

National Park Service  
U.S. Department of the Interior



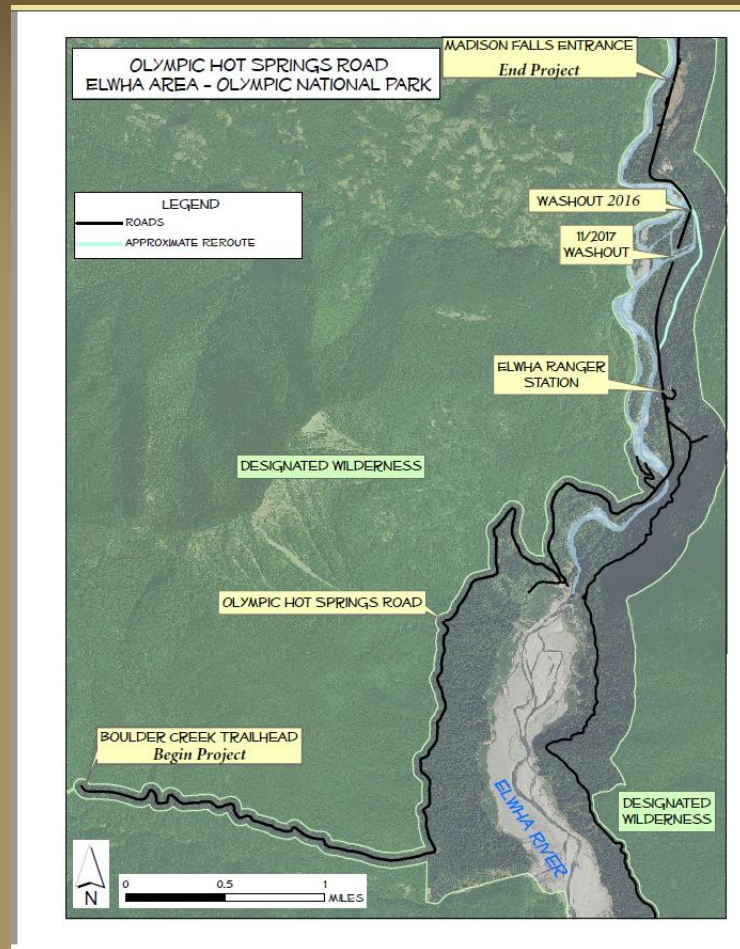
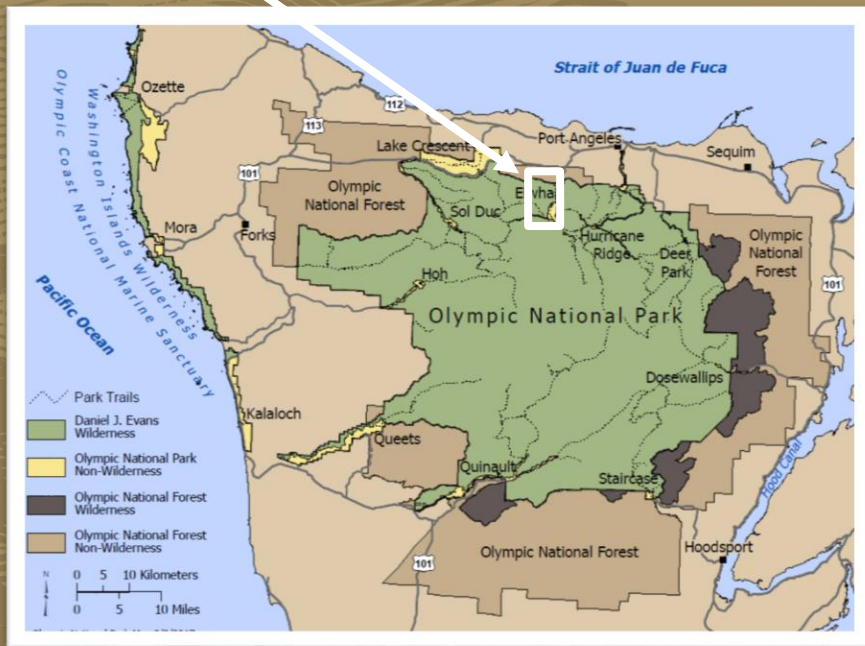
# Olympic Hot Springs Road

## Olympic National Park



# Project Location

## Olympic Hot Springs Road





# Background-Elwha Valley Access

- Trailheads
- Visitor Contact Station
- Picnic Areas
- River Boating Access
- Glines Canyon Overlook
- Olympic Hot Springs
- Administrative access
- Annual Visitation -180,000

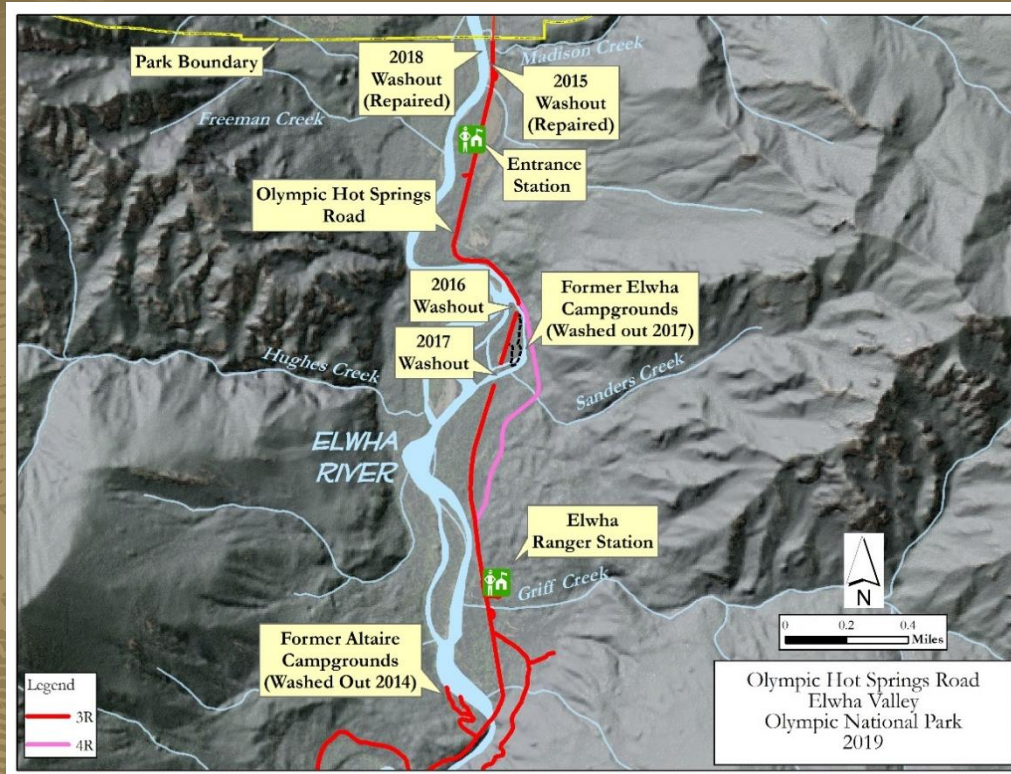


# Background: Dam Removal





# Post Dam Removal Flood Damage



# Road Flood Damage





# Activities to date

## ■ Concept comparisons

- Reroute(s)
- Bridge(s)
- Viaduct(s)
- Minor raise
- Tunnel
- No Action
- Strategic retreat

## ■ Geotechnical investigations

## ■ Field visit with Tribe

## ■ Civic engagement 12/2018

## ■ Agency meetings

## ■ Emergency repairs

- Bridges
- Bank stabilization
- Road closure

# Schematic Alternatives

- **No Action (\$650,000—Limited “retreat”)**

2004 Dam Removal SEIS states that road access shall be maintained  
NEPA requires consideration of no action alternative

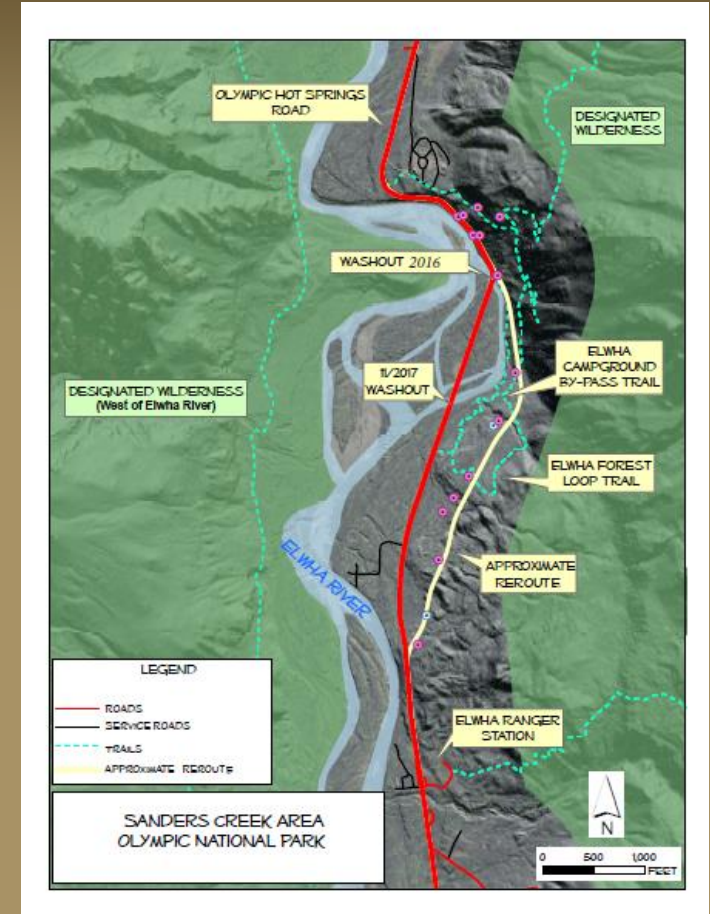
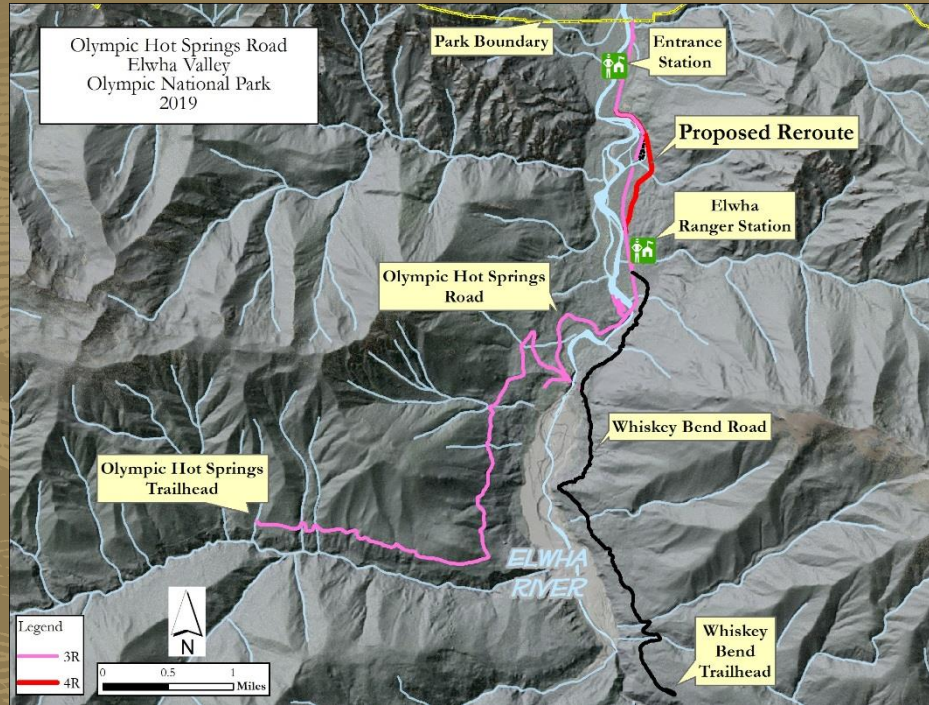
- **Realign road to edge of floodplain (\$8,300,000)**  
**PREFERRED ALTERNATIVE**

- **Raise road across floodplain (\$31,000,000)**

Raises road approximately 15' above current elevation

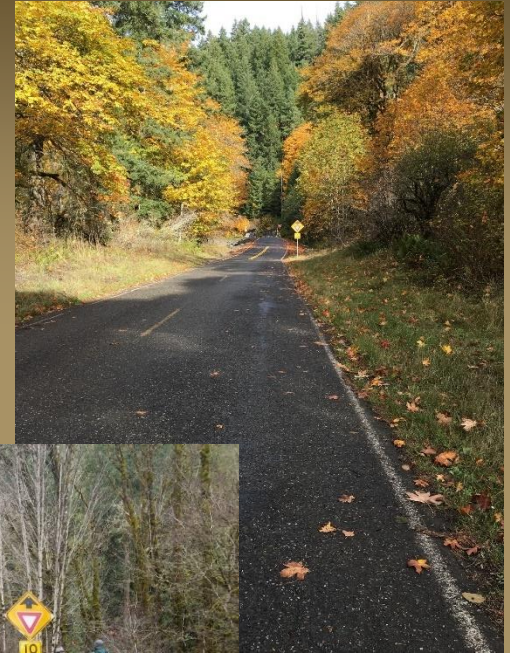


# Schematic Alternatives



# Criteria Considered

- Sustainable/reliable visitor access
- Character of existing corridor access
- Restoration of habitat for T&E fish species
- Riparian habitat disturbance
- Archeological disturbance
- Upland habitat disturbance
- Operational efficiency
- Capital costs
- Private property access
- Constructability





# Project Scope

## 1. Realign One Mile Olympic Hot Springs Road (Elwha Road) out of Elwha River Floodplain

- New alignment east of existing road
- 20' wide road, paved
- Almost entirely out of flood plain
- Retaining walls where constrained
- Bank stabilization/log jams
- Utilities underground

## 2. Rehabilitate 7+ Miles of Olympic Hot Springs Road

# Advantages Preferred Alternative

- Restore Access while minimizing riparian impacts
- Restore Administrative Facilities
- Reduce long-term impacts of road on river flood plain and fisheries
- Reasonable capital costs
- Least vulnerable action alternative to future flood events
- Lowest O&M costs
- Constructability





# Compliance

- Environmental Assessment released to public November 2019.
- Expected FONSI March 31, 2020
- On-going consultation and coordination with LEKT, SHPO, NMFS and USFWS
- Permitting by FHWA

# Schedule

Activity	Date
Decision Document (FONSI)	March 2020
Permits	March 2021
Design Complete	April 2021
Construction Award	June 2021
Begin Construction	July 2021
In water work (Diversion)	Early August 2021*
Construction Complete	October 2023

\* In water work is best in late July/early August



# Thank You

## Questions?

