	0	7	0	0	15	59
1:00 PM	0	0	0	0	0	0	0	0	0	7	0	0	0	5	0	0	12	58
1:15 PM	0	0	0	0	0	0	0	0	1	6	0	0	0	7	1	0	15	58
1:30 PM	0	0	0	0	1	0	0	0	1	5	0	0	0	4	0	0	11	53
1:45 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	5	0	0	14	52
2:00 PM	0	0	0	0	1	0	2	0	0	10	0	0	0	10	1	0	24	64
2:15 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	10	0	0	15	64
2:30 PM	0	0	0	0	0	0	0	0	1	14	0	0	0	6	1	0	22	75
2:45 PM	0	0	0	0	2	0	1	0	0	9	0	0	0	6	1	0	19	80
3:00 PM	0	0	0	0	0	0	0	0	0	14	0	0	0	5	0	0	19	75
3:15 PM	0	0	0	0	0	0	0	0	0	12	0	0	0	7	1	0	20	80
3:30 PM	0	0	0	0	0	0	1	0	0	15	0	0	0	12	2	0	30	88

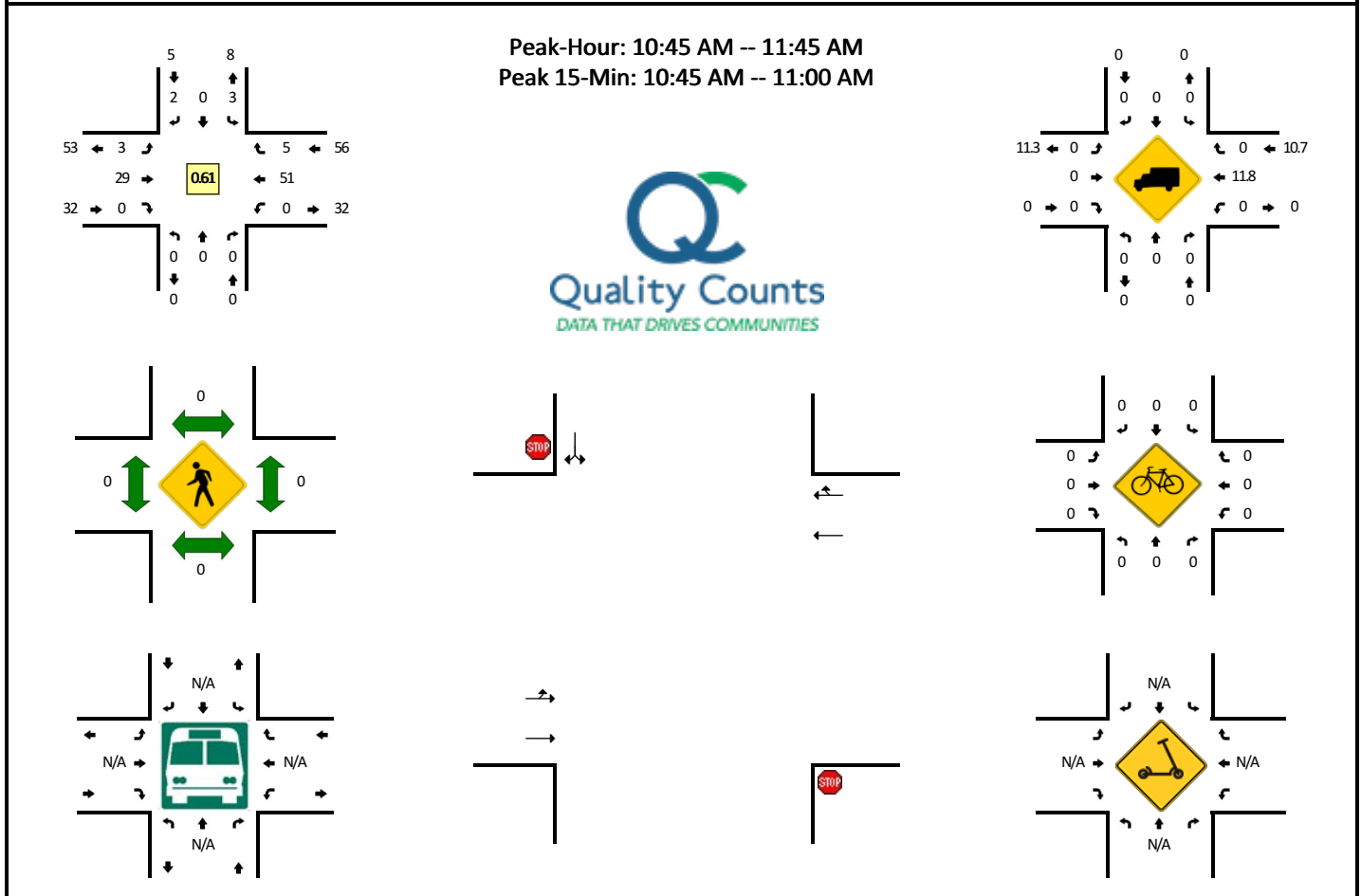
15-Min Count Period Beginning At	(Peak) Colbert Ferry Rd (Northbound)				(Peak) Colbert Ferry Rd (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:45 PM	0	0	0	0	0	0	1	0	0	12	0	0	0	10	3	0	26	95
4:00 PM	0	0	0	0	1	0	1	0	0	11	0	0	0	9	0	0	22	98
4:15 PM	0	0	0	0	0	0	2	0	0	9	0	0	0	13	0	0	24	102
4:30 PM	0	0	0	0	0	0	1	0	0	18	0	0	0	10	1	0	30	102
4:45 PM	0	0	0	0	0	0	0	0	0	15	0	0	0	5	0	0	20	96
5:00 PM	0	0	0	0	0	0	0	0	0	12	0	0	0	3	0	0	15	89
5:15 PM	0	0	0	0	2	0	0	0	1	11	0	0	0	10	0	1	25	90
5:30 PM	0	0	0	0	1	0	0	0	1	11	0	0	0	5	0	0	18	78
5:45 PM	0	0	0	0	0	0	0	0	0	2	0	0	0	9	0	0	11	69
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	4	0	0	60	0	0	0	48	8	0	120	
Heavy Trucks	0	0	0		0	0	0		0	4	0		0	16	8		28	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Colbert Ferry Rd -- Natchez Trace Pkwy
CITY/STATE: Colbert, AL

QC JOB #: 15889214
DATE: Sat, Nov 12 2022



15-Min Count Period Beginning At	(Peak) Colbert Ferry Rd (Northbound)				(Peak) Colbert Ferry Rd (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	4	0	0	7	
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	
6:30 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	2	0	0	4	
6:45 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	3	18
7:00 AM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	12
7:15 AM	0	0	0	0	1	0	0	0	1	12	0	0	0	5	0	0	19	27
7:30 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	3	0	0	6	29
7:45 AM	0	0	0	0	0	0	1	0	0	3	0	0	0	2	0	0	6	32
8:00 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	3	0	0	8	39
8:15 AM	0	0	0	0	1	0	0	0	1	3	0	0	0	4	1	0	10	30
8:30 AM	0	0	0	0	0	0	1	0	0	4	0	0	0	4	1	0	10	34
8:45 AM	0	0	0	0	0	0	1	0	0	4	0	0	0	6	1	0	12	40
9:00 AM	0	0	0	0	1	0	0	0	1	2	0	0	0	3	0	0	7	39
9:15 AM	0	0	0	0	1	0	0	0	1	1	0	0	0	2	0	0	5	34
9:30 AM	0	0	0	0	1	0	0	0	0	1	0	0	0	2	0	0	4	28
9:45 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	7	1	0	16	32
10:00 AM	0	0	0	0	0	0	1	0	0	8	0	0	0	5	1	0	15	40
10:15 AM	0	0	0	0	1	0	0	0	2	9	0	0	0	3	0	0	15	50
10:30 AM	0	0	0	0	2	0	0	0	0	6	0	0	0	2	0	0	10	56
10:45 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	32	1	0	38	78
11:00 AM	0	0	0	0	0	0	0	0	1	7	0	0	0	7	0	0	15	78
11:15 AM	0	0	0	0	2	0	1	0	1	10	0	0	0	6	2	0	22	85
11:30 AM	0	0	0	0	1	0	1	0	1	7	0	0	0	6	2	0	18	93
11:45 AM	0	0	0	0	2	0	1	0	2	6	0	0	0	3	2	0	16	71
12:00 PM	0	0	0	0	1	0	1	0	0	10	0	0	0	6	1	0	19	75
12:15 PM	0	0	0	0	0	0	1	0	0	4	0	0	0	6	0	0	11	64
12:30 PM	0	0	0	0	1	0	0	0	0	4	0	0	0	5	0	0	10	56
12:45 PM	0	0	0	0	0	0	0	0	6	5	0	0	0	6	0	0	17	57
1:00 PM	0	0	0	0	6	0	2	0	0	4	0	0	0	6	2	0	20	58
1:15 PM	0	0	0	0	1	0	0	0	1	2	0	0	0	11	1	0	16	63
1:30 PM	0	0	0	0	1	0	1	0	0	5	0	0	0	6	0	0	13	66
1:45 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	5	0	0	10	59
2:00 PM	0	0	0	0	1	0	1	0	2	1	0	0	0	5	1	0	11	50
2:15 PM	0	0	0	0	1	0	0	0	0	3	0	0	0	9	1	1	15	49
2:30 PM	0	0	0	0	1	0	0	0	1	6	0	0	0	7	0	0	15	51
2:45 PM	0	0	0	0	1	0	1	0	1	8	0	0	0	4	1	0	16	57
3:00 PM	0	0	0	0	1	0	1	0	0	9	0	0	0	5	1	0	17	63
3:15 PM	0	0	0	0	0	0	0	0	0	12	0	0	0	9	0	0	21	69

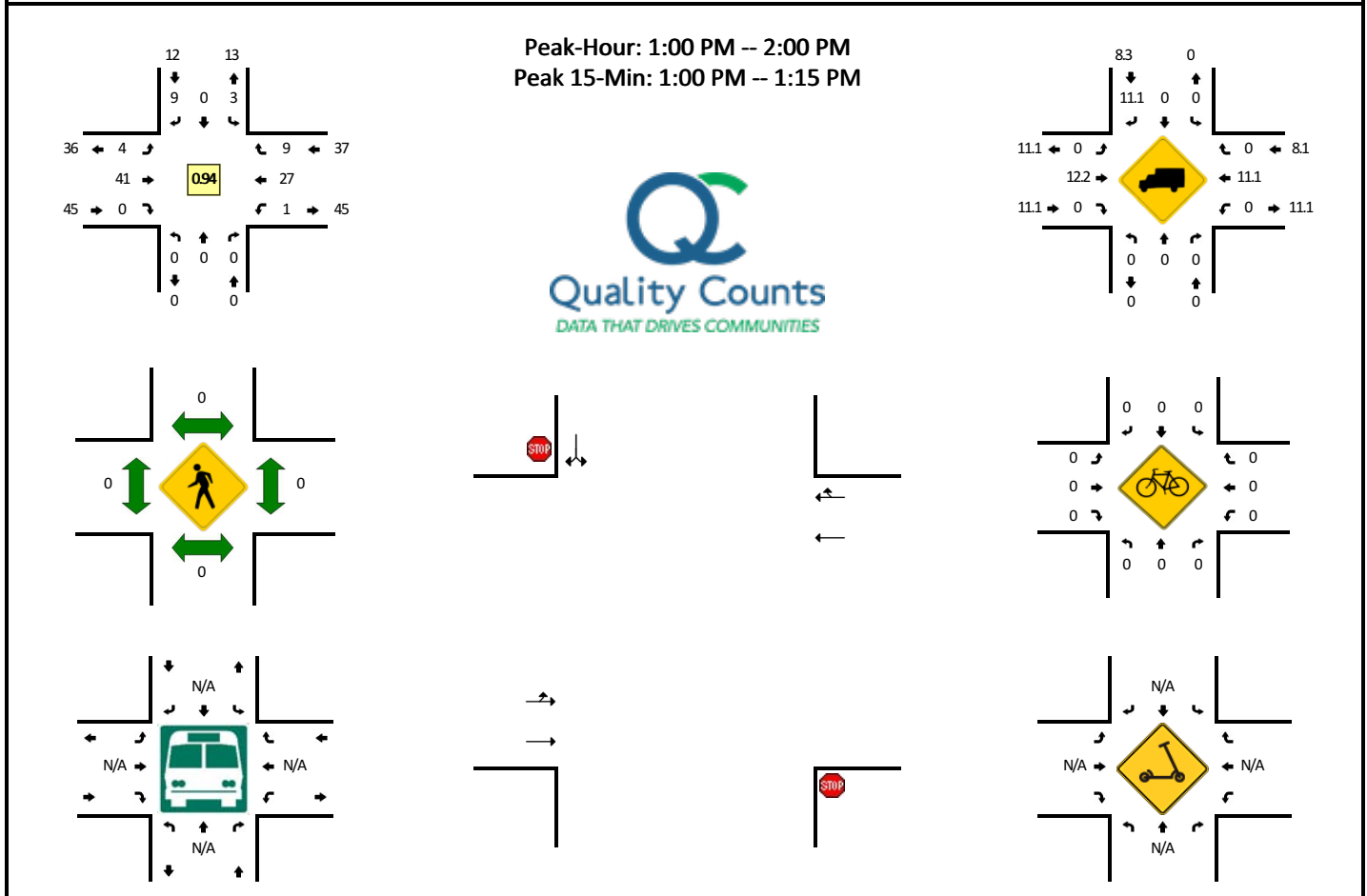
15-Min Count Period Beginning At	(Peak) Colbert Ferry Rd (Northbound)				(Peak) Colbert Ferry Rd (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:30 PM	0	0	0	0	0	0	2	0	0	5	0	0	0	6	3	0	16	70
3:45 PM	0	0	0	0	1	0	0	0	0	5	0	0	0	5	0	0	11	65
4:00 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	6	0	0	12	60
4:15 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	7	0	0	11	50
4:30 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	7	0	0	16	50
4:45 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	6	0	0	15	54
5:00 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	3	0	0	12	54
5:15 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	0	1	0	6	49
5:30 PM	0	0	0	0	0	0	1	0	0	7	0	0	0	6	0	0	14	47
5:45 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	6	0	0	10	42
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	0	0	0	20	0	0	0	128	4	0	152	
Heavy Trucks	0	0	0		0	0	0		0	0	0		0	12	0		12	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Colbert Ferry Rd -- Natchez Trace Pkwy
CITY/STATE: Colbert, AL

QC JOB #: 15889215
DATE: Sun, Nov 13 2022



15-Min Count Period Beginning At	(Peak) Colbert Ferry Rd (Northbound)				(Peak) Colbert Ferry Rd (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	4	0	0	6	
6:15 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	2	0	0	4	
6:30 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	0	2	
6:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12
7:00 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	2	8
7:15 AM	0	0	0	0	1	0	0	0	0	6	0	0	0	1	0	0	8	12
7:30 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	5	0	0	10	20
7:45 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	3	0	0	5	25
8:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	2	0	0	5	28
8:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	2	1	0	4	24
8:30 AM	0	0	0	0	0	0	1	0	0	5	0	0	0	3	0	0	9	23
8:45 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	2	0	0	3	21
9:00 AM	0	0	0	0	0	0	1	0	1	3	0	0	0	2	0	0	7	23
9:15 AM	0	0	0	0	0	0	0	0	1	2	0	0	0	5	0	0	8	27
9:30 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	2	0	0	5	23
9:45 AM	0	0	0	0	2	0	0	0	1	7	0	0	0	3	0	0	13	33
10:00 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	4	0	0	8	34
10:15 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	1	0	0	6	32
10:30 AM	0	0	0	0	0	0	0	0	0	7	0	0	0	2	1	0	10	37
10:45 AM	0	0	0	0	2	0	0	0	2	7	0	0	0	0	2	0	13	37
11:00 AM	0	0	0	0	2	0	2	0	0	4	0	0	0	5	1	0	14	43
11:15 AM	0	0	0	0	0	0	0	0	0	7	0	0	0	4	1	0	12	49
11:30 AM	0	0	0	0	0	0	0	0	1	11	0	0	0	8	3	0	23	62
11:45 AM	0	0	0	0	1	0	2	0	0	4	0	0	0	10	1	0	18	67
12:00 PM	0	0	0	0	2	0	1	0	1	7	0	0	0	2	3	0	16	69
12:15 PM	0	0	0	0	1	0	3	0	0	7	0	0	0	8	1	0	20	77
12:30 PM	0	0	0	0	2	0	0	0	1	6	0	0	0	6	0	0	15	69
12:45 PM	0	0	0	0	0	0	0	0	1	10	0	0	0	10	1	0	22	73
1:00 PM	0	0	0	0	0	0	2	0	1	15	0	0	0	6	1	0	25	82
1:15 PM	0	0	0	0	0	0	3	0	1	9	0	0	0	6	3	1	23	85
1:30 PM	0	0	0	0	2	0	1	0	2	6	0	0	0	7	3	0	21	91
1:45 PM	0	0	0	0	1	0	3	0	0	11	0	0	0	8	2	0	25	94
2:00 PM	0	0	0	0	2	0	3	0	0	6	0	0	0	3	3	0	17	86
2:15 PM	0	0	0	0	0	0	3	0	1	6	0	0	0	4	3	0	17	80
2:30 PM	0	0	0	0	1	0	0	0	1	10	0	0	0	7	0	0	19	78
2:45 PM	0	0	0	0	3	0	4	0	3	5	0	0	0	11	2	0	28	81
3:00 PM	0	0	0	0	1	0	0	0	0	9	0	0	0	5	0	0	15	79
3:15 PM	0	0	0	0	0	0	0	0	0	8	0	0	0	7	8	0	23	85

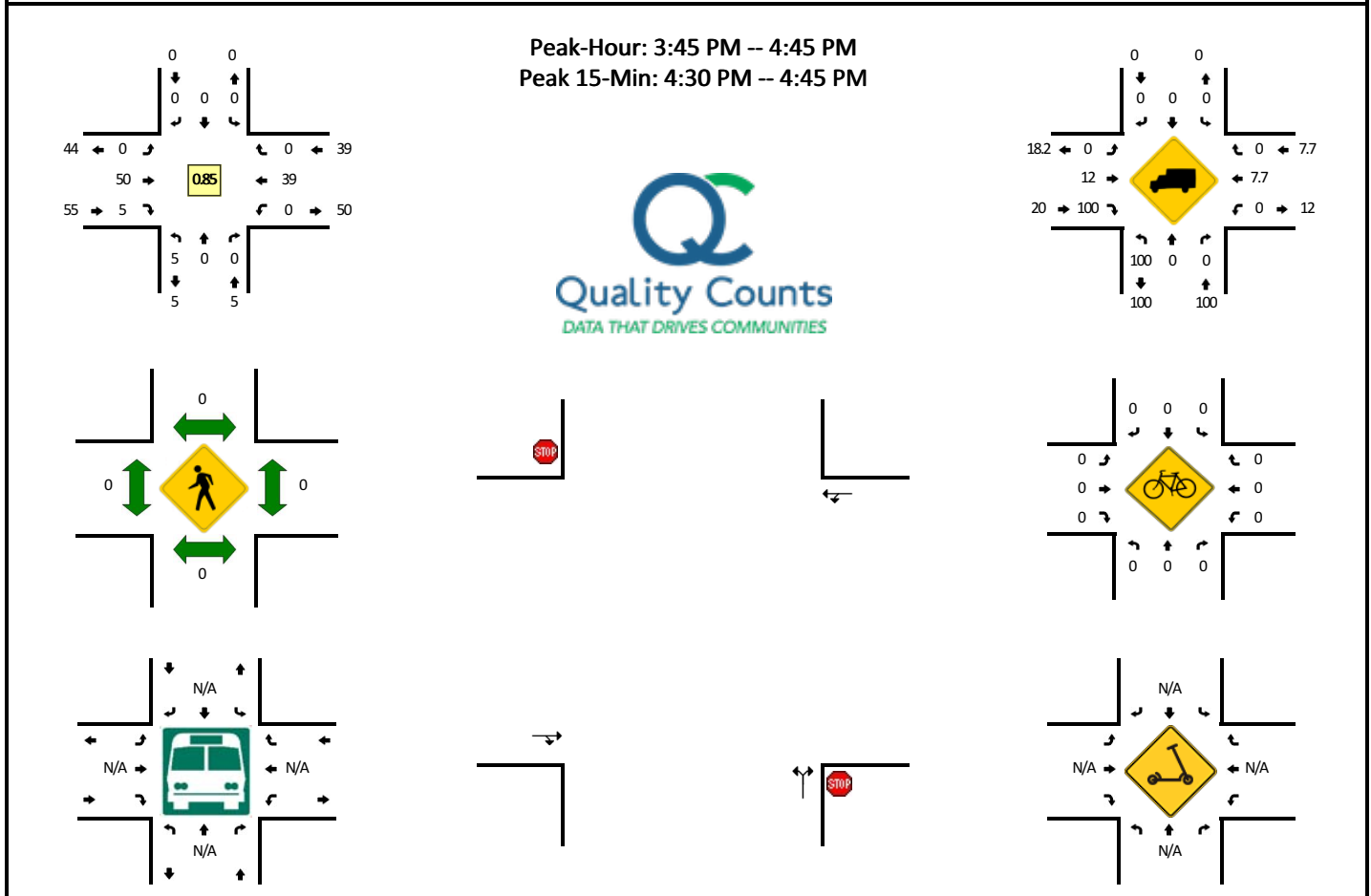
15-Min Count Period Beginning At	(Peak) Colbert Ferry Rd (Northbound)				(Peak) Colbert Ferry Rd (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:30 PM	0	0	0	0	1	0	0	0	0	9	0	0	0	6	4	0	20	86
3:45 PM	0	0	0	0	0	0	3	0	0	11	0	0	0	2	0	0	16	74
4:00 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	14	1	0	24	83
4:15 PM	0	0	0	0	4	0	1	0	1	9	0	0	0	5	2	1	23	83
4:30 PM	0	0	0	0	4	0	0	0	0	5	0	0	0	5	1	0	15	78
4:45 PM	0	0	0	0	2	0	0	0	1	4	0	0	0	5	0	0	12	74
5:00 PM	0	0	0	0	0	0	1	0	0	3	0	1	0	5	0	0	10	60
5:15 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	5	1	0	10	47
5:30 PM	0	0	0	0	1	0	1	0	0	7	0	0	0	5	1	0	15	47
5:45 PM	0	0	0	0	0	0	2	0	1	3	0	0	0	9	2	0	17	52
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	8	0	4	60	0	0	0	24	4	0	100	
Heavy Trucks	0	0	0		0	0	4		0	8	0		0	0	0		12	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Parking Area -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889222
DATE: Wed, Nov 9 2022



15-Min Count Period Beginning At	(Peak) Parking Area (Northbound)				(Peak) Parking Area (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	9	0	0	13	
6:15 AM	0	0	0	0	0	0	0	0	0	6	0	0	0	14	0	0	20	
6:30 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	13	0	0	21	
6:45 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	12	0	0	17	71
7:00 AM	0	0	1	0	0	0	0	0	0	7	2	0	0	8	0	0	18	76
7:15 AM	0	0	0	0	0	0	0	0	0	19	0	0	0	9	0	0	28	84
7:30 AM	0	0	1	0	0	0	0	0	0	7	0	0	0	10	0	0	18	81
7:45 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	4	0	0	9	73
8:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	11	0	0	14	69
8:15 AM	0	0	0	0	0	0	0	0	0	6	0	0	0	2	0	0	8	49
8:30 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	4	0	0	8	39
8:45 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	4	0	0	9	39
9:00 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	1	0	0	4	29
9:15 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	3	0	0	10	31
9:30 AM	0	0	1	0	0	0	0	0	0	7	0	0	0	1	8	0	18	41
9:45 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	3	5	0	12	44
10:00 AM	1	0	0	0	0	0	0	0	0	7	0	0	0	0	3	0	11	51
10:15 AM	2	0	0	0	0	0	0	0	0	3	0	0	0	0	3	0	8	49
10:30 AM	0	0	0	0	0	0	0	0	0	7	1	0	0	0	7	0	15	46
10:45 AM	1	0	0	0	0	0	0	0	0	9	1	1	0	0	5	0	17	51
11:00 AM	3	0	0	0	0	0	0	0	0	4	0	2	0	0	5	0	14	54
11:15 AM	0	0	0	0	0	0	0	0	0	6	0	0	0	0	5	0	11	57
11:30 AM	1	0	2	0	0	0	0	0	0	5	0	0	0	1	6	0	15	57
11:45 AM	0	0	1	0	0	0	0	0	0	5	0	0	0	2	8	0	16	56
12:00 PM	2	0	0	0	0	0	0	0	0	7	2	0	0	0	5	0	16	58
12:15 PM	3	0	0	0	0	0	0	0	0	4	2	0	0	1	8	0	18	65
12:30 PM	1	0	0	0	0	0	0	0	0	8	2	0	0	0	4	0	15	65
12:45 PM	2	0	0	0	0	0	0	0	0	7	1	0	0	0	5	0	15	64
1:00 PM	0	0	0	0	0	0	0	0	0	5	1	0	0	0	6	0	12	60
1:15 PM	2	0	0	0	0	0	0	0	0	7	0	0	0	0	4	0	13	55
1:30 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	0	5	0	12	52
1:45 PM	1	0	0	0	0	0	0	0	0	7	1	0	0	0	3	0	12	49
2:00 PM	1	0	0	0	0	0	0	0	0	7	1	0	0	0	10	0	19	56
2:15 PM	0	0	0	0	0	0	0	0	0	6	2	0	0	0	10	0	18	61
2:30 PM	1	0	0	0	0	0	0	0	0	9	0	0	0	0	6	0	16	65
2:45 PM	2	0	0	0	0	0	0	0	0	15	2	0	0	0	6	0	25	78
3:00 PM	0	0	0	0	0	0	0	0	0	7	1	0	0	0	9	0	17	76
3:15 PM	0	0	0	0	0	0	0	0	0	12	2	0	0	0	6	0	20	78
3:30 PM	3	0	0	0	0	0	0	0	0	13	1	0	0	0	10	0	28	90

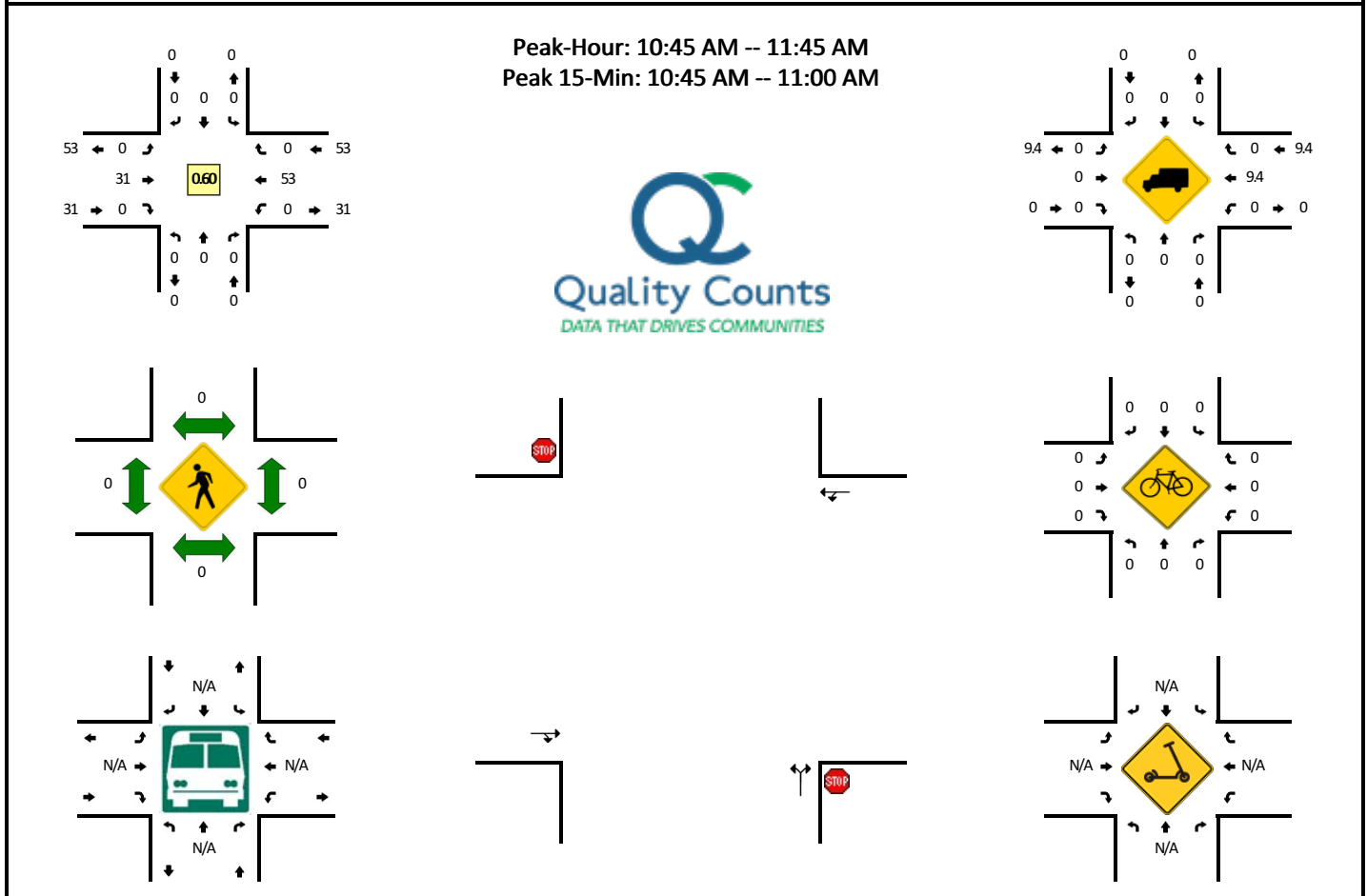
15-Min Count Period Beginning At	(Peak) Parking Area (Northbound)				(Peak) Parking Area (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:45 PM	1	0	0	0	0	0	0	0	0	9	1	0	0	10	0	0	21	86
4:00 PM	1	0	0	0	0	0	0	0	0	13	1	0	0	7	0	0	22	91
4:15 PM	1	0	0	0	0	0	0	0	0	10	1	0	0	15	0	0	27	98
4:30 PM	2	0	0	0	0	0	0	0	0	18	2	0	0	7	0	0	29	99
4:45 PM	0	0	1	0	0	0	0	0	0	12	0	0	0	4	0	0	17	95
5:00 PM	0	0	0	0	0	0	0	0	0	13	0	1	0	4	0	0	18	91
5:15 PM	0	0	0	0	0	0	0	0	0	13	0	0	0	9	0	0	22	86
5:30 PM	0	0	0	0	0	0	0	0	0	14	0	0	0	7	0	0	21	78
5:45 PM	1	0	1	0	0	0	0	0	0	3	0	0	0	10	0	0	15	76
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	8	0	0	0	0	0	0	0	0	72	8	0	0	28	0	0	116	
Heavy Trucks	8	0	0		0	0	0		0	4	8		0	0	0		20	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
Comments:																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Parking Area -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889223
DATE: Sat, Nov 12 2022



15-Min Count Period Beginning At	(Peak) Parking Area (Northbound)				(Peak) Parking Area (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	4	0	0	7	
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	4	0	0	4	
6:30 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	3	0	0	5	
6:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	17
7:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	2	12
7:15 AM	0	0	0	0	0	0	0	0	0	13	0	0	0	4	0	0	17	25
7:30 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	3	0	0	6	26
7:45 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	2	0	0	5	30
8:00 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	3	0	0	8	36
8:15 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	5	0	0	8	27
8:30 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	5	0	0	10	31
8:45 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	8	0	0	12	38
9:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	2	0	0	5	35
9:15 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	2	0	0	4	31
9:30 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	2	0	0	4	25
9:45 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	8	0	0	16	29
10:00 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	7	0	0	15	39
10:15 AM	0	0	0	0	0	0	0	0	0	9	0	0	0	2	0	0	11	46
10:30 AM	0	0	0	0	0	0	0	0	0	9	0	0	0	5	0	0	14	56
10:45 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	30	0	0	35	75
11:00 AM	0	0	0	0	0	0	0	0	0	7	0	0	0	9	0	0	16	76
11:15 AM	0	0	0	0	0	0	0	0	0	11	0	0	0	7	0	0	18	83
11:30 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	7	0	0	15	84
11:45 AM	0	0	0	0	0	0	0	0	0	9	0	0	0	6	0	0	15	64
12:00 PM	0	0	0	0	0	0	0	0	0	11	0	0	0	6	0	0	17	65
12:15 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	6	0	0	11	58
12:30 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	6	0	0	10	53
12:45 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	6	0	0	12	50
1:00 PM	0	0	0	0	0	0	0	0	0	10	0	0	0	10	0	0	20	53
1:15 PM	0	0	0	0	0	0	0	0	0	3	0	0	0	10	0	0	13	55
1:30 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	6	0	0	12	57
1:45 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	6	0	2	12	57
2:00 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	5	0	1	10	47
2:15 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	14	0	0	19	53
2:30 PM	0	0	0	0	0	0	0	0	0	8	0	0	0	6	0	0	14	55
2:45 PM	0	0	0	0	0	0	0	0	0	7	0	0	0	7	0	0	14	57
3:00 PM	0	0	0	0	0	0	0	0	0	11	0	0	0	3	0	0	14	61
3:15 PM	0	0	0	0	0	0	0	0	0	10	0	0	0	14	0	0	24	66

15-Min Count Period Beginning At	(Peak) Parking Area (Northbound)				(Peak) Parking Area (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:30 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	5	0	0	14	66
3:45 PM	0	0	0	0	0	0	0	0	0	7	0	0	0	6	0	0	13	65
4:00 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	5	0	1	12	63
4:15 PM	0	0	0	0	0	0	0	0	0	3	0	0	0	7	0	0	10	49
4:30 PM	0	0	0	0	0	0	0	0	0	10	0	0	0	7	0	0	17	52
4:45 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	6	0	0	15	54
5:00 PM	0	0	0	0	0	0	0	0	0	11	0	0	0	3	0	0	14	56
5:15 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	1	0	0	6	52
5:30 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	7	0	0	13	48
5:45 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	6	0	0	12	45
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	0	0	0	20	0	0	0	120	0	0	140	
Heavy Trucks	0	0	0		0	0	0		0	0	0		0	8	0		8	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		

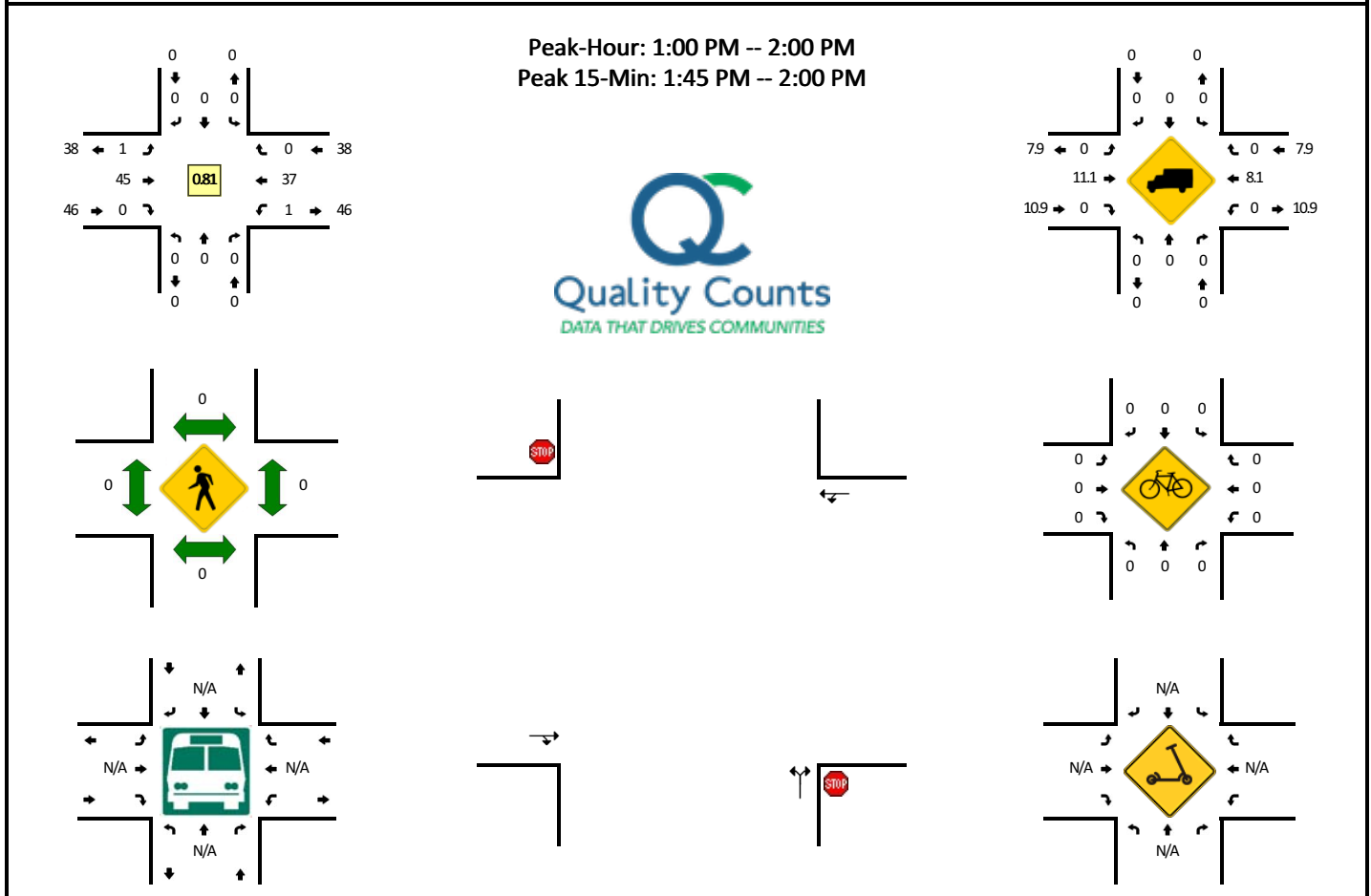
Comments:

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Parking Area -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889224
DATE: Sun, Nov 13 2022



15-Min Count Period Beginning At	(Peak) Parking Area (Northbound)				(Peak) Parking Area (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	4	0	0	5	
6:15 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	2	0	0	4	
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
6:45 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	3	12
7:00 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	2	9
7:15 AM	0	0	0	0	0	0	0	0	0	10	0	0	0	2	0	0	12	17
7:30 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	6	0	0	11	28
7:45 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	3	0	0	6	31
8:00 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	0	0	0	3	32
8:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	3	0	0	4	24
8:30 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	3	0	0	7	20
8:45 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	3	0	0	5	19
9:00 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	1	0	0	5	21
9:15 AM	0	0	0	0	0	0	0	0	0	2	0	0	0	6	0	0	8	25
9:30 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	3	0	0	7	25
9:45 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	3	0	0	11	31
10:00 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	4	0	1	9	35
10:15 AM	0	0	0	0	0	0	0	0	0	5	0	0	0	2	0	0	7	34
10:30 AM	0	0	0	0	0	0	0	0	0	8	0	0	0	2	0	0	10	37
10:45 AM	0	0	0	0	0	0	0	0	0	10	0	0	0	4	0	0	14	40
11:00 AM	0	0	0	0	0	0	0	0	0	7	0	0	0	6	0	2	15	46
11:15 AM	0	0	0	0	0	0	0	0	0	7	0	0	0	7	0	0	14	53
11:30 AM	0	0	0	0	0	0	0	0	0	11	0	0	0	12	0	0	23	66
11:45 AM	0	0	0	0	0	0	0	0	0	4	0	0	0	9	0	0	13	65
12:00 PM	0	0	0	0	0	0	0	0	0	10	0	0	0	5	0	0	15	65
12:15 PM	0	0	0	0	0	0	0	0	0	7	0	0	0	8	0	0	15	66
12:30 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	11	0	0	20	63
12:45 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	7	0	0	16	66
1:00 PM	0	0	0	0	0	0	0	0	0	12	0	0	0	8	0	0	20	71
1:15 PM	0	0	0	0	0	0	0	0	0	11	0	1	0	7	0	0	19	75
1:30 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	10	0	0	19	74
1:45 PM	0	0	0	0	0	0	0	0	0	13	0	0	0	12	0	1	26	84
2:00 PM	0	0	0	0	0	0	0	0	0	8	0	0	0	6	0	0	14	78
2:15 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	5	0	0	11	70
2:30 PM	0	0	0	0	0	0	0	0	0	11	0	0	1	10	0	0	22	73
2:45 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	11	0	0	17	64
3:00 PM	0	0	0	0	0	0	0	0	0	12	0	0	0	5	0	0	17	67
3:15 PM	0	0	0	0	0	0	0	0	0	7	0	0	0	14	0	0	21	77

15-Min Count Period Beginning At	(Peak) Parking Area (Northbound)				(Peak) Parking Area (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:30 PM	0	0	0	0	0	0	0	0	0	9	1	0	0	10	0	0	20	75
3:45 PM	0	0	1	0	0	0	0	0	0	10	0	0	0	2	0	0	13	71
4:00 PM	0	0	0	0	0	0	0	0	0	9	0	0	0	17	0	0	26	80
4:15 PM	0	0	0	0	0	0	0	0	0	15	0	1	0	6	0	0	22	81
4:30 PM	0	0	0	0	0	0	0	0	0	10	0	0	0	6	0	0	16	77
4:45 PM	0	0	0	0	0	0	0	0	0	3	0	0	0	6	0	0	9	73
5:00 PM	0	0	0	0	0	0	0	0	0	6	0	0	0	4	0	0	10	57
5:15 PM	0	0	0	0	0	0	0	0	0	4	0	0	0	7	0	0	11	46
5:30 PM	0	0	0	0	0	0	0	0	0	8	0	0	0	8	0	0	16	46
5:45 PM	0	0	0	0	0	0	0	0	0	3	0	0	0	9	0	0	12	49
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	0	0	0	52	0	0	0	48	0	4	104	
Heavy Trucks	0	0	0		0	0	0		0	4	0		0	8	0		12	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		

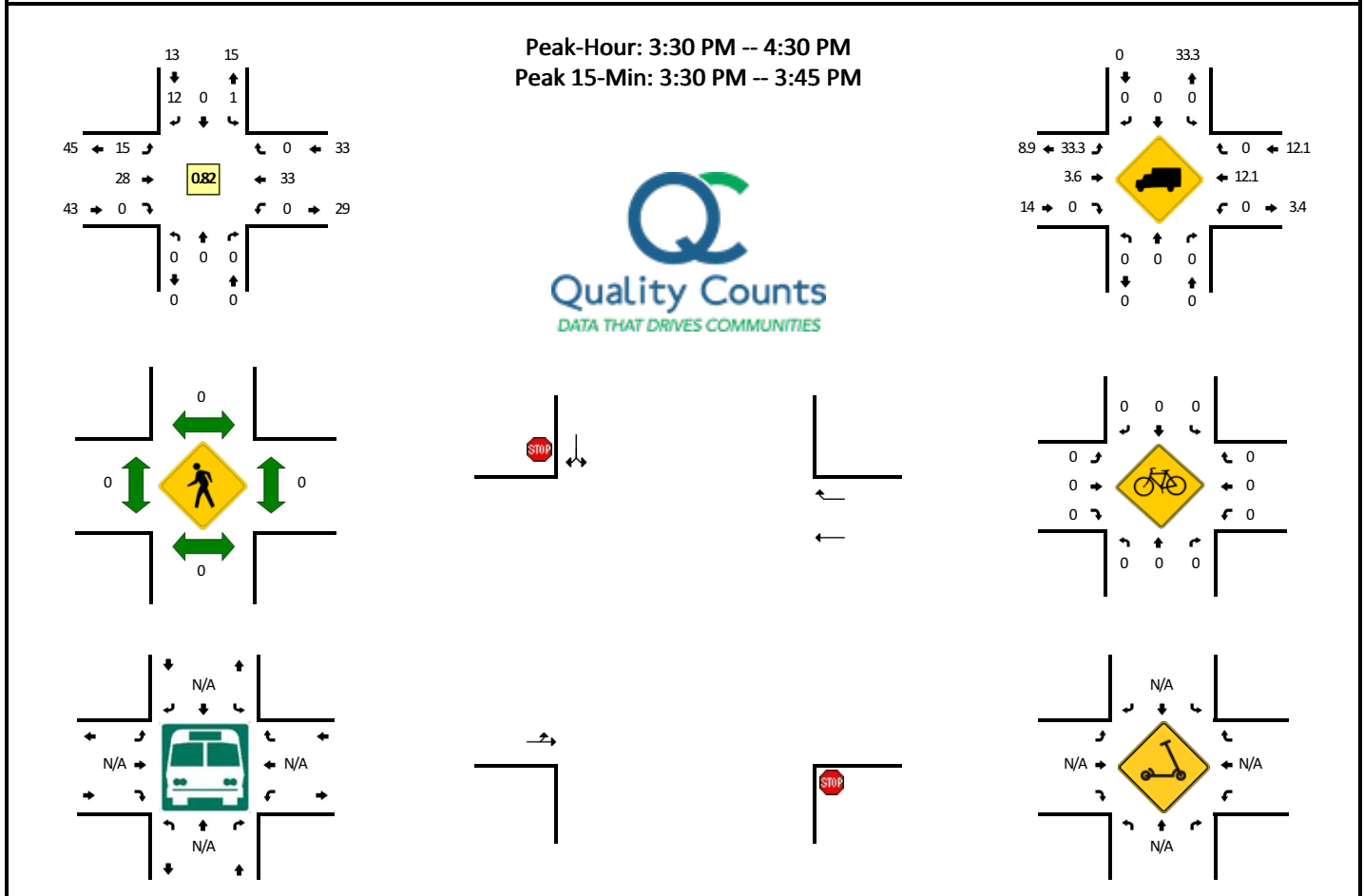
Comments:

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Ramp to Lauderdale County Rd 2 (East) -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889216
DATE: Wed, Nov 9 2022



15-Min Count Period Beginning At	(Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)				(Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	2	0	0	4	0	0	0	7	0	0	13	
6:15 AM	0	0	0	0	0	0	2	0	4	3	0	0	0	13	0	0	22	
6:30 AM	0	0	0	0	0	0	4	0	3	5	0	0	0	8	0	0	20	
6:45 AM	0	0	0	0	3	0	3	0	1	3	0	0	0	9	0	0	19	74
7:00 AM	0	0	0	0	0	0	3	0	2	8	0	0	0	6	0	0	19	80
7:15 AM	0	0	0	0	0	0	1	0	2	16	0	0	0	6	0	0	25	83
7:30 AM	0	0	0	0	0	0	1	0	2	5	0	0	0	9	0	1	18	81
7:45 AM	0	0	0	0	0	0	4	0	1	3	0	0	0	3	0	0	11	73
8:00 AM	0	0	0	0	0	0	2	0	1	2	0	0	0	6	0	0	11	65
8:15 AM	0	0	0	0	0	0	0	0	0	6	0	0	0	2	0	1	9	49
8:30 AM	0	0	0	0	1	0	2	0	2	4	0	0	0	2	1	0	12	43
8:45 AM	0	0	0	0	0	0	1	0	0	3	0	0	0	4	0	0	8	40
9:00 AM	0	0	0	0	0	0	1	0	2	3	0	0	0	2	0	0	8	37
9:15 AM	0	0	0	0	0	0	3	0	1	1	0	0	0	5	0	0	10	38
9:30 AM	0	0	0	0	0	0	1	0	2	5	0	0	0	8	0	0	16	42
9:45 AM	0	0	0	0	0	0	0	0	1	1	0	1	0	9	0	0	12	46
10:00 AM	0	0	0	0	0	0	1	0	3	4	0	0	0	1	1	0	10	48
10:15 AM	0	0	0	0	0	0	1	0	1	2	0	0	0	3	0	0	7	45
10:30 AM	0	0	0	0	0	0	0	0	3	4	0	0	0	6	0	0	13	42
10:45 AM	0	0	0	0	2	0	0	0	2	7	0	0	0	4	0	0	15	45
11:00 AM	0	0	0	0	0	0	2	0	2	2	0	0	0	4	1	0	11	46
11:15 AM	0	0	0	0	1	0	1	0	4	2	0	0	0	5	0	0	13	52
11:30 AM	0	0	0	0	0	0	2	0	2	6	0	0	0	4	0	0	14	53
11:45 AM	0	0	0	0	0	0	5	0	4	4	0	0	0	3	0	0	16	54
12:00 PM	0	0	0	0	1	0	3	0	4	3	0	0	0	3	1	0	15	58
12:15 PM	0	0	0	0	0	0	3	0	1	3	0	0	0	5	0	0	12	57
12:30 PM	0	0	0	0	0	0	1	0	1	5	0	0	0	4	1	0	12	55
12:45 PM	0	0	0	0	0	0	1	0	3	4	0	0	0	4	1	0	13	52
1:00 PM	0	0	0	0	0	0	3	0	0	5	0	0	0	4	0	0	12	49
1:15 PM	0	0	0	0	0	0	0	0	1	5	0	0	0	5	0	0	11	48
1:30 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	4	1	0	10	46
1:45 PM	0	0	0	0	0	0	2	0	1	6	0	1	0	4	0	0	14	47
2:00 PM	0	0	0	0	0	0	2	0	2	5	0	1	0	9	0	0	19	54
2:15 PM	0	0	0	0	0	0	1	0	3	4	0	0	0	6	0	0	14	57
2:30 PM	0	0	0	0	0	0	1	0	1	7	0	0	0	4	0	0	13	60
2:45 PM	0	0	0	0	0	0	0	0	3	12	0	0	0	3	0	0	18	64
3:00 PM	0	0	0	0	0	0	2	0	3	7	0	0	0	7	0	0	19	64

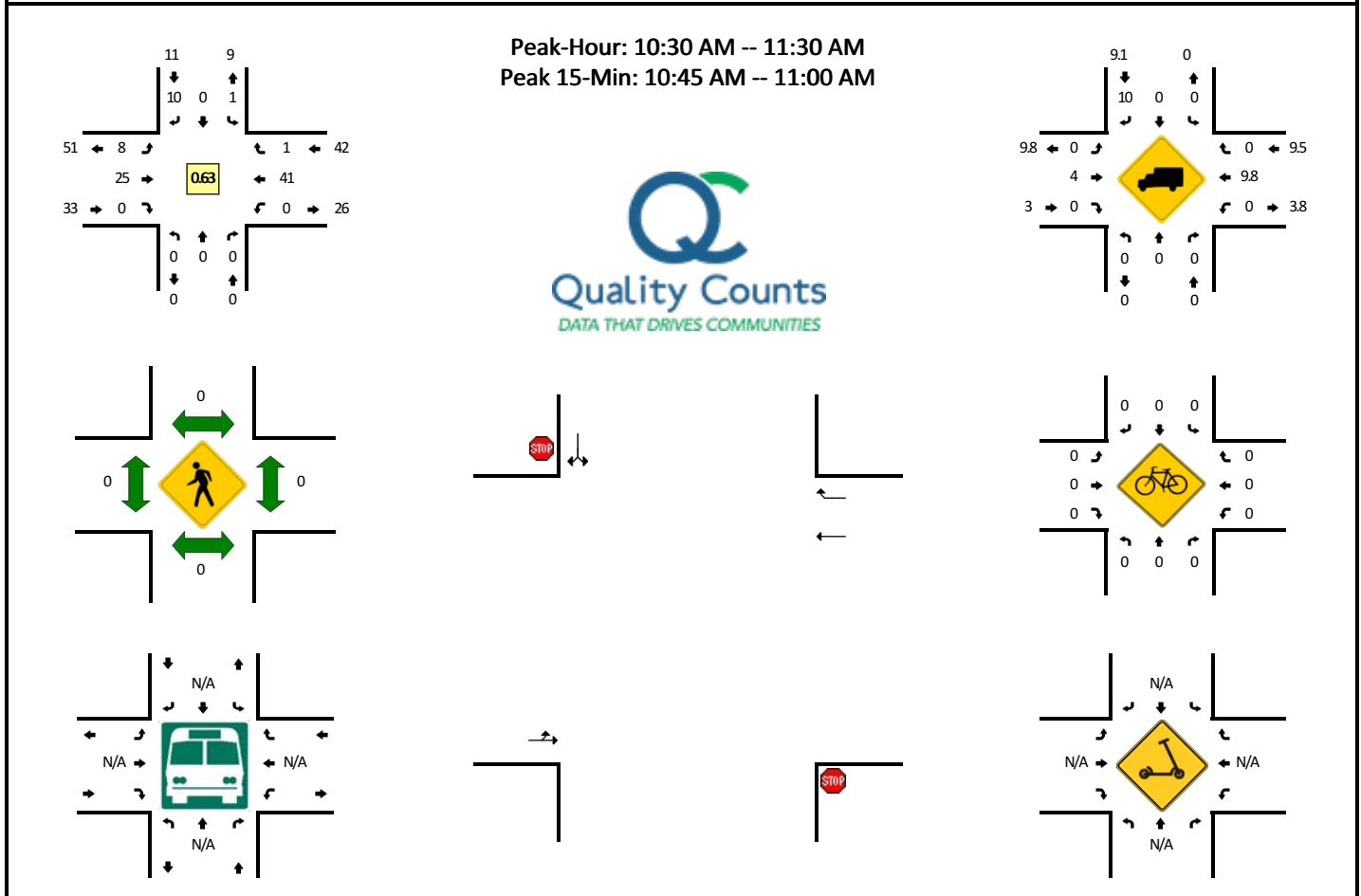
15-Min Count Period Beginning At	(Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)				(Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:15 PM	0	0	0	0	0	0	2	0	3	9	0	1	0	4	0	0	19	69
3:30 PM	0	0	0	0	1	0	2	0	6	8	0	0	0	10	0	0	27	83
3:45 PM	0	0	0	0	0	0	5	0	1	7	0	0	0	5	0	0	18	83
4:00 PM	0	0	0	0	0	0	1	0	5	6	0	0	0	9	0	0	21	85
4:15 PM	0	0	0	0	0	0	4	0	3	7	0	0	0	9	0	0	23	89
4:30 PM	0	0	0	0	0	0	2	0	4	15	0	0	0	4	1	0	26	88
4:45 PM	0	0	0	0	0	0	0	0	0	12	0	0	0	5	1	0	18	88
5:00 PM	0	0	0	0	0	0	3	0	2	11	0	1	0	3	2	0	22	89
5:15 PM	0	0	0	0	0	0	3	0	5	9	0	0	0	3	0	0	20	86
5:30 PM	0	0	0	0	0	0	1	0	6	7	0	0	0	4	0	0	18	78
5:45 PM	0	0	0	0	0	0	2	0	0	4	0	0	0	9	0	0	15	75
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	4	0	8	0	24	32	0	0	0	40	0	0	108	
Heavy Trucks	0	0	0		0	0	0		0	4	0		0	4	0		8	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Ramp to Lauderdale County Rd 2 (East) -- Natchez Trace Pkwy
CITY/STATE: Lauderdale, AL

QC JOB #: 15889217
DATE: Sat, Nov 12 2022



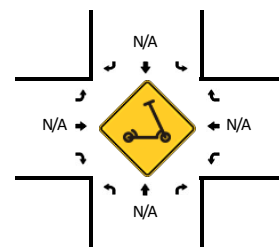
15-Min Count Period Beginning At	(Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)				(Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	0	0	0	0	2	0	1	1	0	0	0	2	0	0	6	
6:15 AM	0	0	0	0	0	0	3	0	0	0	0	0	0	2	0	0	5	
6:30 AM	0	0	0	0	0	0	0	0	1	1	0	0	0	2	0	0	4	
6:45 AM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	16
7:00 AM	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	2	12
7:15 AM	0	0	0	0	0	0	1	0	1	12	0	0	0	3	0	0	17	24
7:30 AM	0	0	0	0	0	0	0	0	2	1	0	0	0	3	0	0	6	26
7:45 AM	0	0	0	0	0	0	0	0	0	3	0	0	0	2	0	0	5	30
8:00 AM	0	0	0	0	0	0	0	0	1	4	0	0	0	3	0	0	8	36
8:15 AM	0	0	0	0	0	0	1	0	1	2	0	0	0	5	0	0	9	28
8:30 AM	0	0	0	0	0	0	1	0	2	3	0	0	0	3	0	0	9	31
8:45 AM	0	0	0	0	0	0	2	0	0	3	0	0	0	6	0	0	11	37
9:00 AM	0	0	0	0	0	0	0	0	2	2	0	0	0	2	0	0	6	35
9:15 AM	0	0	0	0	0	0	1	0	0	2	0	0	0	1	0	0	4	30
9:30 AM	0	0	0	0	1	0	1	0	0	1	0	0	0	1	0	0	4	25
9:45 AM	0	0	0	0	0	0	2	0	3	6	0	0	0	5	0	0	16	30
10:00 AM	0	0	0	0	0	0	0	0	4	4	0	0	0	7	0	0	15	39
10:15 AM	0	0	0	0	0	0	1	0	2	6	0	0	0	1	0	0	10	45
10:30 AM	0	0	0	0	1	0	3	0	4	6	0	0	0	3	1	0	18	59
10:45 AM	0	0	0	0	0	0	2	0	1	4	0	0	0	27	0	0	34	77
11:00 AM	0	0	0	0	0	0	4	0	0	7	0	0	0	5	0	0	16	78
11:15 AM	0	0	0	0	0	0	1	0	3	8	0	0	0	6	0	0	18	86
11:30 AM	0	0	0	0	0	0	2	0	6	2	0	0	0	5	0	0	15	83
11:45 AM	0	0	0	0	0	0	2	0	1	6	0	0	0	4	0	0	13	62
12:00 PM	0	0	0	0	0	0	1	0	3	10	0	0	0	5	0	0	19	65
12:15 PM	0	0	0	0	0	0	1	0	2	3	0	0	0	5	0	0	11	58
12:30 PM	0	0	0	0	1	0	0	0	1	3	0	0	0	6	0	0	11	54
12:45 PM	0	0	0	0	0	0	1	0	1	4	0	0	0	5	0	0	11	52
1:00 PM	0	0	0	0	1	0	6	0	8	3	0	0	0	4	0	0	22	55
1:15 PM	0	0	0	0	0	0	2	0	1	2	0	0	0	8	0	0	13	57
1:30 PM	0	0	0	0	0	0	3	0	3	3	0	0	0	3	1	0	13	59
1:45 PM	0	0	0	0	0	0	1	0	1	4	0	0	0	6	0	0	12	60
2:00 PM	0	0	0	0	0	0	1	0	1	4	0	0	0	5	0	0	11	49
2:15 PM	0	0	0	0	0	0	4	0	1	3	0	0	0	10	0	0	18	54
2:30 PM	0	0	0	0	0	0	5	0	2	7	0	0	0	2	0	0	16	57
2:45 PM	0	0	0	0	0	0	2	0	0	6	0	0	0	5	1	0	14	59
3:00 PM	0	0	0	0	0	0	1	0	4	7	0	0	0	2	0	0	14	62

15-Min Count Period Beginning At	(Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)				(Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:15 PM	0	0	0	0	0	0	4	0	1	7	0	0	0	10	0	0	22	66
3:30 PM	0	0	0	0	2	0	1	0	0	10	0	0	0	5	0	0	18	68
3:45 PM	0	0	0	0	0	0	2	0	1	6	0	0	0	3	0	0	12	66
4:00 PM	0	0	0	0	0	0	1	0	1	6	0	0	0	6	1	0	15	67
4:15 PM	0	0	0	0	0	0	0	0	0	2	0	0	0	6	1	0	9	54
4:30 PM	0	0	0	0	1	0	0	0	3	8	0	0	0	7	0	0	19	55
4:45 PM	0	0	0	0	0	0	1	0	3	6	0	0	0	5	0	0	15	58
5:00 PM	0	0	0	0	0	0	2	0	4	6	0	0	0	1	1	0	14	57
5:15 PM	0	0	0	0	0	0	0	0	2	3	0	0	0	1	0	0	6	54
5:30 PM	0	0	0	0	0	0	2	0	3	3	0	0	0	5	0	0	13	48
5:45 PM	0	0	0	0	0	0	1	0	3	3	0	0	0	5	0	0	12	45
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	8	0	4	16	0	0	0	108	0	0	136	
Heavy Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	8	0	0	8	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

QC JOB #: 15889218
DATE: Sun, Nov 13 2022

Page 1 of 2

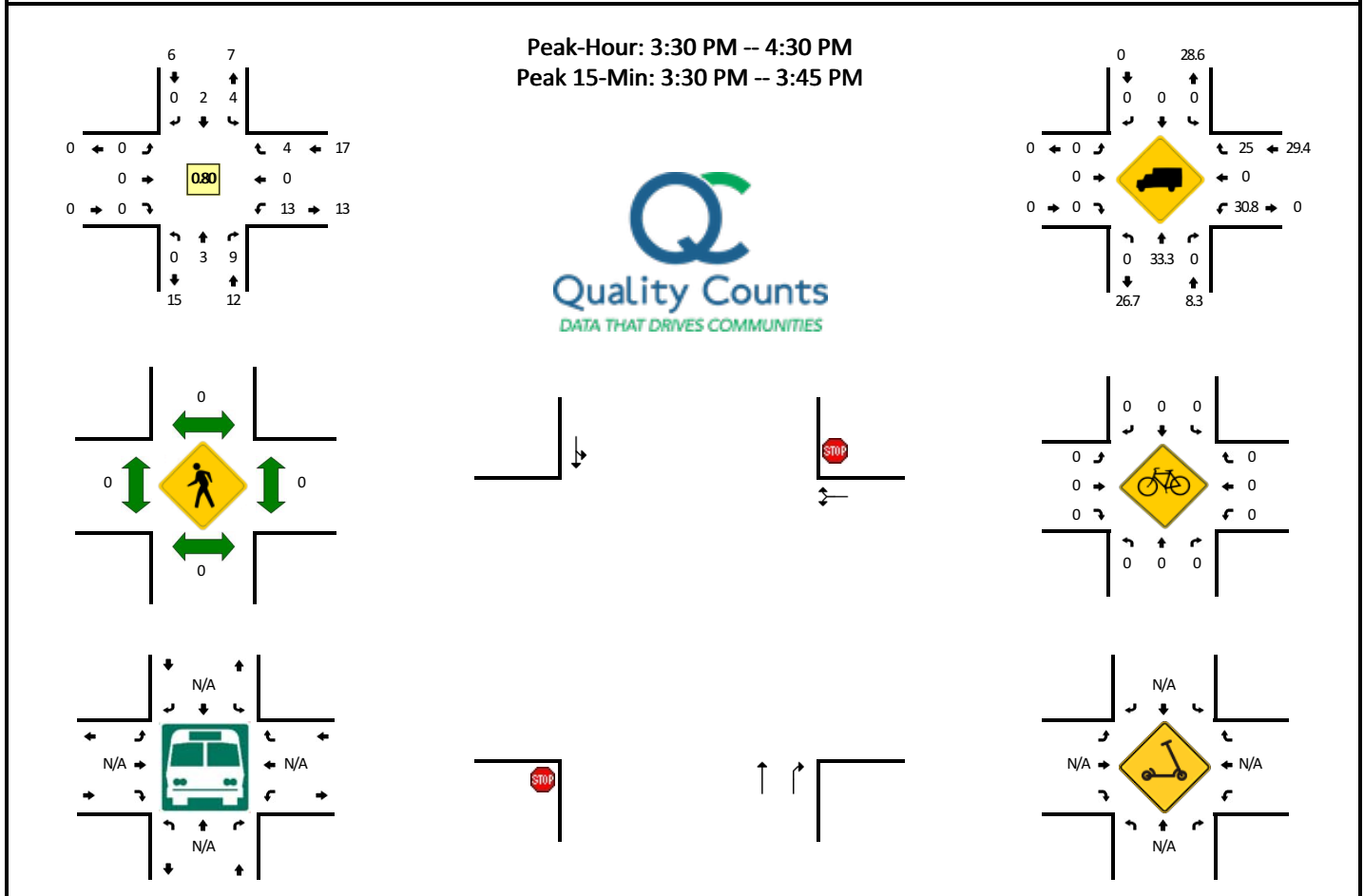
15-Min Count Period Beginning At	(Peak) Ramp to Lauderdale County Rd 2 (East) (Northbound)				(Peak) Ramp to Lauderdale County Rd 2 (East) (Southbound)				Natchez Trace Pkwy (Eastbound)				Natchez Trace Pkwy (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:15 PM	0	0	0	0	0	0	2	0	1	6	0	0	0	11	0	0	20	77
3:30 PM	0	0	0	0	0	0	4	0	1	8	0	0	0	7	0	0	20	74
3:45 PM	0	0	0	0	0	0	1	0	3	8	0	0	0	0	0	0	12	69
4:00 PM	0	0	0	0	0	0	3	0	2	7	0	0	0	14	0	0	26	78
4:15 PM	0	0	0	0	1	0	6	0	6	9	0	0	0	0	0	0	22	80
4:30 PM	0	0	0	0	0	0	3	0	2	7	0	0	0	3	1	0	16	76
4:45 PM	0	0	0	0	0	0	2	0	1	3	0	0	0	3	0	0	9	73
5:00 PM	0	0	0	0	0	0	2	0	1	5	0	0	0	2	0	0	10	57
5:15 PM	0	0	0	0	1	0	1	0	1	2	0	0	0	5	1	0	11	46
5:30 PM	0	0	0	0	0	0	3	0	2	7	0	0	0	5	1	0	18	48
5:45 PM	0	0	0	0	0	0	1	0	1	2	0	0	0	8	0	0	12	51
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	0	0	0	0	20	0	24	32	0	0	0	28	0	0	104	
Heavy Trucks	0	0	0	0	0	0	0	0	0	4	0	0	0	8	0	0	12	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Lauderdale County 2 -- Ramp to Natchez Trace Pkwy (west)
CITY/STATE: Lauderdale, AL

QC JOB #: 15889219
DATE: Wed, Nov 9 2022



15-Min Count Period Beginning At	(Peak) Lauderdale County 2 (Northbound)				(Peak) Lauderdale County 2 (Southbound)				Ramp to Natchez Trace Pkwy (west) (Eastbound)				Ramp to Natchez Trace Pkwy (west) (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	
6:15 AM	0	0	1	0	1	0	0	0	0	0	0	0	2	0	1	0	5	
6:30 AM	0	0	3	0	1	0	0	0	0	0	0	0	4	0	0	0	8	
6:45 AM	0	0	5	0	0	2	0	0	0	0	0	0	1	0	0	0	8	23
7:00 AM	0	1	4	0	0	0	0	0	0	0	0	0	2	0	0	0	7	28
7:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	2	25
7:30 AM	0	2	1	0	0	0	0	0	0	0	0	0	1	0	0	0	4	21
7:45 AM	0	0	1	0	1	1	0	0	0	0	0	0	2	0	0	0	5	18
8:00 AM	0	0	3	0	1	1	0	0	0	0	0	0	1	0	0	0	6	17
8:15 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	16
8:30 AM	0	0	3	0	0	0	0	0	0	0	0	0	2	0	0	0	5	17
8:45 AM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2	14
9:00 AM	0	2	1	0	0	1	0	0	0	0	0	0	1	0	0	2	7	15
9:15 AM	0	3	2	0	0	0	0	0	0	0	0	0	1	0	0	0	6	20
9:30 AM	0	2	2	0	0	1	0	0	0	0	0	0	1	0	1	1	8	23
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	23
10:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	2	0	0	0	3	19
10:15 AM	0	0	1	0	1	0	0	0	0	0	0	0	3	0	0	0	5	18
10:30 AM	0	0	0	0	0	1	0	0	0	0	0	0	1	0	1	0	3	13
10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	0	0	3	14
11:00 AM	0	1	1	0	1	1	0	0	0	0	0	0	2	0	1	0	7	18
11:15 AM	0	2	1	0	1	0	0	0	0	0	0	0	2	0	1	0	7	20
11:30 AM	0	0	1	0	1	0	0	0	0	0	0	0	3	0	0	0	5	22
11:45 AM	0	1	3	0	2	2	0	0	0	0	0	0	1	0	2	0	11	30
12:00 PM	0	2	4	0	0	1	0	0	0	0	0	0	2	0	2	0	11	34
12:15 PM	0	0	1	0	2	0	0	0	0	0	0	0	2	0	1	0	6	33
12:30 PM	0	0	1	0	0	1	0	0	0	0	0	0	0	0	1	0	3	31
12:45 PM	0	3	1	0	0	1	0	0	0	0	0	0	3	0	1	0	9	29
1:00 PM	0	0	2	0	1	1	0	0	0	0	0	0	0	0	1	0	5	23
1:15 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	1	0	3	20
1:30 PM	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	19
1:45 PM	0	0	1	0	0	2	0	0	0	0	0	0	1	0	0	0	4	14
2:00 PM	0	0	3	0	0	1	0	0	0	0	0	0	2	0	1	0	7	16
2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	15
2:30 PM	0	0	0	0	1	0	0	0	0	0	0	0	1	0	1	0	3	16
2:45 PM	0	1	1	0	0	2	0	0	0	0	0	0	2	0	1	0	7	19
3:00 PM	0	0	2	0	0	2	0	0	0	0	0	0	1	0	0	0	5	17

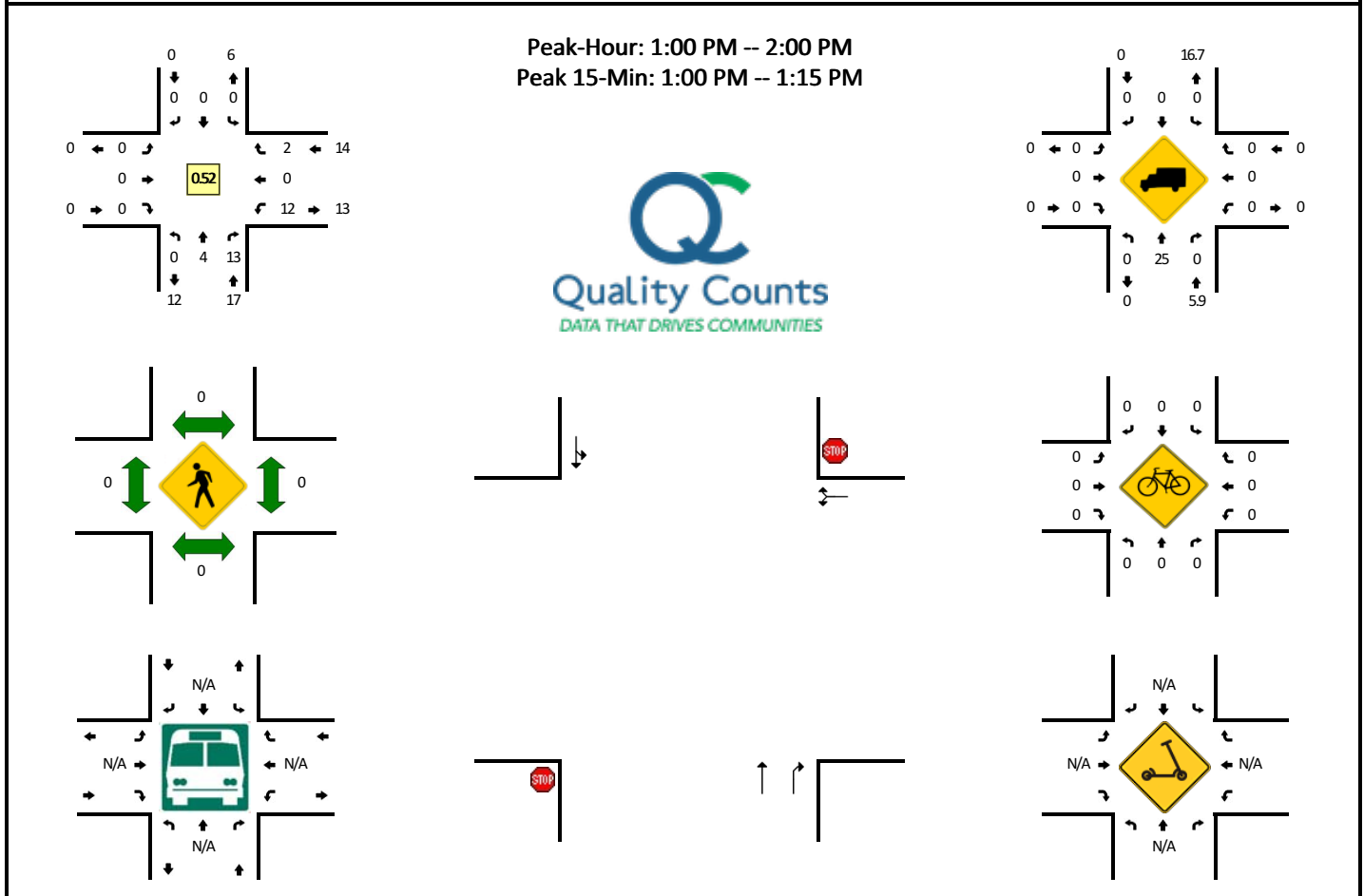
15-Min Count Period Beginning At	(Peak) Lauderdale County 2 (Northbound)				(Peak) Lauderdale County 2 (Southbound)				Ramp to Natchez Trace Pkwy (west) (Eastbound)				Ramp to Natchez Trace Pkwy (west) (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:15 PM	0	0	1	0	1	2	0	0	0	0	0	0	3	0	0	0	7	22
3:30 PM	0	2	1	0	1	0	0	0	0	0	0	0	4	0	3	0	11	30
3:45 PM	0	1	3	0	3	0	0	0	0	0	0	0	2	0	0	0	9	32
4:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	4	0	0	0	5	32
4:15 PM	0	0	4	0	0	2	0	0	0	0	0	0	3	0	1	0	10	35
4:30 PM	0	1	2	0	0	0	0	0	0	0	0	0	2	0	2	0	7	31
4:45 PM	0	1	0	0	0	2	0	0	0	0	0	0	2	0	0	0	5	27
5:00 PM	0	0	2	0	0	2	0	0	0	0	0	0	1	0	1	0	6	28
5:15 PM	0	1	3	0	0	0	0	0	0	0	0	0	6	0	0	0	10	28
5:30 PM	0	0	1	0	0	0	0	0	0	0	0	0	5	0	1	0	7	28
5:45 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	8	4	0	4	0	0	0	0	0	0	0	16	0	12	0	44	
Heavy Trucks	0	4	0		0	0	0		0	0	0		0	0	0		4	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Lauderdale County 2 -- Ramp to Natchez Trace Pkwy (west)
CITY/STATE: Lauderdale, AL

QC JOB #: 15889220
DATE: Sat, Nov 12 2022



15-Min Count Period Beginning At	(Peak) Lauderdale County 2 (Northbound)				(Peak) Lauderdale County 2 (Southbound)				Ramp to Natchez Trace Pkwy (west) (Eastbound)				Ramp to Natchez Trace Pkwy (west) (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	1	0	1	0	0	0	0	0	0	0	1	0	0	0	3	
6:15 AM	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	
6:45 AM	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	8
7:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	6
7:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	2	5
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	6
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	5
8:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	2	5
8:30 AM	0	0	0	0	1	1	0	0	0	0	0	0	1	0	0	0	3	6
8:45 AM	0	2	2	0	0	0	0	0	0	0	0	0	1	0	0	0	5	11
9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	2	12
9:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	11
9:30 AM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	10
9:45 AM	0	0	0	0	2	0	0	0	0	0	0	0	3	0	0	0	5	10
10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	3	0	1	0	4	12
10:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	1	0	1	0	3	14
10:30 AM	0	1	2	0	2	0	0	0	0	0	0	0	2	0	2	0	9	21
10:45 AM	0	0	1	0	1	0	0	0	0	0	0	0	1	0	0	0	3	19
11:00 AM	0	0	2	0	1	2	0	0	0	0	0	0	0	0	0	0	5	20
11:15 AM	0	0	1	0	0	0	0	0	0	0	0	0	2	0	1	0	4	21
11:30 AM	0	1	2	0	0	0	0	0	0	0	0	0	3	0	3	0	9	21
11:45 AM	0	1	2	0	0	0	0	0	0	0	0	0	1	0	0	0	4	22
12:00 PM	0	0	1	0	0	1	0	0	0	0	0	0	1	0	2	0	5	22
12:15 PM	0	1	1	0	0	2	0	0	0	0	0	0	1	0	1	0	6	24
12:30 PM	0	1	1	0	0	2	0	0	0	0	0	0	1	0	0	0	5	20
12:45 PM	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	0	3	19
1:00 PM	0	0	7	0	0	0	0	0	0	0	0	0	7	0	1	0	15	29
1:15 PM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	1	0	5	28
1:30 PM	0	2	1	0	0	0	0	0	0	0	0	0	4	0	0	0	7	30
1:45 PM	0	2	1	0	0	0	0	0	0	0	0	0	1	0	0	0	4	31
2:00 PM	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	0	3	19
2:15 PM	0	0	3	0	0	0	0	0	0	0	0	0	1	0	0	0	4	18
2:30 PM	0	2	3	0	1	0	0	0	0	0	0	0	2	0	0	0	8	19
2:45 PM	0	1	2	0	0	1	0	0	0	0	0	0	1	0	0	0	5	20
3:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	3	0	1	0	5	22

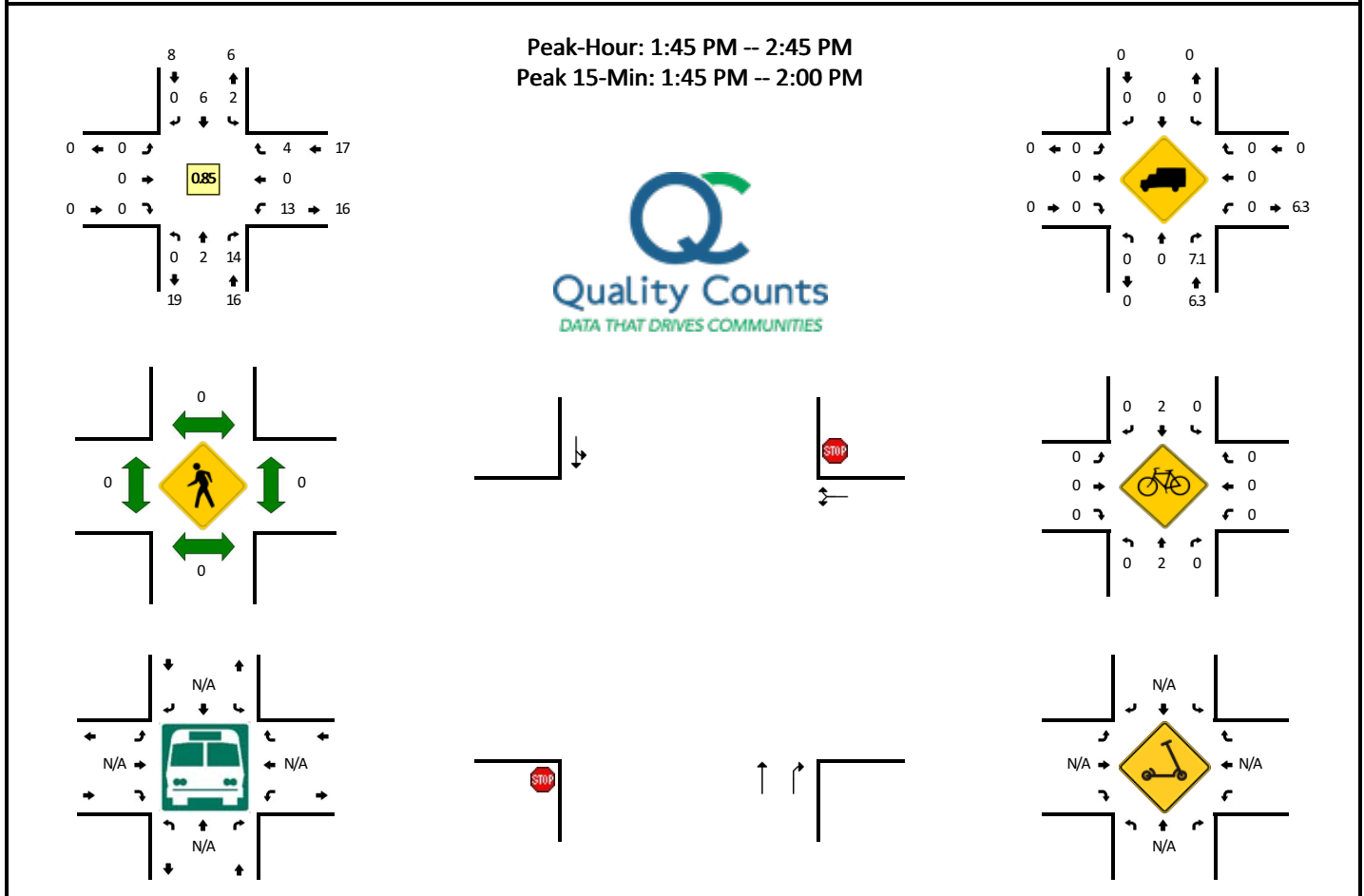
15-Min Count Period Beginning At	(Peak) Lauderdale County 2 (Northbound)				(Peak) Lauderdale County 2 (Southbound)				Ramp to Natchez Trace Pkwy (west) (Eastbound)				Ramp to Natchez Trace Pkwy (west) (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:15 PM	0	2	2	0	2	2	0	0	0	0	0	0	1	0	0	0	9	27
3:30 PM	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	22
3:45 PM	0	0	1	0	1	0	0	0	0	0	0	0	1	0	0	0	3	20
4:00 PM	0	0	1	0	0	0	0	0	0	0	0	0	1	0	1	0	3	18
4:15 PM	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	2	11
4:30 PM	0	0	1	0	0	1	0	0	0	0	0	0	2	0	1	0	5	13
4:45 PM	0	1	1	0	0	1	0	0	0	0	0	0	3	0	0	0	6	16
5:00 PM	0	0	2	1	0	1	0	0	0	0	0	0	5	0	0	0	9	22
5:15 PM	0	1	0	0	0	3	0	0	0	0	0	0	1	0	1	0	6	26
5:30 PM	0	0	2	0	0	1	0	0	0	0	0	0	3	0	0	0	6	27
5:45 PM	0	1	1	0	0	2	0	0	0	0	0	0	3	0	0	0	7	28
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	28	0	0	0	0	0	0	0	0	0	28	0	4	0	60	
Heavy Trucks	0	0	0		0	0	0		0	0	0		0	0	0		0	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

LOCATION: (Peak) Lauderdale County 2 -- Ramp to Natchez Trace Pkwy (west)
CITY/STATE: Lauderdale, AL

QC JOB #: 15889221
DATE: Sun, Nov 13 2022



15-Min Count Period Beginning At	(Peak) Lauderdale County 2 (Northbound)				(Peak) Lauderdale County 2 (Southbound)				Ramp to Natchez Trace Pkwy (west) (Eastbound)				Ramp to Natchez Trace Pkwy (west) (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
6:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	
6:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
6:45 AM	0	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	2	4
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	3
7:30 AM	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	6
7:45 AM	0	1	1	0	0	0	0	0	0	0	0	0	1	0	0	0	3	7
8:00 AM	0	0	0	0	0	1	0	0	0	0	0	0	1	0	0	0	2	9
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	6
8:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	2	5
9:00 AM	0	2	0	0	0	0	0	0	0	0	0	0	1	0	0	0	3	6
9:15 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	7
9:30 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	7
9:45 AM	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	6
10:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	3	0	0	0	4	7
10:15 AM	0	0	2	0	0	1	0	0	0	0	0	0	1	0	0	0	4	10
10:30 AM	0	0	0	0	1	0	0	0	0	0	0	0	2	0	1	0	4	13
10:45 AM	0	0	0	0	1	2	0	0	0	0	0	0	1	0	2	0	6	18
11:00 AM	0	0	3	0	1	1	0	0	0	0	0	0	5	0	1	0	11	25
11:15 AM	0	0	2	0	0	1	0	0	0	0	0	0	4	0	0	0	7	28
11:30 AM	0	0	3	0	0	0	0	0	0	0	0	0	2	0	0	0	5	29
11:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	0	2	25
12:00 PM	0	1	3	0	0	3	0	0	0	0	0	0	1	0	1	0	9	23
12:15 PM	0	0	3	0	0	0	0	0	0	0	0	0	1	0	0	0	4	20
12:30 PM	0	0	2	0	0	0	0	0	0	0	0	0	1	0	1	0	4	19
12:45 PM	0	1	2	0	0	0	0	0	0	0	0	0	3	0	0	0	6	23
1:00 PM	0	1	1	0	1	0	0	0	0	0	0	0	4	0	1	0	8	22
1:15 PM	0	0	3	0	0	0	0	0	0	0	0	0	3	0	0	0	6	24
1:30 PM	0	0	4	0	0	3	0	0	0	0	0	0	1	0	0	0	8	28
1:45 PM	0	0	5	0	0	1	0	0	0	0	0	0	4	0	2	0	12	34
2:00 PM	0	1	0	0	0	3	0	0	0	0	0	0	3	0	1	0	8	34
2:15 PM	0	1	3	0	1	1	0	0	0	0	0	0	3	0	1	0	10	38
2:30 PM	0	0	6	0	1	1	0	0	0	0	0	0	3	0	0	0	11	41
2:45 PM	0	0	3	0	1	1	0	0	0	0	0	0	1	0	0	0	6	35
3:00 PM	0	0	2	0	0	1	0	0	0	0	0	0	4	0	0	0	7	34

15-Min Count Period Beginning At	(Peak) Lauderdale County 2 (Northbound)				(Peak) Lauderdale County 2 (Southbound)				Ramp to Natchez Trace Pkwy (west) (Eastbound)				Ramp to Natchez Trace Pkwy (west) (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:15 PM	0	1	5	0	0	1	0	0	0	0	0	0	2	0	0	0	9	33
3:30 PM	0	0	1	0	0	0	0	0	0	0	0	0	1	0	0	0	2	24
3:45 PM	0	0	0	0	1	0	0	0	0	0	0	0	2	0	1	0	4	22
4:00 PM	0	1	2	0	1	0	0	0	0	0	0	0	1	0	1	0	6	21
4:15 PM	0	2	6	0	1	0	0	0	0	0	0	0	5	0	1	0	15	27
4:30 PM	0	2	3	0	0	2	0	0	0	0	0	0	3	0	0	0	10	35
4:45 PM	0	2	2	0	0	1	0	0	0	0	0	0	1	0	0	0	6	37
5:00 PM	0	2	2	0	1	2	0	0	0	0	0	0	1	0	0	0	8	39
5:15 PM	0	0	1	0	0	2	0	0	0	0	0	0	2	0	0	0	5	29
5:30 PM	0	0	3	0	0	0	0	0	0	0	0	0	1	0	1	0	5	24
5:45 PM	0	0	1	0	0	1	0	0	0	0	0	0	2	0	0	0	4	22
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	0	20	0	0	4	0	0	0	0	0	0	16	0	8	0	48	
Heavy Trucks	0	0	0		0	0	0		0	0	0		0	0	0		0	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		
<i>Comments:</i>																		

Report generated on 12/9/2022 4:44 PM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

Appendix C:

Historic Counts

Natchez Trace PKWY

Current calendar year data are preliminary and subject to change. Data will be finalized by the end of the first quarter of next calendar year.

	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	Annual Total	Change From Previous
TRAFFIC COUNT AT CHEROKEE (NORTH)														
2022	20,416	21,954	26,430	29,360	28,256	27,004	25,728	27,230	24,000	24,180	21,000	21,700	297,258	4.9%
2021	19,530	19,600	22,320	23,400	24,800	24,800	31,000	26,350	24,800	24,180	21,000	21,700	283,480	0.3%
2020	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	0.2%
2019	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2018	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2017	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	-0.2%
2016	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	0.2%
2015	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2014	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2013	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	-0.2%
2012	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	0.2%
2011	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2010	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2009	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	-0.2%
2008	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	-0.3%
2007	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,800	24,180	21,700	21,700	283,380	0.5%
2006	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
2005	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	-0.2%
2004	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	0.5%
2003	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	23,400	21,000	21,700	281,100	0.4%
2002	17,640	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	279,990	8.3%
2001	19,530	19,600	22,320	0	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	258,480	-8.8%
2000	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,700	21,700	283,280	0.5%
1999	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
1998	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	0.0%
1997	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	-0.2%
1996	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	0.2%
1995	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	281,880	15.6%
1994	19,530	19,600	22,320	19,368	12,474	24,000	31,000	26,350	24,000	24,180	21,000	0	243,822	-13.3%
1993	19,530	19,600	22,320	23,400	24,800	24,000	31,000	26,350	24,000	23,400	21,000	21,700	281,100	-0.5%
1992	19,530	20,300	22,320	23,400	24,800	24,000	31,000	26,350	24,000	24,180	21,000	21,700	282,580	0.1%
1991	19,530	19,600	22,320	23,192	25,072	27,174	27,528	27,124	24,000	24,180	21,000	21,700	282,420	

Appendix D:
Capacity Analysis Reports

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection

Int Delay, s/veh 0.8

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	6	64	47	3	4	3
Future Vol, veh/h	6	64	47	3	4	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	7	76	56	4	5	4




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	60	0	0 110 30
Stage 1	-	-	- 58 -
Stage 2	-	-	- 52 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1534	-	- 872 1034
Stage 1	-	-	- 955 -
Stage 2	-	-	- 961 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1534	-	- 868 1034
Mov Cap-2 Maneuver	-	-	- 868 -
Stage 1	-	-	- 950 -
Stage 2	-	-	- 961 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	8.9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1534	-	-	-	932
HCM Lane V/C Ratio	0.005	-	-	-	0.009
HCM Control Delay (s)	7.4	0	-	-	8.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection

Int Delay, s/veh 0.2

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	68	1	1	48	1	1
Future Vol, veh/h	68	1	1	48	1	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	86	1	1	61	1	1

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	87
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1503
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1503
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.2	9
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	899	-	-	1503	-
HCM Lane V/C Ratio	0.003	-	-	0.001	-
HCM Control Delay (s)	9	-	-	7.4	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection

Int Delay, s/veh 2.5

Movement EBL EBT WBT WBR SBL SBRLane Configurations 

Traffic Vol, veh/h 17 52 32 0 4 16

Future Vol, veh/h 17 52 32 0 4 16

Conflicting Peds, #/hr 0 0 0 0 0 0

Sign Control Free Free Free Free Stop Stop

RT Channelized - None - Yield - None

Storage Length - - - - 0 -

Veh in Median Storage, # - 0 0 - 0 -

Grade, % - 0 0 - 0 -

Peak Hour Factor 82 82 82 82 82 82

Heavy Vehicles, % 3 3 3 3 3 3

Mvmt Flow 21 63 39 0 5 20

Major/Minor Major1 Major2 Minor2

Conflicting Flow All 39 0 - 0 144 39

Stage 1 - - - - 39 -

Stage 2 - - - - 105 -

Critical Hdwy 4.13 - - - 6.43 6.23

Critical Hdwy Stg 1 - - - - 5.43 -

Critical Hdwy Stg 2 - - - - 5.43 -

Follow-up Hdwy 2.227 - - - 3.527 3.327

Pot Cap-1 Maneuver 1565 - - - 846 1030

Stage 1 - - - - 981 -

Stage 2 - - - - 917 -

Platoon blocked, % - - - -

Mov Cap-1 Maneuver 1565 - - - 834 1030

Mov Cap-2 Maneuver - - - - 834 -

Stage 1 - - - - 967 -

Stage 2 - - - - 917 -

Approach EB WB SB

HCM Control Delay, s 1.8 0 8.8

HCM LOS A

Minor Lane/Major Mvmt EBL EBT WBT WBR SBLn1




Capacity (veh/h) 1565 - - - 984

HCM Lane V/C Ratio 0.013 - - - 0.025

HCM Control Delay (s) 7.3 0 - - 8.8

HCM Lane LOS A A - - A

HCM 95th %tile Q(veh) 0 - - - 0.1




Intersection						
Int Delay, s/veh	3.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	14	4	3	20	2	3
Future Vol, veh/h	14	4	3	20	2	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	68	68	68	68	68	68
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	21	6	4	29	3	4
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	29	19	0	0	4	0
Stage 1	19	-	-	-	-	-
Stage 2	10	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	983	1056	-	-	1611	-
Stage 1	1001	-	-	-	-	-
Stage 2	1010	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	981	1056	-	-	1611	-
Mov Cap-2 Maneuver	981	-	-	-	-	-
Stage 1	1001	-	-	-	-	-
Stage 2	1008	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.7	0	2.9			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	997	1611	-	
HCM Lane V/C Ratio	-	-	0.027	0.002	-	
HCM Control Delay (s)	-	-	8.7	7.2	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection

Int Delay, s/veh 1.8

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	7	37	44	10	8	10
Future Vol, veh/h	7	37	44	10	8	10
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	8	42	50	11	9	11




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	61	0	93
Stage 1	-	-	56
Stage 2	-	-	37
Critical Hdwy	4.16	-	6.86
Critical Hdwy Stg 1	-	-	5.86
Critical Hdwy Stg 2	-	-	5.86
Follow-up Hdwy	2.23	-	3.53
Pot Cap-1 Maneuver	1533	-	894
Stage 1	-	-	957
Stage 2	-	-	978
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1533	-	890
Mov Cap-2 Maneuver	-	-	890
Stage 1	-	-	952
Stage 2	-	-	978

Approach	EB	WB	SB
HCM Control Delay, s	1.2	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1533	-	-	-	964
HCM Lane V/C Ratio	0.005	-	-	-	0.021
HCM Control Delay (s)	7.4	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection

Int Delay, s/veh 0.4

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	62	1	3	42	2	1
Future Vol, veh/h	62	1	3	42	2	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	73	73	73	73	73	73
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	85	1	4	58	3	1




Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	86
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1504
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1504
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.5	9.1
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	875	-	-	1504	-
HCM Lane V/C Ratio	0.005	-	-	0.003	-
HCM Control Delay (s)	9.1	-	-	7.4	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection




Int Delay, s/veh 1.7

Movement	EBL	EBT	WBU	WBT	WBR	SBL	SBR
Lane Configurations							
Traffic Vol, veh/h	15	48	1	37	0	0	9
Future Vol, veh/h	15	48	1	37	0	0	9
Conflicting Peds, #/hr	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	-	Yield	-	None
Storage Length	-	-	-	-	-	0	-
Veh in Median Storage, #	-	0	-	0	-	0	-
Grade, %	-	0	-	0	-	0	-
Peak Hour Factor	72	72	72	72	72	72	72
Heavy Vehicles, %	3	3	3	3	3	3	3
Mvmt Flow	21	67	1	51	0	0	13




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	51	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.13	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.227	-	-
Pot Cap-1 Maneuver	1549	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1549	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	1.8		8.6
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1549	-	-	-	1014
HCM Lane V/C Ratio	0.013	-	-	-	0.012
HCM Control Delay (s)	7.4	0	-	-	8.6
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0




Intersection						
Int Delay, s/veh	4.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	17	1	3	17	5	4
Future Vol, veh/h	17	1	3	17	5	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	26	2	5	26	8	6
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	40	18	0	0	5	0
Stage 1	18	-	-	-	-	-
Stage 2	22	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	969	1058	-	-	1610	-
Stage 1	1002	-	-	-	-	-
Stage 2	998	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	964	1058	-	-	1610	-
Mov Cap-2 Maneuver	964	-	-	-	-	-
Stage 1	1002	-	-	-	-	-
Stage 2	993	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	8.8	0		4		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	-		969	1610	
HCM Lane V/C Ratio	-	-		0.029	0.005	
HCM Control Delay (s)	-	-		8.8	7.2	
HCM Lane LOS	-	-		A	A	
HCM 95th %tile Q(veh)	-	-		0.1	0	

Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	0	47	44	5	1	5
Future Vol, veh/h	0	47	44	5	1	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	0	55	52	6	1	6
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	58	0	-	0	83	29
Stage 1	-	-	-	-	55	-
Stage 2	-	-	-	-	28	-
Critical Hdwy	4.16	-	-	-	6.86	6.96
Critical Hdwy Stg 1	-	-	-	-	5.86	-
Critical Hdwy Stg 2	-	-	-	-	5.86	-
Follow-up Hdwy	2.23	-	-	-	3.53	3.33
Pot Cap-1 Maneuver	1537	-	-	-	907	1036
Stage 1	-	-	-	-	958	-
Stage 2	-	-	-	-	988	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1537	-	-	-	907	1036
Mov Cap-2 Maneuver	-	-	-	-	907	-
Stage 1	-	-	-	-	958	-
Stage 2	-	-	-	-	988	-
Approach	EB	WB		SB		
HCM Control Delay, s	0	0		8.6		
HCM LOS	A					
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1537	-	-	-	1012	
HCM Lane V/C Ratio	-	-	-	-	0.007	
HCM Control Delay (s)	0	-	-	-	8.6	
HCM Lane LOS	A	-	-	-	A	
HCM 95th %tile Q(veh)	0	-	-	-	0	

Intersection						
Int Delay, s/veh	0.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	50	5	0	39	5	0
Future Vol, veh/h	50	5	0	39	5	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	59	6	0	46	6	0
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	65	0	108	62
Stage 1	-	-	-	-	62	-
Stage 2	-	-	-	-	46	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1531	-	887	1000
Stage 1	-	-	-	-	958	-
Stage 2	-	-	-	-	974	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1531	-	887	1000
Mov Cap-2 Maneuver	-	-	-	-	887	-
Stage 1	-	-	-	-	958	-
Stage 2	-	-	-	-	974	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		9.1	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	887	-	-	1531	-	
HCM Lane V/C Ratio	0.007	-	-	-	-	
HCM Control Delay (s)	9.1	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	0	-	-	0	-	

Intersection

Int Delay, s/veh 2.5

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	15	28	33	0	1	12
Future Vol, veh/h	15	28	33	0	1	12
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	18	34	40	0	1	15

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	40	0	0 110 40
Stage 1	-	-	- 40 -
Stage 2	-	-	- 70 -
Critical Hdwy	4.13	-	- 6.43 6.23
Critical Hdwy Stg 1	-	-	- 5.43 -
Critical Hdwy Stg 2	-	-	- 5.43 -
Follow-up Hdwy	2.227	-	- 3.527 3.327
Pot Cap-1 Maneuver	1563	-	- 885 1028
Stage 1	-	-	- 980 -
Stage 2	-	-	- 950 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1563	-	- 874 1028
Mov Cap-2 Maneuver	-	-	- 874 -
Stage 1	-	-	- 968 -
Stage 2	-	-	- 950 -

Approach	EB	WB	SB
HCM Control Delay, s	2.6	0	8.6
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1563	-	-	-	1014
HCM Lane V/C Ratio	0.012	-	-	-	0.016
HCM Control Delay (s)	7.3	0	-	-	8.6
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0




Intersection						
Int Delay, s/veh	5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	13	4	3	9	4	2
Future Vol, veh/h	13	4	3	9	4	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	80	80
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	16	5	4	11	5	3
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	23	10	0	0	4	0
Stage 1	10	-	-	-	-	-
Stage 2	13	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	991	1068	-	-	1611	-
Stage 1	1010	-	-	-	-	-
Stage 2	1007	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	988	1068	-	-	1611	-
Mov Cap-2 Maneuver	988	-	-	-	-	-
Stage 1	1010	-	-	-	-	-
Stage 2	1004	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	8.7	0		4.8		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	- 1006		1611	-	
HCM Lane V/C Ratio	-	- 0.021		0.003	-	
HCM Control Delay (s)	-	- 8.7		7.2	0	
HCM Lane LOS	-	- A		A	A	
HCM 95th %tile Q(veh)	-	- 0.1		0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

05/23/2023

Intersection




Int Delay, s/veh 0.7

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	3	29	51	5	3	2
Future Vol, veh/h	3	29	51	5	3	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	61	61	61	61	61	61
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	5	48	84	8	5	3

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	92	0	0 122 46
Stage 1	-	-	- 88 -
Stage 2	-	-	- 34 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1493	-	- 858 1010
Stage 1	-	-	- 922 -
Stage 2	-	-	- 981 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1493	-	- 855 1010
Mov Cap-2 Maneuver	-	-	- 855 -
Stage 1	-	-	- 919 -
Stage 2	-	-	- 981 -

Approach	EB	WB	SB
HCM Control Delay, s	0.7	0	9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1493	-	-	-	911
HCM Lane V/C Ratio	0.003	-	-	-	0.009
HCM Control Delay (s)	7.4	0	-	-	9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	31	0	0	53	0	0
Future Vol, veh/h	31	0	0	53	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	60	60	60	60	60	60
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	52	0	0	88	0	0
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	52	0	140	52
Stage 1	-	-	-	-	52	-
Stage 2	-	-	-	-	88	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1548	-	851	1013
Stage 1	-	-	-	-	968	-
Stage 2	-	-	-	-	933	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1548	-	851	1013
Mov Cap-2 Maneuver	-	-	-	-	851	-
Stage 1	-	-	-	-	968	-
Stage 2	-	-	-	-	933	-
Approach	EB	WB		NB		
HCM Control Delay, s	0	0		0		
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	-	-	-	1548	-	
HCM Lane V/C Ratio	-	-	-	-	-	
HCM Control Delay (s)	0	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	-	-	-	0	-	

Intersection

Int Delay, s/veh 1.8

Movement EBL EBT WBT WBR SBL SBRLane Configurations 

Traffic Vol, veh/h 8 25 41 1 1 10

Future Vol, veh/h 8 25 41 1 1 10

Conflicting Peds, #/hr 0 0 0 0 0 0

Sign Control Free Free Free Free Stop Stop

RT Channelized - None - Yield - None

Storage Length - - - - 0 -

Veh in Median Storage, # - 0 0 - 0 -

Grade, % - 0 0 - 0 -

Peak Hour Factor 63 63 63 63 63 63

Heavy Vehicles, % 3 3 3 3 3 3

Mvmt Flow 13 40 65 2 2 16

Major/Minor Major1 Major2 Minor2

Conflicting Flow All 65 0 - 0 132 66

Stage 1 - - - - 66 -

Stage 2 - - - - 66 -

Critical Hdwy 4.13 - - - 6.43 6.23

Critical Hdwy Stg 1 - - - - 5.43 -

Critical Hdwy Stg 2 - - - - 5.43 -

Follow-up Hdwy 2.227 - - - 3.527 3.327

Pot Cap-1 Maneuver 1531 - - - 860 995

Stage 1 - - - - 954 -

Stage 2 - - - - 954 -

Platoon blocked, % - - - -

Mov Cap-1 Maneuver 1531 - - - 852 995

Mov Cap-2 Maneuver - - - - 852 -

Stage 1 - - - - 945 -

Stage 2 - - - - 954 -

Approach EB WB SB

HCM Control Delay, s 1.8 0 8.7

HCM LOS A

Minor Lane/Major Mvmt EBL EBT WBT WBR SBLn1




Capacity (veh/h) 1531 - - - 980

HCM Lane V/C Ratio 0.008 - - - 0.018

HCM Control Delay (s) 7.4 0 - - 8.7

HCM Lane LOS A A - - A

HCM 95th %tile Q(veh) 0 - - - 0.1




Intersection						
Int Delay, s/veh	3.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	12	2	4	13	0	0
Future Vol, veh/h	12	2	4	13	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	52	52	52	52	52	52
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	23	4	8	25	0	0
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	23	21	0	0	8	0
Stage 1	21	-	-	-	-	-
Stage 2	2	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	991	1054	-	-	1606	-
Stage 1	999	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	991	1054	-	-	1606	-
Mov Cap-2 Maneuver	991	-	-	-	-	-
Stage 1	999	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.7	0	0			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	- 1000	1606	-		
HCM Lane V/C Ratio	-	- 0.027	-	-		
HCM Control Delay (s)	-	- 8.7	0	-		
HCM Lane LOS	-	- A	A	-		
HCM 95th %tile Q(veh)	-	- 0.1	0	-		

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection

Int Delay, s/veh 0.8

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	9	95	70	4	6	4
Future Vol, veh/h	9	95	70	4	6	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	11	113	83	5	7	5




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	88	0	0 165 44
Stage 1	-	-	- 86 -
Stage 2	-	-	- 79 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1498	-	- 807 1013
Stage 1	-	-	- 925 -
Stage 2	-	-	- 932 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1498	-	- 801 1013
Mov Cap-2 Maneuver	-	-	- 801 -
Stage 1	-	-	- 918 -
Stage 2	-	-	- 932 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	9.2
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1498	-	-	-	874
HCM Lane V/C Ratio	0.007	-	-	-	0.014
HCM Control Delay (s)	7.4	0	-	-	9.2
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection

Int Delay, s/veh 0.1

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	101	1	1	72	1	1
Future Vol, veh/h	101	1	1	72	1	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	128	1	1	91	1	1

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	129
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1451
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1451
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.1	9.3
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	833	-	-	1451	-
HCM Lane V/C Ratio	0.003	-	-	0.001	-
HCM Control Delay (s)	9.3	-	-	7.5	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection

Int Delay, s/veh 2.5

Movement EBL EBT WBT WBR SBL SBRLane Configurations 

Traffic Vol, veh/h 25 77 48 0 6 24

Future Vol, veh/h 25 77 48 0 6 24

Conflicting Peds, #/hr 0 0 0 0 0 0

Sign Control Free Free Free Free Stop Stop

RT Channelized - None - Yield - None

Storage Length - - - - 0 -

Veh in Median Storage, # - 0 0 - 0 -

Grade, % - 0 0 - 0 -

Peak Hour Factor 82 82 82 82 82 82

Heavy Vehicles, % 3 3 3 3 3 3

Mvmt Flow 30 94 59 0 7 29

Major/Minor Major1 Major2 Minor2

Conflicting Flow All 59 0 - 0 213 59

Stage 1 - - - - 59 -

Stage 2 - - - - 154 -

Critical Hdwy 4.13 - - - 6.43 6.23

Critical Hdwy Stg 1 - - - - 5.43 -

Critical Hdwy Stg 2 - - - - 5.43 -

Follow-up Hdwy 2.227 - - - 3.527 3.327

Pot Cap-1 Maneuver 1538 - - - 773 1004

Stage 1 - - - - 961 -

Stage 2 - - - - 872 -

Platoon blocked, % - - - -

Mov Cap-1 Maneuver 1538 - - - 757 1004

Mov Cap-2 Maneuver - - - - 757 -

Stage 1 - - - - 941 -

Stage 2 - - - - 872 -

Approach EB WB SB

HCM Control Delay, s 1.8 0 9

HCM LOS A

Minor Lane/Major Mvmt EBL EBT WBT WBR SBLn1




Capacity (veh/h) 1538 - - - 942

HCM Lane V/C Ratio 0.02 - - - 0.039

HCM Control Delay (s) 7.4 0 - - 9

HCM Lane LOS A A - - A

HCM 95th %tile Q(veh) 0.1 - - - 0.1




Intersection						
Int Delay, s/veh	3.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	21	6	4	30	3	4
Future Vol, veh/h	21	6	4	30	3	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	68	68	68	68	68	68
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	31	9	6	44	4	6
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	42	28	0	0	6	0
Stage 1	28	-	-	-	-	-
Stage 2	14	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	967	1044	-	-	1608	-
Stage 1	992	-	-	-	-	-
Stage 2	1006	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	965	1044	-	-	1608	-
Mov Cap-2 Maneuver	965	-	-	-	-	-
Stage 1	992	-	-	-	-	-
Stage 2	1004	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	8.8	0		3.1		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	982	1608	-	
HCM Lane V/C Ratio	-	-	0.04	0.003	-	
HCM Control Delay (s)	-	-	8.8	7.2	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection

Int Delay, s/veh 1.8

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	10	55	66	15	12	15
Future Vol, veh/h	10	55	66	15	12	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	11	63	75	17	14	17




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	92	0	138
Stage 1	-	-	84
Stage 2	-	-	54
Critical Hdwy	4.16	-	6.86
Critical Hdwy Stg 1	-	-	5.86
Critical Hdwy Stg 2	-	-	5.86
Follow-up Hdwy	2.23	-	3.53
Pot Cap-1 Maneuver	1493	-	838
Stage 1	-	-	927
Stage 2	-	-	959
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1493	-	831
Mov Cap-2 Maneuver	-	-	831
Stage 1	-	-	920
Stage 2	-	-	959

Approach	EB	WB	SB
HCM Control Delay, s	1.1	0	9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1493	-	-	-	922
HCM Lane V/C Ratio	0.008	-	-	-	0.033
HCM Control Delay (s)	7.4	0	-	-	9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection

Int Delay, s/veh 0.4

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	92	1	4	63	3	1
Future Vol, veh/h	92	1	4	63	3	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	73	73	73	73	73	73
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	126	1	5	86	4	1




Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	127
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1453
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1453
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.4	9.6
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	795	-	-	1453	-
HCM Lane V/C Ratio	0.007	-	-	0.004	-
HCM Control Delay (s)	9.6	-	-	7.5	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection




Int Delay, s/veh 1.7

Movement	EBL	EBT	WBU	WBT	WBR	SBL	SBR
Lane Configurations							
Traffic Vol, veh/h	22	72	1	55	0	0	13
Future Vol, veh/h	22	72	1	55	0	0	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	-	Yield	-	None
Storage Length	-	-	-	-	-	0	-
Veh in Median Storage, #	-	0	-	0	-	0	-
Grade, %	-	0	-	0	-	0	-
Peak Hour Factor	72	72	72	72	72	72	72
Heavy Vehicles, %	3	3	3	3	3	3	3
Mvmt Flow	31	100	1	76	0	0	18

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	76	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.13	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.227	-	-
Pot Cap-1 Maneuver	1517	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1517	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	1.7		8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1517	-	-	-	982
HCM Lane V/C Ratio	0.02	-	-	-	0.018
HCM Control Delay (s)	7.4	0	-	-	8.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1




Intersection						
Int Delay, s/veh	4.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	25	1	4	25	7	6
Future Vol, veh/h	25	1	4	25	7	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	38	2	6	38	11	9
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	56	25	0	0	6	0
Stage 1	25	-	-	-	-	-
Stage 2	31	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	949	1048	-	-	1608	-
Stage 1	995	-	-	-	-	-
Stage 2	989	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	942	1048	-	-	1608	-
Mov Cap-2 Maneuver	942	-	-	-	-	-
Stage 1	995	-	-	-	-	-
Stage 2	982	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9	0		3.9		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	946	1608	-	
HCM Lane V/C Ratio	-	-	0.042	0.007	-	
HCM Control Delay (s)	-	-	9	7.3	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection




Int Delay, s/veh 0.5




Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	0	70	66	7	1	7
Future Vol, veh/h	0	70	66	7	1	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	0	82	78	8	1	8




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	86	0	0 123 43
Stage 1	-	-	- 82 -
Stage 2	-	-	- 41 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1501	-	- 856 1015
Stage 1	-	-	- 929 -
Stage 2	-	-	- 973 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1501	-	- 856 1015
Mov Cap-2 Maneuver	-	-	- 856 -
Stage 1	-	-	- 929 -
Stage 2	-	-	- 973 -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1501	-	-	-	992
HCM Lane V/C Ratio	-	-	-	-	0.009
HCM Control Delay (s)	0	-	-	-	8.7
HCM Lane LOS	A	-	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	75	7	0	58	7	0
Future Vol, veh/h	75	7	0	58	7	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	88	8	0	68	8	0
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	96	0	160	92
Stage 1	-	-	-	-	92	-
Stage 2	-	-	-	-	68	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1491	-	829	963
Stage 1	-	-	-	-	929	-
Stage 2	-	-	-	-	952	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1491	-	829	963
Mov Cap-2 Maneuver	-	-	-	-	829	-
Stage 1	-	-	-	-	929	-
Stage 2	-	-	-	-	952	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		9.4	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	829	-	-	1491	-	
HCM Lane V/C Ratio	0.01	-	-	-	-	
HCM Control Delay (s)	9.4	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	0	-	-	0	-	

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	22	42	49	0	1	18
Future Vol, veh/h	22	42	49	0	1	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	27	51	60	0	1	22
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	60	0	-	0	165	60
Stage 1	-	-	-	-	60	-
Stage 2	-	-	-	-	105	-
Critical Hdwy	4.13	-	-	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	2.227	-	-	-	3.527	3.327
Pot Cap-1 Maneuver	1537	-	-	-	823	1003
Stage 1	-	-	-	-	960	-
Stage 2	-	-	-	-	917	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1537	-	-	-	808	1003
Mov Cap-2 Maneuver	-	-	-	-	808	-
Stage 1	-	-	-	-	943	-
Stage 2	-	-	-	-	917	-
Approach	EB	WB		SB		
HCM Control Delay, s	2.5	0		8.7		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1537	-	-	-	990	
HCM Lane V/C Ratio	0.017	-	-	-	0.023	
HCM Control Delay (s)	7.4	0	-	-	8.7	
HCM Lane LOS	A	A	-	-	A	
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1	




Intersection						
Int Delay, s/veh	5.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	19	6	4	13	6	3
Future Vol, veh/h	19	6	4	13	6	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	80	80
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	24	8	5	16	8	4
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	33	13	0	0	5	0
Stage 1	13	-	-	-	-	-
Stage 2	20	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	978	1064	-	-	1610	-
Stage 1	1007	-	-	-	-	-
Stage 2	1000	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	973	1064	-	-	1610	-
Mov Cap-2 Maneuver	973	-	-	-	-	-
Stage 1	1007	-	-	-	-	-
Stage 2	995	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	8.7	0		4.8		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	-		993	1610	
HCM Lane V/C Ratio	-	-		0.031	0.005	
HCM Control Delay (s)	-	-		8.7	7.2	
HCM Lane LOS	-	-		A	A	
HCM 95th %tile Q(veh)	-	-		0.1	0	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection




Int Delay, s/veh 0.7




Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	4	43	76	7	4	3
Future Vol, veh/h	4	43	76	7	4	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	61	61	61	61	61	61
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	7	70	125	11	7	5




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	136	0	0 180 68
Stage 1	-	-	- 131 -
Stage 2	-	-	- 49 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1439	-	- 789 978
Stage 1	-	-	- 878 -
Stage 2	-	-	- 964 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1439	-	- 785 978
Mov Cap-2 Maneuver	-	-	- 785 -
Stage 1	-	-	- 874 -
Stage 2	-	-	- 964 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	9.3
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1439	-	-	-	858
HCM Lane V/C Ratio	0.005	-	-	-	0.013
HCM Control Delay (s)	7.5	0	-	-	9.3
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	46	0	0	79	0	0
Future Vol, veh/h	46	0	0	79	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	60	60	60	60	60	60
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	77	0	0	132	0	0
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	77	0	209	77
Stage 1	-	-	-	-	77	-
Stage 2	-	-	-	-	132	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1515	-	777	981
Stage 1	-	-	-	-	943	-
Stage 2	-	-	-	-	892	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1515	-	777	981
Mov Cap-2 Maneuver	-	-	-	-	777	-
Stage 1	-	-	-	-	943	-
Stage 2	-	-	-	-	892	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		0	
HCM LOS					A	
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	-	-	-	1515	-	
HCM Lane V/C Ratio	-	-	-	-	-	
HCM Control Delay (s)	0	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	-	-	-	0	-	

Intersection						
Int Delay, s/veh	1.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	12	37	61	1	1	15
Future Vol, veh/h	12	37	61	1	1	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	63	63	63	63	63	63
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	19	59	97	2	2	24
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	97	0	-	0	195	98
Stage 1	-	-	-	-	98	-
Stage 2	-	-	-	-	97	-
Critical Hdwy	4.13	-	-	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	2.227	-	-	-	3.527	3.327
Pot Cap-1 Maneuver	1490	-	-	-	792	955
Stage 1	-	-	-	-	923	-
Stage 2	-	-	-	-	924	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1490	-	-	-	782	955
Mov Cap-2 Maneuver	-	-	-	-	782	-
Stage 1	-	-	-	-	911	-
Stage 2	-	-	-	-	924	-
Approach	EB	WB		SB		
HCM Control Delay, s	1.8	0		8.9		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1490	-	-	-	942	
HCM Lane V/C Ratio	0.013	-	-	-	0.027	
HCM Control Delay (s)	7.4	0	-	-	8.9	
HCM Lane LOS	A	A	-	-	A	
HCM 95th %tile Q(veh)	0	-	-	-	0.1	

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	18	3	6	19	0	0
Future Vol, veh/h	18	3	6	19	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	52	52	52	52	52	52
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	35	6	12	37	0	0
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	33	31	0	0	12	0
Stage 1	31	-	-	-	-	-
Stage 2	2	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	978	1040	-	-	1600	-
Stage 1	989	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	978	1040	-	-	1600	-
Mov Cap-2 Maneuver	978	-	-	-	-	-
Stage 1	989	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.8	0	0			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	986	1600	-	
HCM Lane V/C Ratio	-	-	0.041	-	-	
HCM Control Delay (s)	-	-	8.8	0	-	
HCM Lane LOS	-	-	A	A	-	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

08/31/2023

Intersection




Int Delay, s/veh 0.8




Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	9	95	70	4	6	4
Future Vol, veh/h	9	95	70	4	6	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	11	113	83	5	7	5




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	88	0	0 165 44
Stage 1	-	-	- 86 -
Stage 2	-	-	- 79 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1498	-	- 807 1013
Stage 1	-	-	- 925 -
Stage 2	-	-	- 932 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1498	-	- 801 1013
Mov Cap-2 Maneuver	-	-	- 801 -
Stage 1	-	-	- 918 -
Stage 2	-	-	- 932 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	9.2
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1498	-	-	-	874
HCM Lane V/C Ratio	0.007	-	-	-	0.014
HCM Control Delay (s)	7.4	0	-	-	9.2
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	101	1	1	72	1	1
Future Vol, veh/h	101	1	1	72	1	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	128	1	1	91	1	1
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	129	0	222	129
Stage 1	-	-	-	-	129	-
Stage 2	-	-	-	-	93	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1451	-	764	918
Stage 1	-	-	-	-	894	-
Stage 2	-	-	-	-	928	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1451	-	763	918
Mov Cap-2 Maneuver	-	-	-	-	763	-
Stage 1	-	-	-	-	894	-
Stage 2	-	-	-	-	927	-
Approach	EB	WB		NB		
HCM Control Delay, s	0	0.1		9.3		
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	833	-	-	1451	-	
HCM Lane V/C Ratio	0.003	-	-	0.001	-	
HCM Control Delay (s)	9.3	-	-	7.5	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0	-	-	0	-	




Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	25	77	48	0	6	24
Future Vol, veh/h	25	77	48	0	6	24
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	30	94	59	0	7	29
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	59	0	-	0	213	59
Stage 1	-	-	-	-	59	-
Stage 2	-	-	-	-	154	-
Critical Hdwy	4.13	-	-	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	2.227	-	-	-	3.527	3.327
Pot Cap-1 Maneuver	1538	-	-	-	773	1004
Stage 1	-	-	-	-	961	-
Stage 2	-	-	-	-	872	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1538	-	-	-	757	1004
Mov Cap-2 Maneuver	-	-	-	-	757	-
Stage 1	-	-	-	-	941	-
Stage 2	-	-	-	-	872	-
Approach	EB	WB		SB		
HCM Control Delay, s	1.8	0		9		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1538	-	-	-	942	
HCM Lane V/C Ratio	0.02	-	-	-	0.039	
HCM Control Delay (s)	7.4	0	-	-	9	
HCM Lane LOS	A	A	-	-	A	
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1	

Intersection						
Int Delay, s/veh	3.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	21	6	4	30	3	4
Future Vol, veh/h	21	6	4	30	3	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	68	68	68	68	68	68
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	31	9	6	44	4	6
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	42	28	0	0	6	0
Stage 1	28	-	-	-	-	-
Stage 2	14	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	967	1044	-	-	1608	-
Stage 1	992	-	-	-	-	-
Stage 2	1006	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	965	1044	-	-	1608	-
Mov Cap-2 Maneuver	965	-	-	-	-	-
Stage 1	992	-	-	-	-	-
Stage 2	1004	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.8	0	3.1			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	982	1608	-	
HCM Lane V/C Ratio	-	-	0.04	0.003	-	
HCM Control Delay (s)	-	-	8.8	7.2	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

Intersection						
Int Delay, s/veh	1.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕↕	↕↕		↕↕	
Traffic Vol, veh/h	10	55	66	15	12	15
Future Vol, veh/h	10	55	66	15	12	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	11	63	75	17	14	17
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	92	0	-	0	138	46
Stage 1	-	-	-	-	84	-
Stage 2	-	-	-	-	54	-
Critical Hdwy	4.16	-	-	-	6.86	6.96
Critical Hdwy Stg 1	-	-	-	-	5.86	-
Critical Hdwy Stg 2	-	-	-	-	5.86	-
Follow-up Hdwy	2.23	-	-	-	3.53	3.33
Pot Cap-1 Maneuver	1493	-	-	-	838	1010
Stage 1	-	-	-	-	927	-
Stage 2	-	-	-	-	959	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1493	-	-	-	831	1010
Mov Cap-2 Maneuver	-	-	-	-	831	-
Stage 1	-	-	-	-	920	-
Stage 2	-	-	-	-	959	-
Approach	EB	WB		SB		
HCM Control Delay, s	1.1	0		9		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1493	-	-	-	922	
HCM Lane V/C Ratio	0.008	-	-	-	0.033	
HCM Control Delay (s)	7.4	0	-	-	9	
HCM Lane LOS	A	A	-	-	A	
HCM 95th %tile Q(veh)	0	-	-	-	0.1	

Intersection




Int Delay, s/veh 0.4




Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	92	1	4	63	3	1
Future Vol, veh/h	92	1	4	63	3	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	73	73	73	73	73	73
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	126	1	5	86	4	1

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	127
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1453
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1453
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.4	9.6
HCM LOS			A




Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	795	-	-	1453	-
HCM Lane V/C Ratio	0.007	-	-	0.004	-
HCM Control Delay (s)	9.6	-	-	7.5	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection							
Int Delay, s/veh	1.7						
Movement	EBL	EBT	WBU	WBT	WBR	SBL	SBR
Lane Configurations							
Traffic Vol, veh/h	22	72	1	55	0	0	13
Future Vol, veh/h	22	72	1	55	0	0	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	-	Yield	-	None
Storage Length	-	-	-	-	-	0	-
Veh in Median Storage, #	-	0	-	0	-	0	-
Grade, %	-	0	-	0	-	0	-
Peak Hour Factor	72	72	72	72	72	72	72
Heavy Vehicles, %	3	3	3	3	3	3	3
Mvmt Flow	31	100	1	76	0	0	18
Major/Minor	Major1	Major2		Minor2			
Conflicting Flow All	76	0	-	-	0	238	76
Stage 1	-	-	-	-	-	76	-
Stage 2	-	-	-	-	-	162	-
Critical Hdwy	4.13	-	-	-	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	-	5.43	-
Follow-up Hdwy	2.227	-	-	-	-	3.527	3.327
Pot Cap-1 Maneuver	1517	-	-	-	-	748	982
Stage 1	-	-	-	-	-	944	-
Stage 2	-	-	-	-	-	865	-
Platoon blocked, %		-		-	-		
Mov Cap-1 Maneuver	1517	-	-	-	-	732	982
Mov Cap-2 Maneuver	-	-	-	-	-	732	-
Stage 1	-	-	-	-	-	923	-
Stage 2	-	-	-	-	-	865	-
Approach	EB	WB		SB			
HCM Control Delay, s	1.7			8.7			
HCM LOS				A			
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1		
Capacity (veh/h)	1517	-	-	-	982		
HCM Lane V/C Ratio	0.02	-	-	-	0.018		
HCM Control Delay (s)	7.4	0	-	-	8.7		
HCM Lane LOS	A	A	-	-	A		
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1		

Intersection						
Int Delay, s/veh	4.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	25	1	4	25	7	6
Future Vol, veh/h	25	1	4	25	7	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	38	2	6	38	11	9
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	56	25	0	0	6	0
Stage 1	25	-	-	-	-	-
Stage 2	31	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	949	1048	-	-	1608	-
Stage 1	995	-	-	-	-	-
Stage 2	989	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	942	1048	-	-	1608	-
Mov Cap-2 Maneuver	942	-	-	-	-	-
Stage 1	995	-	-	-	-	-
Stage 2	982	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9	0		3.9		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	-		946	1608	
HCM Lane V/C Ratio	-	-		0.042	0.007	
HCM Control Delay (s)	-	-		9	7.3	
HCM Lane LOS	-	-		A	A	
HCM 95th %tile Q(veh)	-	-		0.1	0	

Intersection

Int Delay, s/veh 0.5

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	0	70	66	7	1	7
Future Vol, veh/h	0	70	66	7	1	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	0	82	78	8	1	8




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	86	0	0 123 43
Stage 1	-	-	- 82 -
Stage 2	-	-	- 41 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1501	-	- 856 1015
Stage 1	-	-	- 929 -
Stage 2	-	-	- 973 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1501	-	- 856 1015
Mov Cap-2 Maneuver	-	-	- 856 -
Stage 1	-	-	- 929 -
Stage 2	-	-	- 973 -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1501	-	-	-	992
HCM Lane V/C Ratio	-	-	-	-	0.009
HCM Control Delay (s)	0	-	-	-	8.7
HCM Lane LOS	A	-	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection

Int Delay, s/veh 0.4

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	75	7	0	58	7	0
Future Vol, veh/h	75	7	0	58	7	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	88	8	0	68	8	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	96
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1491
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1491
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	9.4
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	829	-	-	1491	-
HCM Lane V/C Ratio	0.01	-	-	-	-
HCM Control Delay (s)	9.4	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection

Int Delay, s/veh 2.5

Movement EBL EBT WBT WBR SBL SBRLane Configurations 

Traffic Vol, veh/h 22 42 49 0 1 18

Future Vol, veh/h 22 42 49 0 1 18

Conflicting Peds, #/hr 0 0 0 0 0 0

Sign Control Free Free Free Free Stop Stop

RT Channelized - None - Yield - None

Storage Length - - - - 0 -

Veh in Median Storage, # - 0 0 - 0 -

Grade, % - 0 0 - 0 -

Peak Hour Factor 82 82 82 82 82 82

Heavy Vehicles, % 3 3 3 3 3 3

Mvmt Flow 27 51 60 0 1 22

Major/Minor Major1 Major2 Minor2

Conflicting Flow All 60 0 - 0 165 60

Stage 1 - - - - 60 -

Stage 2 - - - - 105 -

Critical Hdwy 4.13 - - - 6.43 6.23

Critical Hdwy Stg 1 - - - - 5.43 -

Critical Hdwy Stg 2 - - - - 5.43 -

Follow-up Hdwy 2.227 - - - 3.527 3.327

Pot Cap-1 Maneuver 1537 - - - 823 1003

Stage 1 - - - - 960 -

Stage 2 - - - - 917 -

Platoon blocked, % - - - -

Mov Cap-1 Maneuver 1537 - - - 808 1003

Mov Cap-2 Maneuver - - - - 808 -

Stage 1 - - - - 943 -

Stage 2 - - - - 917 -

Approach EB WB SB

HCM Control Delay, s 2.5 0 8.7

HCM LOS A

Minor Lane/Major Mvmt EBL EBT WBT WBR SBLn1

Capacity (veh/h) 1537 - - - 990

HCM Lane V/C Ratio 0.017 - - - 0.023




HCM Control Delay (s) 7.4 0 - - 8.7

HCM Lane LOS A A - - A

HCM 95th %tile Q(veh) 0.1 - - - 0.1

Intersection

Int Delay, s/veh 5.1

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	19	6	4	13	6	3
Future Vol, veh/h	19	6	4	13	6	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	80	80
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	24	8	5	16	8	4

Major/Minor	Minor1	Major1	Major2
Conflicting Flow All	33	13	0
Stage 1	13	-	-
Stage 2	20	-	-
Critical Hdwy	6.43	6.23	-
Critical Hdwy Stg 1	5.43	-	-
Critical Hdwy Stg 2	5.43	-	-
Follow-up Hdwy	3.527	3.327	-
Pot Cap-1 Maneuver	978	1064	-
Stage 1	1007	-	-
Stage 2	1000	-	-
Platoon blocked, %		-	-
Mov Cap-1 Maneuver	973	1064	-
Mov Cap-2 Maneuver	973	-	-
Stage 1	1007	-	-
Stage 2	995	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.7	0	4.8
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	993	1610
HCM Lane V/C Ratio	-	-	0.031	0.005
HCM Control Delay (s)	-	-	8.7	7.2
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

08/31/2023

Intersection




Int Delay, s/veh 0.7




Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	4	43	76	7	4	3
Future Vol, veh/h	4	43	76	7	4	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	61	61	61	61	61	61
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	7	70	125	11	7	5




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	136	0	0 180 68
Stage 1	-	-	- 131 -
Stage 2	-	-	- 49 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1439	-	- 789 978
Stage 1	-	-	- 878 -
Stage 2	-	-	- 964 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1439	-	- 785 978
Mov Cap-2 Maneuver	-	-	- 785 -
Stage 1	-	-	- 874 -
Stage 2	-	-	- 964 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	9.3
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1439	-	-	-	858
HCM Lane V/C Ratio	0.005	-	-	-	0.013
HCM Control Delay (s)	7.5	0	-	-	9.3
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0




Intersection						
Int Delay, s/veh	0					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	46	0	0	79	0	0
Future Vol, veh/h	46	0	0	79	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	60	60	60	60	60	60
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	77	0	0	132	0	0
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	77	0	209	77
Stage 1	-	-	-	-	77	-
Stage 2	-	-	-	-	132	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1515	-	777	981
Stage 1	-	-	-	-	943	-
Stage 2	-	-	-	-	892	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1515	-	777	981
Mov Cap-2 Maneuver	-	-	-	-	777	-
Stage 1	-	-	-	-	943	-
Stage 2	-	-	-	-	892	-
Approach	EB	WB		NB		
HCM Control Delay, s	0	0		0		
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	-	-	-	1515	-	
HCM Lane V/C Ratio	-	-	-	-	-	
HCM Control Delay (s)	0	-	-	0	-	
HCM Lane LOS	A	-	-	A	-	
HCM 95th %tile Q(veh)	-	-	-	0	-	

Intersection						
Int Delay, s/veh	1.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	12	37	61	1	1	15
Future Vol, veh/h	12	37	61	1	1	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	63	63	63	63	63	63
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	19	59	97	2	2	24
Major/Minor	Major1	Major2		Minor2		
Conflicting Flow All	97	0	-	0	195	98
Stage 1	-	-	-	-	98	-
Stage 2	-	-	-	-	97	-
Critical Hdwy	4.13	-	-	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	2.227	-	-	-	3.527	3.327
Pot Cap-1 Maneuver	1490	-	-	-	792	955
Stage 1	-	-	-	-	923	-
Stage 2	-	-	-	-	924	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1490	-	-	-	782	955
Mov Cap-2 Maneuver	-	-	-	-	782	-
Stage 1	-	-	-	-	911	-
Stage 2	-	-	-	-	924	-
Approach	EB	WB		SB		
HCM Control Delay, s	1.8	0		8.9		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1490	-	-	-	942	
HCM Lane V/C Ratio	0.013	-	-	-	0.027	
HCM Control Delay (s)	7.4	0	-	-	8.9	
HCM Lane LOS	A	A	-	-	A	
HCM 95th %tile Q(veh)	0	-	-	-	0.1	

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	18	3	6	19	0	0
Future Vol, veh/h	18	3	6	19	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	52	52	52	52	52	52
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	35	6	12	37	0	0
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	33	31	0	0	12	0
Stage 1	31	-	-	-	-	-
Stage 2	2	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	978	1040	-	-	1600	-
Stage 1	989	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	978	1040	-	-	1600	-
Mov Cap-2 Maneuver	978	-	-	-	-	-
Stage 1	989	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.8	0	0			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	986	1600	-	
HCM Lane V/C Ratio	-	-	0.041	-	-	
HCM Control Delay (s)	-	-	8.8	0	-	
HCM Lane LOS	-	-	A	A	-	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

Intersection




Int Delay, s/veh 0.8




Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	9	95	70	4	6	4
Future Vol, veh/h	9	95	70	4	6	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	84	84	84	84	84	84
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	11	113	83	5	7	5

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	88	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.16	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.23	-	-
Pot Cap-1 Maneuver	1498	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1498	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	9.2
HCM LOS			A




Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1498	-	-	-	874
HCM Lane V/C Ratio	0.007	-	-	-	0.014
HCM Control Delay (s)	7.4	0	-	-	9.2
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	0.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	101	1	1	72	1	1
Future Vol, veh/h	101	1	1	72	1	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	128	1	1	91	1	1
Major/Minor	Major1	Major2		Minor1		
Conflicting Flow All	0	0	129	0	222	129
Stage 1	-	-	-	-	129	-
Stage 2	-	-	-	-	93	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1451	-	764	918
Stage 1	-	-	-	-	894	-
Stage 2	-	-	-	-	928	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1451	-	763	918
Mov Cap-2 Maneuver	-	-	-	-	763	-
Stage 1	-	-	-	-	894	-
Stage 2	-	-	-	-	927	-
Approach	EB	WB		NB		
HCM Control Delay, s	0	0.1		9.3		
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	833	-	-	1451	-	
HCM Lane V/C Ratio	0.003	-	-	0.001	-	
HCM Control Delay (s)	9.3	-	-	7.5	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0	-	-	0	-	

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	25	77	48	0	6	24
Future Vol, veh/h	25	77	48	0	6	24
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	30	94	59	0	7	29
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	59	0	-	0	213	59
Stage 1	-	-	-	-	59	-
Stage 2	-	-	-	-	154	-
Critical Hdwy	4.13	-	-	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	2.227	-	-	-	3.527	3.327
Pot Cap-1 Maneuver	1538	-	-	-	773	1004
Stage 1	-	-	-	-	961	-
Stage 2	-	-	-	-	872	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1538	-	-	-	757	1004
Mov Cap-2 Maneuver	-	-	-	-	757	-
Stage 1	-	-	-	-	941	-
Stage 2	-	-	-	-	872	-
Approach	EB	WB		SB		
HCM Control Delay, s	1.8	0		9		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1538	-	-	-	942	
HCM Lane V/C Ratio	0.02	-	-	-	0.039	
HCM Control Delay (s)	7.4	0	-	-	9	
HCM Lane LOS	A	A	-	-	A	
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1	

Intersection

Int Delay, s/veh 3.8

Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	21	6	4	30	3	4
Future Vol, veh/h	21	6	4	30	3	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	68	68	68	68	68	68
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	31	9	6	44	4	6




Major/Minor	Minor1	Major1	Major2
Conflicting Flow All	42	28	0
Stage 1	28	-	-
Stage 2	14	-	-
Critical Hdwy	6.43	6.23	-
Critical Hdwy Stg 1	5.43	-	-
Critical Hdwy Stg 2	5.43	-	-
Follow-up Hdwy	3.527	3.327	-
Pot Cap-1 Maneuver	967	1044	-
Stage 1	992	-	-
Stage 2	1006	-	-
Platoon blocked, %		-	-
Mov Cap-1 Maneuver	965	1044	-
Mov Cap-2 Maneuver	965	-	-
Stage 1	992	-	-
Stage 2	1004	-	-

Approach	WB	NB	SB
HCM Control Delay, s	8.8	0	3.1
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	982	1608
HCM Lane V/C Ratio	-	-	0.04	0.003
HCM Control Delay (s)	-	-	8.8	7.2
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0

Intersection

Int Delay, s/veh 1.8

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	10	55	66	15	12	15
Future Vol, veh/h	10	55	66	15	12	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	11	63	75	17	14	17




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	92	0	0 138 46
Stage 1	-	-	- 84 -
Stage 2	-	-	- 54 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1493	-	- 838 1010
Stage 1	-	-	- 927 -
Stage 2	-	-	- 959 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1493	-	- 831 1010
Mov Cap-2 Maneuver	-	-	- 831 -
Stage 1	-	-	- 920 -
Stage 2	-	-	- 959 -

Approach	EB	WB	SB
HCM Control Delay, s	1.1	0	9
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1493	-	-	-	922
HCM Lane V/C Ratio	0.008	-	-	-	0.033
HCM Control Delay (s)	7.4	0	-	-	9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Intersection

Int Delay, s/veh 0.4

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	92	1	4	63	3	1
Future Vol, veh/h	92	1	4	63	3	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	73	73	73	73	73	73
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	126	1	5	86	4	1




Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	127
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1453
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1453
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.4	9.6
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	795	-	-	1453	-
HCM Lane V/C Ratio	0.007	-	-	0.004	-
HCM Control Delay (s)	9.6	-	-	7.5	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Intersection




Int Delay, s/veh 1.7

Movement	EBL	EBT	WBU	WBT	WBR	SBL	SBR
Lane Configurations							
Traffic Vol, veh/h	22	72	1	55	0	0	13
Future Vol, veh/h	22	72	1	55	0	0	13
Conflicting Peds, #/hr	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	-	Yield	-	None
Storage Length	-	-	-	-	-	0	-
Veh in Median Storage, #	-	0	-	0	-	0	-
Grade, %	-	0	-	0	-	0	-
Peak Hour Factor	72	72	72	72	72	72	72
Heavy Vehicles, %	3	3	3	3	3	3	3
Mvmt Flow	31	100	1	76	0	0	18

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	76	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.13	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.227	-	-
Pot Cap-1 Maneuver	1517	-	-
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1517	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	1.7		8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1517	-	-	-	982
HCM Lane V/C Ratio	0.02	-	-	-	0.018
HCM Control Delay (s)	7.4	0	-	-	8.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1




Intersection						
Int Delay, s/veh	4.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	25	1	4	25	7	6
Future Vol, veh/h	25	1	4	25	7	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	65	65	65	65	65	65
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	38	2	6	38	11	9
Major/Minor	Minor1	Major1		Major2		
Conflicting Flow All	56	25	0	0	6	0
Stage 1	25	-	-	-	-	-
Stage 2	31	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	949	1048	-	-	1608	-
Stage 1	995	-	-	-	-	-
Stage 2	989	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	942	1048	-	-	1608	-
Mov Cap-2 Maneuver	942	-	-	-	-	-
Stage 1	995	-	-	-	-	-
Stage 2	982	-	-	-	-	-
Approach	WB	NB		SB		
HCM Control Delay, s	9	0		3.9		
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1		SBL	SBT	
Capacity (veh/h)	-	- 946		1608	-	
HCM Lane V/C Ratio	-	- 0.042		0.007	-	
HCM Control Delay (s)	-	- 9		7.3	0	
HCM Lane LOS	-	- A		A	A	
HCM 95th %tile Q(veh)	-	- 0.1		0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection




Int Delay, s/veh 0.5

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	0	70	66	7	1	7
Future Vol, veh/h	0	70	66	7	1	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	0	82	78	8	1	8

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	86	0	0 123 43
Stage 1	-	-	- 82 -
Stage 2	-	-	- 41 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1501	-	- 856 1015
Stage 1	-	-	- 929 -
Stage 2	-	-	- 973 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1501	-	- 856 1015
Mov Cap-2 Maneuver	-	-	- 856 -
Stage 1	-	-	- 929 -
Stage 2	-	-	- 973 -




Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1501	-	-	-	992
HCM Lane V/C Ratio	-	-	-	-	0.009
HCM Control Delay (s)	0	-	-	-	8.7
HCM Lane LOS	A	-	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	75	7	0	58	7	0
Future Vol, veh/h	75	7	0	58	7	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	85	85	85	85	85	85
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	88	8	0	68	8	0
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	96	0	160	92
Stage 1	-	-	-	-	92	-
Stage 2	-	-	-	-	68	-
Critical Hdwy	-	-	4.13	-	6.43	6.23
Critical Hdwy Stg 1	-	-	-	-	5.43	-
Critical Hdwy Stg 2	-	-	-	-	5.43	-
Follow-up Hdwy	-	-	2.227	-	3.527	3.327
Pot Cap-1 Maneuver	-	-	1491	-	829	963
Stage 1	-	-	-	-	929	-
Stage 2	-	-	-	-	952	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1491	-	829	963
Mov Cap-2 Maneuver	-	-	-	-	829	-
Stage 1	-	-	-	-	929	-
Stage 2	-	-	-	-	952	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		0		9.4	
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1		EBT	EBR	WBL	WBT
Capacity (veh/h)	829		-	-	1491	-
HCM Lane V/C Ratio	0.01		-	-	-	-
HCM Control Delay (s)	9.4		-	-	0	-
HCM Lane LOS	A		-	-	A	-
HCM 95th %tile Q(veh)	0		-	-	0	-

Intersection




Int Delay, s/veh 2.5

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	22	42	49	0	1	18
Future Vol, veh/h	22	42	49	0	1	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	Yield	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	82	82	82	82	82	82
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	27	51	60	0	1	22

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	60	0	0 165 60
Stage 1	-	-	- 60 -
Stage 2	-	-	- 105 -
Critical Hdwy	4.13	-	- 6.43 6.23
Critical Hdwy Stg 1	-	-	- 5.43 -
Critical Hdwy Stg 2	-	-	- 5.43 -
Follow-up Hdwy	2.227	-	- 3.527 3.327
Pot Cap-1 Maneuver	1537	-	- 823 1003
Stage 1	-	-	- 960 -
Stage 2	-	-	- 917 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1537	-	- 808 1003
Mov Cap-2 Maneuver	-	-	- 808 -
Stage 1	-	-	- 943 -
Stage 2	-	-	- 917 -

Approach	EB	WB	SB
HCM Control Delay, s	2.5	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1537	-	-	-	990
HCM Lane V/C Ratio	0.017	-	-	-	0.023
HCM Control Delay (s)	7.4	0	-	-	8.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1




Intersection						
Int Delay, s/veh	5.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	19	6	4	13	6	3
Future Vol, veh/h	19	6	4	13	6	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	80	80	80	80	80	80
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	24	8	5	16	8	4
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	33	13	0	0	5	0
Stage 1	13	-	-	-	-	-
Stage 2	20	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	978	1064	-	-	1610	-
Stage 1	1007	-	-	-	-	-
Stage 2	1000	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	973	1064	-	-	1610	-
Mov Cap-2 Maneuver	973	-	-	-	-	-
Stage 1	1007	-	-	-	-	-
Stage 2	995	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.7	0	4.8			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	993	1610	-	
HCM Lane V/C Ratio	-	-	0.031	0.005	-	
HCM Control Delay (s)	-	-	8.7	7.2	0	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

HCM 6th TWSC
1: Natchez Trace Parkway & Colbert Ferry Road

06/02/2023

Intersection

Int Delay, s/veh 0.7

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	4	43	76	7	4	3
Future Vol, veh/h	4	43	76	7	4	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	61	61	61	61	61	61
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	7	70	125	11	7	5




Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	136	0	0 180 68
Stage 1	-	-	- 131 -
Stage 2	-	-	- 49 -
Critical Hdwy	4.16	-	- 6.86 6.96
Critical Hdwy Stg 1	-	-	- 5.86 -
Critical Hdwy Stg 2	-	-	- 5.86 -
Follow-up Hdwy	2.23	-	- 3.53 3.33
Pot Cap-1 Maneuver	1439	-	- 789 978
Stage 1	-	-	- 878 -
Stage 2	-	-	- 964 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1439	-	- 785 978
Mov Cap-2 Maneuver	-	-	- 785 -
Stage 1	-	-	- 874 -
Stage 2	-	-	- 964 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	9.3
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1439	-	-	-	858
HCM Lane V/C Ratio	0.005	-	-	-	0.013
HCM Control Delay (s)	7.5	0	-	-	9.3
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection

Int Delay, s/veh 0

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	46	0	0	79	0	0
Future Vol, veh/h	46	0	0	79	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	60	60	60	60	60	60
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	77	0	0	132	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	77
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	4.13
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	2.227
Pot Cap-1 Maneuver	-	-	1515
Stage 1	-	-	-
Stage 2	-	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	1515
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	-	-	-	1515	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	-	0	-
HCM Lane LOS	A	-	-	A	-
HCM 95th %tile Q(veh)	-	-	-	0	-

Intersection

Int Delay, s/veh 1.8

Movement EBL EBT WBT WBR SBL SBRLane Configurations 

Traffic Vol, veh/h 12 37 61 1 1 15

Future Vol, veh/h 12 37 61 1 1 15

Conflicting Peds, #/hr 0 0 0 0 0 0

Sign Control Free Free Free Free Stop Stop

RT Channelized - None - Yield - None

Storage Length - - - - 0 -

Veh in Median Storage, # - 0 0 - 0 -

Grade, % - 0 0 - 0 -

Peak Hour Factor 63 63 63 63 63 63

Heavy Vehicles, % 3 3 3 3 3 3

Mvmt Flow 19 59 97 2 2 24

Major/Minor Major1 Major2 Minor2

Conflicting Flow All 97 0 - 0 195 98

Stage 1 - - - - 98 -

Stage 2 - - - - 97 -

Critical Hdwy 4.13 - - - 6.43 6.23

Critical Hdwy Stg 1 - - - - 5.43 -

Critical Hdwy Stg 2 - - - - 5.43 -

Follow-up Hdwy 2.227 - - - 3.527 3.327

Pot Cap-1 Maneuver 1490 - - - 792 955

Stage 1 - - - - 923 -

Stage 2 - - - - 924 -

Platoon blocked, % - - - -

Mov Cap-1 Maneuver 1490 - - - 782 955

Mov Cap-2 Maneuver - - - - 782 -

Stage 1 - - - - 911 -

Stage 2 - - - - 924 -

Approach EB WB SB

HCM Control Delay, s 1.8 0 8.9

HCM LOS A

Minor Lane/Major Mvmt EBL EBT WBT WBR SBLn1




Capacity (veh/h) 1490 - - - 942

HCM Lane V/C Ratio 0.013 - - - 0.027

HCM Control Delay (s) 7.4 0 - - 8.9

HCM Lane LOS A A - - A

HCM 95th %tile Q(veh) 0 - - - 0.1

Intersection						
Int Delay, s/veh	3.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	18	3	6	19	0	0
Future Vol, veh/h	18	3	6	19	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	Yield	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	52	52	52	52	52	52
Heavy Vehicles, %	3	3	3	3	3	3
Mvmt Flow	35	6	12	37	0	0
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	33	31	0	0	12	0
Stage 1	31	-	-	-	-	-
Stage 2	2	-	-	-	-	-
Critical Hdwy	6.43	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.327	-	-	2.227	-
Pot Cap-1 Maneuver	978	1040	-	-	1600	-
Stage 1	989	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	978	1040	-	-	1600	-
Mov Cap-2 Maneuver	978	-	-	-	-	-
Stage 1	989	-	-	-	-	-
Stage 2	1019	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	8.8	0	0			
HCM LOS	A					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	986	1600	-	
HCM Lane V/C Ratio	-	-	0.041	-	-	
HCM Control Delay (s)	-	-	8.8	0	-	
HCM Lane LOS	-	-	A	A	-	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

Appendix E:
Volume Development

Natchez Trace Parkway - John Coffee Memorial Bridge (Tennessee River Bridge) - Environmental Assessment - Traffic Study

Table 1. Growth Rate Comparison - Historic and Applied

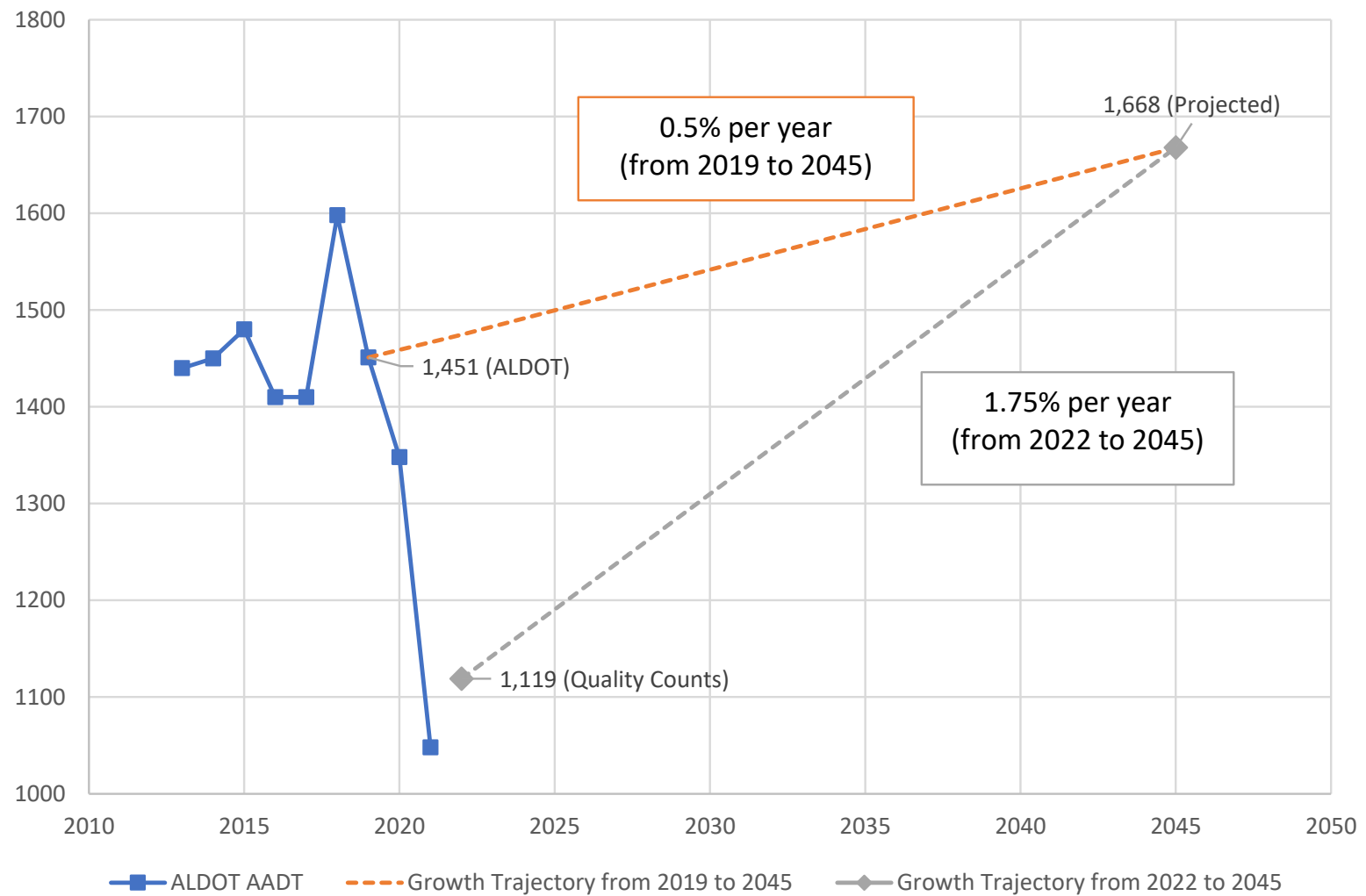
ROUTE	COUNTY and STATION NUMBER	LOCATION	ALDOT AADT ¹									Annual Growth Rate based on Historic AADT		Applied Annual Growth Rate ²	ADT		Projected ADT		Effective Annual Growth Rate
			2013	2014	2015	2016	2017	2018	2019	2020	2021	2013 - 2021 8 year	2017 - 2021 4 year		2022 Aug	2022 Nov	2045 Aug	2045 Nov	2019 - 2045 26 year
ALDOT Data																			
Natchez Trace Parkway	Lauderdale (636)	NORTH of Lauderdale County Route 14	770	780	800	740	740	955	720	688	683	-1.49%	-1.98%	1.75%	--	--	--	--	--
Natchez Trace Parkway	Lauderdale (637)	NORTH of Colbert Creek	940	950	970	980	980	1242	1022	948	985	0.59%	0.13%	1.75%	--	--	--	--	--
Natchez Trace Parkway	Lauderdale (638)	EAST of Tennessee River	1440	1450	1480	1410	1410	1598	1451	1348	1048	-3.89%	-7.15%	1.75%	1119	864	1668	1288	0.54%
Lauderdale County Route 14	Lauderdale (963)	EAST of Natchez Trace Parkway	1340	1350	1380	1080	910	951	1516	1376	1420	0.73%	11.77%	N/A	--	--	--	--	--
Lauderdale County Route 14	Lauderdale (906)	WEST of Natchez Trace Parkway	1340	1350	1380	1040	890	930	1308	1153	1215	-1.22%	8.09%	N/A	--	--	--	--	--
Natchez Trace Parkway	Colbert (8)	WEST of Colbert County Route 21	800	720	730	690	690	721	714	653	698	-1.69%	0.29%	1.75%	--	--	--	--	--
Natchez Trace Parkway	Colbert (502)	EAST of Colbert County Route 21	800	720	730	690	690	721	714	653	698	-1.69%	0.29%	1.75%	--	--	--	--	--
Colbert County Route 21	Colbert (951)	SOUTH of Natchez Trace Parkway	890	880	900	880	880	900	795	812	881	-0.13%	0.03%	N/A	--	--	--	--	--
Colbert County Route 21	Colbert (952)	NORTH of Natchez Trace Parkway	880	210	220	170	170	170	170	200	197	-17.06%	3.75%	N/A	--	--	--	--	--
Average Area AADT			1022	934	954	853	818	910	934	870	869	-2.01%	1.52%	1.75%	--	--	--	--	--

Notes:

1 - Data prior to 2013 is not available for most roadways in this dataset.

2 - Annual growth rate is applicable to Natchez Trace Parkway and turning movements for its intersections.

Historic and Projected ADTs



APPENDIX H:
FLOODPLAIN STATEMENT OF FINDINGS

**United States Department of the Interior
National Park Service**

Natchez Trace Parkway

**Replacement of the John Coffee Memorial Bridge
(Tennessee River Bridge)**

Statement of Findings for Director's Order #77-2:
Floodplain Management

August 2023

**Statement of Findings for Protection of Floodplains
(Executive Order 11988)
Replacement of the John Coffee Memorial Bridge
(Tennessee River Bridge)
August 2023**

**National Park Service
Natchez Trace Parkway**

Recommended:

Superintendent	Signature	Date
----------------	-----------	------

Certification of Technical Adequacy and Servicewide Consistency:

Chief, NPS Water Resources Division	Signature	Date
--	-----------	------

Approved:

Regional Director	Signature	Date
-------------------	-----------	------

CONTENTS

1.0 INTRODUCTION	1
2.0 PROJECT DESCRIPTION.....	2
2.1 Background.....	2
2.2 Project Location.....	2
Study Area.....	2
Project Area.....	3
2.3 Purpose and Need.....	7
2.4 Proposed Action.....	7
Construction	7
Operations and Maintenance	8
General Natural Resources Conservation Measures	9
3.0 PROJECT AREA DESCRIPTION.....	10
3.1 Existing Conditions.....	10
Topography	10
Hydrology	10
Vegetation	10
Geology and Soils.....	11
3.2 Floodplains in the Project Area.....	12
500-year Floodplain.....	16
Freeboard Value Approach	16
Summary	16
3.3 Floodplain Values	17
3.4 Flood Risk in the Project Area.....	18
Recurrence Interval of Flooding in the Project Area	18
Hydraulics of Flooding in the Project Area.....	18
Time Required for Flooding to Occur	20
Opportunity for Evaluation of the Project Area in the Event of Flooding	20
Geomorphic Considerations.....	20
3.5 Action Class.....	21
4.0 JUSTIFICATION FOR THE USE OF FLOODPLAINS.....	22
4.1 Impacts of the Proposed Action.....	22
5.0 MITIGATION	23
6.0 CONCLUSIONS.....	24
7.0 REFERENCES	25

LIST OF FIGURES

Figure 1. Project study area	4
Figure 2. Site vicinity map.....	5
Figure 3. Proposed alternatives for bridge replacement	6
Figure 4A. Floodplain evaluation map	13
Figure 4B. Floodplain evaluation map (east).....	14
Figure 4C. Floodplain evaluation map (west).....	15
Figure 5A. Annual reservoir elevation at Wilson Dam	19
Figure 5B. Annual reservoir elevation at Pickwick Landing Dam.....	20

LIST OF TABLES

Table 1. Construction disturbance under the proposed action (includes the two alternative alignments).....	8
Table 2. Dominant plant species in study area.....	11

ACRONYMS AND ABBREVIATIONS

AMSL	above mean sea level
BFE	Base Flood Elevation
ESRI	Environmental Systems Research Institute, Inc.
FEMA	Federal Emergency Management Agency
FFRMS	Federal Flood Risk Management Standard
FHWA	Federal Highway Administration
FIS	Flood Insurance Study
FSOF	Floodplains Statement of Findings
HUC	Hydrologic Unit Code
NFHL	National Flood Hazard Layer
NPS	National Park Service
NWS	National Weather Service
TVA	Tennessee Valley Authority
UTM	Universal Transverse Mercator
WSP	WSP USA Inc.

1.0 INTRODUCTION

The National Park Service (NPS) is proposing to replace the John Coffee Memorial Bridge (Tennessee River Bridge or bridge) along the Natchez Trace Parkway (Parkway), spanning the Tennessee River, approximately 6 miles north of Cherokee, Alabama (project or proposed action). The purpose of the project is to maintain a safe and reliable bridge crossing for users over the Tennessee River, while minimizing adverse effects on Parkway resources. The project is needed because this bridge, which opened in 1964, has exceeded its 50-year design life. The bridge has not undergone a major rehabilitation since 1964, but several temporary repairs have kept it operational and safe. The bridge is regularly inspected by the Federal Highway Administration (FHWA) in accordance with generally recognized structural engineering guidelines and standards to confirm that it is safe for travel. The bridge lacks redundancy in its design, making it fracture critical, and detailed structural inspections and studies note that the bridge exhibits widespread cracking in the bridge deck and notable deterioration of the bridge piers. As the bridge deteriorates beyond the ability of routine maintenance to address structural issues, and ultimately no longer meets bridge safety requirements, the NPS and FHWA would be required to close the bridge to vehicular traffic. To avoid the impacts of long term or permanent bridge closure, the bridge needs to be replaced to provide a structurally sound vehicular bridge crossing the Tennessee River.

Executive Order 11988, “Floodplain Management,” requires the NPS and other federal agencies to evaluate the likely impacts of actions in floodplains. The objectives of the executive order are: (1) to avoid, to the extent possible, long-term and short-term, adverse impacts associated with occupancy, modification, or destruction of floodplains; and (2) to avoid indirect support of development and new construction in such areas wherever there is a practicable alternative. The NPS administers floodplain policy through Director’s Order #77-2: *Floodplain Management* and Procedural Manual #77-2: *Floodplain Management*.

It is NPS’s policy to preserve floodplain functions and values and minimize potentially hazardous conditions associated with flooding, including threats to human health/life, risk to capital (NPS) investment, and impacts on natural and beneficial floodplain values. If a proposed action is found to cause adverse impacts in an applicable regulatory floodplain, and relocating the action to a non-floodplain location is considered not to be a practicable alternative, then a formal floodplain Statement of Findings (FSOF) must be prepared. The FSOF must (1) describe the rationale for selection of a floodplain site; (2) quantify flood conditions and associated hazards as a basis for management decision-making; (3) disclose the resources and amount of risk associated with the chosen site; and (4) explain flood mitigation plans. The FSOF must be available for public review and comment, generally by including it with applicable National Environmental Policy Act compliance documentation.

The NPS contracted WSP USA Inc. (WSP) to prepare this FSOF for the project. This FSOF provides sufficient information for assessing the potential floodplain impacts of the project to meet NPS requirements under Director’s Order #77-2. This FSOF presents the rationale for replacing the Tennessee River Bridge, which is located entirely within an area in which flooding is possible, but hazards have not been determined by the Federal Emergency Management Agency (FEMA). This FSOF clearly explains the rationale for site selection, documents how impacts on floodplain natural resources are or would be minimized, and how flood hazard mitigation would be achieved. The project design has not yet been finalized. Therefore, the following discussion and analysis assumes a “worst case scenario,” based on the maximum impacts that could occur from the two project alternatives. However, impacts could be reduced depending on the option selected. In accordance with Procedural Manual #77-2, this FSOF also documents that project infrastructure would be designed to the 100-year standard.

2.0 PROJECT DESCRIPTION

2.1 BACKGROUND

The Parkway is a 444-mile roadway, commemorating the Old Natchez Trace. Established in 1938, the Parkway bisected the traditional homelands of several Native American nations, including but not limited to, the Natchez, Chickasaw, and Choctaw Nations. Native American nations were some of the first to establish the Natchez Trace, ushering in an era of trade and travel through this region for centuries. When the United States began to expand westward in the late 1700s and early 1800s, numerous travelers traversed the area and eventually created a clearly marked path, which became known as the Old Natchez Trace. In 1800, President John Adams designated the Old Natchez Trace as a national postal road for mail delivery between Nashville and Natchez. However, over time, new roads and population centers were developed and steamships began to carry people and supplies upstream. As a result, the Old Natchez Trace fell out of use. The Parkway was established as a unit of the national park system by an act of Congress on May 18, 1938, to commemorate and preserve the Old Natchez Trace, an overland route connecting Nashville, Tennessee, and Natchez, Mississippi. As one of the oldest transportation routes in North America, its human use dates back to 8000 Before the Common Era.

The Parkway is eligible for listing in the National Register of Historic Places (NRHP) and has been designated an All-American Road (1996), which means that it meets at least two of the six intrinsic qualities required for listing as a national scenic byway, with scenic, historic, natural, cultural, archeological, and/or recreational values that are distinctive. The Parkway, including both the roadway and its associated facilities, is a historic designed landscape. The Parkway is a national historic landmark, as well as one of America's 150 national scenic byways.

The Tennessee River Bridge opened to traffic in 1964. Today, the bridge continues to provide access and connections for local communities across Alabama and for Parkway travelers. The nearly 1 mile-long bridge passes over a navigable portion of the Tennessee River, which allows for passage of barge traffic underneath the bridge, and to date, no incidents with barge traffic have not been recorded. TVA controls water levels on the Tennessee River within the project area through a series of dams. The bridge currently averages a traffic volume of approximately 1,400 vehicles per day. Although the Parkway prohibits commercial traffic, commercial vehicles frequently use the Parkway and the bridge, which further deteriorates the bridge's condition. Large recreational vehicles create similar issues for the bridge because of their size and weight.

The Tennessee River Bridge is approximately 4,955 feet long with a 35 feet wide bridge deck, and the travel speed is 50 miles per hour (mph) throughout the project area. The existing bridge is a 38-span structure composed of a two-girder system that supports a cast-in-place deck centered over the navigable portion of the Tennessee River. The substructure consists of two reinforced concrete abutments founded on steel piles and reinforced concrete, hammerhead piers founded on rock. While the bridge is currently safe, it has exceeded its design life and is considered a fracture-critical structure with widespread cracking in the bridge deck and shows notable deterioration of the bridge piers. Therefore, routine maintenance can no longer address these structural issues. A replacement structure is expected to have a 100-year service life with up to a 125-year potential life.

2.2 PROJECT LOCATION

Study Area

The project is located along the Parkway, spanning the Tennessee River, within Colbert and Lauderdale Counties, Alabama, approximately 6 miles north of the Town of Cherokee. The bridge crossing is approximately 7 miles due north of the US Highway 72 ramp, which intersects the Parkway. As shown in

Figure 1, the study area includes approximately 148 acres, bounded by the Tennessee River to the east and west, and includes a potential construction staging area in a grazing pasture southeast of the bridge. The study area includes approximately 300 feet from either side of the existing Parkway centerline, west of the Colbert Ferry Recreation Area entrance to east of the ramps connecting the Parkway to Lauderdale County Road 2. An agricultural lease within the study area, just southeast of the bridge, could serve as a construction staging area.

Project Area

The project area encompasses the total acreage of the construction areas associated with the two proposed alternatives for bridge replacement (15.66 acres) and the proposed 24.72-acre construction staging area within the study area. The two proposed alternatives are briefly described below.

- (1) Replace the bridge on a skewed alignment that follows the existing bridge alignment on the west side of the Tennessee River and partially skews to the south of the existing alignment on the east side of the Tennessee River.
- (2) Replace the bridge to the south of the existing alignment.

The project area in Figure 2 depicts the combined total construction footprint from the two proposed alternatives, and Figure 3 depicts the alignments of the two proposed alternatives as they would span the Tennessee River.

The project area extends approximately 1.85 miles east to west between latitude 34.832688° North, longitude -87.947056° West at the west end to latitude 34.844138° North, longitude, -87.918103° West at the east end.

- Approximate center coordinates: 34.838840° North, -87.933566° West
- Universal Transverse Mercator (UTM): 414642.55 Easting, 3855568.55 Northing, UTM Zone 16S
- Public Land Survey System: Sections 31, 32, and 33 of Township 2 South, Range 13 West; Section 6 of Township 3 South, Range 13 West; and Section 1 of Township 3 South, Range 14 East
- Name of nearest waterbody: Tennessee River
- Name of subwatershed: Colbert Creek – Pickwick Lake; Hydrologic Unit Code (HUC) 060300051002

Natchez Trace Parkway

Tennessee River Bridge Replacement

National Park Service
US Department of the Interior

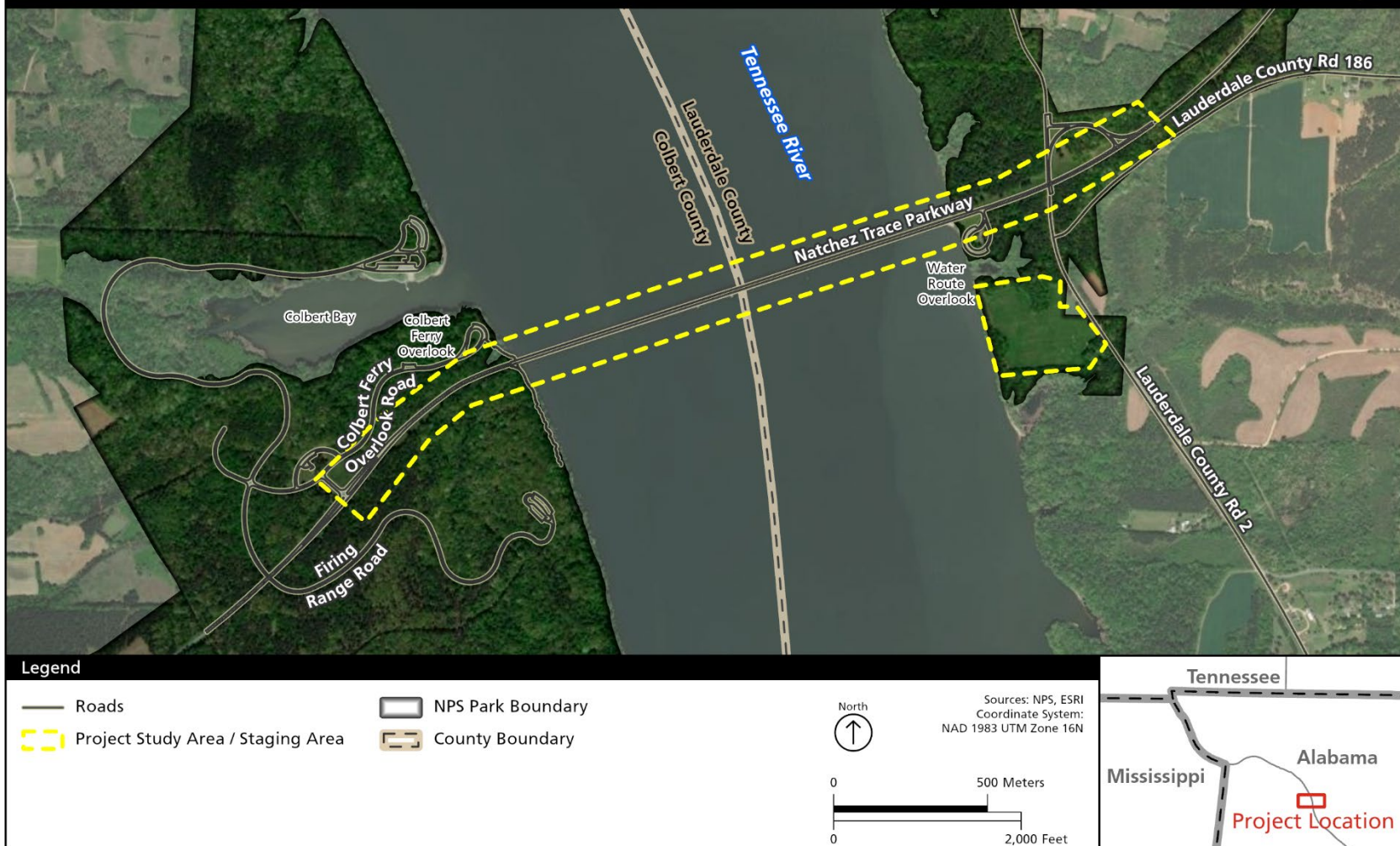


Figure 1. Project study area

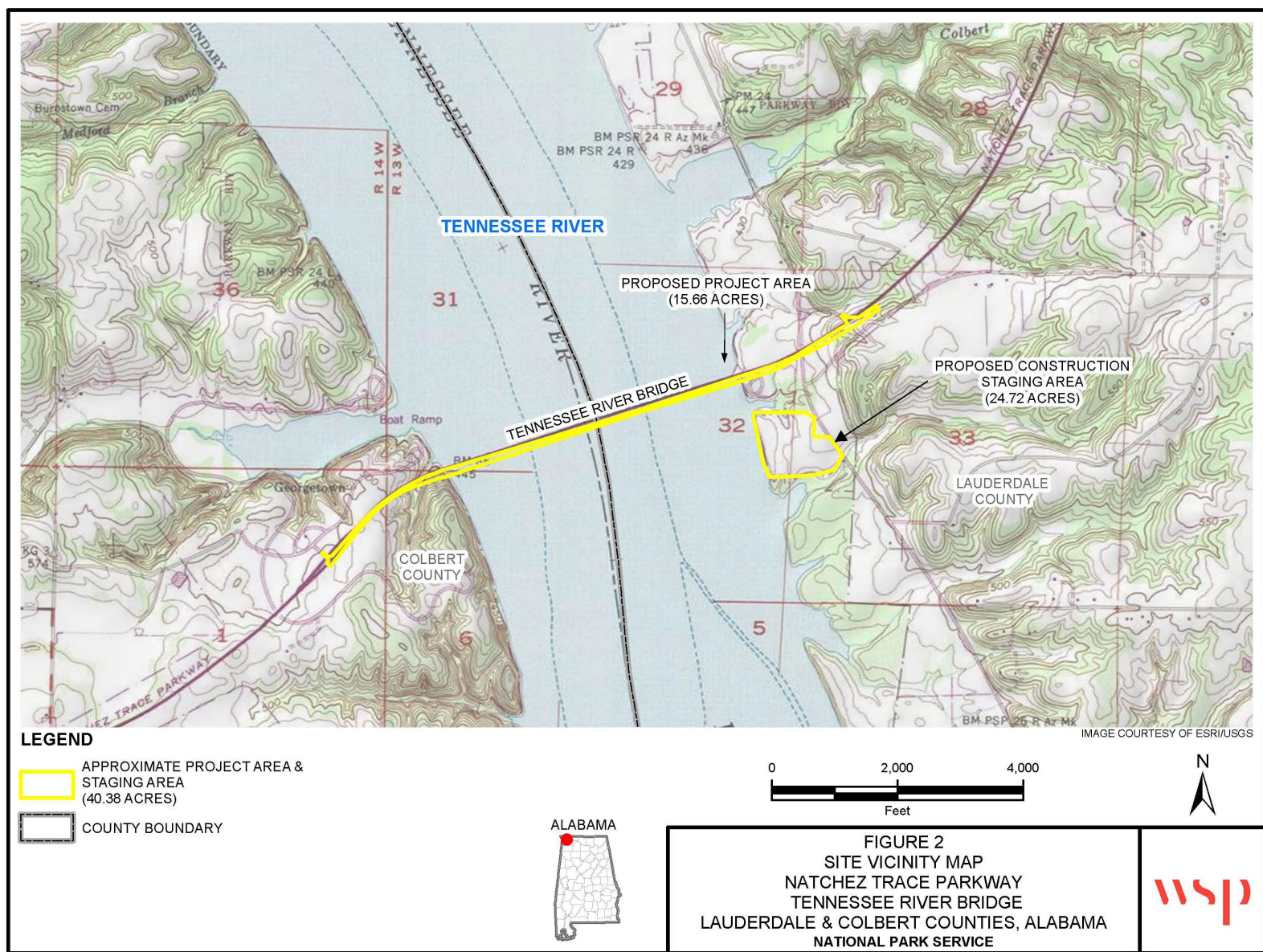


Figure 2. Site vicinity map

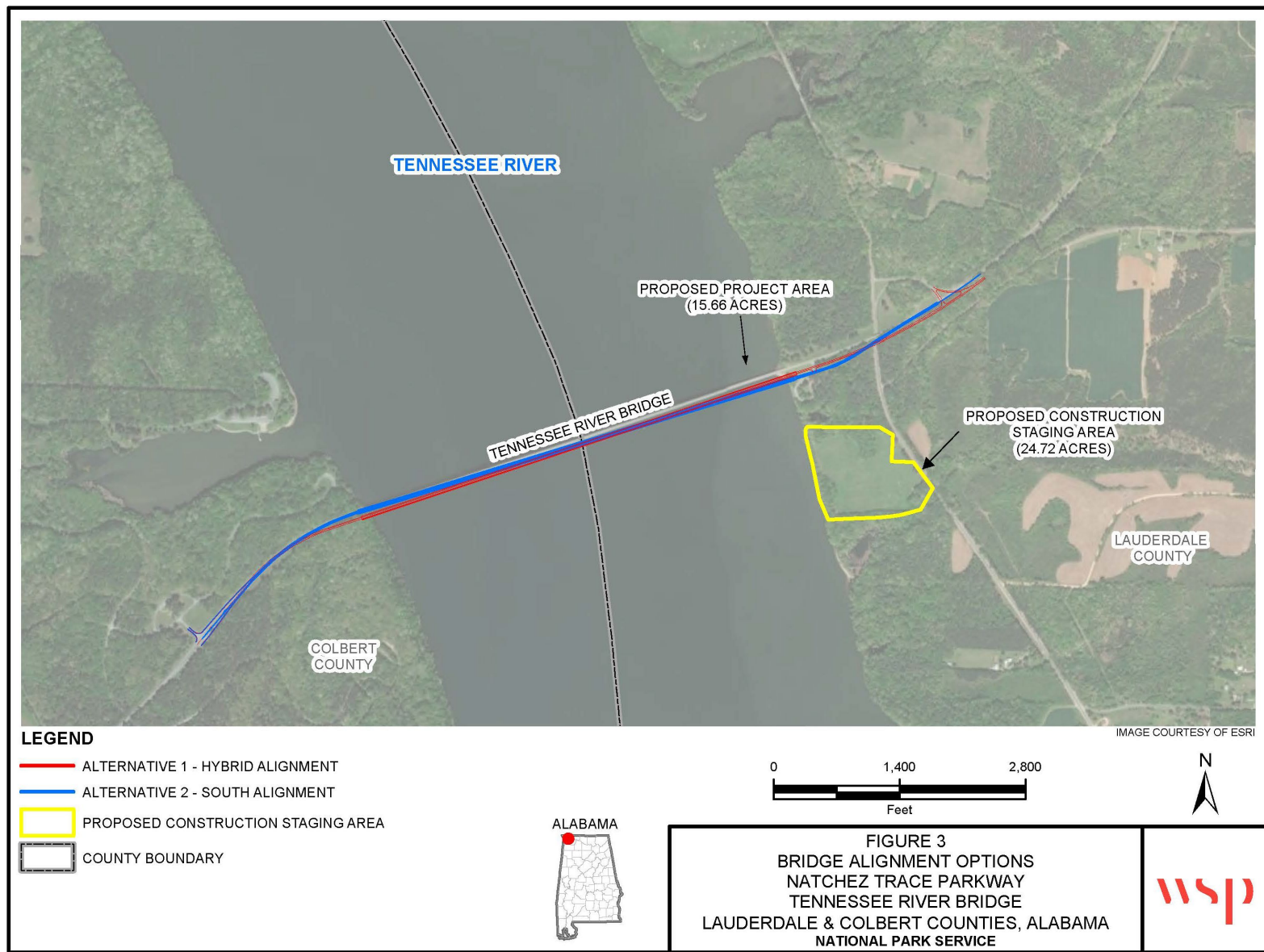


Figure 3. Proposed alternatives for bridge replacement

2.3 PURPOSE AND NEED

The purpose of the project is to maintain a safe and reliable bridge crossing for users over the Tennessee River, while minimizing adverse effects on Parkway resources.

The project is needed because this bridge, which opened in 1964, has exceeded its 50-year design life. The bridge has not undergone a major rehabilitation since 1964, but several temporary repairs have kept it operational and safe. The bridge is regularly inspected by FHWA in accordance with generally recognized structural engineering guidelines and standards to confirm that it is safe for travel.

The bridge lacks redundancy in its design, making it fracture critical, and detailed structural inspections and studies note that the bridge exhibits widespread cracking in the bridge deck and notable deterioration of the bridge piers. As the bridge deteriorates beyond the ability of routine maintenance to address structural issues, and ultimately no longer meets bridge safety requirements, the NPS and FHWA would be required to close the bridge to vehicular traffic. To avoid the impacts of long term or permanent bridge closure, the bridge needs to be replaced to provide a structurally sound vehicular bridge crossing the Tennessee River.

2.4 PROPOSED ACTION

Construction

Under the proposed action, the Tennessee River Bridge would be demolished, and a new bridge would be constructed. The new bridge would either follow the existing alignment in part or follow a parallel alignment that would be located south of the existing alignment. In the project area, the bridge would be reconstructed to a typical cross section containing approximately one 11-foot travel lane in each direction, a 6-foot-wide shoulder on each side of the bridge, and a 5-foot walkway on the southern side of the bridge.

Implementation of either of the two proposed alternatives for bridge replacement would result in temporary and permanent disturbance to terrestrial and aquatic habitats and FEMA-designated floodplains. Temporary and permanent disturbance associated with construction is shown in Table 1.

Construction of the proposed project would permanently disturb up to 10 acres of vegetated upland habitat, including forest, with some disturbance on either side of the bridge. Permanent effects on terrestrial habitats would include vegetation clearing and tree removal. Additional temporary disturbance of terrestrial habitats associated with construction is not anticipated. However, if any areas outside the footprint of permanent disturbance were disturbed, these areas would be revegetated with a native seed mix upon completion of construction.

Construction of the new bridge would result in 0.53 acres of permanent disturbance to aquatic habitats in the Tennessee River to accommodate the abutment and piers and an additional 0.7 acres of temporary disturbance of riverine habitat to accommodate a temporary work platform. Demolition of the existing bridge would result in an additional 0.76 acres of aquatic habitat on the river bottom.

Table 1. Construction disturbance under the proposed action (includes the two alternative alignments)

Infrastructure/Construction Activity	Permanent Disturbance (Acres)	Temporary Disturbance (Acres)
Roadway approach	10 (terrestrial)	
Bridge construction	0.53 (aquatic)	0.7 (aquatic)
Demolition of existing bridge		0.76 (aquatic)
Total	10.53	1.46

The agricultural lease immediately southeast of the bridge would be closed during construction for equipment and material staging. During construction, the volume of construction vehicle traffic to, from, and through the project area and staging area would be higher because of the amount of subgrade removal of old road and bridge material and trucking associated with transporting new materials for the project. After construction, the staging area would be restored to a riparian forest, and there would be no change in size or location of the existing area.

Demolition of the existing bridge and construction of a new bridge would result in a temporary increase in noise in the project area associated with the use of heavy equipment. Equipment that would be necessary to implement the proposed action would include:

- Dump trucks
- Hauling trucks
- Excavators
- Cranes
- Bulldozers
- Skid loaders
- Rolling machines
- Paving machines
- Barges
- Tugboats
- Concrete saws
- Jackhammers
- Cutting torches

Project construction would be completed in phases, with various phases being implemented concurrently. The total duration of project construction would be up to 1,665 working days but could be less depending on the final design alternative selected.

Operations and Maintenance

Ongoing operation and maintenance activities would continue after implementation of the proposed action. Periodic bridge and roadway maintenance activities would be similar to existing conditions but could vary slightly based on characteristics of the final alternative selected. Regular maintenance would

consist of bearing and expansion joint maintenance or replacement, superstructure and substructure concrete patching, and bridge deck maintenance. Regular maintenance activities would occur at 5-to-10-year intervals for the life of the bridge. Asphalt resurfacing and patching is currently conducted as needed to maintain the road surface and would continue under the proposed action.

Vegetation in the Parkway right-of-way is maintained with regular mowing during the growing season, and weed trimmers are used to keep vegetation low around signposts. Herbicides are only used as necessary to control nonnative or invasive vegetation.

General Natural Resources Conservation Measures

The following measures would be implemented to avoid or minimize potential that may occur as a result of the proposed action:

- Conduct a preconstruction biological survey for the length of the existing bridge to reduce impacts on a variety of species in the project area.
- Clearly state all resource protection measures in the construction specifications and instruct workers to avoid conducting activities outside the project area. Limit disturbances to roadsides and other areas inside the project area. Clearly indicate areas of concern on construction drawings.
- Hold a preconstruction meeting to inform contractors about sensitive areas, including natural and cultural resources, and provide procedures for identifying and addressing any unanticipated discoveries.
- Delineate construction zones outside existing disturbed areas with flagging and confine all surface disturbance to the construction zone.
- Establish staging and storage areas for construction vehicles, equipment, materials, and soils in previously disturbed or paved areas approved by the NPS. These areas would be outside high-use visitor areas and clearly identified in advance of construction.
- Require contractors to properly maintain construction equipment to minimize noise and do not allow construction vehicle engines to idle for extended periods.
- Remove all tools, equipment, barricades, signs, and surplus materials from the project area upon completion of the project.
- Use fill material that meets FHWA Eastern Federal Lands Highway Division and NPS requirements to avoid the introduction of nonnative invasive plants.
- Revegetate disturbed areas with a native seed mix.

3.0 PROJECT AREA DESCRIPTION

3.1 EXISTING CONDITIONS

Topography

The study area is relatively flat, with a steep bank to the Tennessee River. Elevations range from 414.00 feet above mean sea level (AMSL) up to 650.00 feet AMSL.

Hydrology

The study area is located within the Colbert Creek – Pickwick Lake subwatershed (12-digit HUC 060300051002), a subset of the Bluff Creek-Tennessee River watershed (HUC 0603000510) that drains through a series of dams to the Ohio River. The Tennessee River runs under the Tennessee River Bridge from southeast to northwest into Pickwick Lake. Pickwick Landing Dam is located approximately 26.5 river miles downstream of the study area. Wilson Dam is located approximately 19 river miles upstream of the study area. TVA owns and operates both dams, which are hydroelectric and were built in the early 1900s. The Tennessee River continues to flow north into the Ohio River approximately 236 river miles downstream from the study area.

Vegetation

LANDFIRE vegetation community data were used to determine potential vegetation communities in the study area. Thirteen vegetation communities were identified (LANDFIRE 2016):

- Southeastern Ruderal Grassland
- South-Central Interior Large Floodplain Herbaceous
- Southeastern Native Ruderal Forest
- South-Central Interior Large Floodplain Forest
- Eastern Cool Temperate Pasture and Hayland
- Eastern Cool Temperate Urban Shrubland
- Eastern Cool Temperate Urban Herbaceous
- Eastern Cool Temperate Urban Mixed Forest
- Eastern Cool Temperate Urban Evergreen Forest
- Eastern Cool Temperate Urban Deciduous Forest
- South-Central Interior Mesophytic Forest
- Southern Interior Low Plateau Dry-Mesic Oak Forest
- Open Water

Volkert, Inc. conducted a wetland and waters delineation in the study area in August 2022 (Volkert 2022). Dominant plant species identified during the delineation are listed in Table 2.

Table 2. Dominant plant species in study area

Common Name	Scientific Name
Dallis Grass	<i>Paspalum dilatatum</i>
Marsh Bristlegrass	<i>Setaria parviflora</i>
Alligator Weed	<i>Alternanthera philoxeroides</i>
Redtop	<i>Agrostis gigantea</i>
Bahiagrass	<i>Paspalum notatum</i>
Silver Maple	<i>Acer saccharinum</i>
American Sweetgum	<i>Liquidambar styraciflua</i>
Boxelder Maple	<i>Acer negundo</i>
Green Ash	<i>Fraxinus pennsylvanica</i>
Asian Crab Grass	<i>Digitaria bicornis</i>
White Clover	<i>Trifolium repens</i>
Yellow Nutsedge	<i>Cyperus esculentus</i>

Source: Volkert, Inc. 2022

Forests in the study area comprise mid-successional, moderately mesic sweetgum (*Liquidambar styraciflua*), tulip poplar (*Liriodendron tulipifera*), American elm (*Ulmus americana*), boxelder (*Acer negundo*), shortleaf pine (*Pinus echinata*), and mixed forest (WSP 2023).

The NPS Vegetation Mapping Inventory Program mapped the vegetation and open water of the Natchez Trace Parkway between 2014 to 2016. Vegetation maps, a geodatabase, and summary reports were published in the 2016 National Park Service Vegetation Mapping Inventory Program – Natchez Trace Parkway Vegetation Mapping Project. Vegetation in the study area was primarily mapped as Northern Oak (Hardwood) Forest, Ruderal Conifer (Hardwood) Forest, Ruderal Herbaceous, and Ruderal Hardwood Flooded Forest (NPS 2016).

Geology and Soils

The study area spans the Tennessee River between Colbert and Lauderdale Counties, Alabama.

Soils in the study area consist predominantly of silt loam, with steep slopes and large soil particle sizes. Soils on the Colbert County side of the Tennessee River consist of the Decatur and Fullerton series, and soils on the Lauderdale County side consist of the Dickson series (USDA-NRCS 2023a). These soils are described briefly below:

- The Decatur series consists of very deep, well-drained, moderately permeable kaolinitic soils that formed in residuum derived from limestone. These soils are on level to strongly sloping uplands in valleys, with mean annual temperatures around 62 degrees Fahrenheit, and mean annual precipitation of more than 49 inches.
- The Fullerton series consists of very deep, well-drained, moderately permeable kaolinitic soils that are strongly acidic. Gravel and chert cobbles range from 10% to 45% of soil content, and soil aggregates are highly friable. These soils are found throughout river valleys in southern Appalachian ridges.
- The Dickson series consists of very deep, moderately well-drained soils that have a slowly permeable fragipan in the subsoil. These soils formed in a silty mantle 2 to 4 feet thick and the

underlying residuum of limestone. They are on nearly level to sloping uplands. Slopes range from 0% to 12% (USDA-NRCS 2023b).

Bedrock geology on the Colbert County side of the study area consists of Tuscumbia limestone, a partly oolitic white-gray limestone that often contains fine to very coarse-grained bioclastic crinoidal limestone. Chert nodules and concretions are scattered throughout and are abundant locally. Bedrock on the Lauderdale County side is formed from Fort Payne chert, a bioclastic (abundant pelmatozoans) limestone containing abundant nodules, lenses, and beds of light to dark-grey chert with a broad range of grain and bed sizes. The upper part of the bedrock layer consists of laminated siltstone containing vugs lined or filled with quartz, with shale, shaly limestone, and siltstone scattered throughout. Claystone and shale belonging to the Maury Formation occur at greater depths (McKay 2021; Szabo et al. 1998).

3.2 FLOODPLAINS IN THE PROJECT AREA

The FEMA National Flood Hazard Layer (NFHL) data was reviewed for floodplains within the project area. The FEMA 100-year floodplain (Zone A) overlaps 9.71 acres (approximately 24%) of the project area in the vicinity of the Tennessee River as shown in Figures 4A, 4B, and 4C (FEMA 2023). The majority of the 100-year floodplain within the project area (approximately 7.5 acres) is within the banks of the Tennessee River (Figure 4A). In the proposed construction staging area, the 100-year floodplain fluctuates between 35 and 40 feet inland from the edge of the eroded bluff adjacent to the Tennessee River, placing the 100-year floodplain boundary just inside the existing tree line adjacent to the grazing pasture. No portions of the project area are mapped within the 500-year floodplain (Zone B or Zone X [shaded]; FEMA 2023).

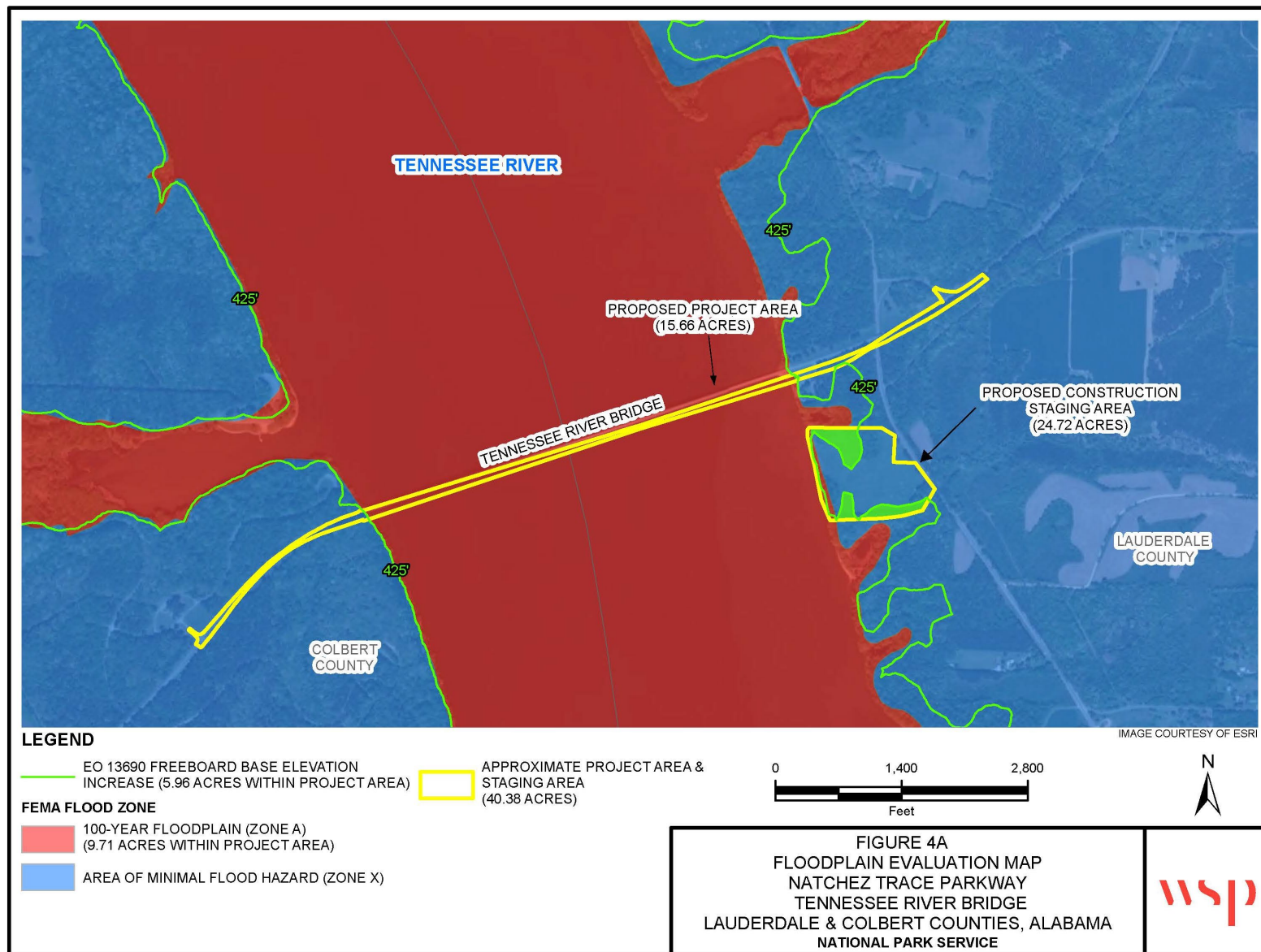


Figure 4A. Floodplain evaluation map

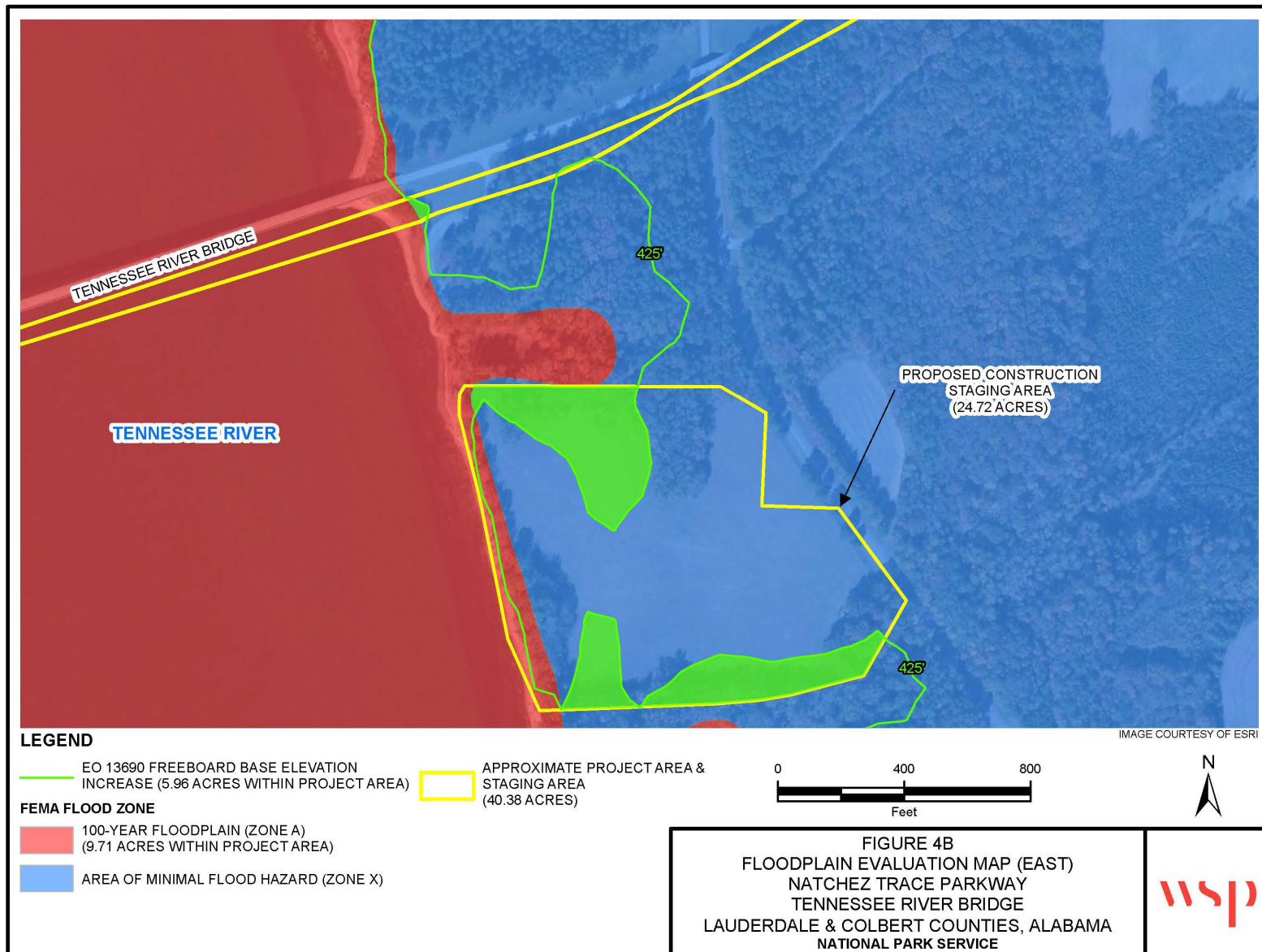


Figure 4B. Floodplain evaluation map (east)

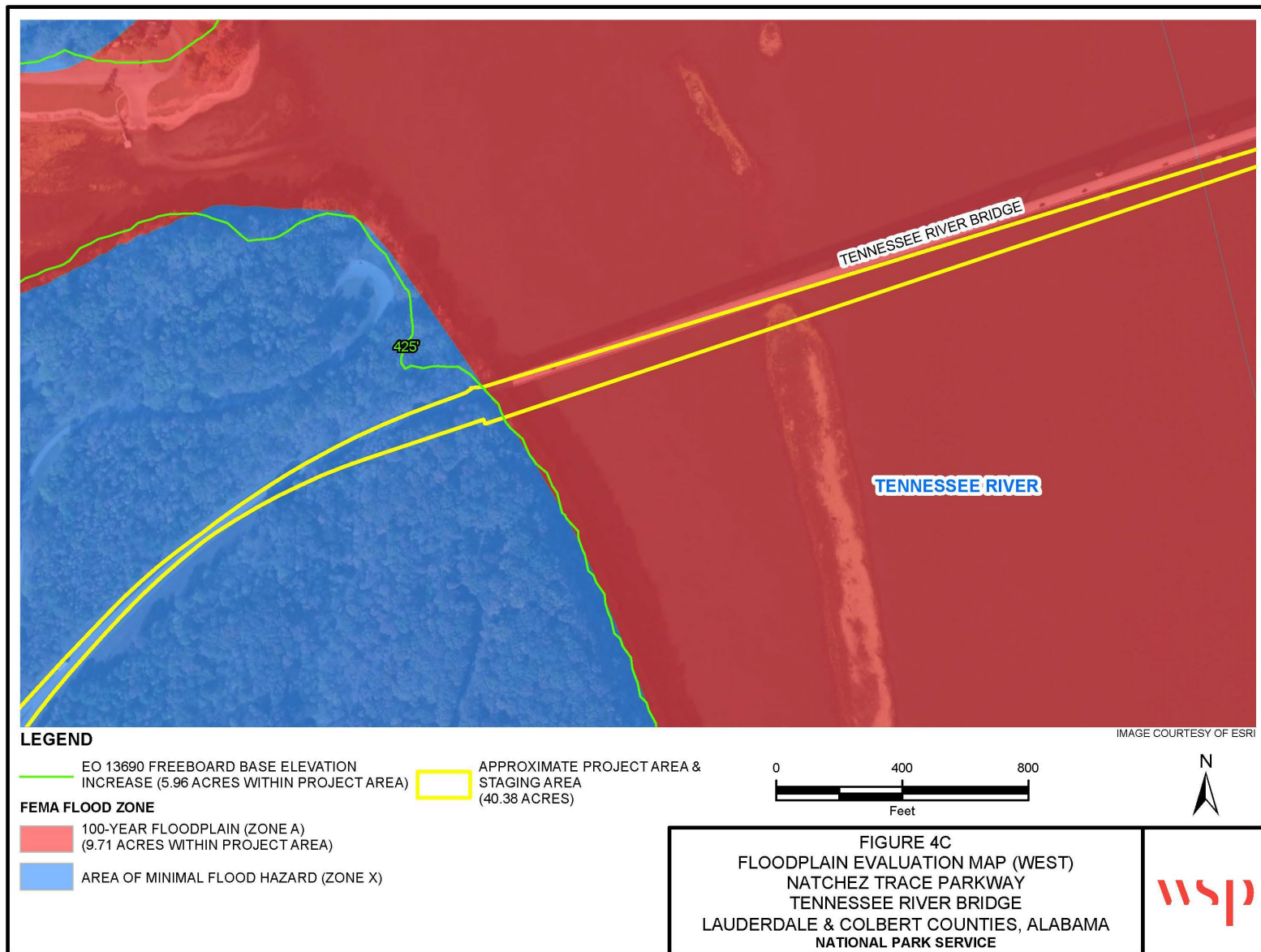


Figure 4C. Floodplain evaluation map (west)

The proposed action would qualify as a non-critical, Class I Action, as defined in Procedural Manual #77-2, subject to the floodplain policies and procedures, if occurring within the 100-year floodplain. Pursuant to Executive Order 13690, “Federal Flood Risk Management Standard (FFRMS),” federal agencies must choose one of three methods for establishing a higher vertical flood elevation beyond the guidelines provided in Executive Order 11988. The NPS used two of the three approaches identified in the FFRMS to evaluate the flood elevation and corresponding flood hazard area for project siting, design, and construction:

- 500-year Floodplain: The area subject to flooding by the 0.2%-annual-chance flood.
- Freeboard Value Approach: The elevation and flood hazard area that results from adding an additional 2 feet to the 100-year flood (1% annual chance) Base Flood Elevation (BFE) for non-critical actions and by adding an additional 3 feet to the BFE for critical actions.

500-year Floodplain

No areas within the project area have been mapped in the FEMA 500-year floodplain. FEMA has mapped an area approximately 12 river miles upstream of the project area, just west of Tusculumbia, Alabama, as 500-year floodplain; the elevation in this area is similar to the mapped 100-year floodplain along the banks of the Tennessee River. Additionally, approximately 12 river miles downstream of the project area, just east of Eastport, Alabama, FEMA also mapped the 500-year floodplain elevation, and this area is similar to the 100-year floodplain elevation along the banks of the Tennessee River.

FEMA has not published a map in the vicinity of the project area depicting the 100-year flood BFE in the FEMA NFHL. The FEMA-mapped area 12 miles upstream of the project area identifies the 100-year floodplain BFE range between 426 feet AMSL and 430 feet AMSL.¹ The FEMA-mapped area 12 miles downstream of the project area identifies the 100-year floodplain BFE range between 420 feet AMSL and 421 feet AMSL (FEMA 2023).

The FEMA Flood Insurance Study (FIS) for Colbert County, Alabama, identifies the preliminary 100-year flood elevation at the Tennessee River Bridge at 422.80 feet AMSL and the preliminary 500-year floodplain elevation at 424.50 feet AMSL (FEMA 2021).

Freeboard Value Approach

As noted above, FEMA has not published a map for the project area depicting the floodplain limits or hazards with the 100-year flood BFE on the FEMA NFHL. The FEMA FIS for Colbert County identifies the preliminary 100-year floodplain elevation at the Tennessee River Bridge at 422.80 feet AMSL (FEMA 2021). To maintain a conservative approach for evaluation, the NPS used 423.00 feet AMSL as the BFE for flood hazard analysis, then added 2 additional feet of freeboard to comply with the FFRMS. Publicly available US Geological Survey elevation data were used to add the additional 2 feet of freeboard elevation (USGS 2023). The resulting FFRMS floodplain elevation is 425.00 feet AMSL. The 425.00-foot FFRMS floodplain elevation also encompasses the FEMA FIS 424.50-foot AMSL 500-year floodplain elevation.

Summary

In an effort to err on the side of caution and protect federal investments for development that may occur within the 100-year floodplain and may be vulnerable to flood damage, the NPS concluded all areas within the 425.00-foot AMSL elevation contour in the project area are within the FFRMS floodplain for

¹ All flood elevation data are referenced to the North American Vertical Datum of 1988 (NAVD 88).

project floodplain evaluation and flood hazard analysis. The FFRMS includes an additional 5.96 acres of project area for floodplain evaluation and flood hazard analysis in addition to the 100-year floodplain area, totaling 15.67 acres. The remaining 24.71 acres of project area are in FEMA-mapped Zone X (unshaded) – Area of Minimal Flood Hazard (Figures 4B and 4C; FEMA 2023).

3.3 FLOODPLAIN VALUES

The proposed action would occur within the 100-year regulatory floodplain. The bridge replacement would result in temporary and permanent effects on floodplains. Work within the floodplain could require 1,655 working days but could be shorter depending on the final design alternative selected. Flooding near the project area typically occurs following substantial amounts of regional rainfall. The risk of flooding during project implementation is possible, but low, due to project-specific mitigation measures and TVA river management from the dams and reservoirs along the Tennessee River.

Floodplain values are typically defined in terms of hydrologic functions such as stormwater retention and infiltration capacity, groundwater recharge, and the ability to buffer or dissipate energy from surface flows to prevent erosion.

The floodplains within the project area are primarily restricted to the edges of the Tennessee River and are currently affected by the existing bridge and Parkway. Bridge piers impede the flow of water, and the existing roadway cuts through the floodplains along the Tennessee River bank, preventing continuous floodplain access during high-water flows.

Areas within the agricultural lease proposed for the temporary construction staging area are within the FFRMS floodplain, and the removal of trees and riparian vegetation has affected these areas, reducing the ability to dissipate energy and retain water. The agricultural lease would be restored to a riparian forest after construction is completed to mitigate impacts on vegetation, floodplains, and water quality. Much of the FFRMS floodplain within the project area supports riparian vegetation, which helps prevent erosion and disperses energy during high-water flow events. The floodplains provide minimal stormwater retention, infiltration, and groundwater recharge other than the areas within the immediate vicinity of the river.

The project area includes woodland and riverine habitats for species of bat and mussels. The Tennessee River Bridge itself acts as a seasonal roosting place for maternity colonies of big brown bats. However, analysis has shown that none of the bat or mussel species present within the project area are listed on state or federal lists of threatened or endangered species. After evaluation, it was determined the proposed action either *may affect, but is not likely to adversely affect*, or would have *no effect* on state or federally listed species (WSP 2023).

As indicated above, the Tennessee River Bridge is itself a historic resource, as part of the Natchez Trace Trail Parkway system, which was determined eligible for listing in the National Register of Historic Places in 2004. As such, all proposed alignment alternatives would result in impacts on historic properties. Additionally, submerged archeological sites are present upstream and downstream of the project area and have the potential to be impacted by the proposed alternative alignments. As such, the Alabama State Historic Preservation Office asserted that demolition of the historic Tennessee River Bridge would result in an adverse effect and that any new alignment may have an adverse effect on the cultural landscape (Alabama Historical Commission 2023).

Potential adverse impacts on natural floodplain values include removal of vegetation and installation of bridge piers and road materials. The existing bridge piers would be removed from the floodplain, and the new bridge would have the same number or fewer piers, depending on the chosen design. There may also be minor permanent vegetation impacts associated with bridge shading over floodplain areas if design plans include realignment; however, removal of the previous bridge alignment after construction would minimize shading impacts. Overall, the decrease in floodplain values is expected to be negligible.

3.4 FLOOD RISK IN THE PROJECT AREA

Recurrence Interval of Flooding in the Project Area

The 100-year BFE at the Florence, Alabama, TVA river gauge located approximately 19 river miles upstream of the project Area is 431.00 feet AMSL (FEMA 2023). Based on historic crests documented by the National Weather Service (NWS) at the gauge, the Tennessee River has exceeded its 100-year floodplain BFE (431.00 feet AMSL) twice since 1897. NWS-mapped flood categories (in river stage footage) for the Tennessee River at Florence include: action stage at 16 feet; flood stage at 18 feet; moderate flood stage at 27 feet; and major flood stage at 32 feet. The reference “gauge 0” level at the Florence River gauge is at 401.12 feet AMSL. The designated NWS flood stage category of 419.12 feet has been exceeded at the TVA river gauge at Florence 61 times since 1897 (NWS 2023).

Intensity and variability in precipitation are expected to increase in Alabama due to climate change. Floods and droughts are expected to become more severe, while higher intensity and more frequent rainfall may increase sediment runoff and water levels. Both the Wilson and Pickwick Landing Dams would mitigate issues of flooding or droughts by managing the river’s water accordingly. Warmer temperatures are expected to decrease the amount of water recharging rivers and groundwater by 2.5% to 5%, which may lead to the depletion of water resources (USEPA 2016).

Hydraulics of Flooding in the Project Area

TVA’s web-based Environmental Systems Research Institute, Inc. (ESRI) mapping platform indicates the Tennessee River Bridge is situated at least 5 to 6 feet higher than the 500-year flood elevation. Additionally, the 100-year flood discharge at the bridge is 464,060 cubic feet per second (TVA, Williamson, pers. comm. 2023a). Flood conditions in the project area typically occur during periods of heavy regional rainfall but are mitigated by the numerous upstream TVA-managed dams and reservoirs. The presence of the Wilson Dam located approximately 19 river miles upstream, and the Pickwick Landing Dam located approximately 26.5 river miles downstream of the project area reduce Tennessee River flow velocities, and the proposed project is designed to withstand predicted river hydraulics.

Historically, the Tennessee River was routinely flooded, causing major issues for those who lived in the area. However, the Tennessee River has been dammed numerous times to combat flooding events. TVA uses the dams and reservoirs to keep flooding under control by managing water levels accordingly through different times of the year. Despite these holds and releases of water, the main-channel reservoirs do not fluctuate as much as the tributaries. As shown in Figure 5A and Figure 5B, waters at Wilson and Pickwick Landing Reservoirs have fluctuated less than 10 feet since the start of 2022 (TVA 2023b, c).

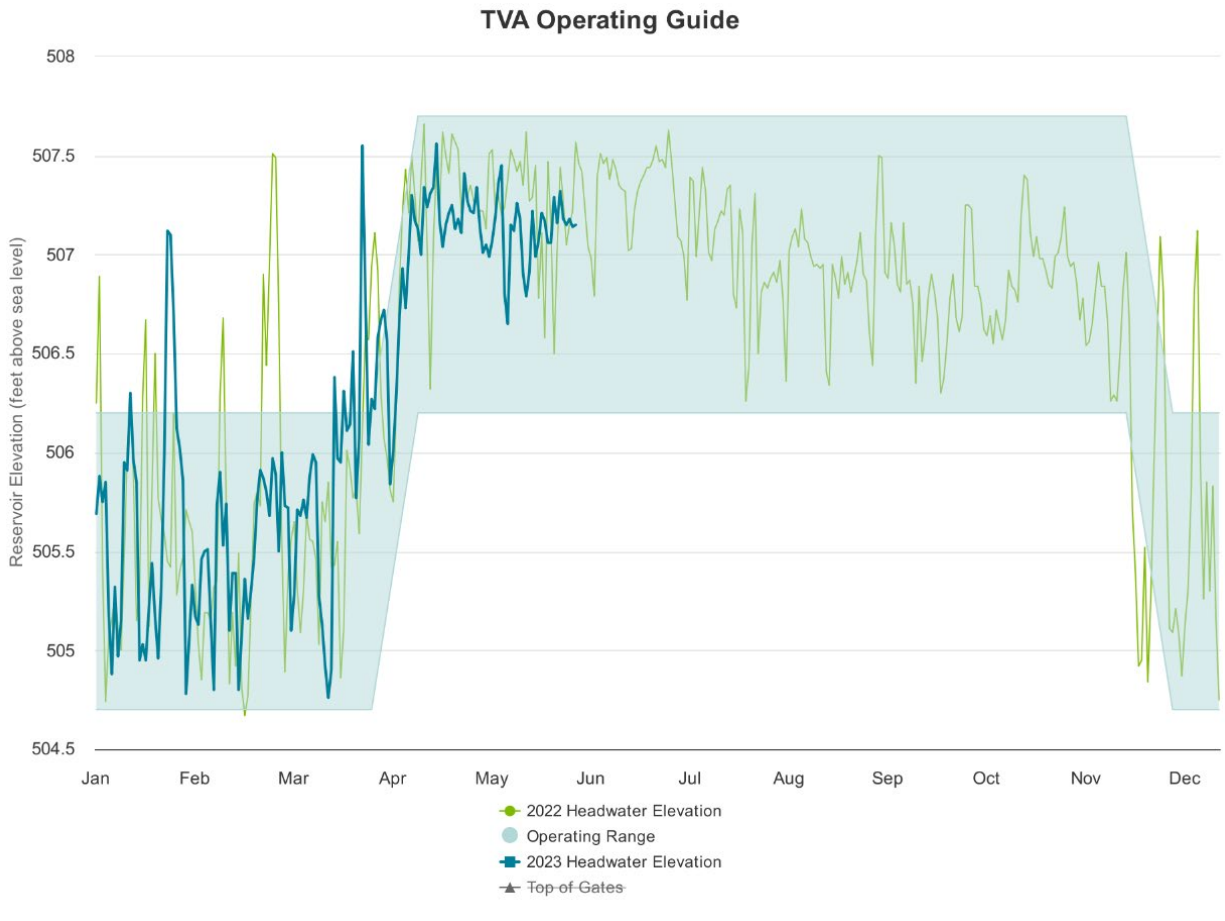


Figure 5A. Annual reservoir elevation at Wilson Dam

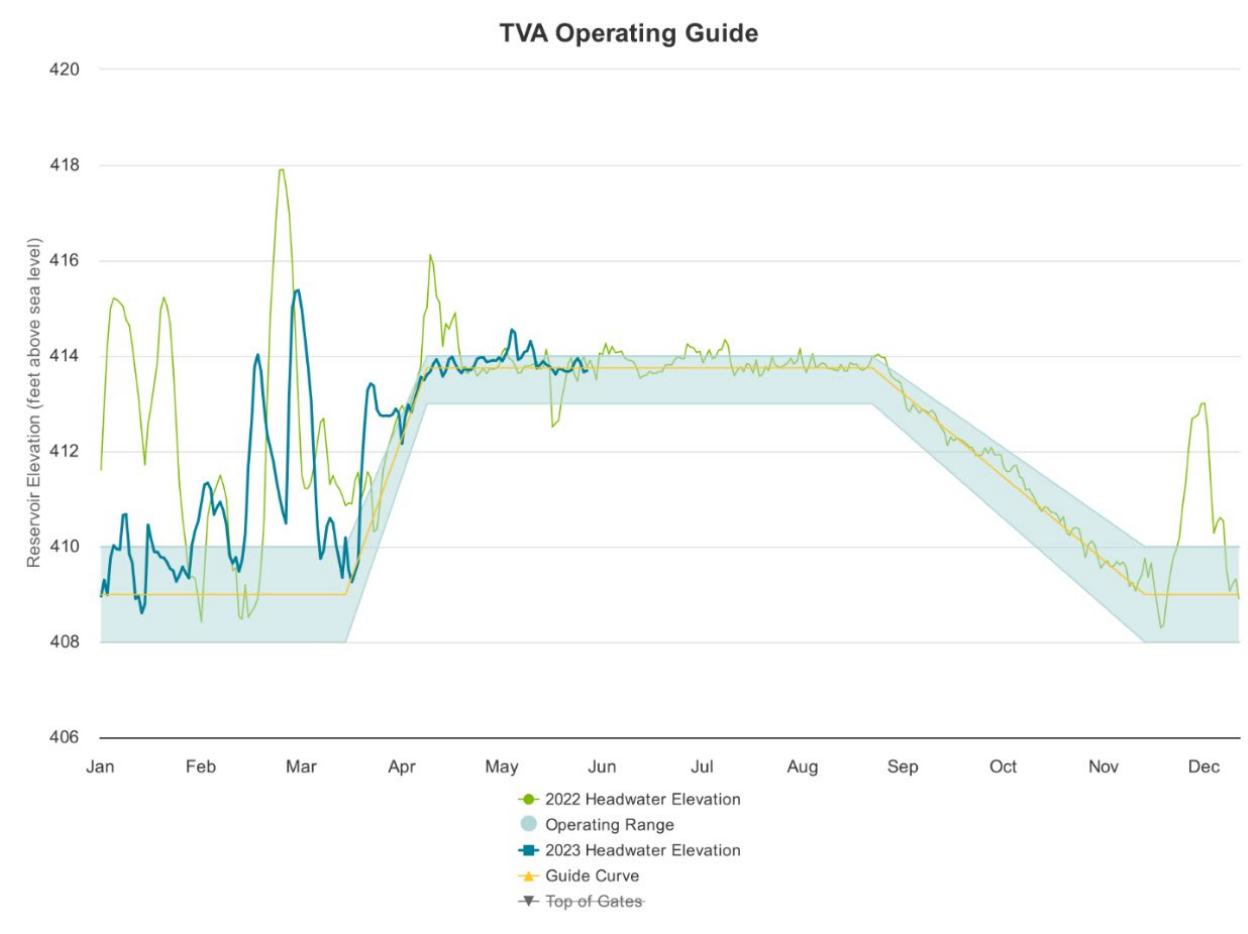


Figure 5B. Annual reservoir elevation at Pickwick Landing Dam

Time Required for Flooding to Occur

The Tennessee River Bridge is not directly served by an emergency flood notification system. Flood conditions at the Pickwick Reservoir may develop as soon as 12 to 24 hours after large regional precipitation events, though conditions often change 2 to 3 days or more following large regional precipitation events. Flood crests are broad and typically last several hours (TVA, Williamson, pers. comm. 2023a). NPS-managed lands are adjacent to the Tennessee River for several hundred feet upstream and downstream on both sides of the river adjacent to the project area, and minimal private property is located within the project's FFRMS floodplain. Land around the project area is sparsely populated, thus the number of individuals to evacuate during flood conditions would be minimal.

Opportunity for Evaluation of the Project Area in the Event of Flooding

The existing road and the Natchez Trace Boat Ramp located approximately 0.2 miles north of the project area on the west side of the Tennessee River provide opportunities to evaluate the project area during and immediately after flood events.

Geomorphic Considerations

Terrain on both sides of the Tennessee River at the Tennessee River Bridge rises steeply from the riverbank to the overbank, and the floodplain generally hugs the river channel. Because of the dams

upstream and downstream of the project area, flows within the project area are strictly managed, and the Tennessee River channel is stable and experiences minimal adjustments. The vegetated riparian corridor limits channel and bank erosion, and sediment deposition is also minimal because of the nature of the TVA-managed dam system.

3.5 ACTION CLASS

The proposed action lies within the 100-year regulatory floodplain (the Base Floodplain). Pursuant to Director's Order #77-2: *Floodplain Management*, the proposed project would qualify as a non-critical Class I Action:

The location or construction of administrative, residential, warehouse and maintenance buildings, non-excepted parking lots or other man-made features, which by their nature entice or require individuals to occupy the site, are prone to flood damage, or result in impacts to natural floodplain values. Actions in this class are subject to the floodplain policies and procedures if they lie within the 100-year regulatory floodplain (the Base Floodplain).

4.0 JUSTIFICATION FOR THE USE OF FLOODPLAINS

4.1 IMPACTS OF THE PROPOSED ACTION

The project area overlaps with 15.67 acres of the FFRMS floodplain. Within that area the project would disturb up to 10 acres of vegetated upland habitat (most of which is outside the FFRMS floodplain) and 0.53 acres of aquatic habitat (most of which is within the Tennessee River). Construction of the new bridge would result in an additional 0.7 acres of temporary disturbance of riverine habitat to accommodate a temporary work platform, and demolition of the existing bridge would result in an additional 0.76 acres of impacts to aquatic habitat on the river bottom.

If a new bridge is built, the bridge would have fewer piers, which would reduce the volume occupied by the structures within the Tennessee River. Many of the old piers would be removed to the mudline or below to eliminate obstacles in the river. If the bridge is built on a new alignment, the existing bridge approaches would be decommissioned, and the acreage would be restored to natural conditions. Impervious surfaces that are not part of the new alignment would also be restored to natural conditions, and the proposed construction staging area would be restored to riparian forest after construction is complete.

There is no practicable non-floodplain location for bridge replacement. Each alternative alignment would have similar impacts on floodplains, and no alternative areas were investigated due to the current location of the Tennessee River Bridge and the extent of the proposed project.

5.0 MITIGATION

TVA owns and operates a sophisticated system of dams and reservoirs to control flooding along the Tennessee River watershed. TVA's 49 dams and associated reservoirs on the Tennessee River provide the means to manage the river system to provide a variety of benefits, including flood control, navigation, hydroelectric generation, recreation, and water quality and supply. The Wilson Dam is located approximately 19 river miles upstream of the project area near Florence, Alabama, and the Pickwick Landing Dam is located approximately 26.5 river miles downstream of the project area near Counce, Tennessee. Additionally, Guntersville Dam is located approximately 85 river miles upstream of Wilson Dam, and Nickajack Dam is located approximately 75 river miles upstream of Guntersville Dam.

TVA has the ability to notify public resource systems of flood warnings based on river gauges and dam inputs/outputs. TVA has a river gauge at Florence, Alabama, approximately 19 river miles upstream of the project area to monitor river levels, and the Alabama Department of Economic and Community Affairs supports the Alabama Flood Risk Information System, which provides flood mapping and risk evaluation for the state of Alabama. Flood warning for the area would occur well ahead of flood conditions due to the extent of TVA-managed dams upstream of the project area.

To help protect life, no inhabitable buildings are currently located within the 100-year floodplain in the project area. The bridge and road structures would be designed to withstand flood events while impeding flow as little as possible. The structure design would adhere to applicable floodplain standards.

All temporary disturbance areas would be restored to preconstruction conditions (or better). Disturbed areas would be revegetated with native plant species that reflect surrounding native vegetation. Native soils excavated for construction would be screened, and suitable soils would be reused to restore temporary disturbance areas, as needed, in accordance with the NPS floodplain guidelines and with Executive Order 11988.

The project would comply with all relevant requirements under the Clean Water Act, including management of stormwater-related non-point source pollutants under the National Pollutant Discharge Elimination System. Mitigation to minimize storm damage would include the use of sustainable design principles and implementation of best management practices during and after construction. Contractors would implement best management practices for drainage and sediment control to prevent or reduce non-point source pollution and minimize soil loss and sedimentation in drainage areas. These practices may include silt fencing, filter fabric, temporary sediment ponds, temporary or permanent check dams, and immediate mulching of exposed areas to minimize sedimentation and turbidity impacts from construction activities. Erosion control measures would be left in place at the completion of construction to avoid adverse impacts on water and floodplain resources, after which time NPS staff would be responsible for maintenance and removal.

The project would utilize qualified NPS staff and/or certified wetland scientists to ensure wetlands and floodplains are protected during project implementation. Construction activities would be conducted with caution to prevent damage to wetlands and floodplains from equipment, erosion, siltation, or pollutant discharges. In addition, mitigation would ensure that structures and facilities are designed to be consistent with the intent of the standards and criteria of the National Flood Insurance Program (44 Code of Federal Regulations Part 60).

Construction or clearing of vegetation within the floodplains would be limited to the greatest extent possible. Watershed conditions would be restored after temporary impacts to reduce accelerated runoff caused by soil compaction; poor vegetation cover; or the unnatural conveyance of water by roads, ditches, or trails.

6.0 CONCLUSIONS

The proposed bridge replacement project is necessary to maintain a safe and reliable bridge crossing over the Tennessee River for Parkway users along the historic Natchez Trace Parkway, while minimizing effects on Parkway resources. The project is needed because the bridge has exceeded its 50-year design life. The bridge is a fracture-critical structure, and there are no redundancies in the bridge members. Failure of one girder could cause one or multiple spans to fail leading to the potential for collapse. In addition, there is widespread cracking in the bridge deck and notable deterioration of the bridge piers that routine maintenance can no longer address.

The NPS has determined that there are no practicable, non-floodplain locations for the proposed action. Risks to natural and beneficial floodplain values are minimal because the permanent net loss of floodplains would be minimal. Once the existing bridge piers, approaches, and impervious surfaces are decommissioned and the areas are restored to natural conditions, there may be a net gain of floodplains and improvement of existing floodplain conditions. The temporary disturbance areas would be reclaimed to pre-existing conditions, and the temporary construction staging area would be reclaimed to natural riparian forest. Concrete structures no longer in use would be removed, if applicable, and their disturbance areas would be returned to a condition equal to that which existed prior to the disturbance. Flood conditions in the project area typically occur during periods of heavy regional rainfall and are mitigated by the numerous TVA-managed dams and reservoirs upstream and downstream of the project area. The presence of the Wilson Dam upstream of the project area and Pickwick Landing Dam downstream of the project reduce Tennessee River flow velocities, and the proposed project would be designed to withstand predicted river hydraulics. The NPS finds that the floodplain impacts from the proposed action would be acceptable under the applicable regulations and guidance, including the policies and procedures of NPS Director's Order #77-2 (Floodplain Management) and Executive Orders 11988 and 13690.

7.0 REFERENCES

Alabama Historical Commission

- 2023 Memorandum from L. A. Wofford, to D. Neighbor, NPS. Alabama Historical Commission. AHC 22-0499, John Coffee Memorial Bridge Replacement Project, Colbert County. April 10.

Federal Emergency Management Agency (FEMA)

- 2021 *Flood Insurance Study*. Colbert County, Alabama. February 04.
- 2023 National Flood Hazard Layer. Accessed June 2023. <https://www.fema.gov/flood-maps/national-flood-hazard-layer>

LANDFIRE

- 2016 Existing Vegetation Type Layer, LANDFIRE 2.0.0, US Department of the Interior, Geological Survey, and US Department of Agriculture. Accessed May 2023. <https://landfire.gov/getdata.php>

McKay, M. P.

- 2021 Geologic Map and Cross Section of the Cherokee 7.5-Minute Quadrangle, Colbert and Lauderdale Counties, NW Alabama. Produced in cooperation with the U.S. National Park Service.

National Park Service (NPS)

- No Date *Foundation Document Overview*. Natchez Trace Parkway. Mississippi, Alabama, Tennessee. Accessed June 2023. <http://npshistory.com/publications/foundation-documents/natr-fd-overview.pdf>

2016 National Park Service Vegetation Mapping Inventory Program. Natchez Trace Parkway Vegetation Mapping Project. August. National Weather Service (NWS)

- 2023 Advanced Hydrologic Prediction Service. Accessed June 2023. https://water.weather.gov/ahps2/hydrograph.php?wfo=hun&gage=floal&hydro_type=2

Szabo, M. W., E. W. Osborne, C. W. Copeland, Jr., and T. L. Neathery

- 1988 *Geologic Map of Alabama, Geological Survey of Alabama Special Map 220, scale 1:250,000.*

Tennessee Valley Authority (TVA)

- 2023a Personal Communication Email correspondence between C. Williamson, Program Manager, Flood Risk, River Management, Tennessee Valley Authority, and B. Mays, Senior Environmental Planner, WSP, and others. June 6, 2023.
- 2023b *Wilson Reservoir Operating Guide*. Accessed June 2023 <https://www.tva.com/environment/lake-levels/pickwick/operating-guide>
- 2023c *Pickwick Reservoir Operating Guide*. Accessed June 2023. <https://www.tva.com/environment/lake-levels/wilson/operating-guide>

US Department of Agriculture-Natural Resources Conservation Service (USDA-NRCS)

2023a Web Soil Survey. Accessed February 2023. <http://websoilsurvey.nrcs.usda.gov/>

2023b Official Soil Series Descriptions. Accessed June 2023.
<https://soilseries.sc.egov.usda.gov/osdname.aspx>

US Environmental Protection Agency (USEPA)

2016 What Climate Change Means for Alabama. August. Accessed July 2023.
<https://19january2017snapshot.epa.gov/sites/production/files/2016-09/documents/climate-change-al.pdf>

US Geological Survey (USGS)

2023 TNM Download (v2.0). Accessed June 2023. <https://apps.nationalmap.gov/downloader/#/>

Volkert, Inc.

2022 *Wetland and Waters Delineation Report*. Natchez Trace Parkway John Coffee Memorial Bridge Lauderdale and Colbert Counties, Alabama. December.

WSP USA, Inc. (WSP)

2023 Natchez Trace Parkway Biological Evaluation. August 2023.

APPENDIX I: ACRONYMS AND ABBREVIATIONS

ACRONYMS AND ABBREVIATIONS

AHC	Alabama Historical Commission
APE	area of potential effect to historic properties
ALDOT	Alabama Department of Transportation
CEQ	Council on Environmental Quality
CFR	Code of Federal Regulations
CLI	Cultural Landscapes Inventory
CR	county road
CRIS	(NPS) Cultural Resource Inventory System
dBA	A-weighted decibels
EA	environmental assessment
EO	executive order
EPA	Environmental Protection Agency
ESA	Endangered Species Act
FEMA	Federal Emergency Management Agency
FGDC	Federal Geographic Data Committee
FHWA	Federal Highway Administration
FSOF	Floodplain Statement of Findings
HALS	Historic American Landscapes Survey
HAER	Historic American Engineering Record
HWY	Highway
LOS	level of service
mph	miles per hour
NAGPRA	Native American Graves Protection and Repatriation Act of 1990
NEPA	National Environmental Policy Act
NHPA	National Historic Preservation Act
NPS	National Park Service
NRHP	National Register of Historic Places
Parkway	Natchez Trace Parkway
PEPC	Planning, Environment & Public Comment
SHPO	State Historic Preservation Officer
TCL	traditional cultural landscape
TVA	Tennessee Valley Authority

US	United States
USACE	US Army Corps of Engineers
USC	United States Code
USCG	US Coast Guard
USFWS	US Fish and Wildlife Service
WNS	white-nose syndrome

APPENDIX J: REFERENCES

REFERENCES

Alabama Historical Commission

- 2023 Memorandum from L. A. Wofford to D. Neighbor, NPS. AHC 22-0499, John Coffee Memorial Bridge Replacement Project, Colbert County. April 20, 2023.

Alabama Birding Trails

- n.d. Natchez Trace Parkway – Rock Springs Nature Trail. Accessed May 31, 2023.
<https://alabamabirdingtrails.com/sites/natchez-trace-parkway-rock-springs-nature-trail/>

Alabama Department of Environmental Management (ADEM)

- 2022a 2022 Alabama Integrated Water Quality Monitoring and Assessment Report. Accessed June 6, 2023. <https://adem.alabama.gov/programs/water/waterforms/2022AL-IWQMAR.pdf>
- 2022b Alabama §303(d) List. Accessed June 6, 2023.
<https://adem.alabama.gov/programs/water/wquality/2022AL303dList.pdf>

Alabama Department of Public Health (ADPH)

- 2022 “Alabama Fish Consumption Advisories 2022”. Accessed August 21, 2024.
https://www.alabamapublichealth.gov/tox/assets/2022_fishadvisory.pdf

Alabama Department of Transportation (ALDOT)

- 2023 North Projects: Colbert County; Lauderdale County. Accessed May 26, 2023.
<https://aldotnews.com/project/>

Alabama Natural Heritage Program (ANHP)

- 2022 *Alabama Inventory List: the Rare, Threatened and Endangered Plants & Animals of Alabama*. Alabama Natural Heritage Program, Auburn University, Alabama.

Alabama Tourism Department

- 2022 *Alabama Tourism Industry Economic Impact Report*. 2021. Accessed 25 May 2023.
<https://tourism.alabama.gov/app/uploads/2022-Economic-Impact-Report-FINAL.pdf>

Alabama Wildlife Federation (AWF)

- 2023 Wonders of Wildlife: Little Brown Bat. Accessed May 3, 2023.
<https://www.alabamawildlife.org/wonders-of-wildlife-little-brown-bat/#:~:text=Other%20Common%20Names%3A%20Little%20brown,with%20no%20breeding%20colonies%20known>

AllStar Ecology, LLC (AllStar)

- 2022 Natchez Trace Parkway, John Coffee Memorial Bridge Replacement (Project Code 2022-0062488). Presence/Absence Mist Netting Survey Report for the Indiana Bat (*Myotis sodalis*) and Northern Long-Eared Bat (*Myotis septentrionalis*), Lauderdale and Colbert Counties, Alabama. August 26, 2022.

- 2023 Natchez Trace Parkway – NATR 315976, Replace the John Coffee Memorial Bridge (Tennessee River Bridge). *Environmental Assessment, Qualitative Mussel Survey Report*. USFWS Project Code 2022-0062488, Tennessee River, Colbert and Lauderdale Counties, Alabama. January 20, 2023.

American Association of State Highway and Transportation Officials (AASHTO)

- 2018 *A Policy on Geometric Design of Highways and Streets* (Green Book). 7th ed., September 28.

Burns, J. (RECON Offshore LLC)

- 2022 Memorandum to M. Meyers, New South Associates, Inc. Natchez Trace Parkway Project to replace the John Coffee Memorial Bridge (NATR 315976). Pensacola, FL.

California Department of Transportation (Caltrans)

- 2016 *Technical Guidance for Assessment and Mitigation of the Effects of Road Noise and Road Construction Noise on Birds*. Division of Environmental Analysis. June (Contract 43A0306). Sacramento, CA. Prepared by ICF International, Sacramento, CA, Robert Dooling, Gaithersburg, MD, and Arthur Popper, Silver Spring, MD. <https://dot.ca.gov/-/media/dot-media/programs/environmental-analysis/documents/env/noise-effects-on-birds-jun-2016-ally.pdf>

City of Florence

- 2023 Current Planning and Community Development Projects. Accessed December 31, 2023. https://florenceal.org/departments/planning_and_community_development/current_projects.php

Coco, J., C. Bennett, M. Myers, and R. Freeman (New South Associates, Inc.)

- 2023 “Ethnographic Study for the Replacement of the John Coffee Memorial Bridge/Tennessee River, Natchez Trace Parkway Environmental Assessment, Colbert and Lauderdale Counties, Alabama, NPS Project Number NATR 315976.” Draft Technical Report 4445 submitted to National Park Service Denver Service Center, Denver, CO.

Council on Environmental Quality (CEQ)

- 1997 *Environmental Justice Guidance Under the National Environmental Policy Act*. December 10. <https://ceq.doe.gov/docs/ceq-regulations-and-guidance/regs/ej/justice.pdf>
- 2022 National Environmental Policy Act Implementing Regulations Revisions. Final Rule. Federal Register vol. 87, no. 76, 23543–23470. April 20, 2022.

Decibel Pro

- n.d. *Decibel Sound Level Meter*. <https://decibelpro.app/blog/decibel-chart-of-common-sound-sources/>

De Vries, G., P. M. O'Donnell, and E. Bruce (Heritage Landscapes LLC)

- 2023 “John Coffee Memorial Bridge/Colbert Ferry Park/Water Route Cultural Landscape, Natchez Trace Parkway.” Historic American Landscape Survey. Draft data sheet.

FGDC (Federal Geographic Data Committee)

- 2013 *Classification of wetlands and deepwater habitats of the United States*. FGDC-STD-004-2013. Second Edition. Wetlands Subcommittee, Federal Geographic Data Committee and US Fish and Wildlife Service, Washington, DC.

Federal Highway Administration (FHWA)

- 2016 Guidance on Highway Preservation And Maintenance. February 25. Accessed June 1, 2023. <https://www.fhwa.dot.gov/preservation/memos/160225.cfm>
- 2017 Noise. Analysis and Abatement Guidance Updates. Accessed November 13, 2024. Last updated June 29, 2017. https://www.fhwa.dot.gov/ENVIRONMENT/noise/regulations_and_guidance/analysis_and_abatement_guidance/addguidance.cfm

Federal Emergency Management Agency (FEMA)

- 2009 FEMA Flood Map Service Center. Lauderdale County. 010323. 01077C0450D. Special Flood Hazard Areas. September 11, 2009. Accessed May 31, 2023. <https://msc.fema.gov/portal/search?AddressQuery=Lauderdale%20County%2C%20Alabama#searchresultsanchor>
- 2010 FEMA Flood Map Service Center. Colbert County. 010318. 01033C0100D. Special Flood Hazard Areas. February 17, 2010. Database. Accessed May 31, 2023. <https://msc.fema.gov/portal/search?AddressQuery=colbert%20county%2C%20Alabama#searchresultsanchor>

Francos, M., and X. Úbeda

- 2021 Prescribed fire management. Current Opinion in Environmental Science & Health, 100250.

GEOServices, LLC (GEOServices)

- 2023 “Karst Investigation Report.” May 2023.
- 2024 “Report of Geophysical Investigation, John Coffee Memorial Bridge Replacement or Rehabilitation.” February 2024.

Geological Survey of Alabama (GSA)

- 2018 Assessment of Groundwater Resources in Alabama, 2010-16. Accessed May 31, 2023. https://www.gsa.state.al.us/img/Groundwater/docs/assessment/00_B186_StatewideAssessment_Print_Document.pdf

Heritage Landscapes LLC (Heritage Landscapes)

- 2022 “Historical Context and Thresholds of Significance Draft, Replace the John Coffee Memorial Bridge/Tennessee River, Natchez Trace Parkway, Colbert and Lauderdale Counties, Alabama.” NPS Project Number NATR 315976. Draft report prepared for the National Park Service Denver Service Area, August 26, 2022.
- 2023 “National Park Service Cultural Landscapes Inventory, 2023: John Coffee Memorial Bridge/Colbert Ferry Park/Water Route Overlook, Natchez Trace Parkway.” Draft submitted March 26, 2023.

Hubert, J., E. Booms, R. Witbaard, and H. Slabbekoorn

- 2022 “Responsiveness and habituation to repeated sound exposures and pulse trains in blue mussels.” *Journal of Experimental Marine Biology and Ecology* 547:1–10.

Lauderdale County Government

- n.d. Parks and Recreation. Accessed May 26, 2023.
<https://lauderdalecountyal.gov/index.php/parks-recreation/>

Loar J. M., L. L. Dye, R. R. Turner, and S. G. Hildebrand

- 1980 *Analysis of Environmental Issues Related to Small-scale Hydroelectric Development, I: Dredging*. U.S. Department of Energy, Environment Sciences Division. Publication No. 1565. Oak Ridge National Laboratory. Oak Ridge, TN.

Loosanoff, V. L.

- 1962 *Effects of turbidity on some larval and adult bivalves*. US Bureau of Commercial Fisheries Biological Laboratory, Milford, CT. 17 pp.

Meyers, M., M. Colvin, J. Burns, J. Weber, B. Snyder, C. Bennett, A. Hronek, K. Kilgore, and R. Freeman (New South Associates, Inc.)

- 2024 “Cultural Resource Survey for Replacement of the John Coffee Memorial Bridge/Tennessee River Bridge, Natchez Trace Parkway Environmental Assessment, Colbert and Lauderdale Counties, Alabama, NPS Project Number NATR315976.” Draft Technical Report 4456 submitted to National Park Service Denver Service Center, May 5, 2023.

National Association of Realtors

- 2023 2022 Quarter 3 County Median Prices and Monthly Mortgage Payment by State. Accessed May 25, 2023. <https://cdn.nar.realtor/sites/default/files/documents/2022-q3-county-median-prices-and-monthly-mortgage-payment-by-state-12-20-2022.pdf>

National Park Service (NPS)

- 1998 *NPS-28: Cultural Resource Management Guideline*.
https://www.nps.gov/parkhistory/online_books/nps28/28contents.htm
- 2000 Director’s Order 47: *Sound Preservation and Noise Management*. Washington, DC.
- 2006 *NPS Management Policies 2006*. Washington, DC: US Department of the Interior, National Park Service.
- 2011 Director’s Order 12: *Conservation Planning, Environmental Impact Analysis, and Decision-making*.
- 2014 *Foundation Document Overview. Natchez Trace Parkway, Mississippi, Alabama, Tennessee*. US Department of the Interior, National Park Service.
https://www.nps.gov/natr/learn/management/upload/NATR_OV_2014_508-with-headings.pdf
- 2015a 2015 *NPS NEPA Handbook*.

- 2015b *NPS NEPA Handbook Supplemental Guidance: Preparing Focused and Concise Environmental Assessments*.
- 2016 *Cultural Resources Climate Change Strategy*. Washington, DC: Cultural Resources, Partnerships, and Science and Climate Change Response Program, National Park Service.
- 2018 “What We Do.” Accessed June 1, 2023. <https://www.nps.gov/subjects/infrastructure/what-we-do.htm>
- 2020 *Acoustic Inventory 2015–2017. Katmai National Park and Preserve, Alaska*. Natural Resource Report NPS/NRSS/NSNSD/NRR—2020/2080.
- 2021 Trail of Tears Water Route Overlook on the Natchez Trace. Accessed May 23, 2023. <https://www.nps.gov/places/trail-of-tears-water-route-overlook.htm>
- 2022 Repair or Replace the John Coffee Memorial Bridge. Accessed May 18, 2024. <https://parkplanning.nps.gov/projectHome.cfm?projectId=99418>
- 2023a Personal communication. Comments by D. Boensch, Natural Resource Program Manager, Natchez Trace Parkway, made during a call between NPS and WSP staff, on May 12, 2023, regarding preparation of the biological evaluation.
- 2023b NPS Integrated Resource Management Applications (IRMA) Portal. NPSpecies: Information on Species in National Parks. Species Checklist for Natchez Trace Parkway. Accessed April 11, 2023. <https://irma.nps.gov/NPSpecies/Search/SpeciesList/NATR>
- 2023c Natchez Trace Parkway. Accessed May 16, 2023. <https://www.nps.gov/natr/index.htm>
- 2023d Natchez Trace Visitation Statistics by Month. Accessed May 17, 2023. <https://irma.nps.gov/Stats/SSRSReports/Park%20Specific%20Reports/Visitation%20by%20Month?Park=NATR>
- 2024a *John Coffee Memorial Bridge/Colbert Ferry Park/Water Route Overlook Cultural Landscape, Lauderdale and Colbert Counties, Alabama*. Determination of Eligibility for the National Register of Historic Places Application Form.
- 2024b Mammals. Natchez Trace. <https://www.nps.gov/natr/learn/nature/mammals.htm>
- 2024c Park Visitation and Climate Change. *Warming Temperatures Likely to Alter Visitation across the National Park System*. Accessed May 21, 2024. <https://www.nps.gov/subjects/climatechange/visitation.htm>

Northwest Alabama Council of Local Governments (NACOLG)

- 2020 *2023-2027 CEDS Northwest Alabama Council of Local Governments*. Accessed 25 May 2023. <https://static1.squarespace.com/static/611034c15a42983f9ac71725/t/630d2c1966a77412b7514df0/1661807648444/2023-2027-CEDS.pdf>

Patón, D., R. Francisco, J. Cuenca, and J. C. Escudero

- 2012 “Tolerance to noise in 91 bird species from 27 urban gardens of Iberian Peninsula.” *Landscape and Urban Planning* 104:1–8.

Shannon G., M. F. McKenna, L. M. Angeloni, K. R. Crooks, K. M. Fristrup, E. Brown, K. A. Warner, M. D. Nelson, C. White, J. Briggs, S. McFarland, and G. Wittemyer

- 2016 “A synthesis of two decades of research documenting the effects of noise on wildlife.” *Biological Reviews* 91(4): 982–1005.

Shoals Area Metropolitan Planning Organization

- 2009 *US 72 Corridor Study*. Accessed May 26, 2023. https://static1.squarespace.com/static/611034c15a42983f9ac71725/t/61a67cd21a976a393c19690f/1638300906297/FinalReport_US72+corridor+study.pdf

Szabo, E.W., W. E. Osborne, C. W. Copeland, Jr., and T. L. Neathery

- 1988 *Geologic Map of Alabama*. Geological Survey of Alabama. Accessed May 24, 2023. https://ngmdb.usgs.gov/Prodesc/proddesc_55859.htm

Tennessee Valley Authority (TVA)

- n.d. The Lost Towns of Pickwick. Accessed May 31, 2023. <https://www.tva.com/about-tva/our-history/built-for-the-people/the-lost-towns-of-pickwick>
- 2023a Wilson Operating Guide. <https://www.tva.com/environment/lake-levels/Wilson/Operating-Guide>
- 2023b Pickwick Operating Guide. <https://www.tva.com/environment/lake-levels/Pickwick/Operating-Guide>
- 2024a. Wilson. <https://www.tva.com/energy/our-power-system/hydroelectric/wilson>
- 2024b. Pickwick Landing. <https://www.tva.com/energy/our-power-system/hydroelectric/pickwick-landing>

Toth, G.

- 2017 CSS Design Controls and Criteria: Traffic Projections and Levels of Service Targets. Federal Highway Administration, June 20. Accessed June 1, 2023. https://www.fhwa.dot.gov/planning/css/design/controls/traffic_projections.cfm

United States Army Corps of Engineers (USACE)

- n.d. Wilson Navigation Lock. Accessed May 31, 2023. <https://www.lrn.usace.army.mil/Locations/Navigation-Locks/Tennessee-River/Wilson/>

United States Census Bureau

- 2020 Longitudinal Origin-Destination Employment Statistics (LODES). Available online at <https://lehd.ces.census.gov/data/#lodes>. Accessed 25 May 2023.

- 2022a Decennial Census, Table P1 (Race); Table S1701 (Poverty status in the past 12 months). Accessed May 25, 2023. https://data.census.gov/table?q=p1&g=050XX00US01033,01077_1500000US010330210001,010330210002,010770112002,010770112003,010770113001,010770113002_160XX00US0114152,0180064&tid=DECENNIALPL2020.P1
- 2022b American Community Survey Five-Year Estimates, Table S1903 (Median income in the past 12 months). Accessed May 25, 2023. https://data.census.gov/table?q=median+income&g=050XX00US01033,01077_160XX00US0114152,0126896,0180064&tid=ACSST5Y2020.S1903

US Department of Agriculture, Natural Resources Conservation Service (USDA-NRCS)

- 2013 Official Soil Series Descriptions. Accessed February 13, 2023. <https://soilseries.sc.egov.usda.gov/osdname.aspx>
- 2024 Custom Soil Resource Report for Colbert County, Alabama and Lauderdale County, Alabama. US Department of Agriculture, Natural Resources Conservation Service. February 2024. [Web Soil Survey | Natural Resources Conservation Service \(usda.gov\)](#)

United States Department of Transportation (USDOT)

- 2022 RAISE 2022 Fact Sheets. Accessed May 26, 2023. <https://www.transportation.gov/sites/dot.gov/files/2022-08/RAISE%202022%20Award%20Fact%20Sheets.pdf>

United States Environmental Protection Agency (USEPA)

- 2016a *Promising Practices for EJ Methodologies in NEPA Reviews*. Federal Interagency Working Group on Environmental Justice and NEPA Committee. March. https://www.epa.gov/sites/default/files/2016-08/documents/nepa_promising_practices_document_2016.pdf
- 2016b *What Climate Change Means for Alabama*. August. Accessed May 24, 2023. <https://19january2017snapshot.epa.gov/sites/production/files/2016-09/documents/climate-change-al.pdf>
- 2021 *Climate Change and Social Vulnerability in the United States. A Focus on Six Impacts*. EPA 430-R-21-003. https://www.epa.gov/system/files/documents/2021-09/climate-vulnerability_september-2021_508.pdf

United States Fish and Wildlife Service (USFWS)

- 1985 *Recovery Plan for the Pink Mucket Pearly Mussel (Lampsilis orbiculata)*. US Fish and Wildlife Service, Southeast Region. Atlanta, GA. 47 pp.
- 2007 *Indiana Bat (Myotis sodalis) Draft Recovery Plan*. Department of the Interior, US Fish and Wildlife Service, Great Lakes-Big Rivers Region - Region 3. Fort Snelling, MN. 258 pp.
- 2009 *Gray Bat (Myotis grisescens) 5-Year Review: Summary and Evaluation*. US Fish and Wildlife Service, Midwest Region, Missouri Ecological Services Field Office. Columbia, MO. 34 pp.

- 2018a *Programmatic Biological Opinion for Transportation Projects in the Range of the Indiana Bat and Northern Long-Eared Bat*. US Fish and Wildlife Service Midwest Regional Office. Bloomington, IN.
- 2018b *Pink Mucket (Lampsilis abrupta) 5-Year Review: Summary and Evaluation*. US Fish and Wildlife Service, Asheville Field Office, Asheville, NC. 68 pp.
- 2019 *Indiana Bat (Myotis sodalis) 5-Year Review: Summary and Evaluation*. US Fish and Wildlife Service, Interior Region 3 – Great Lakes. Indiana Ecological Services Field Office Bloomington, Indiana. 91 pp.
- 2020a *Dromedary Pearlymussel (Dromus dromas) 5-Year Review: Summary and Evaluation*. US Fish and Wildlife Service, Southeast Region, Tennessee Ecological Services Field Office Cookeville, TN. 22 pp.
- 2020b *Monarch (Danaus plexippus) Species Status Assessment Report*, version 2.1. 96 pp.
- 2021a *Species Status Assessment (SSA) Report for the Tricolored Bat (Perimyotis subflavus)*. Version 1.1. December 2021. USFWS Northeast Region. Hadley, MA. Accessed May 2, 2023. <https://ecos.fws.gov/ServCat/DownloadFile/221212>
- 2021b *Rough Pigtoe (Pleurobema plenum) 5-Year Review: Summary and Evaluation*. US Fish and Wildlife Service, South Atlantic-Gulf Region, Kentucky Ecological Services Field Office. Frankfort, KY.
- 2023a US Fish and Wildlife Service Environmental Conservation Online System (ECOS). Accessed May 31, 2023. <https://ecos.fws.gov/ecp/>
- 2023b FWS Focus: Gray Myotis. Accessed May 31, 2023. <https://www.fws.gov/species/gray-myotis-myotis-grisescens>
- 2023c Bats in Alabama. US Fish and Wildlife Service, Alabama Ecological Services Field Office. Daphne, AL. Accessed May 31, 2023. <https://www.fws.gov/office/alabama-ecological-services/bats-alabama>
- 2024 Personal communication via email between J. Rowell, USFWS, and D. Boensch, NPS, on November 12, 2024, regarding gray bats.

United States Geological Survey (USGS)

- 2024 National Hydrography Dataset (ver. NHDPlus High Resolution). Accessed January 6, 2024. <https://www.usgs.gov/national-hydrography/access-national-hydrography-products>.

Volkert, Inc. (Volkert)

- 2022 “Wetland and Waters Delineation Report; Natchez Trace Parkway John Coffee Memorial Bridge; Lauderdale and Colbert Counties, Alabama.” Prepared for the National Park Service on behalf of WSP USA Solutions, Inc. December 2022.

Ward, K.

- 2021 What You Can’t See in the Tennessee River. Accessed August 21, 2024. <https://storymaps.arcgis.com/stories/0ae7ea467f5e4701a2968c11d94eb688>

Weather U.S.

- 2023 Climate and monthly weather forecast Florence, AL. Accessed May 31, 2023. <https://www.weather-us.com/en/alabama-usa/florence-climate>

White-nose Syndrome Response Team (WNS Response Team)

- 2023 Where is WNS now? Accessed June 1, 2023. <https://www.whitenosesyndrome.org/where-is-wns>

WSP USA Solutions Inc. (WSP)

- 2023a “Socioeconomic Analysis and Environmental Justice Report.” February 2023.
- 2023b “Statement of Findings for Protection of Floodplains (Executive Order 11988) Replacement of the John Coffee Memorial Bridge (Tennessee River Bridge).”
- 2024a “Draft Soundscape Study Report.” January 2024.
- 2024b “Disturbance Calculations.” August 2024.

**APPENDIX K:
LIST OF PREPARERS**

US Department of Interior, National Park Service

Natchez Trace Parkway

Name	Role
Douglas Neighbor	Superintendent
Deanna Boensch	Natural Resource Program Manager
Mark Holley	Facility Manager/Park Engineer
Chris Smith	Chief of Resource Management
Greg Smith	Landscape Architect

Denver Service Center

Name	Role
Kevin Tyler	Project Manager
Erin Chipps	Natural Resource Specialist
Jeremiah Glasz	Project Specialist
Wesley Wills	Cultural Resource Specialist

South Atlantic- Gulf Region

Name	Role
Amanda Griffis	Cultural Anthropologist and Tribal Liaison
Jami Hammond	Southeast Regional Environmental Coordinator

Other NPS Divisions

Name	Role
Dave Conlin	Archeologist/Chief of Submerged Resources Center
Kelsey White	Biologist/Natural Sounds and Night Skies Specialist

WSP USA

Name	Role	Qualifications
Lori Fox, AICP	Project Manager	BS, Environmental Policy MCP, Land Use and Environmental Planning
Alexandria Sentilles	Deputy Project Manager	BS, Sustainable Development MS, Urban and Environmental Planning
Phil Baigas	Wildlife Biologist	BS, Geography MS, Rangeland Ecology and Watershed Management
Joe Dalrymple	Biologist	BS, Environmental Science BS, Marine Biology MS, Marine Science
Kate Hoffman	GIS Specialist	BS, Environmental Science
Michael Lucia	Environmental Planner	BA, Geography and Environmental Policy MUP, Community and Regional Planning
Deborah Mandell	Senior Technical Editor	BA, Government MBA, Finance and Marketing
Sam Pendyala	Environmental Engineer, Acoustician	MS, Environmental Engineering MEng, Environmental Engineering
Madison Reckman	Environmental Planner	BS, Marine Science
Margaret Stover	Environmental Planner	BS, Environmental Studies MEM, Water Resources Science and Management
Joe Tomberlin	Architectural Historian	BA, History MHP, Historic Preservation
Susan Van Dyke	Environmental Planner	BS, Environmental Science
Erika Zamek	Geologist	BS, Environmental Science MS, Geology