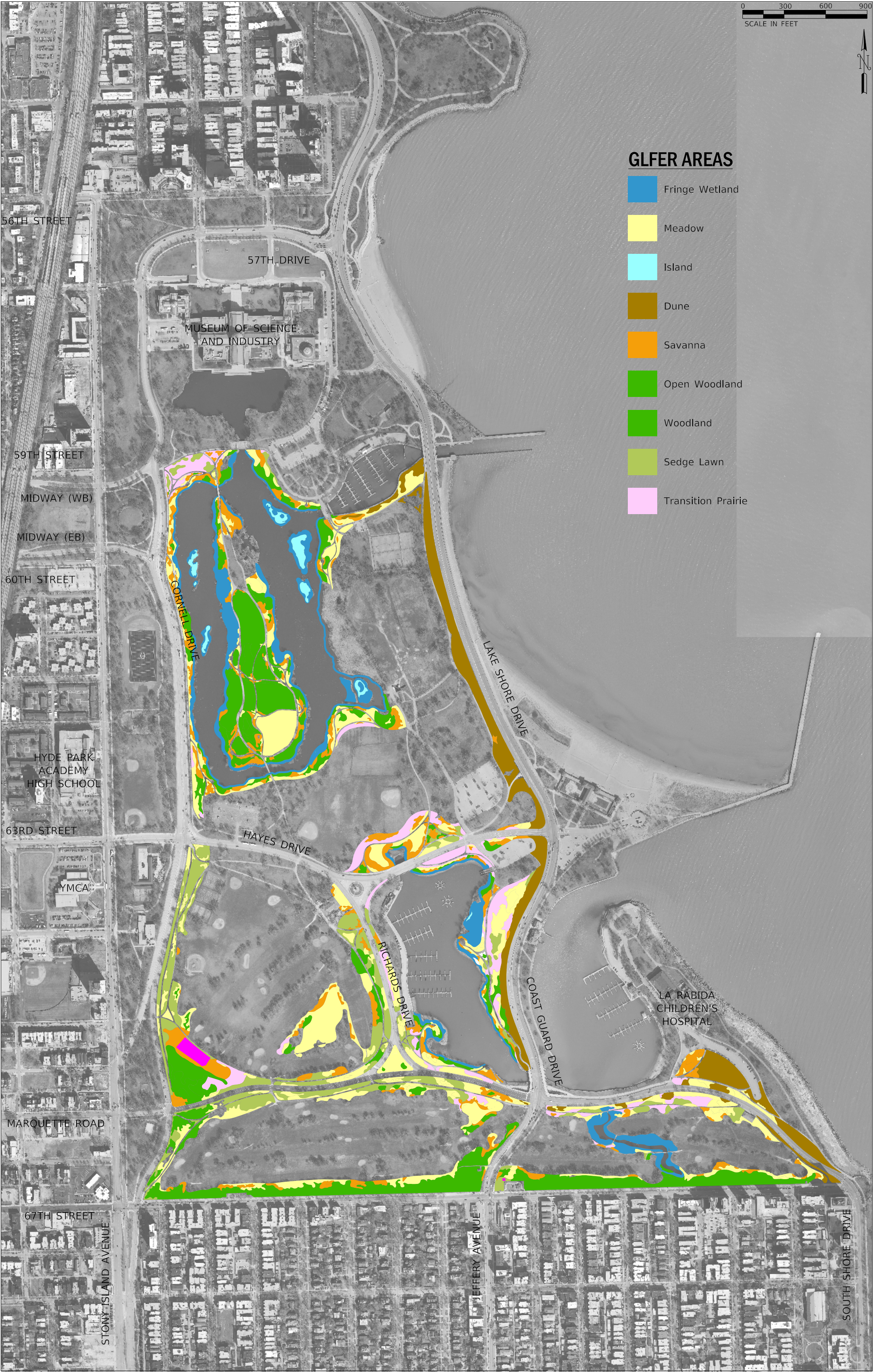


ATTACHMENT J-1

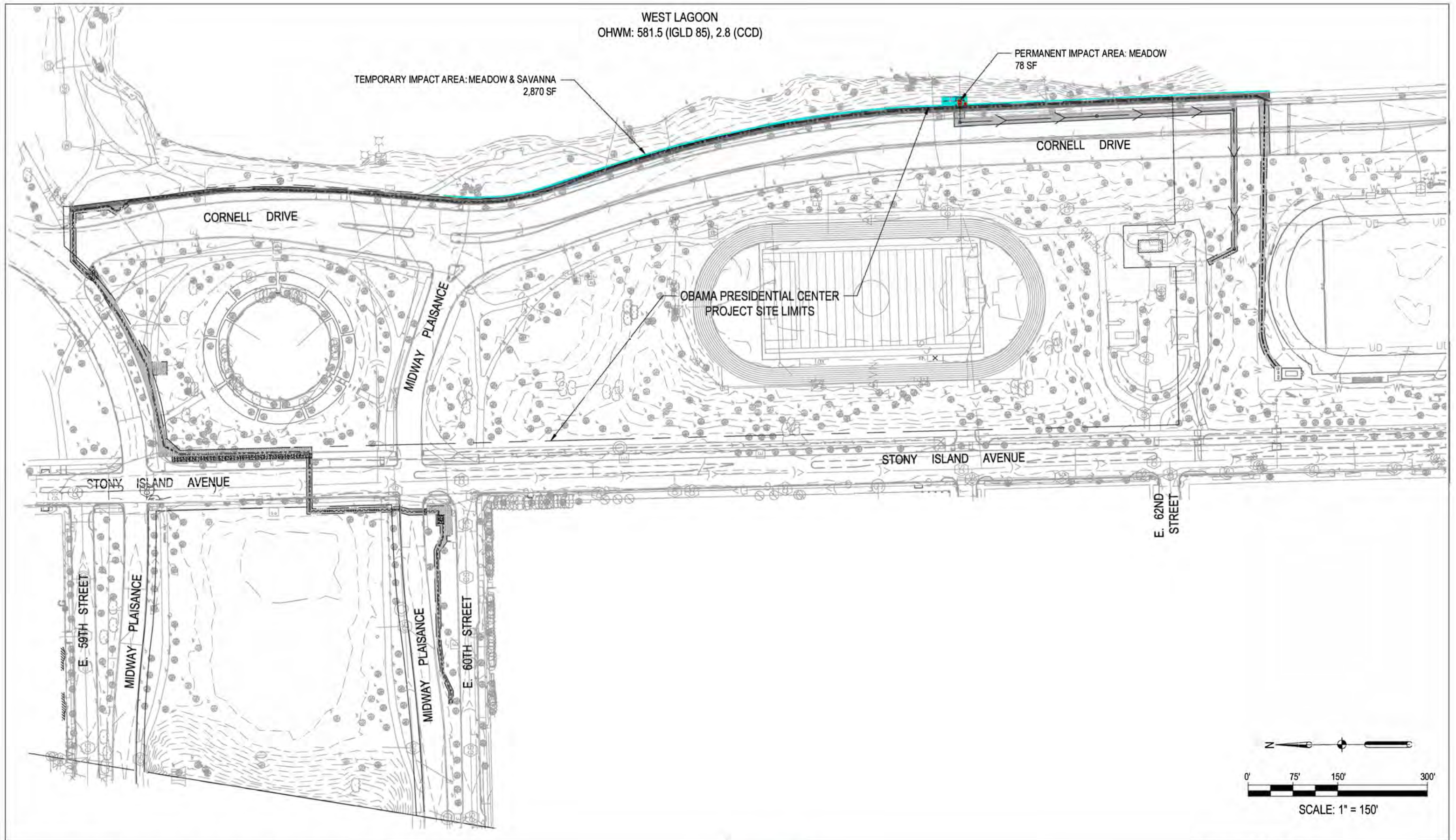
GLFER AREAS



GLFER AREAS

- Fringe Wetland
- Meadow
- Island
- Dune
- Savanna
- Open Woodland
- Woodland
- Sedge Lawn
- Transition Prairie

ATTACHMENT J-2
UTILITY RELOCATION GLFER AREA IMPACTS



JACKSON PARK - UTILITY RELOCATION PROJECT

GREAT LAKES FISHERY & ECOSYSTEM RESTORATION IMPACT EXHIBIT

LEGEND

- PERMANENT IMPACT AREAS (0.00 AC, 78 SF)
- TEMPORARY UTILITY/LAYOUT IMPACT AREAS (0.06 AC, 2,870 SF)

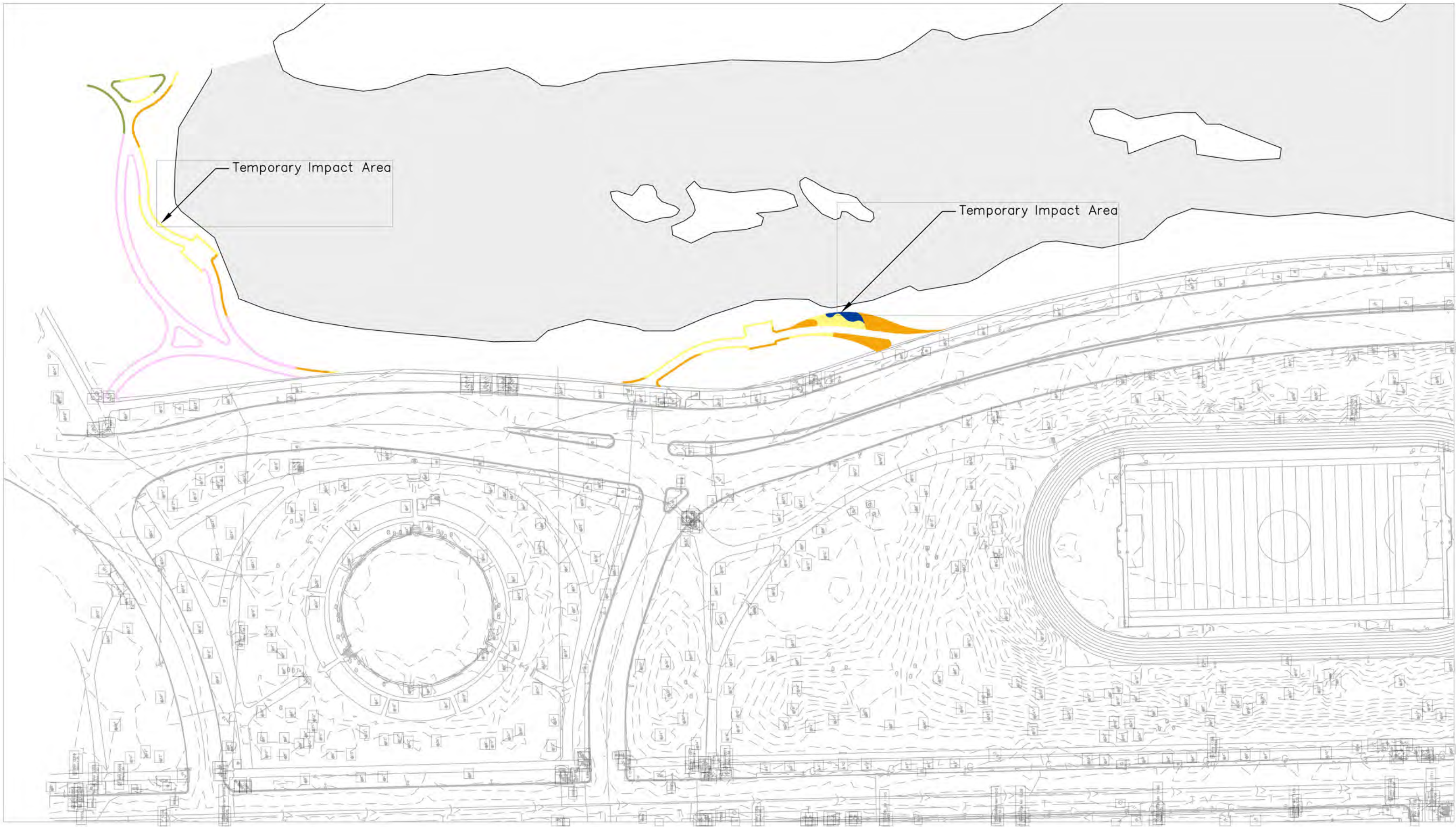
DATE: 04.08.2019

PREPARED BY:

SMITHGROUP

ATTACHMENT J-3

PATHWAY CONNECTIONS GLFER AREA IMPACTS



Temporary Impact Areas: Total: (0.166 AC)

Blue Fringe Wetland (0.005 AC)

Pink Transition Prairie (0.055 AC)

Orange Savanna (0.053 AC)

Yellow Meadow (0.046 AC)

Green Sedge Lawn (0.007 AC)

Permanent Impact Areas:

Red (0.000 AC)

MVVA, Inc.
06/28/2019

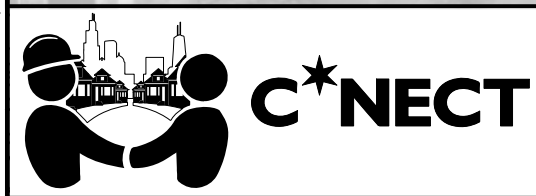
0 50' 100' 200'
Scale: 1" = 100'



ATTACHMENT J-4
GLFER AREA IMPACTS



MODEL: SHOMELNAME
FILE NAME: 17-B7203-00-ES GLFER Impacts.dgn



USER NAME	= untitled	DESIGNED -	MSA	REVISED -	
DRAWN	- MSA	REVIS	MSA	REVISED -	
PLOT SCALE	= 175.0000 ' / in.	CHECKED -	TKL	REVISED -	
PLOT DATE	= 3/9/2020	DATE	- MAR 2020	REVISED -	



OPC MOBILITY IMPROVEMENTS GLFER IMPACTS MAP				
SCALE: 1" = 175'	SHEET	OF	SHEETS	STA. TO STA.

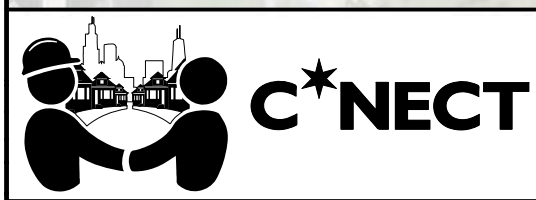
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	17-B7203-00-ES	COOK	1	1
CONTRACT NO. B-7-203				
ILLINOIS FED. AID PROJECT				

ATTACHMENT J-5

PROPOSED GLFER RESTORATION AND REPLACEMENT



MODEL: SHORDEL.NAMES
FILE NAME: ...00303GLFER3153_GLFER_00.dgn

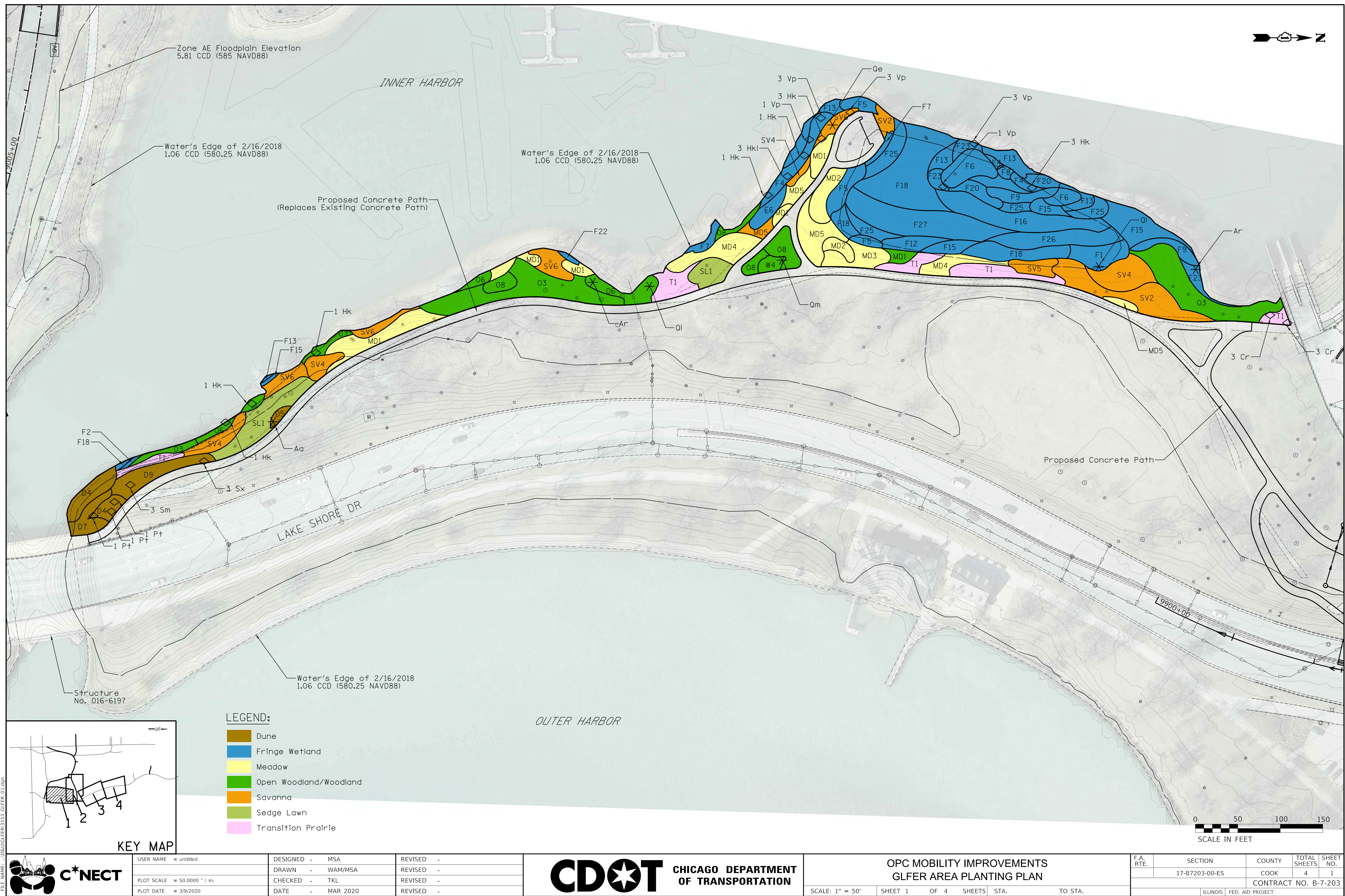


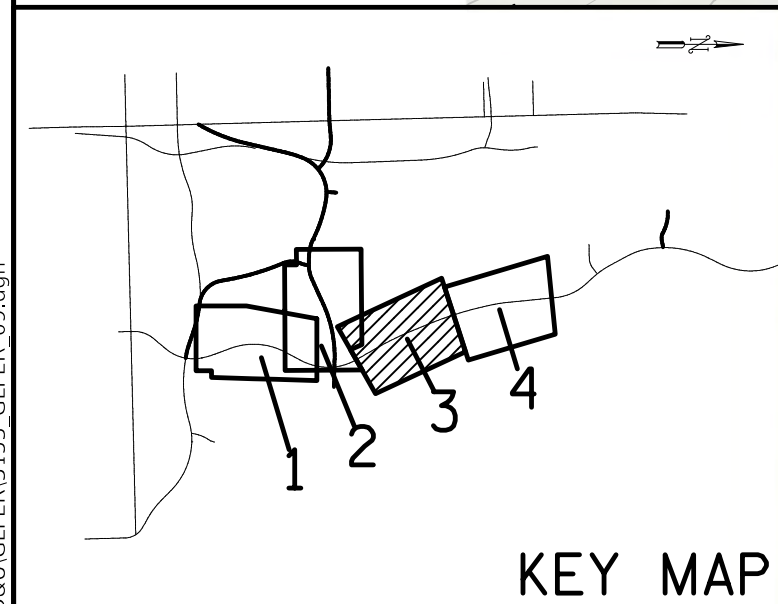
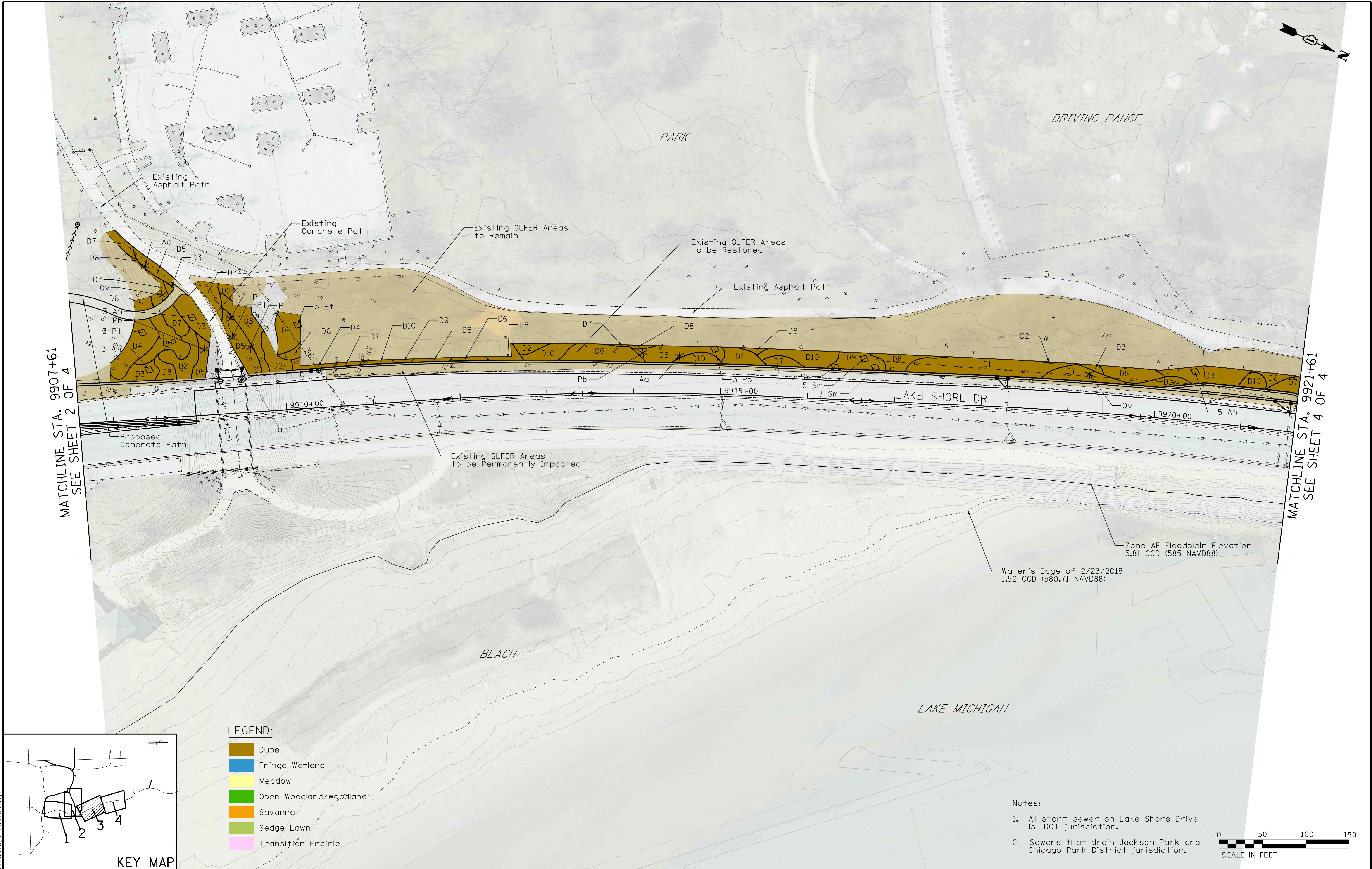
USER NAME	= untitled	DESIGNED -	MSA	REVISED -	
		DRAWN -	WAM/MSA	REVISED -	
PLOT SCALE	= 400.0000 ' / in.	CHECKED -	TKL	REVISED -	
PLOT DATE	= 3/9/2020	DATE -	MAR 2020	REVISED -	



OPC MOBILITY IMPROVEMENTS GLFER AREA PLANTING PLAN KEY MAP				
SCALE: 1" = 400'	SHEET	OF	SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	17-B7203-00-ES	COOK	1	1
CONTRACT NO. B-7-203				
ILLINOIS FED. AID PROJECT				



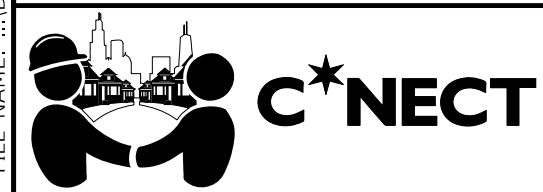


- LEGEND:
- Dune
 - Fringe Wetland
 - Meadow
 - Open Woodland/Woodland
 - Savanna
 - Sedge Lawn
 - Transition Prairie

- Notes:
- All storm sewer on Lake Shore Drive is IDOT jurisdiction.
 - Sewers that drain Jackson Park are Chicago Park District jurisdiction.



MODEL: SHORLNAME2
FILE NAME: C:\GIS\GLFER\3153_GLEER_09.dgn








USER NAME = untitled	DESIGNED - MSA	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN - WAM/MSA	REVISED -
PLOT DATE = 3/9/2020	CHECKED - TKL	REVISED -
	DATE - MAR 2020	REVISED -

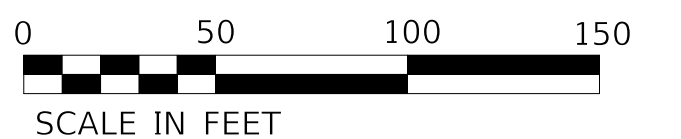


OPC MOBILITY IMPROVEMENTS GLFER AREA PLANTING PLAN			
SCALE: 1" = 50'	SHEET 3	OF 4 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	17-B7203-00-ES	COOK	4	3
CONTRACT NO. B-7-203				
ILLINOIS FED. AID PROJECT				



	Dune
	FrInge Wetland
	Meadow
	Open Woodland/Woodland
	Savanna
	Sedge Lawn
	Transition Prairie



ATTACHMENT J-6
USACE COORDINATION

Meeting Minutes

Re: Mobility Improvements to Support the SLFP
USACE Coordination Meeting

Date: June 26, 2018

Location: 30 N LaSalle Suite 2624

Time: 10:30 a.m.

Attending: See attached.

The primary purpose of the meeting was to begin coordination with the USACE regarding the Section 408 process due to the impacts to the GLFER project in Jackson Park as a result of the proposed OPC Mobility Improvements. The following is a summary of the meeting discussion:

Preferred Improvement Plan

- Lake Shore Drive from north of 57th Street to Hayes Drive would be widened to the west by 11 feet to provide a third southbound lane. The intersections of Lake Shore Drive at 57th Street would also be modified to accommodate the additional southbound lane. No widening is occurring to the east. Additionally, bridge modifications are required at the 63rd Street underpass, the 59th Street underpass, and the bridge over the 59th Street Harbor Inlet to accommodate the roadway widening.
- Stony Island Avenue from 59th Street to 64th Street would be widened to the east to provide an additional through lane in each direction, an all-day onstreet parking or loading zone lane on each side of the road, and a raised median with left-turn channelization at intersections. From 64th Street to 67th Street, Stony Island Avenue would be widened to provide a consistent cross section including three northbound lanes and four southbound lanes separated by a raised median. Intersections on Stony Island Avenue from 59th Street to 67th Street would be reconfigured to accommodate the roadway widening and provide additional turn lanes.
- Parking would be removed on Hayes Drive from Lake Shore Drive to Cornell Drive to provide two lanes in each direction, separated by a barrier median. Modifications are required at the Lake Shore Drive/Coast Guard Drive, Richards Drive, and Cornell Drive/63rd Street intersections with Hayes Drive to accommodate the additional through lanes as well as the proposed closure of Cornell Drive north of 63rd Street. The existing triangular, stop-controlled intersection at Hayes Drive and Richards Drive would be reconfigured to create a signalized T-intersection. Hayes Drive would be realigned at Cornell Drive to provide a through movement for predominant travel through the intersection. The existing portion of Hayes Drive between Stony Island Avenue and

Cornell Drive would be realigned to create a T-intersection with the realigned Hayes Drive.

Section 408 Coordination

- The GLFER project consists of restoration and improvements within Jackson Park that were completed and funded by the USACE and the Chicago Park District.
- Because the OPC Mobility Improvements will impact some of the restored GLFER areas within Jackson Park the Section 408 process will be required.
- The Section 408 process is a separate track from the 404 permit process. However the 408 process will need to be approved prior to the issuance of a 404 permit.
- The current status of the GLFER project is that the proposed improvements surrounding the East and West Lagoons are complete. In general the areas north of Hayes Drive have also been planted, along with the removal of invasive species. The areas south of Hayes Drive have had the invasive species removed, but have not been planted yet.
- The direct impacts to the GLFER project as a result of the OPC Mobility Improvements will need to be quantified and mitigated. It is preferred to mitigate the impacted areas within Jackson Park as close by as possible. The USACE stated that direct impacts may be mitigated through planting of the areas south of Hayes that were part of the original GLFER project but never planted. The Park District stated that they would consider recent updates to the South Lakefront Framework Plan in assessing potential mitigation areas.
- The GLFER project did not complete a detailed hydrologic/hydraulic analysis of the water levels in the lagoons. The water levels in the East and West Lagoons are generally maintained by a control structure located to the west of the 59th Street inlet. When Lake Michigan is low the weir/control structure maintains the levels in the East and West lagoons.
- Under existing conditions there are areas from Jackson Park that drain directly into the lagoons. Cornell Drive, north of Hayes discharges directly to the West Lagoon via an existing storm sewer. The roadway right of way on South Lake Shore Drive south of 59th Street as well as Hayes Drive discharges to the combined sewer on Stoney Island. The system is designed to take the 1-year flow (first flush) with the storm sewer overflow that exceeds the 1-year storm going to the Lagoons. In general the intent of the OPC Mobility Improvements is to maintain the existing drainage patterns as closely as possible.
- The USACE will be concerned with quality of flow/runoff entering the lagoons. If additional flow or volume is proposed the USACE will be concerned if the plants can

tolerate the increase or will be impacted and this could be considered an indirect impact to the GLFER project.

- As part of the 408 approval process a public notice is not required and the submittals will be reviewed at the District level. There is no formal 408 permit application or permit issued.
- The mitigated areas will require a Maintenance and Monitoring Plan. The maintenance and monitoring will likely be required for 5 years.
- The Chicago Park District will act as applicant for all Section 408 coordination with the USACE (including both CDOT and OPC work).

Schedule

- Based on previous coordination, the Section 408 coordination will likely require two months between the Chicago Park District and USACE. The Section 408 coordination will need to conclude prior to the Section 404 permit, which is needed in the February/March of 2019 time frame.

Next Steps/Immediate Action Items

- The USACE can provide the project specifications and quantity of plants that have been put in place.
- CONNECT (TKL) to complete an initial estimate of the GLFER impact areas as a result of the OPC Mobility Improvements.
- CONNECT (TKL) to complete a memorandum that describes and documents the hydrologic and hydraulic impacts to Jackson Park as a result of the OPC Mobility Improvements.

By: Tom Liliensiek, P.E. (CNECT)

Date: July 6, 2018

Meeting Materials:

1. Agenda
2. Attendance Roster

OPC Mobility Improvements – USACE Coordination Meeting

**June 26, 2018, 10:30 AM
30 North LaSalle Suite 2624**

The purpose of the meeting is to discuss with the USACE the proposed OPC Mobility Improvements and potential impacts within the corridor to the GLFER project.

1. Introductions
2. GLFER Project
 - History
 - Project Status
3. Roadway Preferred Improvement Plan
 - Lake Shore Drive
 - Stony Island Avenue
 - Hayes Drive
 - 59th Street Inlet Bridge
 - OPC Site Plan
4. Anticipated GLFER Impacts
 - Adjacent to OPC
 - Roadway Improvements
5. Section 408 Application Process
6. Mitigation
7. Next Steps

Attendance Roster

Re: OPC Mobility Improvements – USACE Coordination Meeting

Date: June 26, 2018

Location: Civiltech Engineering – 30 North LaSalle Suite 2624

Time: 10:30 a.m.

Attending:

NAME	AFFILIATION	INITIAL	EMAIL
John Sadler	CDOT	<i>[Signature]</i>	John.Sadler@cityofchicago.org
David Gleason	CDOT	<i>[Signature]</i>	David.Gleason@cityofchicago.org
Nate Roseberry	CDOT	<i>NK</i>	Nathan.Roseberry@cityofchicago.org
Vasile Jurca	CDOT	<i>VJM</i>	Vasile.Jurca@cityofchicago.org
Lauren Umek	CPD	<i>[Signature]</i>	Lauren.Umek@ChicagoParkDistrict.com
Heather Gleason	CPD	<i>HG</i>	Heather.Gleason@chicagoparkdistrict.com
Nicole Sheehan	CPD	<i>MS</i>	Nichole.Sheehan@ChicagoParkDistrict.com
Sarah Gelder	CPD	<i>SG</i>	Sarah.Gelder@ChicagoParkDistrict.com
William Raffensperger	IDOT	<i>call in</i>	William.Raffensperger@illinois.gov
Michael Murphy	USACE		michael.j.murphy@usace.army.mil
Kriston Buczak	USACE	<i>KAB</i>	kirston.a.buczak@usace.army.mil
Duane Mahone	IEI		DMahone@infrastructure-eng.com
Ken Smorynski	IEI	<i>KS</i>	KSmorynski@infrastructure-eng.com
Tom Liliensiek	Civiltech	<i>TKL</i>	TLiliensiek@civiltechinc.com
Mary Young	Civiltech	<i>conf.</i>	MYoung@civiltechinc.com
Tom Kracun	David Mason	<i>[Signature]</i>	tkracun@davidmason.com
Chris Brooks	Obama Foundation		cbrooks.consultant@obama.org
Aparna Bapu	Obama Foundation		abapu.consultant@obama.org
Roark Frankel	Obama Foundation		rfrankel@obama.org
<i>Robert Rack</i>	<i>LIVING HABITATS</i>	<i>RR</i>	<i>robertrack@livinghabitats.com</i>
<i>Robin Skumski</i>	<i>WACE</i>	<i>[Signature]</i>	

Jenn Hyman
Aaron Perles
Page 1 of 1

Civiltech
OPC consultant
conf

Meeting Minutes

Re: Mobility Improvements to Support the SLFP
USACE Coordination Meeting

Date: Nov. 16, 2018

Location: US Army Corp of Engineers -
231 South LaSalle Suite 1500

Time: 1:30 p.m.

Attending: See attached.

The primary purpose of the meeting was to present/update the USACE on the proposed Mobility Improvements to Support the South Lakefront Framework Plan (SLFP) and to discuss the anticipated permit requirements. The following is a summary of the meeting discussion:

Status of Project Federal Approval Process

The project is being processed as a joint federal approval by Federal Highway Administration (FHWA) and the National Park Service (NPS). NPS is the lead federal agency for the NEPA Environmental Assessment (E.A.), FHWA is the lead federal agency for the NHPA Section 106 Review. The project team presented at the NEPA / 404 agency coordination meetings for the FHWA portion of the E.A. earlier this year. The project team is now working on a combined E.A. for public review. The project team is also currently drafting a Section 106 Assessment of Effects and Section 4(f) document. The goal is to conclude the Phase I portion of the project in April 2019.

Preferred Improvement Plan

- Lake Shore Drive from north of 57th Street to Hayes Drive would be widened to the west by 11 feet to provide a third southbound lane. The intersections of Lake Shore Drive at 57th Street would also be modified to accommodate the additional southbound lane. No widening is occurring to the east (i.e. towards Lake Michigan). Additionally, bridge modifications are required at the 63rd Street underpass, the 59th Street underpass, and the bridge over the 59th Street Harbor Inlet to accommodate the roadway widening.
- The scope of the inlet bridge widening includes removing existing architectural stone cladding and reinstalling to the new bridge structure on the west face. The abutments will be extended to the west and new wingwalls will be constructed on the west side. The construction includes both temporary and permanent impacts to waters of the US, but the areas are minimal totaling less than 0.04 acres for both.
- Stony Island Avenue from 59th Street to 64th Street would be widened to the east to provide an additional through lane in each direction, an all-day on-street parking or

Meeting Minutes

loading zone lane on each side of the road, and a raised median with left-turn channelization at intersections. From 64th Street to 67th Street, Stony Island Avenue would be widened to provide a consistent cross section including three northbound lanes and four southbound lanes separated by a raised median.

- Parking would be removed on Hayes Drive from Lake Shore Drive to Cornell Drive to provide two lanes in each direction, separated by a barrier median. Modifications are required at the Lake Shore Drive/Coast Guard Drive, Richards Drive, and Cornell Drive/63rd Street intersections with Hayes Drive to accommodate the additional through lanes as well as the proposed closure of Cornell Drive north of 63rd Street. The existing triangular, stop-controlled intersection at Hayes Drive and Richards Drive would be reconfigured to create a signalized T-intersection. Hayes Drive would be realigned at Cornell Drive to provide a through movement for predominant travel through the intersection. The existing portion of Hayes Drive between Stony Island Avenue and Cornell Drive would be realigned to create a T-intersection with the realigned Hayes Drive.
- The USACE noted that they will be a cooperating agency for the NEPA E.A. and will be a signatory to the NHPA Section 106 Memorandum of Agreement (MOA) Programmatic Agreement (PA).

Section 404 Permit

- The widening of the Lake Shore Drive Bridge spanning the 59th Street Inlet will result in permanent fill/impact to Lake Michigan and will therefore require a Section 404 Permit from the USACE. This is the only anticipated project permanent impact to USACE jurisdictional wetlands or Waters of the US as a result of the Mobility Improvements. It is anticipated that the bridge construction would be covered under Regional Permit 3 (Transportation Projects) and the cofferdams under Regional Permit 7 (Temporary Impacts). The City is also considering repairs to the existing Hayes Drive Bridge spanning the inner harbor. Proposed repairs to the substructure may require temporary cofferdams and dewatering, but no permanent impacts.
- The USACE inquired if coordination with the Coast Guard had occurred to confirm that they do not have jurisdiction. *Subsequent to the meeting the City/CNECT received correspondence from the US Coast Guard confirming that a Section 9 Permit will not be required. The correspondence with the US Coast Guard will be included the 404 application to the USACE.*

Meeting Minutes

- The USACE has established a permit number, LRC-2017-676, for the proposed OPC Mobility Improvements. This permit number should be used on all future submittals to the USACE.
- The Federal process has initiated the Section 7 (i.e. endangered species) consultation. The coordination is ongoing, however IDNR has already indicated that there are no anticipated impacts to state endangered species.

Section 408 Coordination

- The GLFER project consists of restoration and improvements within Jackson Park that were completed and funded by the USACE and the Chicago Park District.
- Because the OPC Mobility Improvements will impact some of the restored GLFER areas within Jackson Park the Section 408 process will be initiated.
- The Section 408 process is a separate track from the 404 process, with different USACE staff providing the review. However the 408 process will need to be approved prior to the issuance of a 404 permit.
- Currently, GLFER improvements have been completed around the east and west lagoons (north of Hayes Drive). Invasive species have been removed south of Hayes Drive, GLFER planting scope still remains to be completed
- The direct impacts to the GLFER project as a result of the OPC Mobility Improvements have been quantified. CNECT and the City are currently working with the Chicago Park District to develop the restoration and improvement plan.
- The Chicago Park District will be the applicant for the 408 application.

Schedule

- The NEPA Public Hearing for the project is planned for February 2019
- Anticipate NEPA Determination in the Spring of 2019.
- Advertisement of the project construction documents after NEPA Determination, possibly in late April 2019 with construction beginning in the Fall of 2019.

Meeting Minutes

Next Steps/Immediate Action Items

- CNECT/City to inform the USACE when the Environmental Assessment (EA) is ready.
- Submit 404/408 permit applications for USACE review.

By: Tom Liliensiek, P.E. (CNECT)

Date: December 10, 2018

Meeting Materials:

1. Agenda
2. Attendance Roster

OPC Mobility Improvements – USACE Coordination Meeting
November 16, 2018, 1:30 PM
USACE Office – 231 South LaSalle

The purpose of the meeting is to present/update to the USACE the proposed OPC Mobility Improvements and anticipated permit requirements.

1. Introductions
2. Preferred Improvement Plan
 - Lake Shore Drive
 - Stony Island Avenue
 - Hayes Drive
 - 59th Street Inlet Bridge
3. Status of Environmental Reviews
 - NPS / FHWA Co-Federal Leads
 - NEPA E.A. Status (NPS)
 - NHPA Section 106 Status (FHWA)
4. Section 404 Process
 - Status of the bridge design and review
 - Anticipated impacts
 - Schedule
5. Section 408 Process
 - Anticipate GLFER Impacts
 - CDOT / CPD Coordination
6. Overall Project Schedule

Attendance Roster

Re: OPC Mobility Improvements – USACE Coordination
Meeting - LRC-2017-676

Date: November 16, 2018

Location: US Army Corp of Engineers –
231 South LaSalle Suite 1500

Time: 1:30 p.m.

Attending:

NAME	AFFILIATION	INITIAL	EMAIL
John Sadler	CDOT	<i>JS</i>	John.Sadler@cityofchicago.org
Nate Roseberry	CDOT	<i>NR</i>	Nathan.Roseberry@cityofchicago.org
Colin Smalley	USACE	<i>CS</i>	Colin.C.Smalley@usace.army.mil
Kathy Chernich	USACE	<i>via phone</i>	Kathleen.G.Chernich@usace.army.mil
Ken Smorynski	IEI	<i>KS</i>	KSmorynski@infrastructure-eng.com
Tom Liliensiek	Civiltech	<i>TKL</i>	TLiliensiek@civiltechinc.com



DEPARTMENT OF THE ARMY
U.S. ARMY CORPS OF ENGINEERS, CHICAGO DISTRICT
231 SOUTH LASALLE STREET SUITE 1500
CHICAGO, IL 60604

26 February 2020

SUBJECT: Section 408 Request Number: 19-17 Jackson Park

Ms. Cathy Breitenbach
Director of Natural and Cultural Resources
541 North Fairbanks
Chicago, Illinois 60611

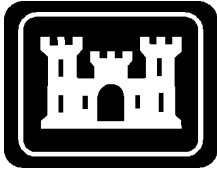
Dear Ms. Breitenbach,

The Chicago District ("District") of the U.S. Army Corps of Engineers (USACE) has received your request for changes related to the City of Chicago Department of Transportation (CDOT) roadway improvements, Chicago Park District (CPD) utility relocation, and the connection of pathways within the Obama Presidential Center (OPC) site to existing project pathways, to the Section 506 Great Lakes Fishery and Ecosystem Restoration (GLFER) Project, Jackson Park, operated and maintained by the CPD under Section 14 of the Rivers and Harbors Act of 1899, 33 USC 408 (Section 408).

The District has reviewed your submittal consistent with Engineer Circular (EC) 1165-2-220 and has determined that your Section 408 request is complete and will proceed to USACE review and decision. The District expects to render a decision on your Section 408 request within 90 days of the date on this letter. You will be notified if additional information is needed to complete the review and decision and/or if the review and decision to exceed the 90 day timeline. If you have questions regarding your Section 408 request, please contact Natalie Mills at Natalie.R.Mills@usace.army.mil.

Sincerely,

Natalie Mills
408 Coordinator
Chicago District
U.S. Army Corps of Engineers



PUBLIC NOTICE

REQUEST FOR PERMISSION TO MODIFY A U.S. ARMY CORPS OF ENGINEERS PROJECT UNDER SECTION 408

**U.S. ARMY CORPS OF ENGINEERS
CHICAGO DISTRICT**

PUBLIC NOTICE/APPLICATION NUMBER: 19-17 Jackson Park

**COMMENT PERIOD BEGINS: 01 April 2020
COMMENT PERIOD IS EXTENDED TO: 15 May 2020**

U.S. Army Corps of Engineers, Chicago District
231 S. LaSalle Street, Suite 1500
Chicago, IL 60604

REQUESTER

Chicago Park District
541 N Fairbanks, Chicago, IL 60611

PROPOSED ACTION

The proposed federal actions are in response to the decision made by the City to allow for the construction of the Obama Presidential Center (OPC) in Jackson Park and to improve the roadway network in and around Jackson Park. These actions would alter the USACE Section 506 Great Lakes Fishery and Ecosystem Restoration (GLFER) Project in Jackson Park. Proposed actions include Chicago Department of Transportation (CDOT) roadway improvements, Chicago Park District (CPD) utility relocation, and the connection of pathways within the Obama Presidential Center (OPC) site to existing GLFER project pathways. GLFER areas that would be temporarily impacted by proposed actions would be restored in place and permanently impacted areas would be replaced at a different location within the GLFER project boundaries. A detailed description of this proposal is provided on page 2 of this notice.

LOCATION OF PROPOSED ACTION

Location: Chicago, Illinois; Jackson Park

Interested parties are hereby notified that a request for permission to modify a Federal project has been received for the activity described herein and as shown on the attached drawings. You are invited to provide your comments by **15 May 2020** on the proposed work, which will become part of the record and will be considered in the decision on the request. Permission will be issued or denied under Section 14 of the Rivers and Harbors Act of 1899 (33 U.S.C. 408).

Written comments shall be mailed to:

U.S. Army Corps of Engineers, Chicago District,
Planning, Programs and Project Management Division,
Project Management Branch
ATTN, Ms. Natalie Mills, Section 408 Program Manager
231 South LaSalle Street, Suite 1500
Chicago, Illinois 60604-1437

Electronic comments may be sent to:

chicagodistrict.pao@usace.army.mil

It should be noted that ALL comments received by this office (via hard copy or electronic) will only be accepted with the full name and address of the individual commenting, and must be received by the close of the public notice period.

PROJECT DESCRIPTION

The Jackson Park Ecosystem Restoration project was completed in 2019. The purpose of the project was to restore ecological health to natural areas at Jackson Park while preserving the historical and cultural integrity of the Park. Native plants and seeds were used to establish habitat including fringe wetland marsh, dune and swale, and oak savanna. New walking paths and scenic overlooks were constructed to allow park users to circulate throughout the park. The completed project provides valuable habitat for a wide variety of insects and wildlife. Presently, CDOT, CPD, and OPC have proposed projects that would have both temporary and permanent impacts to the restored GLFER areas within Jackson Park.

Roadway improvements proposed by CDOT involve the widening of southbound Lake Shore Drive including the 59th Street inlet bridge. Pedestrian underpasses are proposed near the intersections of Lake Shore Drive and Hayes Drive as well as Cornell Drive and Hayes Drive.

OPC and CPD propose to close Cornell Drive from 59th Street to 63rd Street and convert the area to open space. Grading will occur and pathways will be constructed to connect

OPC walking paths with the GLFER trails. GLFER paths in the northwest portion of the project area will be converted from crushed stone to concrete. CPD proposed to construct a new lift station and electrical ductbank east of Cornell Drive.

The proposed work discussed above will have both temporary and permanent impacts to the restored GLFER areas. Temporary impacts are anticipated to be 1.7 acres and will be restored in place using the GLFER planting guidelines. Permanent impacts are expected to be 1.32 acres, and will be replaced with 2.43 acres of restored natural area in GLFER project area 2. Project area 2 was identified as the next highest priority area for the GLFER project had funding been available to execute the work.

ANTICIPATED ENVIRONMENTAL IMPACTS

Based on an initial review of the documentation provided by the CPD, it does not appear that the proposed project would result in any significant adverse environmental impacts within the footprint of the Jackson Park Section 506 Project.

REGULATORY AUTHORITY

This request will be reviewed according to the provisions of Section 14 of the Rivers and Harbors Act of 1899

JURISDICTION

This request will be reviewed according to the provisions of Section 14 of the Rivers and Harbors Act of 1899.

EVALUATION FACTORS

The decision whether to grant the requested permission for project modification under Section 408 will be based on several factors. That decision will reflect the national concern for both protection and utilization of important resources. The benefit which reasonably may be expected to accrue from the proposal must be balanced against its reasonably foreseeable detriments. Review of the requests for modification will be reviewed by a USACE technical review team considering the following factors:

- 1) Impair the Usefulness of the Project Determination. The review team will determine if the proposed alteration would limit the ability of the project to function as authorized, or would compromise or change any authorized project conditions, purposes or outputs. The decision whether to approve a request for modification would be based on a determination of no impairments.
- 2) Injurious to the Public Interest Determination. Proposed alterations will be reviewed to determine the probable impacts, including cumulative impacts, on the public interest. Factors that may be relevant to the public interest depend upon the type of USACE project being altered and may include, but are not limited to, such things as conservation, economic development, historic properties, cultural resources, environmental impacts, water supply, water quality, flood hazards, floodplains, residual risk, induced damages, navigation,

shore erosion or accretion, and recreation. This evaluation will consider information received from the interested parties, including tribes, agencies, and the public. The decision whether to approve the requested modification will be determined by the consideration of whether benefits are commensurate with risks associated with the proposed modification. If the potential detriments are found to outweigh the potential benefits, then it may be determined that the proposed alteration is injurious to the public interest.

- 3) Environmental Compliance. A decision on a Section 408 request is a Federal action, and therefore subject to the National Environmental Policy Act (NEPA) and other environmental compliance requirements. While ensuring compliance is the responsibility of USACE, the requester is responsible for providing all information that the USACE district identifies as necessary to satisfy all applicable Federal laws, executive orders, regulations, policies, and ordinances. NEPA and other analysis completed to comply with other environmental statutes (e.g. Endangered Species Act) should be commensurate with the scale and potential effects of the activity that would alter the USACE project. The district will work with the requester to determine the requirements, which will be scaled to the likely impacts of the proposed alteration and should convey the relevant considerations and impacts in a concise and effective manner.

All factors that may be relevant to the proposal will be considered, including the potential cumulative effects associated with the proposed project. The review will consider whether the proposed project is injurious to the public interest, and its potential impact to the usefulness of the Federal project. Policy and legal compliance will also be considered.

The USACE is also soliciting comments from the public, Federal, state and local agencies, Indian tribes, and other interested parties in order to consider and evaluate the potential impacts of the proposed activity. Once this office completes a review of the comments received, it will determine whether to provide permission to modify the project under Section 408.

To prepare this decision, comments are taken into consideration to assess impacts on the public interest factors listed above, as well as endangered species, historic properties, water quality, and general environmental effects. Comments will be used in the preparation of environmental compliance documentation as required by the National Environmental Policy Act (NEPA).

PRELIMINARY EVALUATION OF SELECTED FACTORS

INJURIOUS TO THE PUBLIC INTEREST DETERMINATION

It has been determined that the proposed alteration to the Jackson Park GLFER project to construct roadway improvements, relocate utilities, resurface and connect walking paths will not be injurious to the public interest. All areas

impacted temporarily for construction will be restored using the GLFER planting plan as a guide and permanent areas will be replaced in GLFER project area 2, so the alteration will not affect any of the public interest factors. (GLFER project area 2 was part of the approved decision document and integrated EA, but was not constructed due to a lack of federal funding.) As a result, there will be a net gain of 1.11 acres of restored native GLFER areas resulting from the proposed actions. Consequently, factors relevant to the public interest including conservation, economic development, historic properties, cultural resources, environmental impacts, water quality, flood hazards, residual risk, and induced damages, etc. are not changed by the alteration. All of these public interest factors were evaluated under NEPA and documented in the original project's environmental assessment (EA) during the pre-authorization phase of the project, and remain unchanged by the proposed alteration.

USEFULNESS OF THE PROJECT

It has been determined that the proposed alteration would have no deleterious impact on the usefulness of the Jackson Park GLFER project. To the contrary, the design of the proposed alteration will improve usefulness of the GLFER project by increasing natural area acreage as well as improving park accessibility through pathway connections to the Obama Presidential Center.

ENVIRONMENTAL COMPLIANCE

An evaluation of environmental compliance has been initiated to support the issuance of permission under Section 408. An initial review of the documentation provided by the applicant, CPD, and the final integrated EA for the Jackson Park 506 project has been completed. The report and FONSI can be found at the following url:

<https://www.lrc.usace.army.mil/Portals/36/docs/projects/Jackson%20Park/Jackson%20Park%20Report%20EA%20FONSI..pdf>. . USACE is also aware of ongoing

coordination associated with the EA for Federal Actions In and Adjacent to Jackson Park. A preliminary determination has been made that the applicant's request will not have a significant effect on the human or natural environment within the footprint of the Section 506 project. Therefore, the EA supports a Finding of No Significant Impact, and a decision has been made that an Environmental Impact Statement (EIS) is not required. Upon completion of the public notice period, consideration will be given to comments received.

ENVIRONMENTAL IMPACT STATEMENT

A preliminary determination has been made that an environmental impact statement is not required for the proposed work.

SUMMARY

It should be noted that materials submitted as part of the Section 408 request become part of the public record and are thus available to the general public under the procedures of the Freedom of Information Act (FOIA). Individuals may submit a written request to obtain materials under FOIA or make an appointment to view the project file at the Chicago District Corps of Engineers, Office of Counsel.

Interested parties wishing to comment on the proposed activity must do so in writing no later **15 May 2020**. It is presumed that all parties receiving this notice will wish to

respond to this public notice; therefore, a lack of response will be interpreted as meaning that there is no objection to the project as described.

This public notice is not a paid advertisement and is for public information only. Issuance of this notice does not imply Corps of Engineers endorsement of the project as described.

If you have any questions, please contact Ms. Natalie Mills, Section 408 Program Manager, by telephone at 312-846-5561 or via email at ChicagoDistrict.PAO@usace.army.mil. **It should be noted that ALL comments received by this office (via hard copy or electronic) will only be accepted with the full name and address of the individual commenting.**

FOR THE DISTRICT COMMANDER:

ORIGINAL SIGNED

A handwritten signature in cursive script that reads "Susanne J. Davis".

Susanne J. Davis, P.E.
Chief, Planning Branch

Jackson Park GLFER Project Impacts

0 500 1,000 2,000 Feet



Pave crushed stone paths with concrete

Relocate utilities

Covert Cornell Dr. to greenspace

Widen 59th Street Inlet Bridge

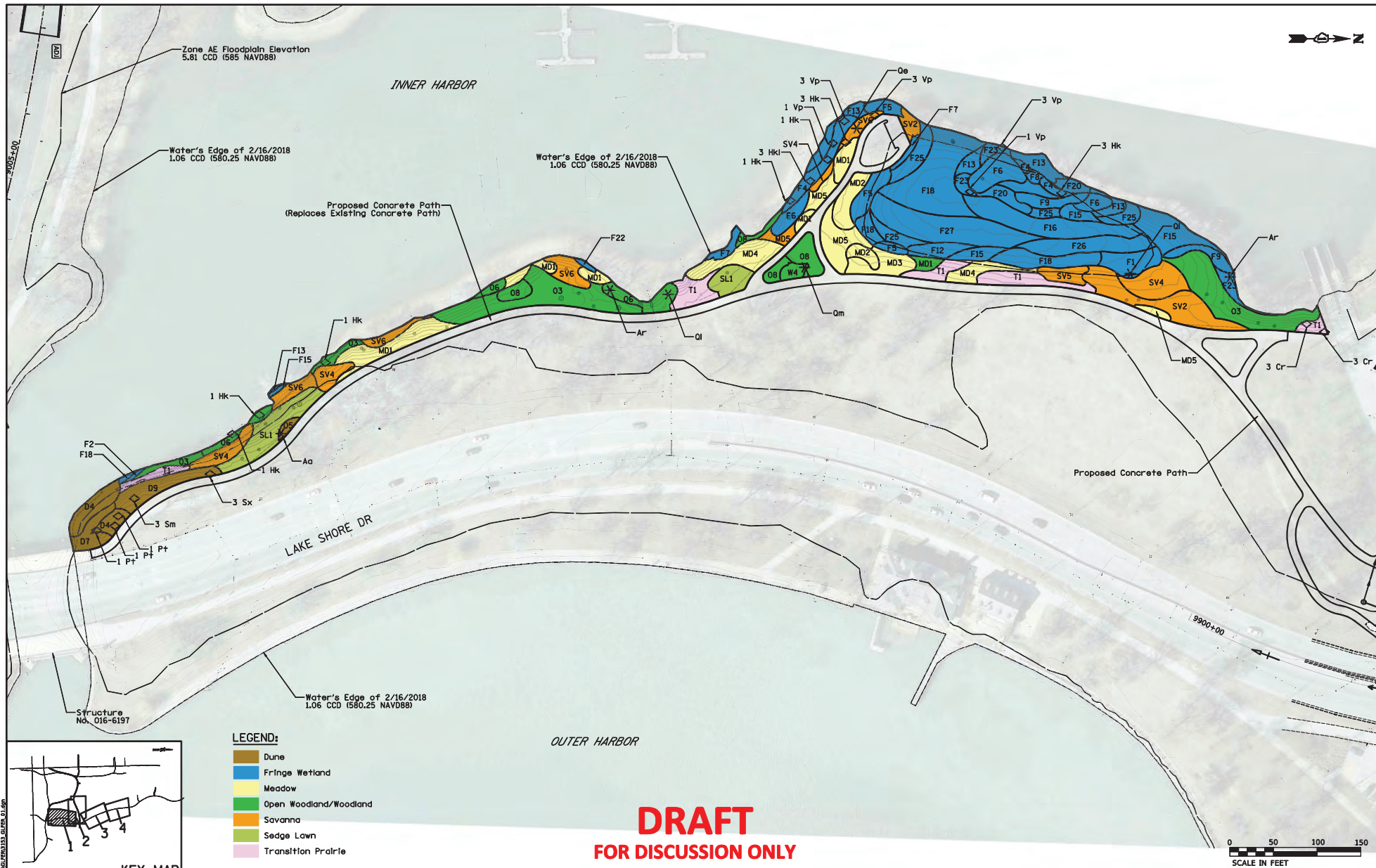
Improve LSD, Hayes, & intersections to accomodate diverted traffic

Proposed pedestrian underpass

Mitigation planting area

Esri, HERE, Garmin, © OpenStreetMap contributors, and the GIS User community, © 2015; Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User community

Mitigation Planting - GLFER Project Area 2



DRAFT
FOR DISCUSSION ONLY



CHICAGO DEPARTMENT
OF TRANSPORTATION

OPC MOBILITY IMPROVEMENTS
GLFER AREA PLANTING PLAN

SCALE: 1" = 50' SHEET 1 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
17-87203-00-ES	COOK	4	1	
CONTRACT NO. B-7-203				
ILLINOIS FED. AID PROJECT				

MODEL ENDPOINTS
FILE NAME: 10510017P133 GLFER 31.gpx



USER NAME	WAM	DESIGNED	MSA	REVISED	-
DRAWN	WAM/MSA	REVISOR	WAM/MSA	REVISED	-
CHECKED	TKL	REVISOR	TKL	REVISED	-
DATE	AUG 2019	REVISOR	TKL	REVISED	-

PLOT SCALE = 50.0000' / 1 in.
PLOT DATE = 8/14/2019